

WIS 21 Corridor Preservation Plan

Rivermoor Road to US 41

February 2006

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Mission statement:
To provide leadership in the development and operation of a safe and efficient transportation system.

Included in this issue:

- Page 1: The plan
- Page 2: Land use
- Page 3: Traffic
- Page 4: Goal and objectives
- Page 4: Public meeting

Congestion/safety focus of WIS 21 planning

Motorists are encountering more congestion and crashes when driving WIS 21 in Winnebago County. The Wisconsin Department of Transportation (WisDOT) is working with local jurisdictions addressing the concerns and challenges facing motorists every day along WIS 21 from Rivermoor Road to US 41.

This is an important state highway serving the local economy and linking major population and economic centers in the state. WIS 21 is experiencing traffic growth and congestion. Increasing community development creates additional demand for WIS 21 capacity. Meeting future economic and travel needs are challenged as WIS 21 conditions further deteriorate.

The department is leading a collaborative, long-range planning effort to identify transportation improvements that meet future economic and travel needs. The plan will depict how WIS 21 will look in the future. This plan will also benefit local communities. Knowing the future of WIS 21 allows communities to make informed land use decisions that are compatible with WIS 21.

Long-range means 20 or more years. WIS 21 long-range recommendations may include locations of interchanges, overpasses and local road and access modifications. Short-term and mid-term recommendations may include changing signal timing, lengthening turn lanes, adding turn lanes and providing

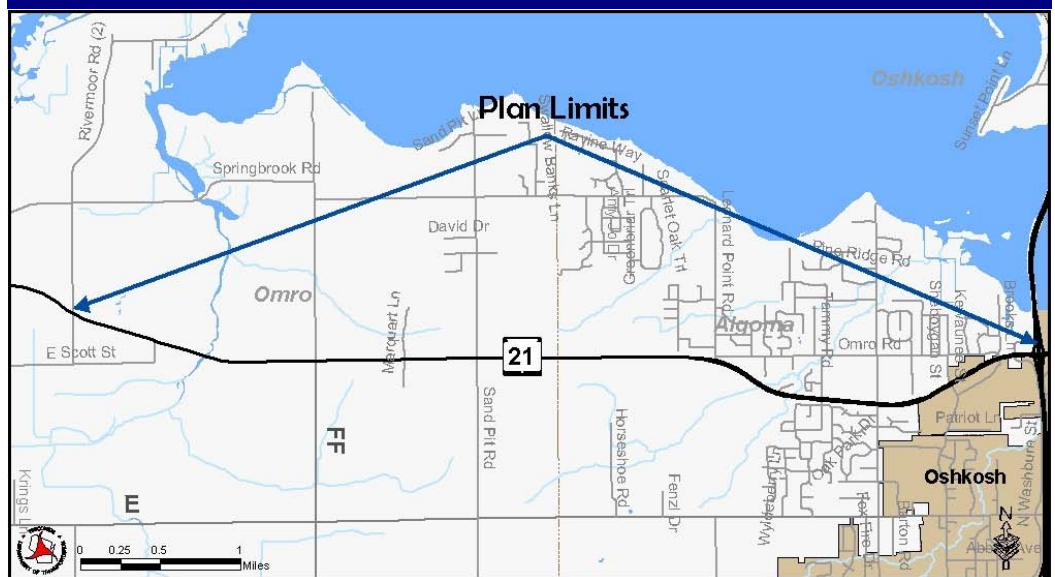
slotted turn lanes.

WisDOT will analyze several transportation improvement alternatives. Public involvement is very important and is necessary for WisDOT to finalize the draft goal and objectives.

Before developing alternatives we want to hear your concerns, thoughts and ideas. **You are invited** to attend a public meeting on **February 21, 2006, 6 to 8 p.m.** at the **Fox Valley Technical College, 150 North Campbell Road, Oshkosh.**

Please note: No construction is scheduled. Plan recommendations will be completed mid-2007 and will help determine improvements and timing of improvements.

Plan Limits: Rivermoor Road to US 41



Development will continue along WIS 21

Winnebago County is experiencing increased development, especially near WIS 21. Using information from local communities, East Central Wisconsin Regional Planning Commission (ECWRPC) developed maps showing existing and future land use.

The future land use map shows significant reduction of agricultural land, shown in brown. Residential (yellow), commercial

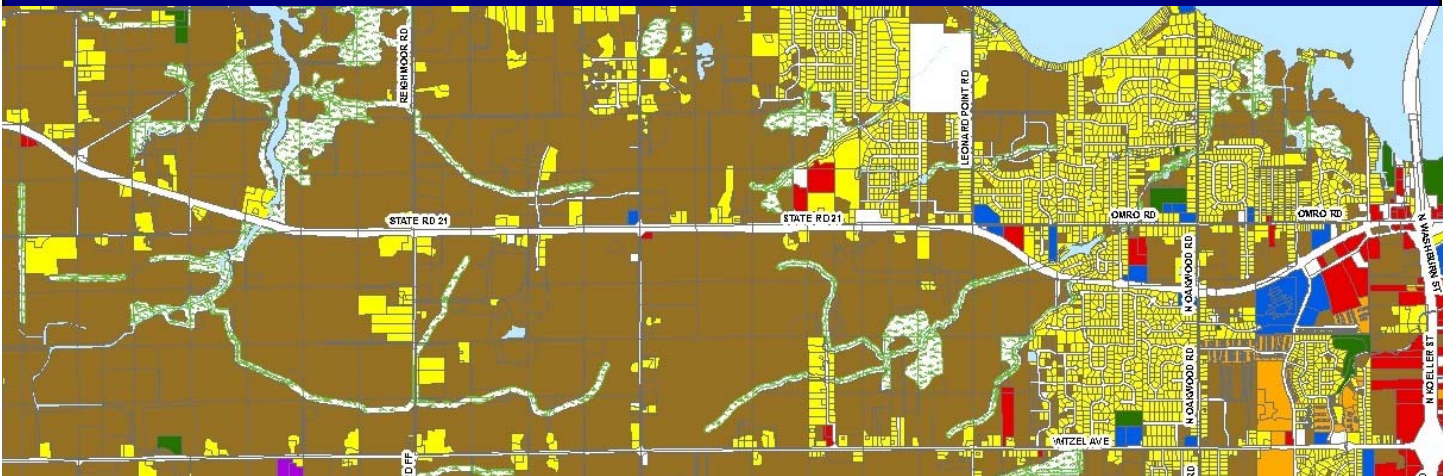
(red) and manufacturing (purple) development will increase.

Land development generates traffic. Different land uses generate different amounts and types of traffic. Most development generates more traffic than agricultural development. Increased traffic generation places additional demand on WIS 21 and the local street system.

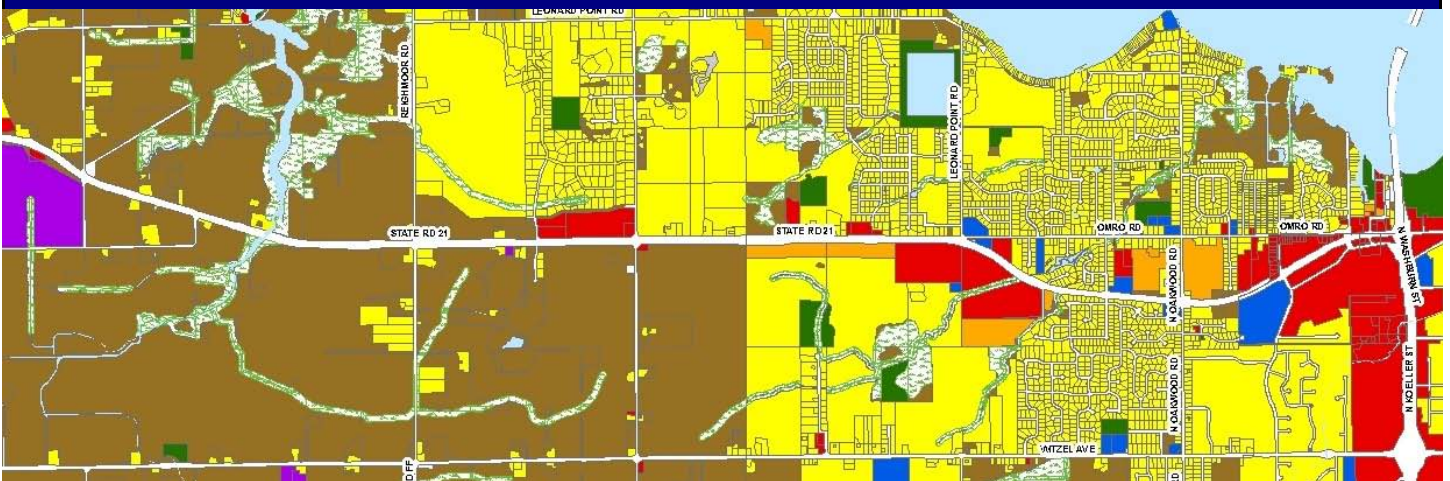
Understanding future land use is critical in determining future traffic conditions and identifying deficiencies on the transportation network serving this area.

“Increased development places additional demands on the transportation system.”

Existing Land Use



Future Land Use



Source: ECWRPC.

Higher than average crash rate, injury rate and fatality rate

The WIS 21 crash rate, injury rate and fatality rate are higher than rates for other comparable state highways. The fatality rate from Omro to Leonard Point Road is 1.8 times the statewide average. However the overall crash rate is lower than the average rate for other two-lane, rural state highways.

The crash rate from Leonard Point Road to US 41 is 2.2 times the statewide average for comparable urban state highways. Not only

is the crash rate higher, but this roadway segment also has a higher than average proportion of crashes involving injuries. The injury rate is almost 2.7 times the statewide average.

Frequent intersections and congestion create less than ideal driving conditions, contributing to higher than average crash rates. Intersections, both signalized and un-signalized, operate poorly during many

times of the day. Intersections and congestion create traffic back-ups causing drivers to stop unexpectedly. Congestion also minimizes the number of large gaps in traffic that allow drivers adequate time to make good driving decisions. Short gaps in traffic give drivers less time to make driving decisions. This results in drivers taking more risks when maneuvering at intersections. **More information is available at the upcoming public meeting.**

WIS 21 crash analysis summary

Segment	All Crashes			Injury Crashes			Fatal Crashes		
	Number	WIS 21 Rate	Statewide Rate	Number	WIS 21 Rate	Statewide Rate	Number	WIS 21 Rate	Statewide Rate
Rural	73	75.26	112	36	37.11	45	3	3.09	1.70
Urban	499	621.97	283	212	264.25	98.20	0	0	0.60

All crash rates expressed as crashes per 100 million vehicle miles traveled.

Congestion and travel time increasing

Commuters traveling WIS 21 can expect more congestion and increased travel times in the future. Increased congestion will force commuters to drive slower, especially between Leonard Point Road and US 41. During the afternoon rush hour in 2035, an eastbound commuter's expected average travel speed is 25 miles per hour between Leonard Point Road to Oakwood Drive. Today, a commuter drives the same segment averaging 49 miles per hour.

How does WisDOT know the future travel conditions? WisDOT is using a traffic model. Locally collected traffic and land use information is used to develop the model. The model produces traffic projections and other related information to help identify problem areas.

Traffic back-ups at intersections will worsen and some will eventually fail to work. It will be more difficult for drivers making turns. This may cause drivers to

Average afternoon rush hour travel speeds on WIS 21

	Algoma West Town Line to Leonard Point Road	Leonard Point Road to Oakwood Drive	Oakwood Drive to US 41
Eastbound			
2005	56.1 mph	49.9 mph	24.2 mph
2015	56.1 mph	49.4 mph	23.1 mph
2035	49.1 mph	24.8 mph	19.5 mph
Westbound			
2005	56.7 mph	49.1 mph	28.6 mph
2015	55.8 mph	49.2 mph	30.2 mph
2035	48.8 mph	36.7 mph	15.4 mph

find alternate travel routes.

Not only will congestion increase on WIS 21, but on other roadways as well. The average travel speed on all roadways in the study area is 39.7 miles per hour today. The average travel speed will decrease to 20.3

miles per hour in 2035.

You are welcome to view the model and additional traffic information at an upcoming **public meeting on February 21, 2005, 6 to 8 p.m. at the Fox Valley Technical College in Oshkosh.**

Wisconsin Department of Transportation



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Address

Improved traffic circulation and economic viability wanted

Local officials, businesses and agencies stated congestion and safety as problems needing improvement. These stakeholders also identified WIS 21 as the communities' economic lifeline. WisDOT will use this information to develop the plan's goal and objectives and determine transportation improvements. Recommended transportation improvements best meet the plan's goal and objectives. The draft goal is:

Meet long-term transportation needs by providing safe, dependable and efficient

movement of traffic while enhancing economic viability.

Objectives are statements describing how the goal will be met. Draft objectives include:

- Improve mobility and operating capacity by reducing congestion and travel time.
- Improve safety by reducing intersection-related crashes and conflict points.
- Improve aesthetics by minimizing billboards and utilizing community

sensitive design techniques.

- Enhance economic viability by reducing congestion and improving accessibility.
- Find an acceptable balance between mobility and accessibility.
- Develop transportation improvements that are compatible with sound comprehensive plans.
- Provide transportation improvements using cost effective strategies.
- Provide a transportation system that minimizes negative impacts on the environment and community.

Public meeting

You are invited to help determine the future of WIS 21 at a public meeting scheduled:

February 21, 2006

6 p.m. to 8 p.m.

Fox Valley Technical College, Oshkosh Campus, 150 North Campbell Road

Brief presentations at 6:15 p.m. and 7:00 p.m.