



Bipartisan Infrastructure Law (BIL)

STP-Local And Local Bridge Supplemental Project Solicitation

Overview

WisDOT Local Programs & Finance Webinar

December 7, 2022

Webinar Outline

- What is BIL?
- What is STP-Local and Local Bridge?
- How to Apply
- Additional Resources



What is BIL?

- Signed on November 15, 2021, the Bipartisan Infrastructure Law (BIL) represents a significant increase of federal funding available for local programs over the next five fiscal years.
- Surface Transportation Block Grant (STBG)
 - Surface Transportation Program (STP)
 - Local Bridge
 - Transportation Alternatives Program (TAP)
 - Congestion Mitigation & Air Quality (CMAQ)



WisDOT FFY2023 Federal Plan

- Section 84.03(2)(b), Wis. Stats., requires WisDOT to submit a Federal Expenditure Plan to the Wisconsin Joint Committee on Finance if the amount of federal-aid highway funds provided by federal transportation authorization and appropriations bills for the current State Fiscal Year (SFY) is less than 95 percent or greater than 105 percent of the amounts appropriated under s. 20.395, Wis. Stats.
- Joint Committee on Finance has the power to make changes to the submitted plan
 - Example: Congestion Mitigation & Air Quality Program (CMAQ) BIL increase restricted to use on ROW projects
- Federal plan requirement needed to be factored into WisDOT's response to the BIL





What is STP-Local?

Surface Transportation Program (STP) Local

- Provides federal funding for up to 80% of the cost of local road improvements
- Eligible Roads
 - Functional classification of Minor Collector or Local Road
 - Outside of urbanized area (less than 50,000 population)
 - Connecting highways are not eligible
 - Appears on STP-Local Eligible Roads Map
- All STP-Local projects are State let and require State oversight
- Projects are selected by statewide discretionary committee



Roadway Functional Classification: STP Eligibility

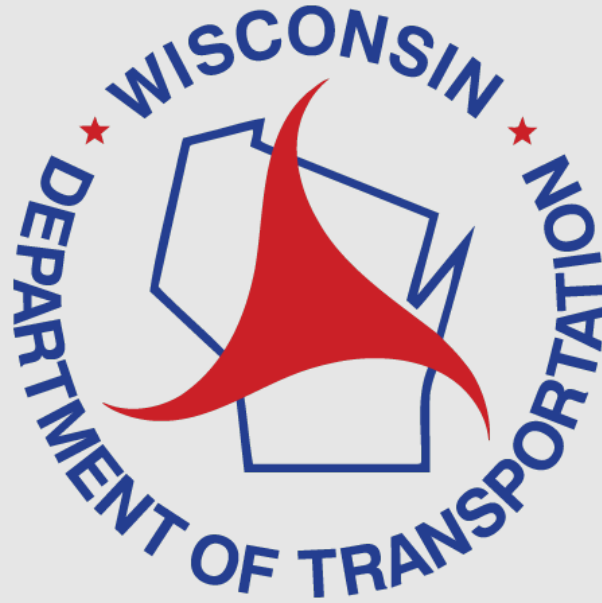
Roadway Functional Classifications		
Arterial	Principal	Interstate
		Expressway
		Other Principal
	Minor Arterial	
Collector	Major Collector	
	Minor Collector	
Local Roads		

STP-Urban & STP-Rural

STP-Local (NEW)

Visit [Wisconsindot.gov](https://wisconsindot.gov) – <https://wisconsindot.gov/Pages/projects/data-plan/plan-res/function.aspx>

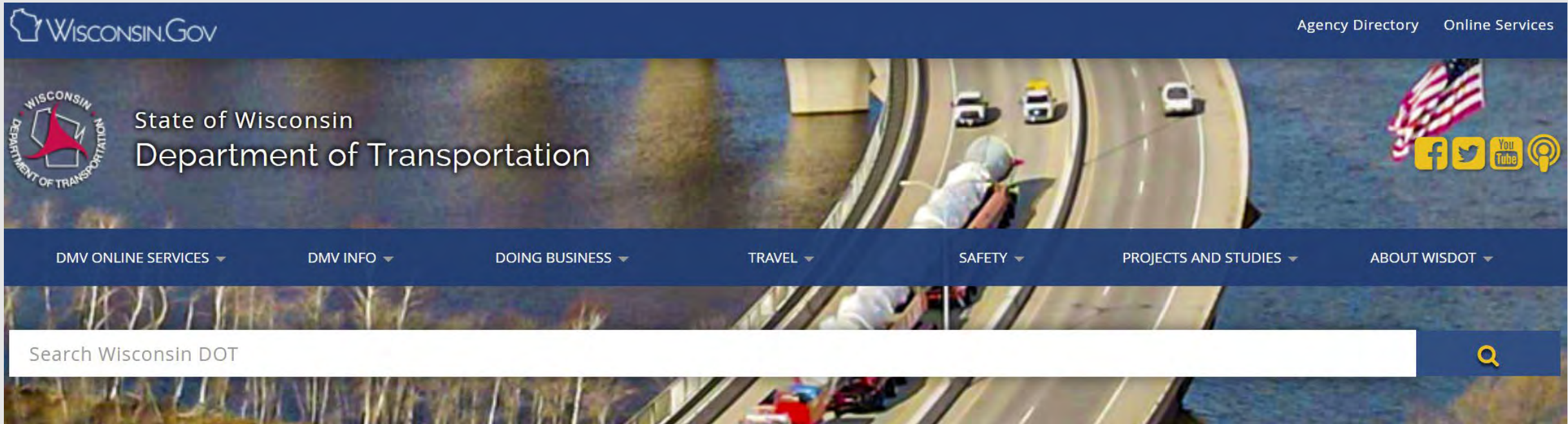




Accessing Application Materials

Accessing Application Materials

- Visit wisconsindot.gov



- In the "Search Wisconsin DOT" type "BIL"
 - Second search result = WisDOT BIL webpage, FFY23-26



Accessing Application Materials

FFY 2023 - 2026 Bipartisan Infrastructure Law (BIL) Program Solicitation

Assistance programs

- BIL - Local Programs
- Road and bridge
- Transit
- Other aid

NEW BIL Solicitations

New BIL solicitations for the Surface Transportation Program (STP) - Local, Local Bridge and the Transportation Alternatives Program (TAP) are available as of November 1, 2022.

All applications for these new solicitations are due by 5:00 PM on Friday March 24, 2023.

FFY 2024 STP-Local

STP-Local roadways are functionally classified as rural minor collectors, rural local roads and urban local roads that are located in urban areas with populations under 50,000.

- [Application Instructions](#)
- [Application](#)

An updated map application to assist with identifying eligible roads for the STP-Local program [is available here](#).

FFY 2024-2026 Local Bridge Improvement Assistance Program

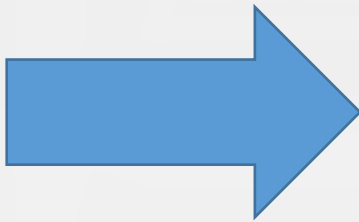
The Local Bridge Improvement Assistance Program helps rehabilitate and replace, on a cost-shared basis, the most seriously deficient existing local bridges on Wisconsin's local highway systems.

- [Application Instructions](#)
- [Application](#)
- [Eligible Bridge List](#)

FFY 2023-2026 Transportation Alternatives Program (TAP)

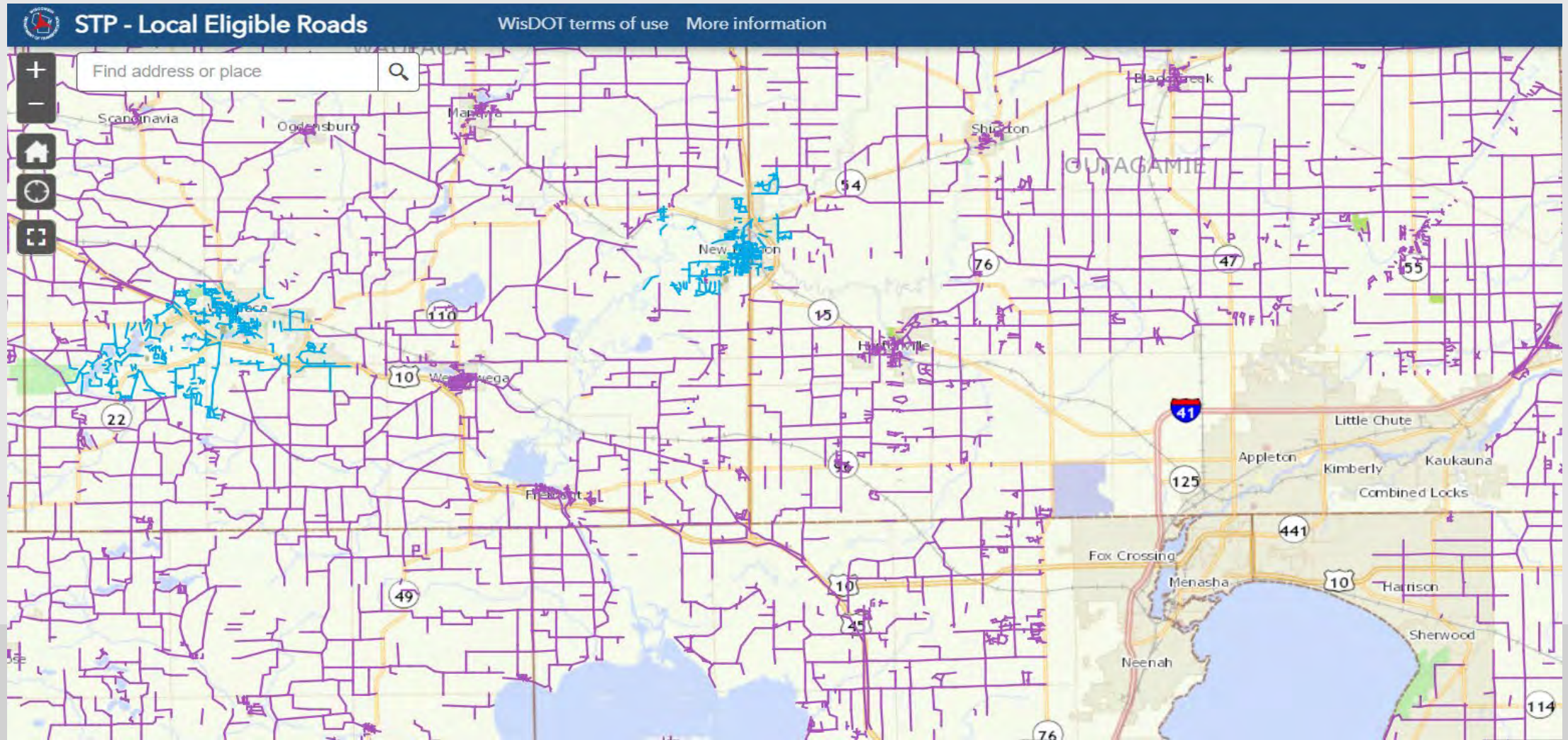
The Transportation Alternatives Program (TAP) provides eligible sponsors with up to 80% federal reimbursement for non-motorized, utilitarian trail projects as well as eligible non-infrastructure planning and safe routes to school activities.

- [Application Instructions](#)
- [Application](#)



STP-L Map

- <https://wisdot.maps.arcgis.com/apps/webappviewer/index.html?id=0c12fe578f204941b3b677b118598005>



Application Instructions-STP Local



WisDOT FFY24 Surface Transportation Program (STP)-Local Program Application Instructions CONSTRUCTION ONLY

NOTE: Only ONE Application Per Sponsor

This application is for Federal Fiscal Year 2024 (FFY24) construction projects funded by the Bipartisan Infrastructure Law (BIL). The BIL allows states to use funds from the small urban and rural federal funding allocations on minor collectors and local roads. This one-year supplemental solicitation has a \$2,000,000 total program funding availability. The roadway must be functionally classified as a Minor Collector or Local Road to be eligible for STP-Local funding. The roadway also must be located outside of urbanized areas (a population of less than 50,000) to be eligible for STP-Local funds. An individual application is required for each new potential FFY24 STP-Local program project. Previous applications cannot be rolled over into this solicitation.

STP-Local projects are selected by a committee of members from the Wisconsin County Highway Association (WCHA), the Wisconsin Towns Association (WTA), and the League of Wisconsin Municipalities (LWM). The discretionary project selection process is competitive and the categories that the selection committee considers are economic impact, state of good repair, connectivity and regional or local significance. Equitable geographic distribution is a consideration both in the formation of the STP-Local project selection committee and for the final project selections.

Project Eligibility

- The 2021 Bipartisan Infrastructure Law (BIL), allows states to use up to 15% of small urban and rural federal funding allocations on minor collectors and local roads.
- The GIS Application linked [here](#) will assist applicants in determining if the proposed project is a minor collector or local road, outside of an urbanized area. Roadways highlighted in purple are eligible areas for rural projects and roadways highlighted in blue are eligible areas for urban projects. While the GIS Application will provide information that has a high degree of accuracy, please cross-validate with the departmental functional classification maps linked [here](#).
- The roadway must be functionally classified as a Minor Collector or Local Road to be eligible for STP-Local funding. In addition, the roadway must be located outside of urbanized areas (populations of less than 50,000) to be eligible for STP-Local funds.
- Local improvement projects on connecting highways are not eligible for STP-Local funds.
- This solicitation for FFY24 STP-Local program will be for construction-only (no design) State-Let projects. An August 2024, Plans, Specifications & Estimates (PS&E) deadline is required to meet a November 2024 letting. The November 2024 let is the latest letting eligible for FFY 2024 funding. Earlier project lets are encouraged.
- Design must be 100% locally funded. Design must be undertaken by qualified consultant. The design must result in a PS&E that is acceptable to both the Wisconsin Department of Transportation (WisDOT) and Federal Highway Administration (FHWA).
- **Federal funding is capped at \$500,000. Project minimum is set at \$250,000.** Project applications for projects greater than the federal cap will be considered, however if successful, the federal share cannot exceed \$500,000.

Recommendations for Completing an Application

- It is **strongly recommended** that the local sponsor contact the Statewide Local Program Manager well in advance of the application deadline to address questions or comments related to the application.
- An application is required for each new potential FFY24 STP-Local program project and must include all pages of the application, WISLR map, project description/justification, and detailed cost estimate calculations. **Incomplete submittals will not be considered.**

- Use the **Tab** key in the application to "tab" to the next box or field.
- Questions on the application process should be directed to the Wisconsin Department of Transportation (WisDOT); contact persons listed below:

Chris Brooks	(608)267-3611	christopher_brooks1@dot.wi.gov
Michael Loughran	(608)266-2870	michael.loughran@dot.wi.gov
Merrill Mechler-Hickson	(608)261-8977	merrill.mechlerhickson@dot.wi.gov
Tim Olusegun	(608)266-0254	tim.olusegun@dot.wi.gov

- Save the application, **including the name of your local unit of government in the filename (e.g., City of ..., Town of ...)**, and email completed applications to: DOTLocalPrograms@dot.wi.gov. **Applications sent to the wrong email address will not be considered.**
- The application deadline is no later than **5:00 p.m. on Friday, March 24, 2023**. Submitting an application prior to the deadline is strongly encouraged. **Late applications will NOT be considered.**
- WisDOT will only accept applications in electronic form as a Microsoft Word document. Applications saved in PDF format **will NOT** be accepted; handwritten signatures **are NOT** required.
- The final project scope, cost, and proposed delivery schedule are the responsibility of the local sponsor.
- There is **only one** project sponsor allowed per project. Only one application per sponsor will be considered **do NOT submit more than one application**.
- Project sponsors must fund a portion of total project costs.
- Federal funding is capped at 80%.
- All costs more than the estimates indicated in the application will be 100% responsibility of the local sponsor. No funds beyond the accepted amount indicated in the State/Municipal Agreement will be authorized.
- Construction must begin in Calendar Year 2025.
- Please note that temporary easements are considered Right-of-Way activities. All project right-of-way activities must be resolved prior to the August 1, 2024 PS&E date to guarantee a November 2024 letting.

FFY22 STP-Local Program Application Instructions

Population Category, Functional Classification and Municipality Type

- Select population category, functional classification, and municipality type from the drop-down menus.
 - The population category is either "RURAL" (highlighted in purple), which is outside all urban and urbanized areas; or "5,000 to 50,000" (highlighted in blue), which is within one of the 54 designated urban areas. Refer to the GIS application linked [here](#) and the maps linked below to determine if roadway lies within an urban area.
 - Functional Classification. Roadway must be functionally classified as a minor collector or local road to be eligible for STP-Local funding. Refer to the following links to view guidance and approved functional classification maps
 - <http://wisconsin.dot.gov/Pages/projects/data-plan/plan-res/function.aspx>

Project Description

- Roadway must be functionally classified as a Minor Collector or Local Road to be eligible for STP-Local funding. The roadway must be located outside of an urbanized area boundary, ie within an area below 50,000 population. Refer to GIS Application linked [here](#) (roadways highlighted in purple are eligible for rural projects and roadways



Application Materials-STP Local



WisDOT FFY24 STP-Local Program Application CONSTRUCTION ONLY

NOTE: This application is for Federal Fiscal Year 2024 (FFY24) construction projects funded by the Bipartisan Infrastructure Law (BIL). The BIL allows states to use funds from small urban and rural federal funding allocations on minor collectors and local roads. The roadway must be functionally classified as a Minor Collector or Local Road to be eligible for STP-Local funding. The roadway must be located outside of urbanized areas (less than 50,000) to be eligible for STP-Local funds. An individual application is required for each new potential FFY24 STP-Local program project. Previous applications cannot be rolled over into this solicitation. Please review the application instructions (see link below) to assist you in completing the application.

[FFY24 STP-Local Application Instructions](#)

Population Category:
SELECT

Functional Classification:
SELECT

Municipality Type:
SELECT

Project Description

Project Sponsor: **SELECT** Facility Owner: **SELECT**

Project Location:

Municipality: **SELECT** County: **SELECT**

On Route: **SELECT**

At Route (Start): **SELECT** Offset: **SELECT** (tenths of a mile)

Toward Route (End): **SELECT**

NOTE: Attach an 8 1/2 x 11 map showing the project location. A WISLR map is REQUIRED (refer to the following link) <http://wisconsindot.gov/Pages/doing-bus/local-gov/wislr/default.aspx>

NOTE: Roadway must be functionally classified as a Minor Collector or Local Road to be eligible for STP-Local funding.

Existing Facility

Number of Lanes: **SELECT** Cross Section: Rural Urban

Pavement Type: **SELECT** Pavement Width: **SELECT**

Pavement Rating: **SELECT** Year Last Improved: **SELECT**

Existing Sidewalk or Bicycle accommodation? Yes, one side Yes, both sides No

Any federal-aid-eligible structures within the existing facility? Yes No If yes, please indicate the Bridge ID # (s): **SELECT**

Does a railroad facility exist within 1000 feet of the project limits? Yes No

Owner of Railroad facility: **SELECT**

NOTE: Rail improvements are not eligible for STP program funding.

Project Description/Justification **(NOTE: This is very important to read!)**

Explain why the project is needed, including the scope and appropriate detail on the project's uniqueness and complexity. Describe specific deficiencies such as pavement cracking, edge raveling, surface deterioration, substandard geometrics, etc. Include up to three pictures to further show the deficiencies and surrounding land use. Describe in detail any known economic impacts to surrounding land use, such as agricultural, commercial, industrial, educational, etc. Describe any known safety issues. Include and separately identify any 100% locally funded components of the project. This portion of the application will be used by the selection committee to rate and rank the projects. **Use a separate single page document to describe the proposed project and justification (one page maximum).**

Proposed Improvement

NOTE: Applicants should reference the links to traffic data and design standards information in the instructions prior to completing this section of the application.

Improvement Type: **SELECT** If Combination, explain: **SELECT** Overall Length: **SELECT** (feet)

Rural Cross Section

Urban Cross Section

New Pavement Type: **SELECT** If Combination, explain: **SELECT** Width: **SELECT** Length: **SELECT**

New Shoulder Type: **SELECT** If Combination, explain: **SELECT** Width: **SELECT** Length: **SELECT**

Sidewalk One side or both: **SELECT** Width: **SELECT** Length: **SELECT**

New bicycle accommodations? Yes No If yes, specify: **SELECT**

Roundabout Location: **SELECT**

NOTE: Refer to [FDM 11-26](#) for modern roundabout information

Structure Structure Type: **SELECT** Work Required: **SELECT**

Structure #(s): **SELECT**

Traffic Management During Construction: **SELECT**



Application Materials-STP Local

Federal Fiscal Year 2024 STP-Local SELECTION CRITERIA FORM

Project Sponsor:

County:

The applicant must show evidence that criteria 1-12 (below) has been addressed to be eligible for the Federal Fiscal Year 2024 STP-Local funding.	Check If Criterion Can Be Met Or Not Applicable	Check If Criterion Cannot Be Met
1. The project will be within the existing operational right-of-way and utilize the existing facilities, preferably staying within the existing subgrade shoulder points or curb and gutter. The proposed project may include minimal grading to allow for minor pavement elevation changes.	<input type="checkbox"/> N/A	<input type="checkbox"/>
2. If the project is a bridge project, the existing bridge structure does not cross a railroad. If the project is a roadway project, the project would not be within 1000' of a railroad crossing.	<input type="checkbox"/> N/A	<input type="checkbox"/>
3. Project must meet WisDOT FDM 26-5-1.1.1.1. Project is not adjacent to or proposing to rehabilitate historic buildings or structures and is not within historic districts or adjacent to archaeological sites.	<input type="checkbox"/> N/A	<input type="checkbox"/>
4. The project is not located in an EPA air quality area designated non-attainment or maintenance for ozone or PM2.5. Nonattainment Areas for Criteria Pollutants (Green Book) US EPA	<input type="checkbox"/> N/A	<input type="checkbox"/>
5. Total NEW ground disturbance will be less than 0.5 acre and there will be no tree clearing.	<input type="checkbox"/> N/A	<input type="checkbox"/>
6. No evidence of migratory bird nesting or bat roosting on structures to be replaced or rehabilitated. If present, impacts would be avoided or minimized through approved avoidance and minimization measures: https://wisconsin.gov/Documents/doing-bus/eng-consultants/cnsit-rsrces/environment/mbtaguidance.pdf ; https://www.fws.gov/media/users-guide-range-wide-programmatic-consultation-indiana-bat-and-northern-long-eared-bat	<input type="checkbox"/> N/A	<input type="checkbox"/>
7. The project may not be located within a Metropolitan Planning Area.	<input type="checkbox"/> N/A	<input type="checkbox"/>
8. The bridge or roadway project will not require diversion of a stream, or modification to a stream channel.	<input type="checkbox"/> N/A	<input type="checkbox"/>
9. Any work to be done or any fill to be permanently placed in the water or wetlands will meet the non-reporting criteria (no pre-construction notification) of the U. S. Army Corps of Engineers Transportation Regional General Permit for Wisconsin/Minnesota. https://www.mvp.usace.army.mil/Portals/57/docs/regulatory/RGP/Transportation_RGP.pdf?ver=2018-02-22-093530-183	<input type="checkbox"/> N/A	<input type="checkbox"/>
10. If the project is a bridge deck replacement over a waterway, debris will be minimized from entering waterway, using WisDOT Standard Specifications Section 203 and CMM Section 645.6. (add links to these two references). https://wisconsin.gov/rdwy/stndspec/ss-02-03.pdf https://wisconsin.gov/rdwy/cmm/cm-06-45.pdf	<input type="checkbox"/> N/A	<input type="checkbox"/>
11. If the project includes culvert replacement or modification, the culvert is not located on a waterway with a waterbody index code (WBIC) as identified onto DNR's surface water data viewer: (Surface Water Data Viewer Wisconsin DNR).	<input type="checkbox"/> N/A	<input type="checkbox"/>
12. No real estate may be acquired in Fee Simple, Permanent Limited Easement or Temporary Limited Easement. No donation or trading of real estate is allowed.	<input type="checkbox"/> N/A	<input type="checkbox"/>

CONFIDENTIAL INFORMATION

Cost Estimate

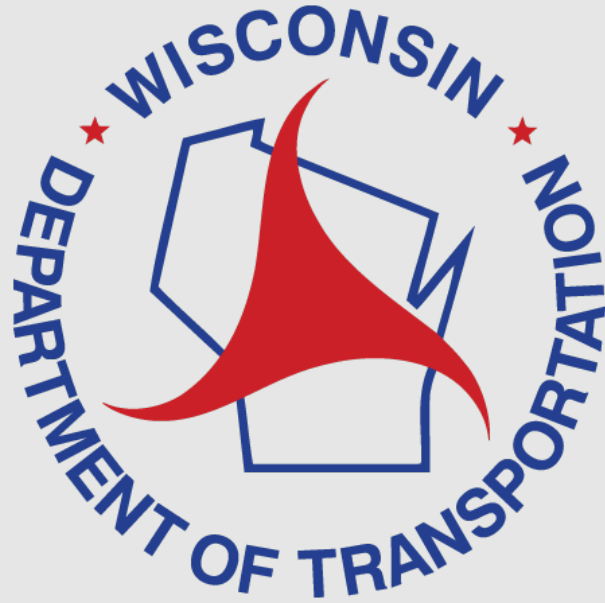
Applicants should reference the following WisDOT web page prior to completing this section of the application: <http://wisconsin.gov/Pages/doing-bus/local-gov/astnce-pgms/highway/tools.aspx>

NOTE: All applications must include a sheet documenting the calculations performed to create the estimate.

Construction	Total	Federal Share	Local Share
Participating Roadway Cost	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
Participating Structure Cost	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
Total Participating Cost	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
Non-Participating Roadway Cost (100% Local)	\$ <input type="text"/>	\$0	\$ <input type="text"/>
Non- Participating Structure Cost (100% Local)	\$ <input type="text"/>	\$0	\$ <input type="text"/>
Total Non-Participating Construction Cost	\$ <input type="text"/>	\$0	\$ <input type="text"/>
A. Subtotal Construction Costs	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
B. Construction Engineering Costs (10% of A.)	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
C. State Review for Construction (5% of A.)	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
Total Construction Cost Estimate with Construction Engineering and State Review (sum lines A, B and C)	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>

NOTE: All estimates will be reviewed by WisDOT Region staff for consistency with current practices and approaches. WisDOT Region staff, in agreement with the local sponsor, may revise estimates in these categories due to the complexity of the project or other factors. WisDOT will notify the sponsor of any changes to estimates within the application and determine whether the sponsor wishes to continue with the application with the revised estimate.





What is Local Bridge?

100% Federal Funding for Off-System Local Bridges

- Local Bridge projects are selected based on LES rating and ranking processes
 - Off-system bridges are not guaranteed selection
- Eligibility for 100% federal funding:
 - Non-Federal aid highway system structures only
 - Structure must be on a roadway functionally classified as a local road or minor collector



Local Bridge Improvement Assistance Program

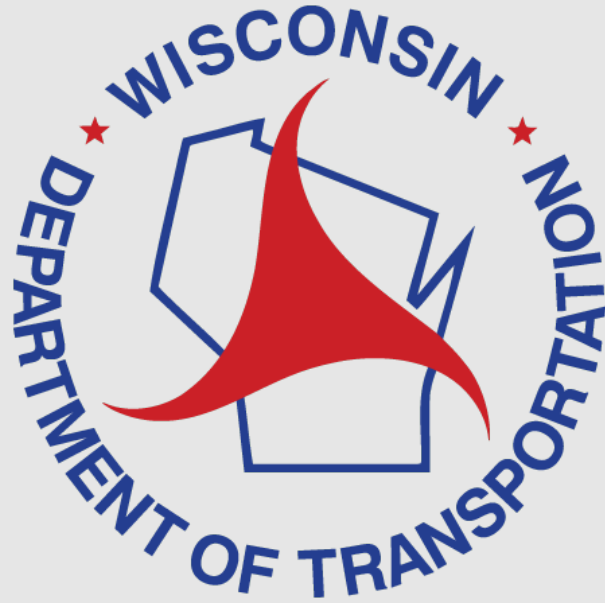
- Project appears on Eligible Bridge List
- Only bridge replacements and bridge rehabilitations as determined by Sufficiency Rating (SR) are eligible
- County Highway Commissioner (CHC) submits a prioritized list to WisDOT and verifies eligibility
- Projects selected through statewide rating and ranking process based on entitlement balance and estimated cost
- All Local Bridge projects are State let and require State oversight



Eligible Bridge List

B020423	NW	ASHLAND	COUNTY	MARENGO	CTH GG	MCCARTHY CREEK	655	42.3	FO	REPLACEMENT
B030725	NW	BARRON	COUNTY	CHETEK	CTH SS	CHETEK RIVER	4305	59.3	SD	REHABILITATION
B040018	NW	BAYFIELD	COUNTY	NAMAKAGON	D	NAMEKAGON LAKE	6583	54.8	SD	REHABILITATION
B040036	NW	BAYFIELD	TOWN	MASON	BIBON RD	WHITE RIVER	2472	64.5	SD	REHABILITATION
B040049	NW	BAYFIELD	TOWN	PORT WING	BEACH RD	LAGOON	2386	73	SD	REHABILITATION
B040413	NW	BAYFIELD	TOWN	BARNES	SOUTH SHORE RD	EAU CLAIRE LAKES	935	46.8	SD and FO	REPLACEMENT
B050008	NE	BROWN	COUNTY	NEW DENMARK	KB	NESHOTA RIVER	1935	26.9	SD	REPLACEMENT
B050026	NE	BROWN	COUNTY	HOBART	FF HILLCREST RD	DUCK CREEK	3650	68.8	SD	REHABILITATION
B050048	NE	BROWN	TOWN	NEW DENMARK	Rosecrans Rd	Devil River State Trail	6085	67.7	SD	REHABILITATION
B050105	NE	BROWN	COUNTY	LAWRENCE	EE	ASHWAUBENON CREEK	2725	68.5	SD	REHABILITATION
B050118	NE	BROWN	COUNTY	HOLLAND	CTH Z (Hill Road)	PLUM CREEK	4046	56.7	SD	REHABILITATION
B050154	NE	BROWN	CITY	GREEN BAY	MASON ST	EAST RIVER	11370	75.7	FO	REHABILITATION

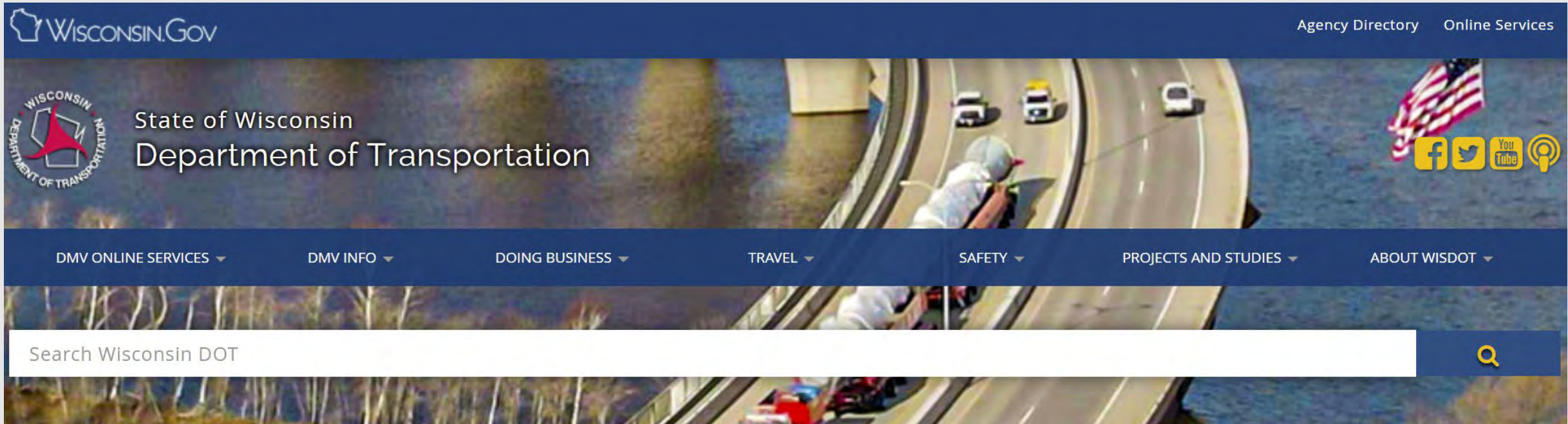




Accessing Application Materials

Accessing Application Materials

- Visit wisconsindot.gov



- In the "Search Wisconsin DOT" type "BIL"
 - Second search result = WisDOT BIL webpage, FFY23-26



Accessing Application Materials

FFY 2023 - 2026 Bipartisan Infrastructure Law (BIL) Program Solicitation

Assistance programs

BIL - Local Programs

Road and bridge

Transit

Other aid

NEW BIL Solicitations

New BIL solicitations for the Surface Transportation Program (STP) - Local, Local Bridge and the Transportation Alternatives Program (TAP) are available as of November 1, 2022.

All applications for these new solicitations are due by 5:00 PM on Friday March 24, 2023.

FFY 2024 STP-Local

STP-Local roadways are functionally classified as rural minor collectors, rural local roads and urban local roads that are located in urban areas with populations under 50,000.

- [Application Instructions](#)
- [Application](#)

An updated map application to assist with identifying eligible roads for the STP-Local program [is available here](#).

FFY 2024-2026 Local Bridge Improvement Assistance Program

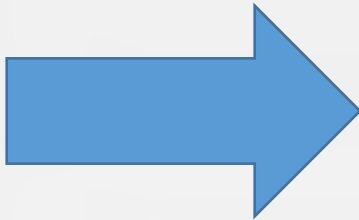
The Local Bridge Improvement Assistance Program helps rehabilitate and replace, on a cost-shared basis, the most seriously deficient existing local bridges on Wisconsin's local highway systems.

- [Application Instructions](#)
- [Application](#)
- [Eligible Bridge List](#)

FFY 2023-2026 Transportation Alternatives Program (TAP)

The Transportation Alternatives Program (TAP) provides eligible sponsors with up to 80% federal reimbursement for non-motorized, utilitarian trail projects as well as eligible non-infrastructure planning and safe routes to school activities.

- [Application Instructions](#)
- [Application](#)



Application Instructions-Local Bridge



FFY 2024-FFY 2026 BIL Local Bridge Program Application Instructions

NOTE: The application is required for each new potential FFY 2024-FFY 2026 program cycle project. Please review these application instructions and use them to assist you in completing the application. Municipalities should work with their County Highway Commissioner when developing an application in the Local Bridge Program.

Project Eligibility

- All eligible structures must be on the current WisDOT Bridge List. The Bridge List is provided to all County Highway Commissioners. A project must be located on a locally owned public roadway (not on a connecting highway) and the overall structure opening must be greater than 20 feet in length along the center of the roadway to be eligible for funding.
- Rail improvements are not eligible for Local Bridge program funding.
- Refer to the following link to TRANS 213 for additional information on project eligibility: https://docs.legis.wisconsin.gov/code/admin_code/trans/213.pdf

Recommendations for Completing the Application

- It is strongly recommended that the local sponsor contact the Regional Local Program Manager well in advance of the application deadline to address any comments or updates to the application.
- The application is required for each new potential FFY 2024-FFY 2026 program cycle project and must include all pages of the application. Please review these application instructions and use them to assist you in completing the application.
- If the Improvement Type is a Bridge Rehabilitation, a copy of the Rehabilitation Report that was approved by the Bureau of Structures is required to be submitted with this application.
- The **Tab** key can be used in the application to tab to the next box or field.
- Questions on the application process should be directed to the appropriate Wisconsin Department of Transportation (WisDOT) regional contact person listed below:

SE Region	Jacob Varnes	jacob.varnes@dot.wi.gov	(262) 548-8789
SW Region	Michael Erickson	michael.erickson@dot.wi.gov	(608) 246-5361
NW Region	Randy Kirk	randall.kirk@dot.wi.gov	(715) 392-7860
NC Region	Ben Roskoskey	benjamin.roskoskey@dot.wi.gov	(715) 365-5783
NE Region	Alex Dums	alex.dums@dot.wi.gov	(920) 492-5707

- Additional project cost estimate information is available on the following WisDOT web page: [WisDOT Bureau of Structures Cost Estimate Tool](#)
- When you have completed the application, include the name of the local unit of government in the file name and email it to the appropriate WisDOT Region email address:

SE Region DOTDTSOSELocalApps@dot.wi.gov

SW Region DOTDTSOSWLocalBridge@dot.wi.gov

NW Region DOTDTSOSWLocalBridge@dot.wi.gov

NC Region DOTDTSOSWLocalBridge@dot.wi.gov

NE Region DOTDTSOSWLocalBridge@dot.wi.gov

- Project application **deadline is no later than 5:00 p.m. Friday, March 24, 2023**. Submitting applications prior to the deadline will allow WisDOT time for quick review and communication with locals on any outstanding questions.
- WisDOT will only accept applications in electronic form as a Microsoft Word document. Applications saved in PDF format will **NOT** be accepted; handwritten signatures are **NOT** required.
- The final project scope, cost and proposed delivery schedule are the responsibility of the local sponsor.

- All costs in excess of the estimates indicated in the application will be 100% responsibility of the local sponsor unless additional funding is approved through the Change Management process. Change Management requests for additional funding will not be approved unless adequate justification is provided based on sound engineering judgement.
- Only one project sponsor is allowed per project.
- Project sponsor must fund a portion of total project costs. Local Bridge program project costs are funded with up to 80% federal/state and a minimum 20% local funds.

Local Bridge Instructions

Project Description

NOTE: Location must be on a locally owned public roadway (not on a connecting highway) and the overall structure opening must be greater than 20 feet in length along the center of the roadway to be eligible for funding. Multiple pipe structures, where the clear distance between openings is less than half of the smaller contiguous opening, that meet the above criteria are eligible for funding.

Project Sponsor: Select a prefix from the drop-down menu. Provide the name of the local unit of government that is financially responsible for the federal-aid improvement.

Facility Owner: Select a prefix from the drop-down menu. Provide the name of the government entity that owns the facility. If the Facility Owner is the Project Sponsor, indicate "Same as Sponsor" from the drop-down menu. If the Owner and Sponsor differ, a signed resolution is required to authorize the Sponsor to act on behalf of the Owner. Note that only a county can act on behalf of another government entity.

Project Location

Municipality: Select a prefix from the drop-down menu and provide the name of the municipality in which the project is located.

County: Provide the name of the county in which the project is located.

On Route: Provide the name of the roadway on which the project is located.

At Route (Start): Provide the name of the intersecting roadway at the start of the project. If the project does not start at an intersection, the At Route is the nearest intersecting roadway outside of the project limits.

Offset: If the project does not start at an intersection, an Offset is required. If the project starts at an intersection, an Offset is not required. An Offset is the distance, expressed as a positive number rounded to a tenth of a mile, between the nearest intersection and the start of the project.

Toward Route (End): Provide the name of the intersecting roadway at the end of the project. If the project does not end at an intersection, the Toward Route is the nearest intersecting roadway outside of the project limits. Enter the word "Termini" if the roadway terminates or a municipal boundary occurs after the end of the project but before the nearest intersection.

NOTE: Refer to the following link for information on the On/At method of describing the location of a project: <http://wisconsin.dot.gov/Documents/doing-bus/local-gov/astnce-ogms/highway/trip/onatmanual.pdf>

NOTE: Attach an 8 ½ x 11 map showing the project location. A WISLR map is **REQUIRED** (refer to the following link: <http://wisconsin.dot.gov/Pages/doing-bus/local-gov/wislr/default.aspx>)

Existing Bridge ID#: Provide the existing bridge number. Include leading zeros to fulfill the following format examples: B-01-1234 or P-21-0036.

Length of Project: Provide the length of the entire project rounded to the nearest 25 feet (Example: 750 feet)



Application Materials-Local Bridge



FFY 2024-FFY 2026 BIL Local Bridge Program Application

NOTE: This application is required for each new potential FFY 2024 - FFY 2026 program cycle project. Please review the application instructions (see link below) to assist you in completing the application.

[FFY24-26 Local Bridge Application Instructions](#)

Project Description

Project Sponsor: **SELECT** Facility Owner: **SELECT**

Project Location:

Municipality: **SELECT** County:

On Route:

At Route (Start): Offset: (tenths of a mile)

Toward Route (End):

Note: Attach an 8½ x 11 map showing the project location. A WISLR map is REQUIRED (refer to the following link: <http://wisconsindot.gov/Pages/doing-bus/local-gov/wislr/default.aspx>)

Existing Bridge ID#:

Length of Project: (feet)

Average Daily Traffic (ADT): ADT Year: Posted or Statutory Speed Limit(s): (mph)

Functional Classification: **SELECT**

Existing Facility

Bridge Type: **SELECT** If Other, specify:

Feature the Structure Passes Over:

Clear Roadway Width of Bridge: (feet) Bridge Length: (feet)

Number of Spans: Approach Pavement Width: (feet)

Is the bridge on the current WisDOT Eligible Bridge List? Yes No

Most Recent Inspection Date:

Bridge Build Year:

Bridge Rehabilitation Year:

Is scour currently a problem? Yes No

Has flooding (overtopping) of the bridge been observed? Yes No

Is debris accumulation and clogging currently a problem? Yes No

Current Load Posting:

Sufficiency Rating:

Structurally Deficient

Functionally Obsolete

Existing sidewalk? Yes, one side Yes, both sides No

Existing bicycle accommodations? Yes No

If YES to either of previous two questions, are bicycle/pedestrian accommodations designated as part of a regional or local bicycle or pedestrian system?

Yes No

Lighting: **SELECT** Lighting Style: **SELECT**

Approach Pavement Type: **SELECT**

Sub-standard alignment? Horizontal: Yes No
Vertical: Yes No

Does a railroad facility exist within 1,000 feet of the project limits? Yes No If yes, specify: **SELECT**

Owner of Railroad facility:

NOTE: Rail improvements are not eligible for Local Bridge program funding.

NOTE: If there are pertinent railroad considerations, design funds may be included for Railroad Review Costs.

Known safety issues? Yes No If yes, specify: (consider applying for Highway Safety Improvement Program [HSIP] funds if applicable)

Is this project within a F4R site? Yes No If yes, has an evaluation been completed:

NOTE: Refer to the following link, Facilities Repeatedly Requiring Repair and Reconstruction (F4R)
<https://wisconsindot.gov/Pages/doing-bus/local-gov/astnce-ngms/highway/f4r.aspx>

Project Justification

Explain why the project is needed, including the scope and appropriate detail on the project's uniqueness and complexity. Describe specific deficiencies such as deck or abutment condition, structural element deterioration, substandard width or substandard features that are problematic. Include and separately identify any 100% locally funded components of the project that are part of the overall improvement.

Proposed Improvement

NOTE: Applicants should refer to the traffic data and design standards information in the instructions prior to completing this section of the application.

NOTE: Per Trans 213, an independently funded engineering study must be undertaken prior to application for all proposed bridge rehabilitation projects. Link to Trans 213:
https://docs.legis.wisconsin.gov/code/admin_code/trans/213.pdf

NOTE: An optimal work recommendation created by the WisDOT Bureau of Structures is available at the WisDOT Local Project Tools webpage:
<http://wisconsindot.gov/Pages/doing-bus/local-gov/astnce-ngms/highway/tools.aspx>

Improvement Type: Bridge Replacement: **SELECT**
 Bridge Rehabilitation: **SELECT** If Other, specify:

NOTE: If the Improvement Type is a Bridge Rehabilitation, a copy of the Rehabilitation Report that was approved by the Bureau of Structures (BOS) is required to be submitted with this application. Submitting the bridge rehabilitation report to the BOS by **January 6, 2023 will ensure that the state review will be complete by the **March 24, 2023** due date for the application.**



Application Materials-Local Bridge

Structure Type: **SELECT** If Other, specify:
 Clear Roadway Width of Bridge: (feet) Bridge Length: (feet) Number of Spans:

NOTE: Approach costs are limited to only those costs that are necessary to render the bridge serviceable (to reach the attainable touchdown points using current standards.) WisDOT approval is needed at application for approach lengths greater than 100 feet. See FDM 3-20-1 for additional details.

Total Approach Work: **SELECT**
 Approach #1 – Direction from Bridge: **SELECT** Approach #1 Length: (feet)
 Approach #2 – Direction from Bridge: **SELECT** Approach #2 Length: (feet)
 Approach Pavement Type: **SELECT** Approach Pavement Width: (feet)
 Approach Shoulder Type: **SELECT** Approach Shoulder Width: (feet)
 Sidewalk One side or both: Width: (feet) Length: (feet)

Are bicycle/pedestrian accommodations required as part of a local or regional plan? Yes No If yes, specify:

Lighting: **SELECT** Lighting Style: **SELECT**
 Bridge Rail
 Beam Guard

Traffic Management During Construction: **SELECT**
 Do you anticipate submittal of an exception to standards request? Yes No
 If yes, please describe:

Environmental/Cultural Issues

Agriculture	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Archaeological sites	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Historical sites	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Lakes, waterways, floodplains	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
If located in floodplain, specify <input type="checkbox"/> Zone A <input type="checkbox"/> Zone AE - if Zone AE, provide Base Flood Elevation <input type="text"/>				
Wetland	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Stormwater management	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Hazardous materials sites	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Hazardous materials on existing structure	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Upland habitat	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Endangered/threatened/migratory species	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Section 4(f)	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Section 6(f)	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>
Through/adjacent to tribal land	<input type="checkbox"/> Yes	<input type="checkbox"/> No	<input type="checkbox"/> Not Investigated	Comments: <input type="text"/>

Miscellaneous Issues

Construction Schedule Restrictions (trout, migratory bird, local events): Yes No
 If yes, please explain.

Has there been any real estate acquired or transferred in anticipation of this project? Yes No
 If yes, please explain.

Right of Way (NOTE: It is recommended that local funds be used to acquire right of way)

Check all that are applicable.

None Less than ½ acre More than ½ acre
 Parklands Large parcels Temporary interests

Other Concept Notes: Provide any additional relevant project information that has not been covered in another section of the application.

Application Materials-Local Bridge

CONFIDENTIAL INFORMATION

Cost Estimate, Project Priority, and Scheduling

Applicants should reference the following WisDOT web page prior to completing this section of the application:
<http://wisconsindot.gov/Pages/doing-bus/local-gov/astnce-pgms/highway/tools.aspx>

NOTE: Requesting design and construction projects to be scheduled in the same fiscal year is not allowed.

NOTE: All applications must include a sheet documenting the calculations performed to create the estimate(s).

Tied Projects? All requests for design must be tied to a construction project. Please indicate which projects will be tied (if applicable):

Construction:

Project Priority:

FY 2023 FY 2024 FY 2025 FY 2026 FY 2027

	Total	State/Fed Share (80%)	Local Share (20%)
Participating Structure Cost	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
Participating Approach Cost	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
Total Participating Construction Cost	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
Non-Participating Structure Cost (100% Local)	\$ <input type="text"/>	\$0	\$ <input type="text"/>
Non-Participating Approach Cost (100% Local)	\$ <input type="text"/>	\$0	\$ <input type="text"/>
Total Non-Participating Construction Cost	\$ <input type="text"/>	\$0	\$ <input type="text"/>
A. Subtotal Construction Costs	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
B. Construction Engineering Costs (Coordinate with WisDOT Region)	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
C. State Review for Construction (Provided by WisDOT Region)	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
Total Construction Cost Estimate with Construction Engineering and State Review (sum lines A, B and C)	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>

NOTE: All estimates will be reviewed by WisDOT Region staff for consistency with current practices and approaches. WisDOT Region staff, in agreement with the local sponsor, may revise estimates in these categories due to the complexity of the project or other factors. WisDOT will notify the sponsor of any changes to estimates within the application and determine whether the sponsor wishes to continue with the application with the revised estimate.

Design:

100% Locally Funded (state review is required to be included as 100% locally funded) OR
 80% State/Federally Funded ("Design Only" projects are not allowed)

Project Priority:

FY 2023 FY 2024 FY 2025 FY 2026 FY 2027

	Total	State/Fed Share	Local Share
A. Design Plan Development	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
B. State Review for Design (Provided by WisDOT Region)	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>
Total Design Cost Estimate with State Review (sum lines A and B)	\$ <input type="text"/>	\$ <input type="text"/>	\$ <input type="text"/>

NOTE: WisDOT Region staff, in agreement with the local sponsor, may revise estimates in the Plan Development, State Review for Design, and State Review for Construction categories based on the complexity of the project or other factors.

NOTE: Costs for Railroad Review of plans will be added to the design costs when there are railroad considerations.

Real Estate: (Must be funded with local funds.)

Project Priority:

FY 2023 FY 2024 FY 2025 FY 2026 FY 2027

Total Real Estate Cost (round to next \$1,000) \$

Utility: (Compensable utility costs must be \$50,000 minimum per utility. Recommend funding with local funds.)

Project Priority:

FY 2023 FY 2024 FY 2025 FY 2026 FY 2027

Total Utility Cost (round to next \$1,000) \$

WisDOT Utility Policy link: <http://wisconsindot.gov/rdwy/fdm/fd-18-00toc.pdf>



University of Wisconsin-Madison

Transportation Information Center



GET PROFESSIONAL HELP

Is R.O.W. wide enough?

How wide does it need to be to meet standards?

overlay or pulverize?

Pavement Width?

Pavement thickness?

replacing culverts?

Is guardrail needed?

Pedestrian and bike facilities?

Railroad crossings?

pavement condition (PASER rating)?

Drainage issues?

safety improvements needed?

Wetlands?

replacing signs and markings?

How much ditch work needs to be done?

trucks traffic count?

Strength of soils?

Endangered Species?

traffic count (ADT)?



Key Considerations

- Verify the segment length
- Check pavement and shoulder widths at several locations
- Verify pavement condition and revise PASER rating if needed
- Identify specific needs such as full depth patching, culvert replacement, guardrail, alignment changes, and other safety improvements
- Review crash data
- Check for utility conflicts, railroads crossings, wetlands, endangered species, historic or culturally significant sites
- Verify that design fits in the existing right-of-way
- Prepare detailed cost estimate with contingency factors



WISLR Maps and Pavement Information

<https://wisconsin.gov/Pages/doing-bus/local-gov/wislr/default.aspx>

Wisconsin Department of Transportation

Wisconsin Information System for Local Roads

application: [home](#) | [main menu](#) | [route name discrepancy](#) | [log-off](#) | [manual and publications](#) | [On/At training quiz](#)

County: TREMPEALEAU (61) Municipality: BURNSIDE (T) (006) County-Muni Certification Year: 2023

Global Location

Rd/St Name: Hunts Valley Ln Retrieve Entire Route At/Toward Certified Mileage: 9768 feet
 At: Termini Rd/St Length: 9768 feet
 Toward: Wolfe Rd (Termini)

Retrieve At/Toward

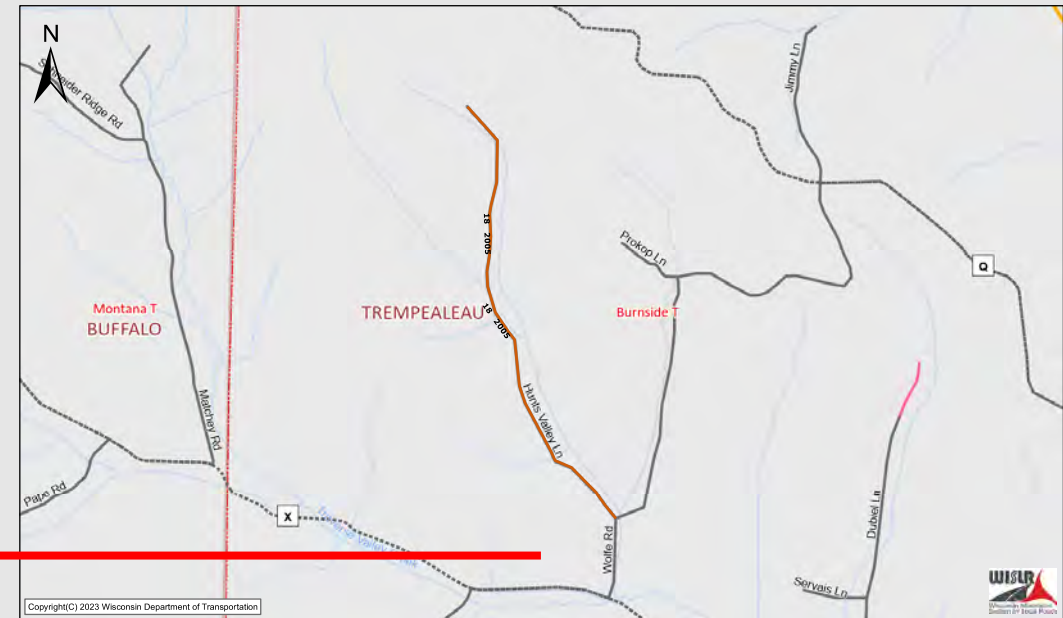
View Physical Inventory

Physical Inventory Administrative Inventory

Map	+	Attribute Name	Occurs	At Intersection	From Offset	To Offset	Section Length	Attribute Value
		Surface	1 of 1	Termini	0	9768	9768	Type: 70-Hot Mix Asphalt Pavement (HMAC), Width: 18 ft, Year: 2005
		Maintenance Treatment	0 of 0		0	0	0	
		Left Shoulder	1 of 2	Termini	0	3696	3696	Type: 0-None, Width: 0 ft
		Right Shoulder	1 of 2	Termini	0	3696	3696	Type: 0-None, Width: 0 ft
		One Way	1 of 1	Termini	0	9768	9768	One Way: No
		Right-of-Way	1 of 1	Termini	0	9768	9768	Indicator: E, Width: 66 ft
		Median	0 of 0		0	0	0	
		Left Curb	1 of 1	Termini	0	9768	9768	Type: 0-None
		Right Curb	1 of 1	Termini	0	9768	9768	Type: 0-None
		Parking	1 of 1	Termini	0	9768	9768	Parking: 4-Rural
		Traffic Lanes	1 of 1	Termini	0	9768	9768	Traffic Lanes: 2 Lanes
		Average Daily Traffic (ADT)	1 of 1	Termini	0	9768	9768	Indicator: E, Count: 50, Year: 1995
		Pavement Rating	1 of 1	Termini	0	9768	9768	System: Paser Asphalt Pavement (Hot Mix or Cold Mix Asphalt), Rating: 4, Hot Mix Asphalt Pavement (HMAC)
		Sidewalk	0 of 0		0	0	0	

WISLR Questions / Help:

wislrinfo@dot.wi.gov



Is the current roadway deficient?

TRANS 204

TABLE A—RECONSTRUCTION

TRAFFIC VOLUME			ROADWAY WIDTH DIMENSIONS IN FEET		
Design Class	Current ADT	Design Speed MPH	Traveled Way	Shoulder	Roadway
T1	Under 250	40	20	3	26
T2	250–750	50	22	4	30
T3	Over 750	55	24	6	36

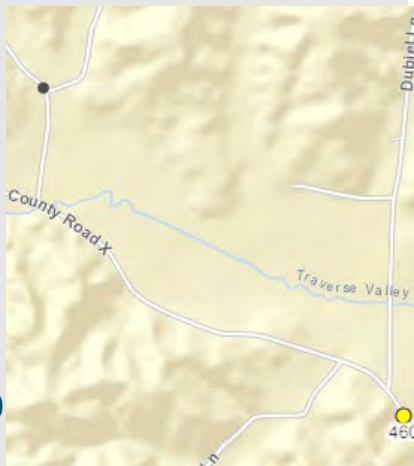
TABLE B—RESURFACING AND RECONDITIONING

TRAFFIC VOLUME			ROADWAY WIDTH DIMENSIONS IN FEET		
Design Class	Current ADT	Design Speed MPH	Traveled Way	Shoulder	Roadway
TR1	Under 250	—	18	2	22
TR2	250 – 400	40	20	2	24
TR3	401 – 750	50	22	2	26
TR4	Over 750	55	22	4	30

Note: Examples of resurfacing and reconditioning improvements which may be appropriate for existing town roads include, but are not limited to, pavement rehabilitation; widening lanes and shoulders; replacing bridge elements to correct structural deficiencies; bridge deck overlays; bridge and culvert replacement; and other related improvements such as minor grading, subgrade work and correction of drainage problems.

82.50 Town road standards. (1) The following minimum geometric design standards are established for improvements on town roads:

Annual Average 24-hour Traffic (ADT)	Minimum Design Standards
(a) Local service, intermittent traffic	
1. Right-of-way	3 rods
2. Roadway width	20 feet
3. Surface width	16 feet
(b) Under 100 ADT	
1. Right-of-way	3 rods
2. Roadway width	24 feet
3. Surface width	18 feet
4. Maximum grades	9%–11%
(c) 100 to 250 ADT	
1. Right-of-way	4 rods
2. Roadway width	26 feet
3. Surface width	20 feet
4. Maximum grades	8%–11%
(d) 251 to 400 ADT	
1. Right-of-way	4 rods
2. Roadway width	32 feet
3. Surface width	22 feet
4. Maximum grades	6%–8%
5. Curvature	6°–12.5°
(e) 401 to 1,000 ADT	
1. Right-of-way	4 rods
2. Roadway width	34 feet
3. Surface width	22 feet
4. Maximum grades	5%–8%
5. Curvature	5°–12.5°
(f) 1,001 to 2,400 ADT	
1. Right-of-way	4 rods
2. Roadway width	44 feet
3. Surface width	24 feet
4. Maximum grades	5%–7%
5. Curvature	4.5°–7.5°
(g) Over 2,400	State trunk standards



ADT = Average Daily Traffic

Sources:

- Conduct a count

- Regional Planning Commission

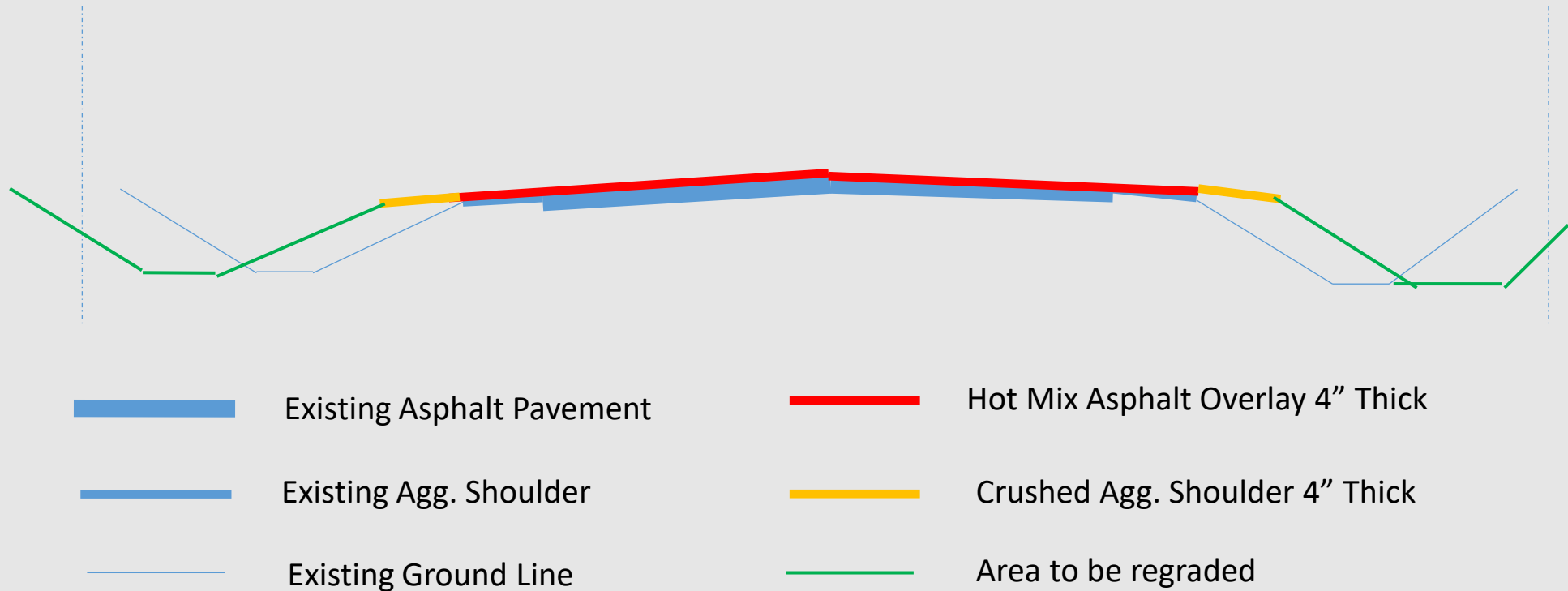
<https://wisconsin.gov/pages/projects/data-plan/traf-counts/default.aspx>

$$\text{Percent Grade} = \frac{\text{Rise} \times 100}{\text{Run}}$$

$$\text{Radius} = \frac{5729.578}{\text{Degree of Curvature}}$$



One change in the road necessitates other changes – widening example



Typical Items on Rural Road Projects

- Asphalt Pavement Removal
- Excavation
- Pulverize and Relay
- Full Depth Patching
- Hot Mix Asphalt
- Driveway, Field Entrance, Mailbox Turnout (Asphalt or Aggregate)
- Crushed Aggregate Base Course
- Crushed Aggregate shoulders
- Grading
- Culvert Removal
- Culvert Installation
- Guardrail
- Traffic Signs
- Pavement Markings
- Work Zone Traffic Control
- Erosion Control

Controlling Item Cost Estimate for Roadway Project				
Item	Quantity	Unit Price	Cost	Remarks
Removals				
Clearing/Grubbing				
Pavements				
Curb & Gutter				
Sidewalk				
Guardrail				
Buildings				
Manholes/Inlets				
Earthwork				
Common Excavation				
Borrow Excavation				
Rock Excavation				
Marsh Excavation				
Excavation for Structures				
Roadway				
Crushed Agg. Base Course				
Asphaltic Pavement				
Concrete Pavement				
Curb & Gutter				
Sidewalk				
Driveways				
Storm Sewer & Drainage				
Storm Sewer Pipe				
Inlets & Covers				
Manholes & Covers				
Culvert Pipe				
Endwalls				
Granular Backfill				
Landscaping/Erosion Control				
Water				
Topsoil				
Seed, Fertilizer & Mulch				
Silt Fence				
Signing & Marking				
Permanent Signs				
Permanent Marking				
Traffic Control				
Beam Guard w/End Treatments				
Electrical				
Traffic Signals				
Overhead Lighting				
Wire & Conduit				
Box Culverts & Structures				
List Each				
Beam Guard				
Beam Guard End Treatments				
Subtotal				
Add 20% - 30% for Misc. Items, E&C, Mob etc..				
Add Real Estate, Utilities, Detour, RR etc...				
Grand Total				



Estimating Resources

- WisDOT Estimating Tools
 - <https://wisconsindot.gov/Pages/doing-bus/eng-consultants/cnslt-rsrcs/tools/estimating/est-tools.aspx>
- Average Unit Prices Last Three Fiscal Years
 - <https://wisconsindot.gov/hccidocs/contracting-info/average-unit-price.pdf>
- WisDOT Asphalt Price Map and Asphalt Estimating Tools
 - <https://wisconsindot.gov/Pages/doing-bus/eng-consultants/cnslt-rsrcs/tools/estimating/est-tools.aspx>
- WisDOT Construction Cost index
 - <https://wisconsindot.gov/Documents/doing-bus/eng-consultants/cnslt-rsrcs/tools/estimating/wisdot-cci.pdf>
- WisDOT Preliminary Estimate for Local Program Roadway Projects form
 - <https://wisconsindot.gov/Documents/doing-bus/local-gov/astnce-pgms/highway/tools/controlling-road.xlsx>



Don't forget about Engineering Costs

- higher percentage costs for projects with federal funds
- local cost responsibility

Construction Cost	Design + Review
Less than \$250,000	\$75,000 minimum
\$250,000 - \$350,000	26% - 30% (of const. est.)
\$350,000 - \$500,000	20% - 25%
\$500,000 - \$1,000,000	18% - 20%
\$1,000,000 - \$3,000,000	15% - 18%
\$3,000,000 - \$7,000,000	13% - 15%
Over \$7,000,000	10% - 13%

Construction Cost	Const. Oversight
Less than \$250,000	\$50,000 minimum
\$250,000 - \$500,000	20% (of construction est.)
\$500,000 - \$2,000,000	20% - 15%
\$2,000,000 - \$3,000,000	15% - 12%
\$3,000,000 - \$4,000,000	12% - 10%
\$4,000,000 - \$7,000,000	10% - 9%
Over \$7,000,000	8%



Source: WisDOT 2018-2022 STP Urban, STP Rural and Local Bridge Programs Presentation, Robert Schmidt, Local Program Symposium, April 4, 2017



Ben Jordan
(608) 265-4478 office
(608) 692-5578 mobile

bjordan@wisc.edu

Toll Free: (800) 442-4615

TIC Email: tic@interpro.wisc.edu

TIC Website: <http://interpro.wisc.edu/tic>



WisDOT Grant Applications: Tips for Successful Applications

- *Actions to take before the application process:*

1. Decide on the parameters of your project

- Clearly define project need
- Clearly define project scope

2. Conduct stakeholder outreach

- Engage interest in your community
- Obtain letters of support
- Identify potential partners to create larger project benefit



WisDOT Grant Applications: Tips for Successful Applications

- *Actions to take during the application process:*
 1. Complete all sections of application materials
 - Map/location of project area
 - Photos
 2. Review application materials for errors
 3. Tell a story – Engage the reviewer
 4. Submit materials by the application deadline



WisDOT Grant Applications: Tips for Successful Applications

- *Actions to take after the application is submitted:*

1. Be Proactive

- Confirm receipt of application materials
- Inform WisDOT of any changes

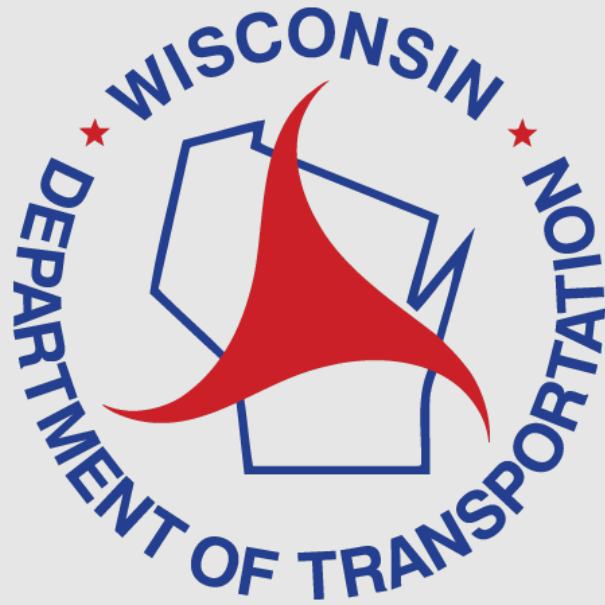
2. Be Patient

- Ask when project awards are likely to be announced
- Do not start work on your project prior to authorization

3. Be Persistent

- Not all applicants are successful the first time
- Improve your application for future submissions





Additional Resources

New BIL Discretionary Webpage

- List of Current BIL Discretionary Funding Opportunities
- How to Apply for a Discretionary Grant
- Grant writing Resources – which also includes links to helpful Federal resources
- Request a Letter of Support from WisDOT's Secretary Thompson
- BIL Discretionary Grant Summary – Opportunity for Locals to let WisDOT know about Discretionary Grant opportunities that they have applied for

The screenshot shows a webpage titled "Bipartisan Infrastructure Law (BIL) - Discretionary funding". On the left, there is a navigation menu with links for "Bipartisan Infrastructure Law funding", "Local programs funding", "Transit assistance programs", and "Other funding". The main content area features a notice: "Information here will be updated as more program details are released by the USDOT." Below this are three expandable sections: "+ Current BIL Discretionary funding opportunities", "+ How to apply for a Discretionary Grant", and "+ Grant writing resources". A section titled "After application is submitted" explains that WisDOT has teamed up with UW TOPS Lab and includes a blue button labeled "Tell us about your Federal Discretionary Grant submission". Another section, "Request Letter of Support", provides instructions on how to request a letter of support from WisDOT, including a blue button labeled "Request Letter of Support". At the bottom, contact information for Jessica L. Wagner, WisDOT Federal Discretionary Fund Coordinator, is provided.



Central Office Local Programs Contacts

Michael Loughran
Local Bridge & STP Statewide Program
Manager
(608) 266-2870
michael.loughran@dot.wi.gov

Tim Olusegun
Program & Policy Supervisor
(608) 266-0254
tim.olusegun@dot.wi.gov

Merrill Mechler-Hickson
Program & Policy Chief
(608) 261-8977
merrill.mechlerhickson@dot.wi.gov

Chris Brooks
BIL Primary Point of Contact
(608) 267-3611
christopher.brooks1@dot.wi.gov



Additional Resources

- WisDOT BIL Homepage
 - <https://wisconsindot.gov/Pages/doing-bus/local-gov/astnce-pgms/highway/bil-lp.aspx>
- WisDOT Local Programs Acronyms & Definitions
 - <https://wisconsindot.gov/Documents/doing-bus/local-gov/lpm/lp-acronyms.pdf>
- YouTube Local Program Project Development Process Training
 - https://www.youtube.com/playlist?list=PLoGzf6P7PsQ-I7QIFRHVVx9BXwD875_yR



Frequently Asked Questions



Thank You!

DOTLocalPrograms@dot.wi.gov

