

# **Public Involvement Meeting Handout**

**Oregon/Jackson Street Bridge, City of Oshkosh  
Fox River Bridge & Approaches  
Winnebago County**

Project ID: 4994-07-00



**April 27, 2016  
5 p.m. – 7 p.m.  
Oshkosh City Hall, Room 404  
215 Church Avenue**



The study team has begun the environmental documentation for this project. Historical and archaeological investigations are underway – if you have knowledge of historical or archaeological significant features along the corridor contact the project manager listed at the end of this handout.

The study team has completed an inspection report on the condition of the existing structure. While the bridge is safe for use, there are numerous deficiencies with the existing bridge that need to be addressed. As the study moves forward and it is determined that improvements are required, any bridge improvement alternatives must address structural and geometric deficiencies of the existing bridge, maintain safe access and passage for all users (including bicyclists, pedestrians, individuals with disabilities, waterway traffic and motor vehicles), meet transportation demand, and comply with all state, regional and local plans.

A summary of the report findings is presented below.

#### Bridge Deficiencies

- Movable bridge machinery shows signs of wear and corrosion and needs replacement
- Deterioration of tack welds and steel plates require repair
- Cracking and spalling on concrete surfaces of bridge piers
- Sidewalks are less than the standard widths
- Approach spans and sidewalks need repair
- Vertical profiles of roadway approaches are below current design standards

#### Bridge Operations

- Bridge tender has poor visibility to monitor vehicles and pedestrians when raising the bridge
- Low vertical clearance – bridge had 400 more openings in 2015 than adjacent bridges

#### Safety

- Vehicle crash rate is above statewide average for similar roadways
- Bridge railing of bascule span is offset into sidewalk, creating potential hazards for pedestrians and bicyclists
- Narrow lane and sidewalk widths do not provide safe bicycle accommodations
- Riverwalk pedestrians and bicycle traffic are required to cross road at grade (cannot cross under bridge), creating a mid-block at-grade pedestrian crossing
- Steel grid panels in deck are offset and present a hazard to bicyclists
- Approach sidewalks have settled and present a tripping hazard

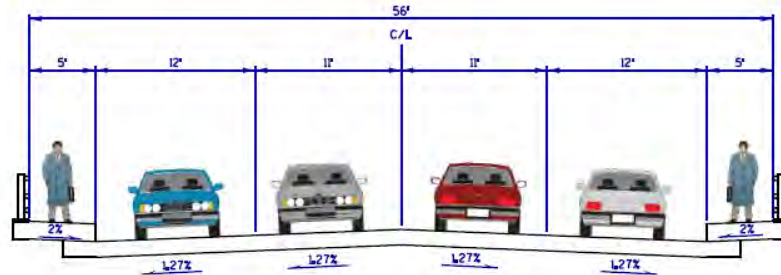
#### **Alternatives that will be evaluated**

**NO-BUILD** – Base alternative ‘do-nothing’

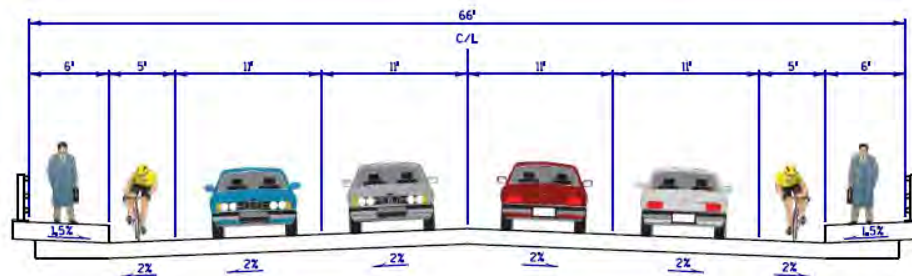
**REHABILITATION** – Repair structure deficiencies (no change to bridge width or clearances)

**FULL REPLACEMENT** – Replace entire bridge on existing or new alignment

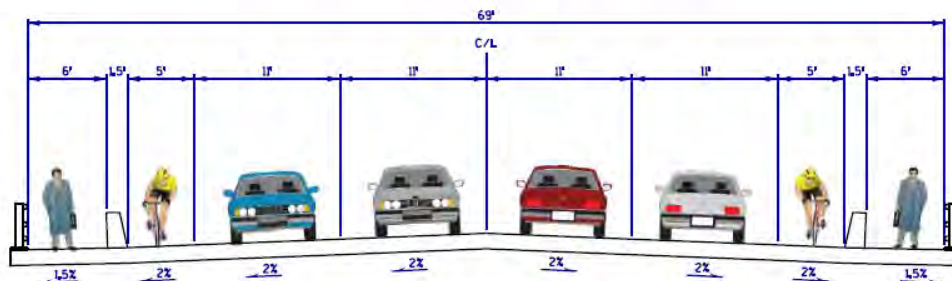
Note: See typical sections on the next page of this handout.



REHABILITATED EXISTING TYPICAL SECTION



RECONSTRUCTED TYPICAL SECTION WITHOUT BARRIER



RECONSTRUCTED TYPICAL SECTION WITH BARRIER

## Proposed traffic impacts

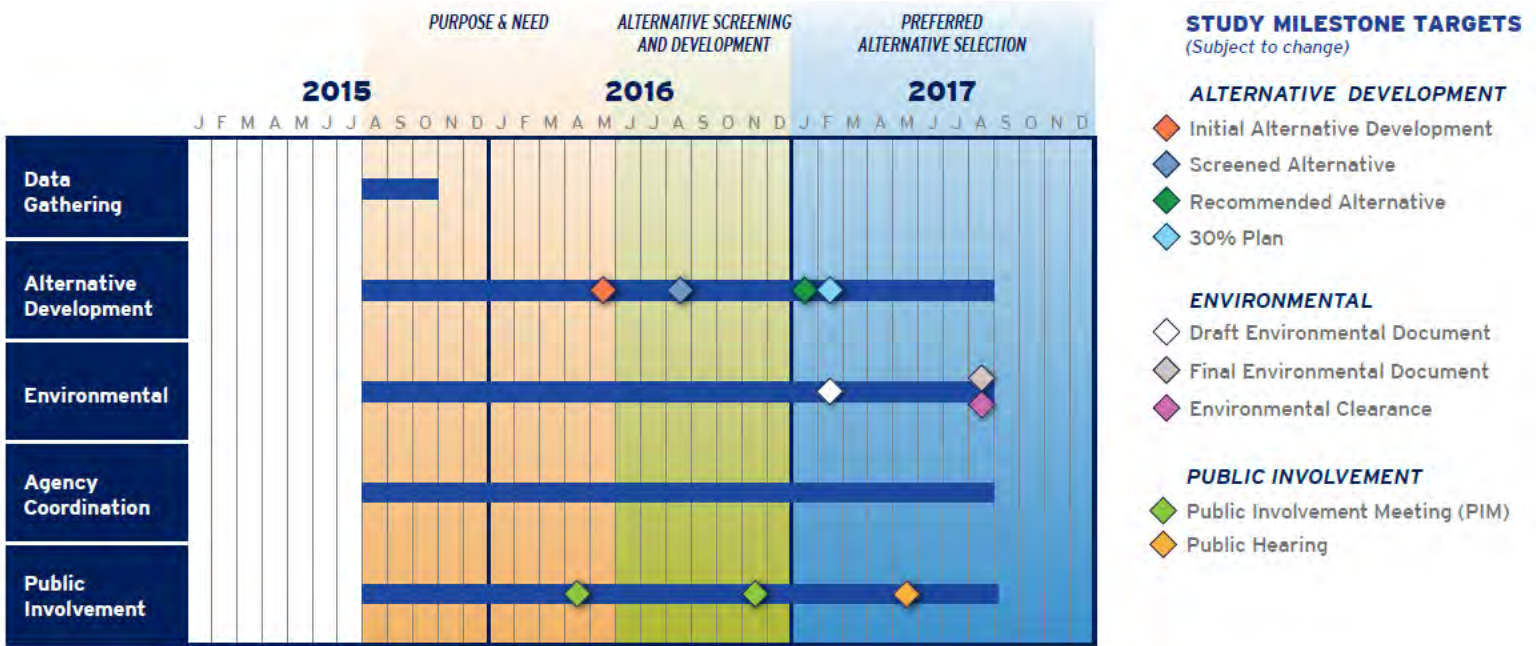
Traffic impacts and potential detours will be addressed at future meetings. It is too early in the process to determine how traffic will be impacted. WisDOT understands minimizing traffic impacts is important along any corridor and will provide updates on any proposed impacts and/or detours as they are determined.

Construction is currently scheduled to begin in 2022 and could extend between 1-2 years depending on the preferred alternative that is selected (see 'Potential Project Schedule' in this handout).

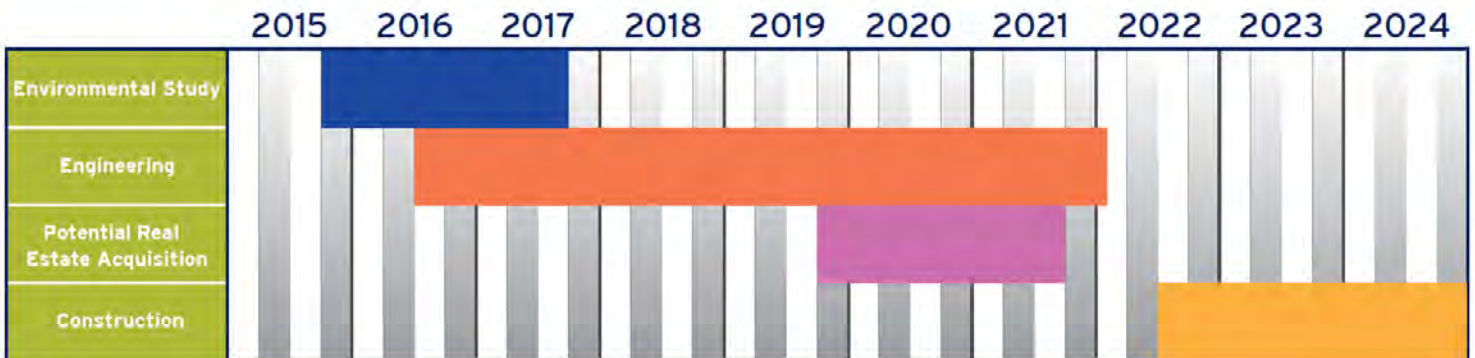
## Project update/next steps

The study team will collect and review all comments received from tonight's meeting. These comments will assist us in completing the study's purpose and need statement and in developing improvement alternatives as needed. We will present the purpose and need and more detailed improvement alternatives at our next public involvement meeting. This next meeting is planned for the fall of 2016. Notice of meeting details will be published and invitations will be sent to those on the project mailing list.

## Study Schedule



## Potential Project Schedule



## **Public input/comments**

We encourage you to talk to the project representatives and ask them questions. Attached to this handout is a sheet for your written comments and input regarding this study. Please mail any written comments about the study before May 13, 2016 or leave them in the comment box tonight. You can also email your comments to the project manager listed below.

Your comments assist us in developing a project that will serve the needs of the traveling public as well as the needs of the local community. Your input is welcome and appreciated throughout the study process.

For more information, please contact:

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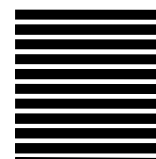
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**Attn: Bill Bertrand, P.E.**

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