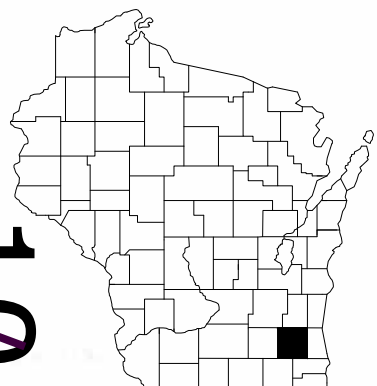


ORDER OF SHEETS

| | |
|---------------|------------------------------|
| Section No. 1 | Title |
| Section No. 2 | General Notes |
| Section No. 2 | Project Overview |
| Section No. 2 | Typical Sections and Details |
| Section No. 2 | Construction Details |
| Section No. 2 | Paving Details |
| Section No. 2 | Erosion Control |
| Section No. 2 | Permanent Signing |
| Section No. 2 | Pavement Marking |
| Section No. 2 | Detour Plan |
| Section No. 2 | Alignment Plan |
| Section No. 3 | Miscellaneous Quantities |
| Section No. 4 | Right of Way Plat |
| Section No. 5 | Plan and Profile |
| Section No. 9 | Earthwork Quantities |
| Section No. 9 | Cross Sections |

TOTAL SHEETS = 94



DESIGN DESIGNATION

| | | |
|---------------|---|-----------|
| A.A.D.T. 2022 | = | 2,800 |
| A.A.D.T. 2042 | = | 3,900 |
| D.H.V. | = | - |
| D.D. | = | - |
| T. | = | 5.24% |
| DESIGN SPEED | = | 50 M.P.H. |
| ESALS | = | 419,175 |

CONVENTIONAL SYMBOLS

| | | | |
|--------------------------------|--|---|--|
| PLAN | | PROFILE | |
| CORPORATE LIMITS | | GRADE LINE | |
| PROPERTY LINE | | ORIGINAL GROUND | |
| LOT LINE | | MARSH OR ROCK PROFILE (To be noted as such) | |
| LIMITED HIGHWAY EASEMENT | | SPECIAL DITCH | |
| EXISTING RIGHT OF WAY | | GRADE ELEVATION | |
| PROPOSED OR NEW R/W LINE | | CULVERT (Profile View) | |
| SLOPE INTERCEPT | | UTILITIES | |
| REFERENCE LINE | | ELECTRIC | |
| EXISTING CULVERT | | FIBER OPTIC | |
| PROPOSED CULVERT (Box or Pipe) | | GAS | |
| COMBUSTIBLE FLUIDS | | SANITARY SEWER | |
| MARSH AREA | | STORM SEWER | |
| WOODED OR SHRUB AREA | | TELEPHONE | |
| | | WATER | |
| | | UTILITY PEDESTAL | |
| | | POWER POLE | |
| | | TELEPHONE POLE | |

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

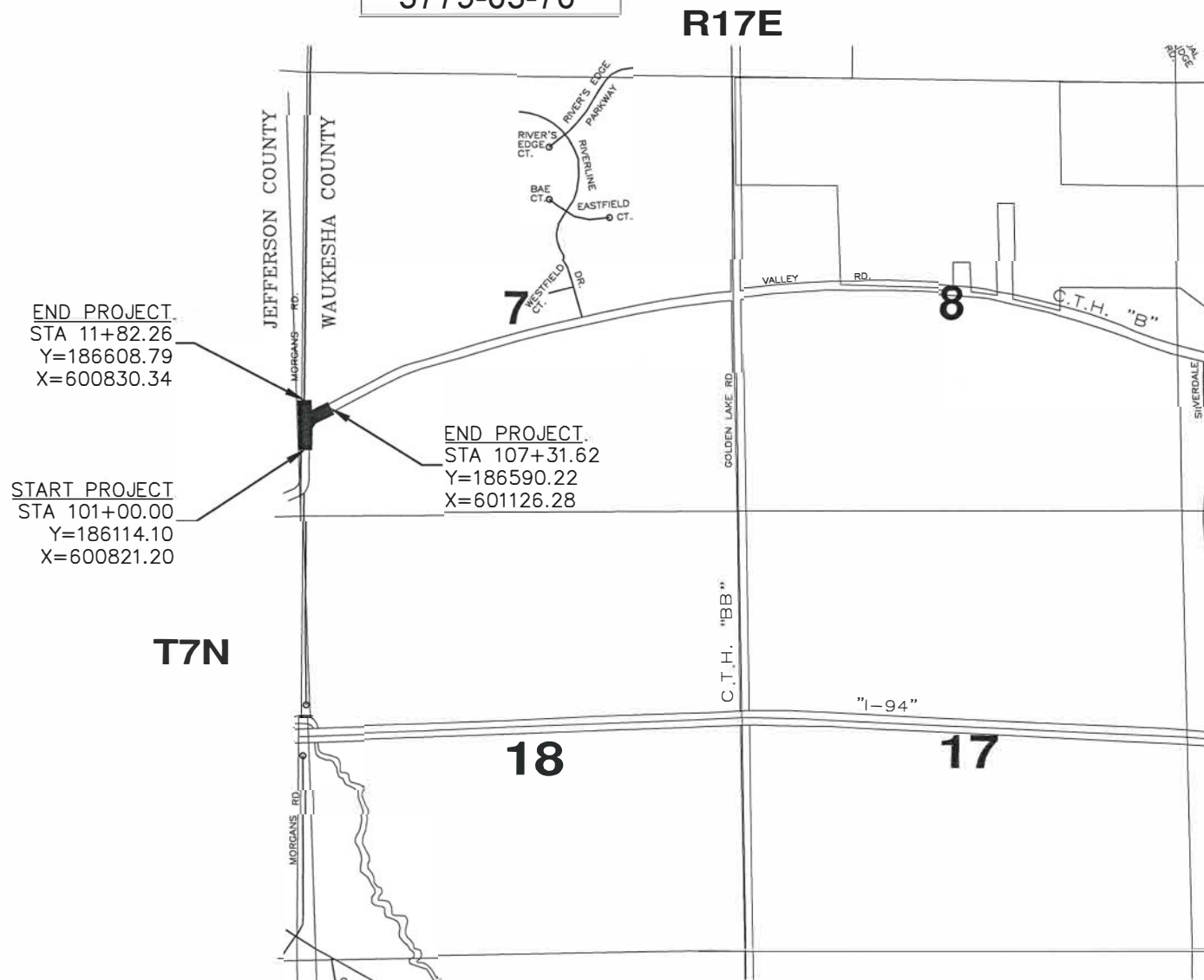
PLAN OF PROPOSED IMPROVEMENT

CTH B

INTERSECTION WITH MORGAN ROAD

CTH B
WAUKESHA COUNTY

STATE PROJECT NUMBER
3779-03-70



LAYOUT
SCALE 0 1/4 MILE
TOTAL NET LENGTH OF CENTERLINE = 0.120 MI. (URBAN)

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, WAUKESHA COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL ELEVATIONS ON THIS PLAN ARE REFERENCED TO NAVD 88 (2012) USING THE WISCORS GEOID 12A IN US SURVEY FEET.

| STATE PROJECT | FEDERAL PROJECT | |
|---------------|-----------------|----------|
| | PROJECT | CONTRACT |
| 3779-03-70 | WISC 2022488 | 1 |
| | | |
| | | |

APPROVED FOR
WAUKESHA COUNTY
DEPARTMENT OF PUBLIC WORKS

4-28-22 *Alison Busher*
Date Director

4/28/22 *Kevin Brown*
Date Engineering Services Manager

ORIGINAL PLANS PREPARED BY



5/2/22 *N.D. Beth*
Date Signature

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor WAUK. CO. DEPT. OF PUBLIC WORKS

Designer WAUK. CO. DEPT. OF PUBLIC WORKS

Project Manager GREG HAFEMAN, P.E.

Regional Examiner _____

Regional Supervisor JEFFREY BOHEN, P.E.

APPROVED FOR THE DEPARTMENT

DATE: 04/28/2022 *Greg Hafeman*
(Signature)

SHEET 1 E

GENERAL NOTES

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS INDICATED FOR REMOVAL BY THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS, AS SHOWN ON THE PLANS, ARE APPROXIMATE. THERE MAY BE OTHER UTILITIES AND UTILITY INSTALLATIONS WITHIN THE PROJECT LIMITS THAT ARE NOT SHOWN.

EXCAVATION BELOW SUBGRADE (EBS) SHALL NOT BE USED TO BALANCE YARDAGE. EBS IS NOT SHOWN ON THE CROSS SECTIONS, BUT WILL BE MEASURED AND PAID FOR AS EXCAVATION COMMON. THE PRECISE LOCATION OF THE EBS WILL BE DETERMINED BY THE ENGINEER.

PAVEMENTS ARE TO BE SAWCUT, AS INDICATED ON THE PLANS, TO PROVIDE A BUTT JOINT AT THE PROJECT LIMITS AND AT ALL ASPHALTIC DRIVEWAYS. SAWCUT AREAS ARE TO BE SQUEEGEED INTO NEAREST GRAVEL SHOULDER OR CURB AND GUTTER AREA AND REMOVED FROM THE SITE BEFORE MOVING TO NEXT SAW CUT AREA.

WHEN THE QUANTITY OF THE ITEMS OF BASE AGGREGATE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE. THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

THE CRUSHED AGGREGATE FOR SHOULDERS ADJACENT TO THE HMA PAVEMENT SHALL NOT BE PLACED UNTIL AFTER THE SURFACE LAYER OF THE HMA PAVEMENT HAS BEEN LAID.

ALL DISTURBED AREAS WITHIN THE RIGHT OF WAY SHALL BE COVERED WITH SALVAGED TOPSOIL OR TOPSOIL AND HYDROSEEDING.

SEED QUANTITIES ARE BASED ON MIXTURE NO. 30.

THE CONTRACTOR SHALL NOTIFY DIGGER'S HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK TO DETERMINE THE LATEST STATUS OF UTILITY RELOCATIONS. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF A ONE-CALL SYSTEM MUST BE CONTACTED SEPARATELY.

RE-TOPSOIL OF GRADED AREAS, AS DESIGNATED BY THE ENGINEER, IMMEDIATELY AFTER GRADING IS COMPLETED WITHIN THOSE AREAS. SEED, FERTILIZE, AND MULCH/EROSION MAT TOP-SOILED AREAS, AS DESIGNATED BY THE ENGINEER, WITHIN FIVE (5) CALENDAR DAYS AFTER PLACEMENT OF TOPSOIL.

ALL DISTURBED/GRADED/SOIL STOCKPILE AREAS SHALL BE PERMANENTLY RESTORED WITHIN 14-DAYS OF ANY CONTRACTOR DISTURBANCE. IF ANY DISTURBED/GRADED/SOIL STOCKPILE AREAS WILL NOT BE PERMANENTLY RESTORED WITHING 14 DAYS, THEY SHALL RECEIVE TEMPORARY SEED AND MULCH WITHIN 5-DAYS OF THE INITIAL DISTURBANCE.

STOCKPILE EXCESS MATERIAL OR SPOILS ON UPLAND AREAS AWAY FROM WETLANDS, FLOODPLAINS AND WATERWAYS. STOCKPILED SOIL SHALL BE PROTECTED AGAINST EROSION.

EROSION CONTROL BMP'S ARE AT SUGGESTED LOCATIONS. THE ACTUAL LOCATIONS WILL BE DETERMINED BY THE CONTRACTORS ECIP AND BY THE ENGINEER. EROSION CONTROL BMP'S SHALL BE MAINTAINED UNTIL PERMANENT VEGETATION IS ESTABLISHED OR UNTIL THE ENGINEER DETERMINES THAT THE BMP IS NO LONGER REQUIRED.

INDEX OF SECTION 2 DRAWINGS

- GENERAL NOTES AND PROJECT CONTACTS
- PROJECT OVERVIEW
- TYPICAL EXISTING SECTIONS
- TYPICAL FINISHED SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- PERMANENT SIGNING
- PAVEMENT MARKING
- SIGNAGE PLANS
- TRAFFIC CONTROL
- MISCELLANEOUS QUANTITIES

STANDARD ABBREVIATIONS

| | | | |
|----------|-------------------------------|------------|--------------------------------------|
| A.D.T. | AVERAGE DAILY TRAFFIC | P.C. | POINT OF CURVATURE |
| AC. | ACRE(S) | P.I. | POINT OF INTERSECTION |
| ASPH. | ASPHALT | P.L. | PROPERTY LINE |
| BM | BENCH MARK | P.R.C. | POINT OF REVERSE CURVATURE |
| C & G | CURB & GUTTER | P.T. | POINT OF TANGENCY |
| CB | CATCH BASIN | PAV'T. | PAVEMENT |
| C.S.C.P. | CORRUGATED STEEL CULVERT PIPE | R | RADIUS |
| C.Y. | CUBIC YARDS | C.P.R.C. | CULVERT PIPE REINFORCED CONCRETE |
| ℄ | CENTERLINE | RHF | RIGHT HAND FORWARD |
| CO. | COUNTY | RT. | RIGHT |
| C.T.H. | COUNTY TRUNK HIGHWAY | R/W | RIGHT OF WAY |
| CWT. | HUNDREDWEIGHT | S | SOUTH |
| D | DEGREE OF CURVE | SAN | SANITARY |
| D.H.V. | DESIGN HOURLY VOLUME | S.B. | SOUTHBOUND |
| DISCH. | DISCHARGE | S.D.D. | STANDARD DETAIL DRAWING |
| E | EAST | S.F. | SQUARE FEET |
| EA. | EACH | S.S.P.R.C. | STORM SEWER PIPE REINFORCED CONCRETE |
| E.B. | EASTBOUND | STA. | STATION |
| ESALS | EQUIVALENT SINGLE AXLE LOADS | S.Y. | SQUARE YARDS |
| ESMT. | EASEMENT | T | TANGENT |
| F.E. | FIELD ENTRANCE | T | TELEPHONE |
| FT. | FOOT (FEET) | T.L.E. | TEMPORARY LIMITED EASEMENT |
| G | GAS | VAR. | VARIES |
| I.P. | IRON PIPE | V.P.C. | VERTICAL POINT OF CURVATURE |
| K | RATE OF VERTICAL CURVATURE | V.P.I. | VERTICAL POINT OF INTERSECTION |
| L | LENGTH | V.P.T. | VERTICAL POINT OF TANGENCY |
| LB. | POUND(S) | W | WATER MAIN |
| L.F. | LINEAR FEET | W | WEST |
| LHF | LEFT HAND FORWARD | W.B. | WESTBOUND |
| LS | LUMP SUM | WV | WATER VALVE |
| LT. | LEFT | YD. | YARDS |
| MH | MANHOLE | | |
| N | NORTH | | |
| N.B. | NORTHBOUND | | |
| NO. | NUMBER | | |

UTILITIES CONTACTS

COMMUNICATIONS:

AT&T
435 S 95th STREET
MILWAUKEE, WI 53214-1226
MATTHEW DINNAUER
PHONE: (262) 237-0012
md9542@att.com

CHARTER / SPECTRUM
1320 N. DR. MARTIN LUTHER KING JR. DR.
MILWAUKEE, WI 53212-3980
NEAL LONG
CONSTRUCTION COORDINATOR II
PHONE: (414) 430-7189
Neal.Long@charter.com

WE-ENERGIES:

ELECTRIC
500 S. 116th STREET
WEST ALLIS, WI 53214
ERIC KICKHAVER
PHONE: (414) 944-5917
eric.kickhaver@we-energies.com

GAS
S13 W33800 STH 18
DELAFIELD, WI 53121
JACOB SPENCER
PHONE: (262) 968-7009
jacob.spencer@we-energies.com

WDNR LIAISON

141 NW BARSTOW ROOM 180
WAUKESHA, WI 53188
CRAIG WEBSTER
ENVIRONMENTAL COORDINATOR - SOUTHEAST REGION
PHONE: (262) 574-2141
craig.webster@wisconsin.gov

SEWRPC LIAISON

W239N1812 ROCKWOOD DR.
P.O. BOX 1607
WAUKESHA, WI 53187-1607
ROB MERRY
PHONE: (262) 953-1036
rmerry@sewrpc.org

WISDOT LIAISON

141 NW BARSTOW ST.
WAUKESHA, WI 53188
GREG HAFEMAN, P.E.
PHONE: (262) 548-8677
greg.hafeman@dot.wi.gov

OTHER CONTACTS

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WAUKESHA, WI 53188

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(262) 548-7740
abussler@waukeshacounty.gov

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kbraun@waukeshacounty.gov

ED HINRICHS
PROJECT MANAGER
(262) 548-7740
ehinrichs@waukeshacounty.gov

NATE BETH
PROJECT DESIGNER
(262) 548-7740
nbeth@waukeshacounty.gov

WAUKESHA COUNTY - HIGHWAY OPERATIONS
1641 WOODBURN RD.
WAUKESHA WI 53188

STEVE SCHULTZ
SIGN/SIGNAL MAINTENANCE
MOBILE: (262)424-9129
sschultz@waukeshacounty.gov

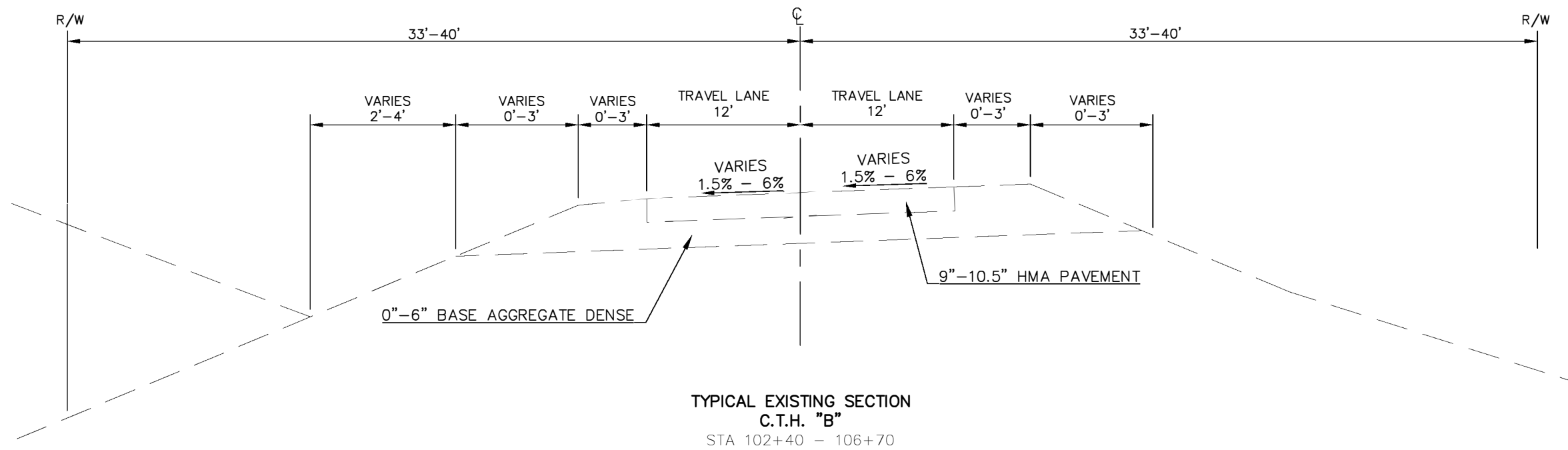
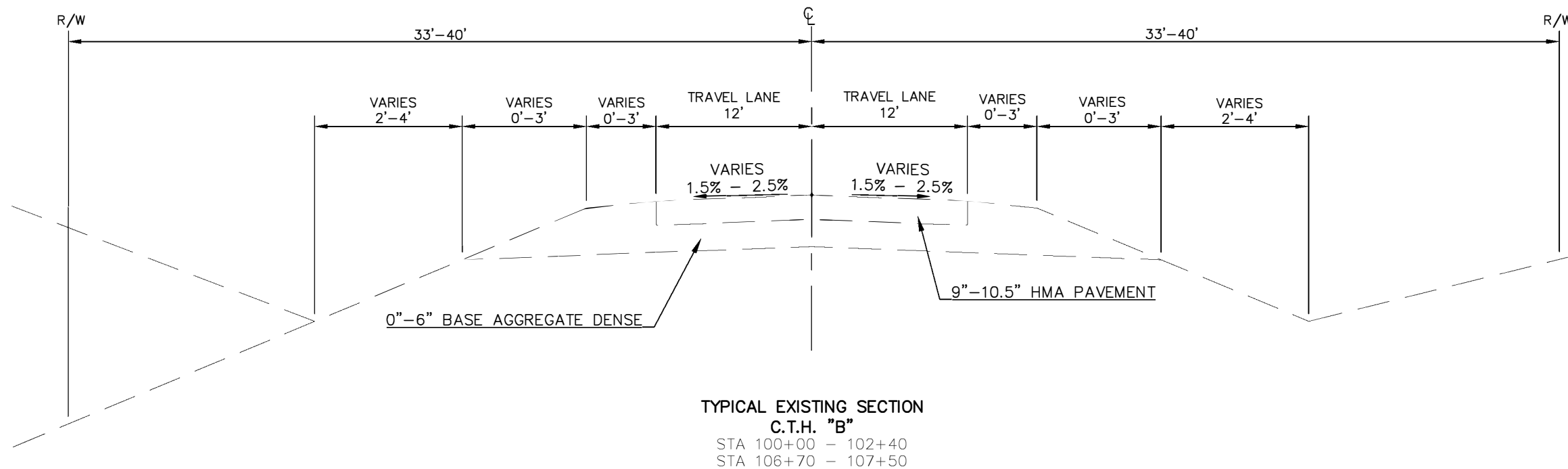
DAN MOUDRY
PATROL SUPERINTENDANT
PHONE: (262) 548-7840
dmoudry@waukeshacounty.gov

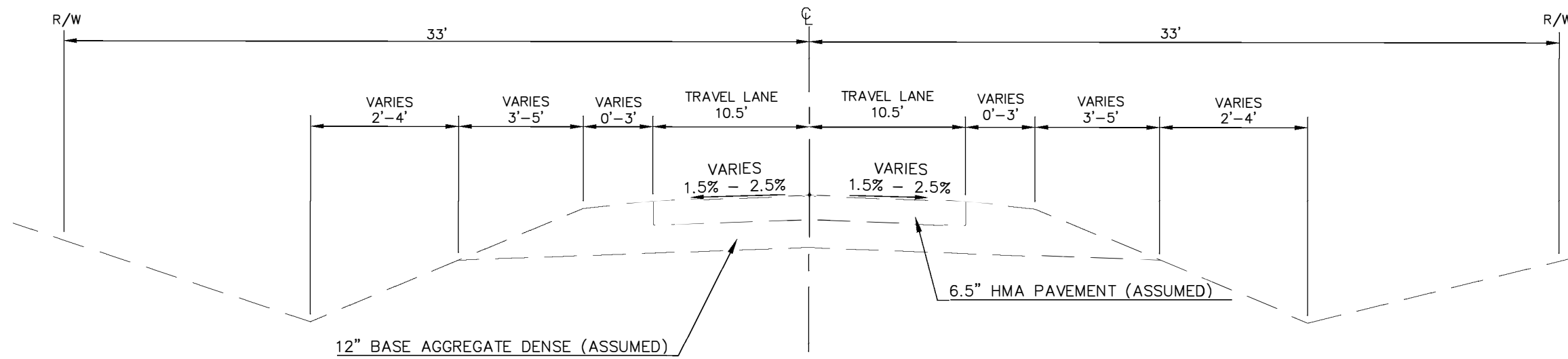


Dial 811 or (800) 242-8511
www.DiggersHotline.com

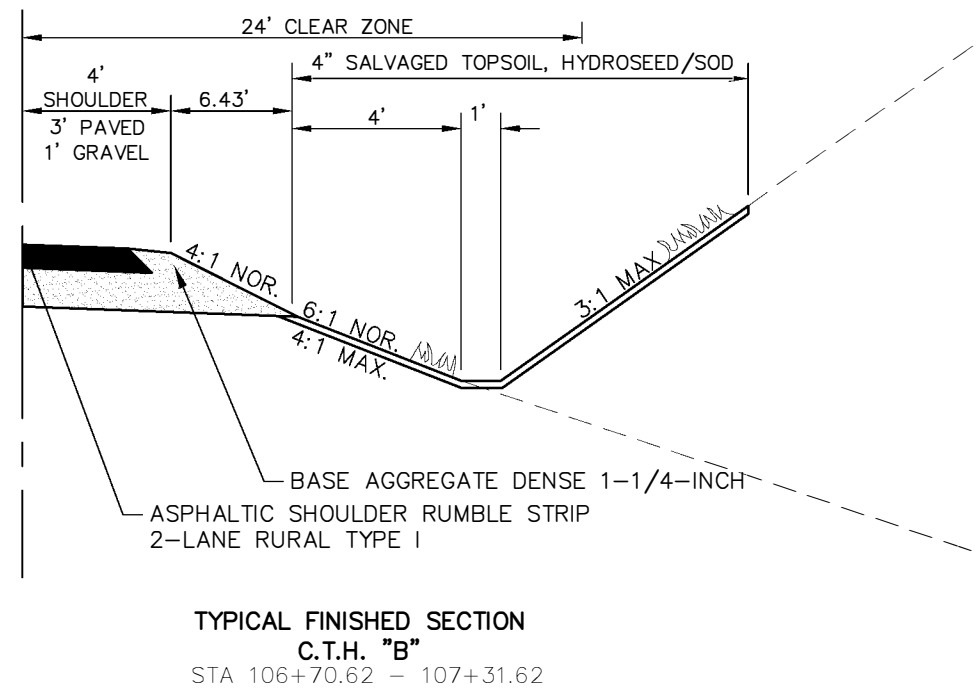
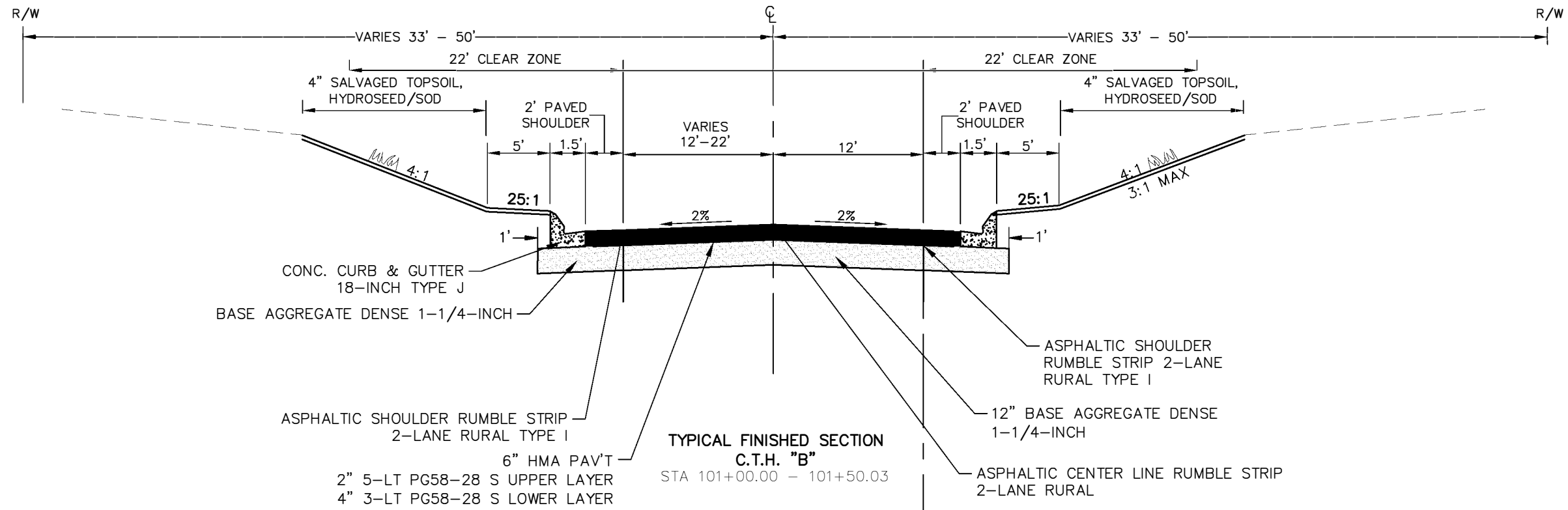
Call 811 3 Work Days Before You Dig
Or Toll Free (800) 242-8511
Hearing Impaired TDD (800) 542-2289
www.DiggersHotline.com

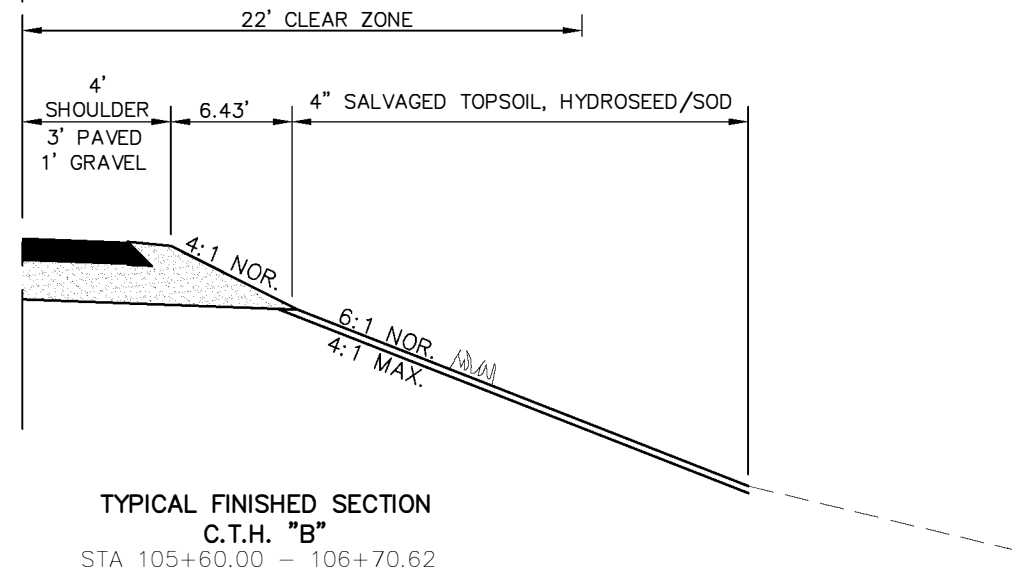
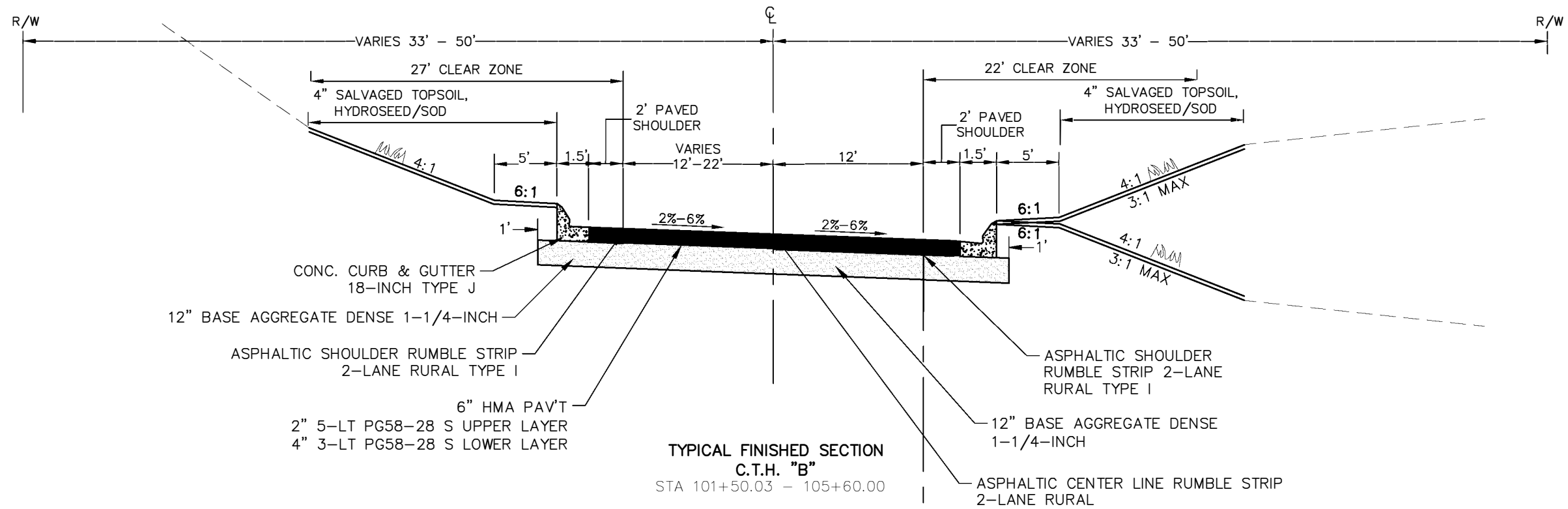


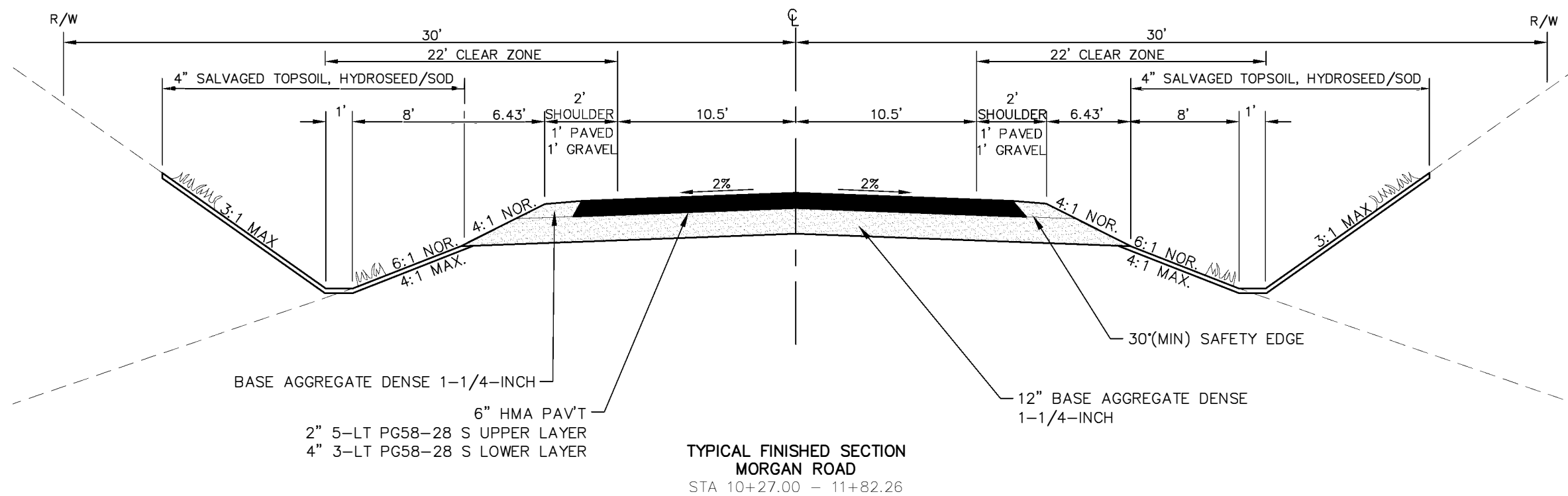


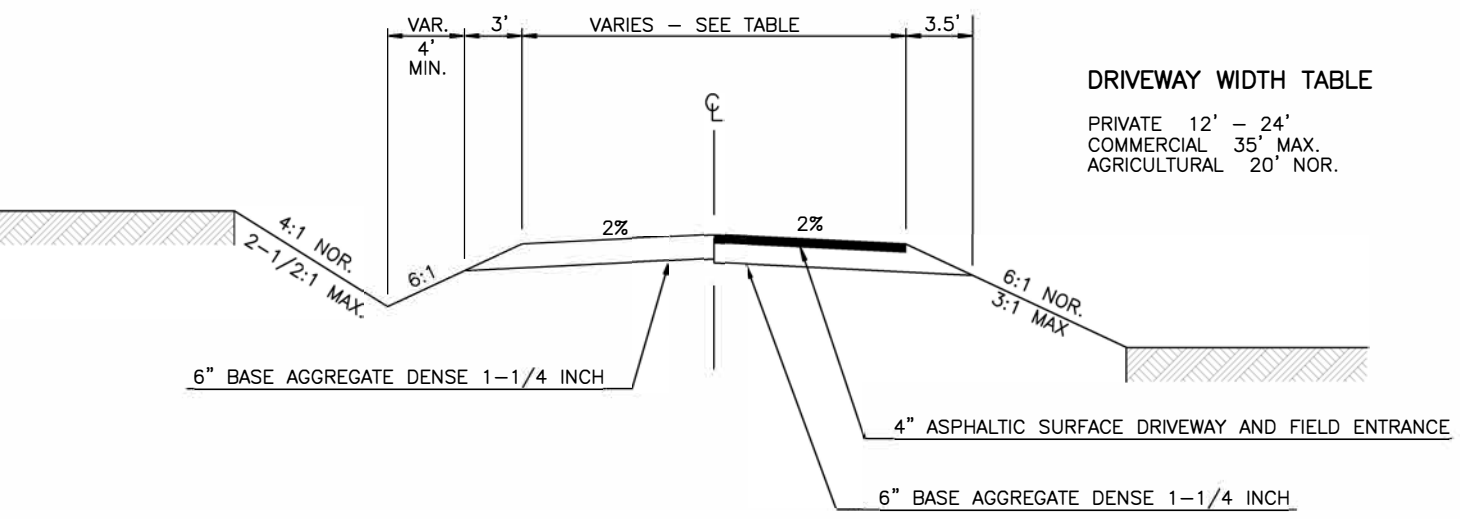
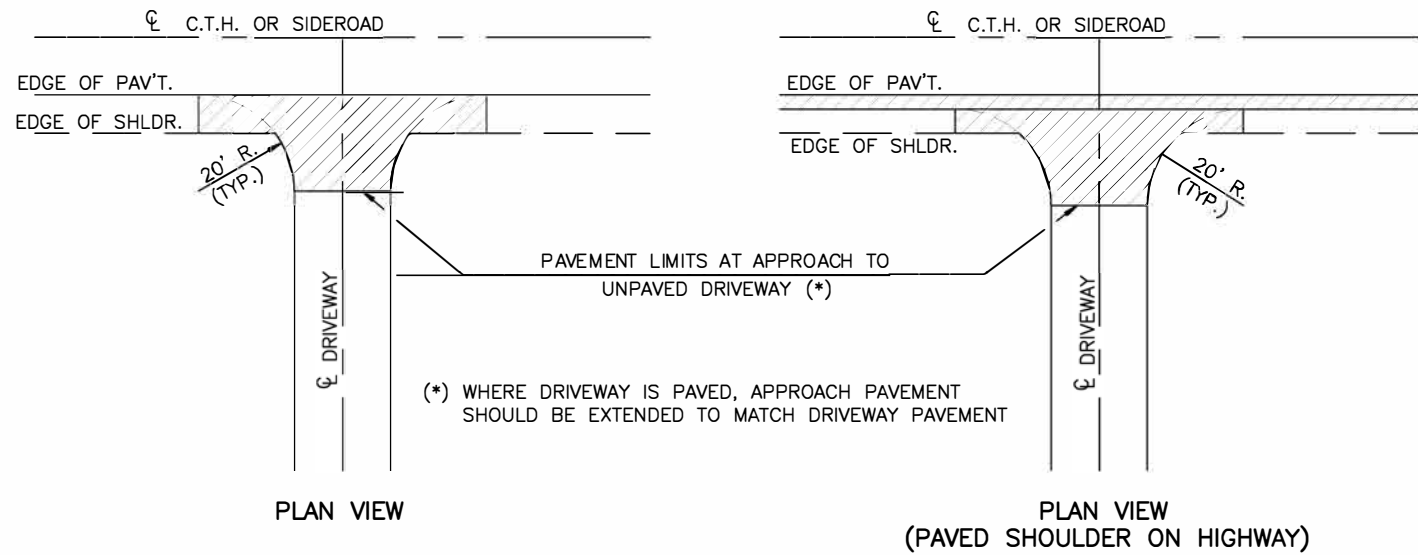


TYPICAL EXISTING SECTION
 MORGAN ROAD
 STA 10+00 - 12+50

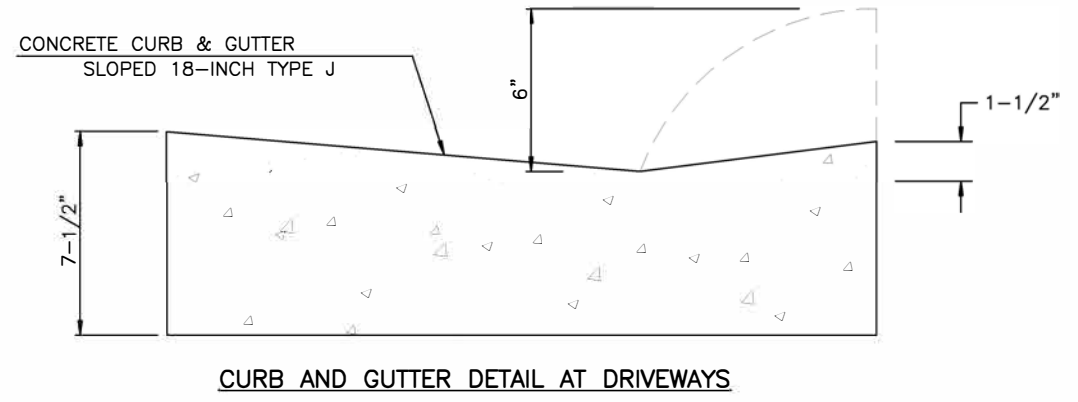
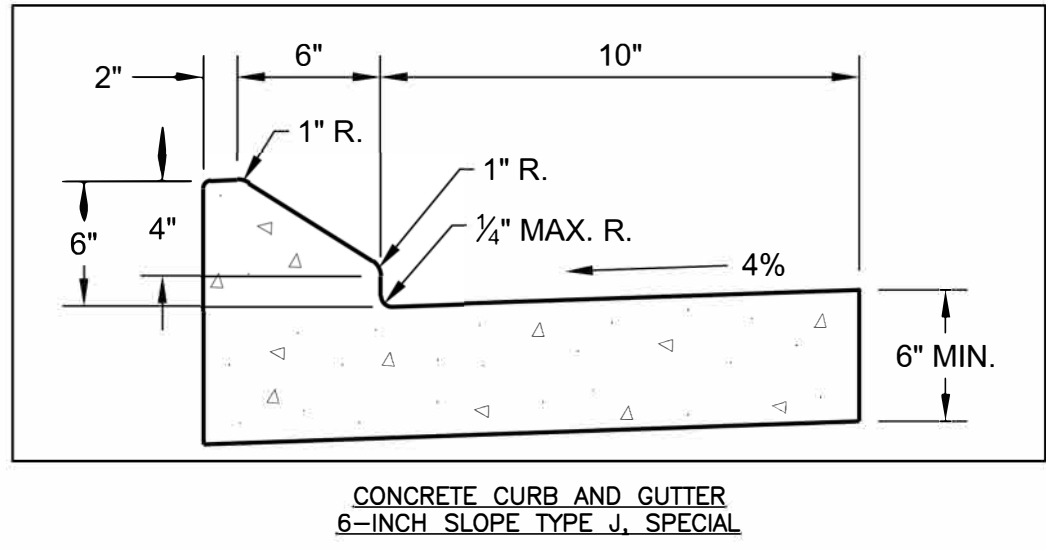
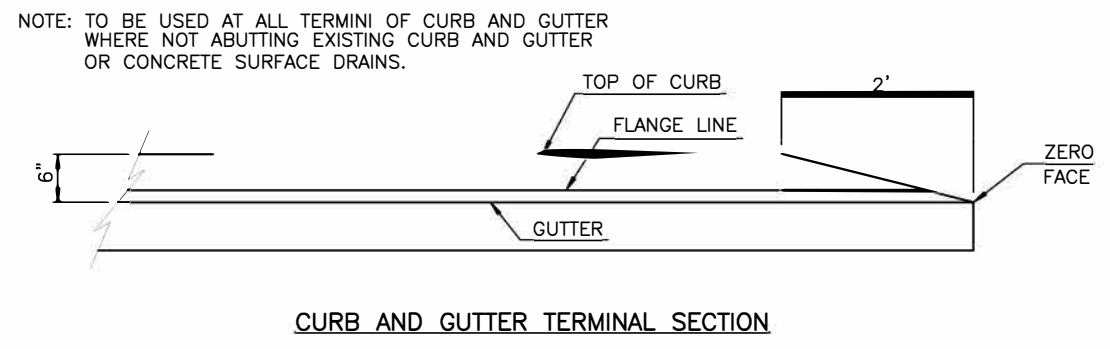
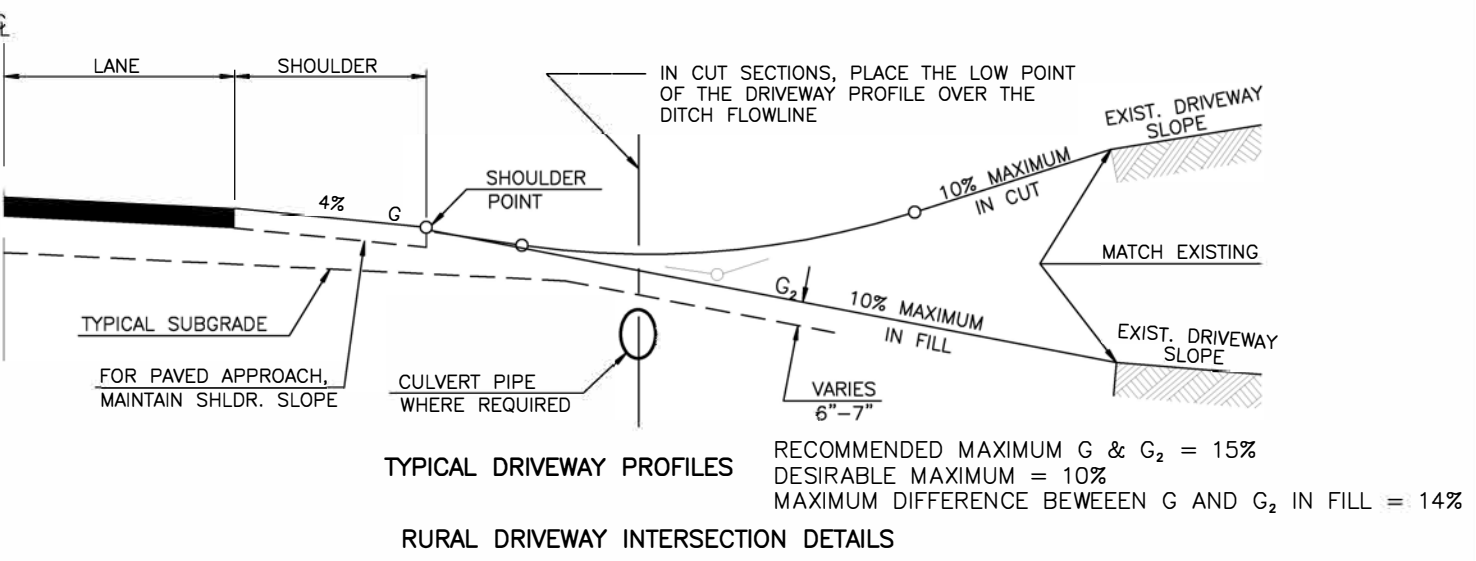


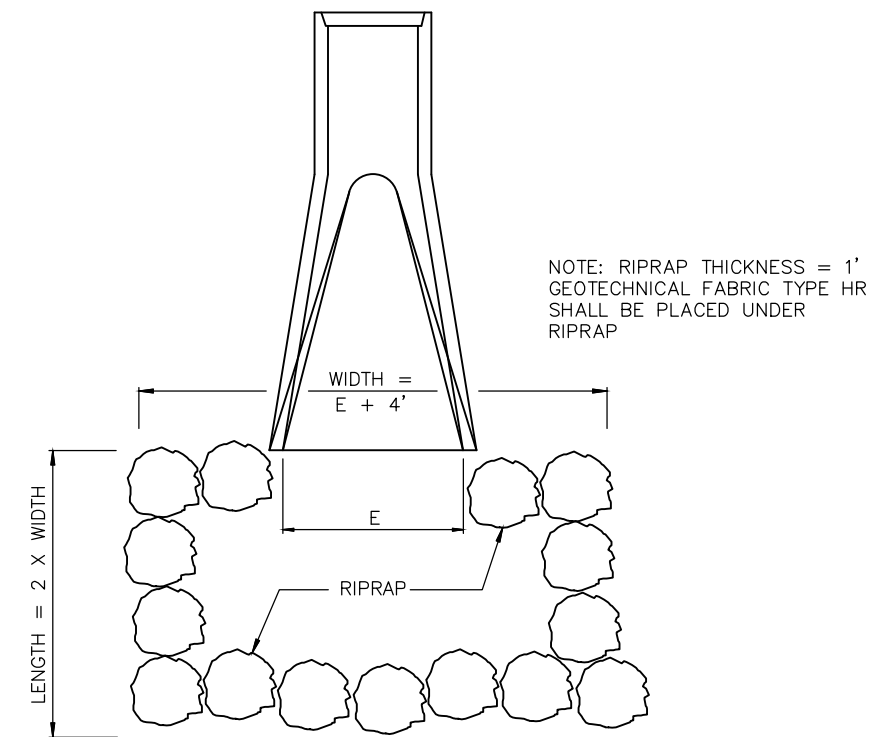




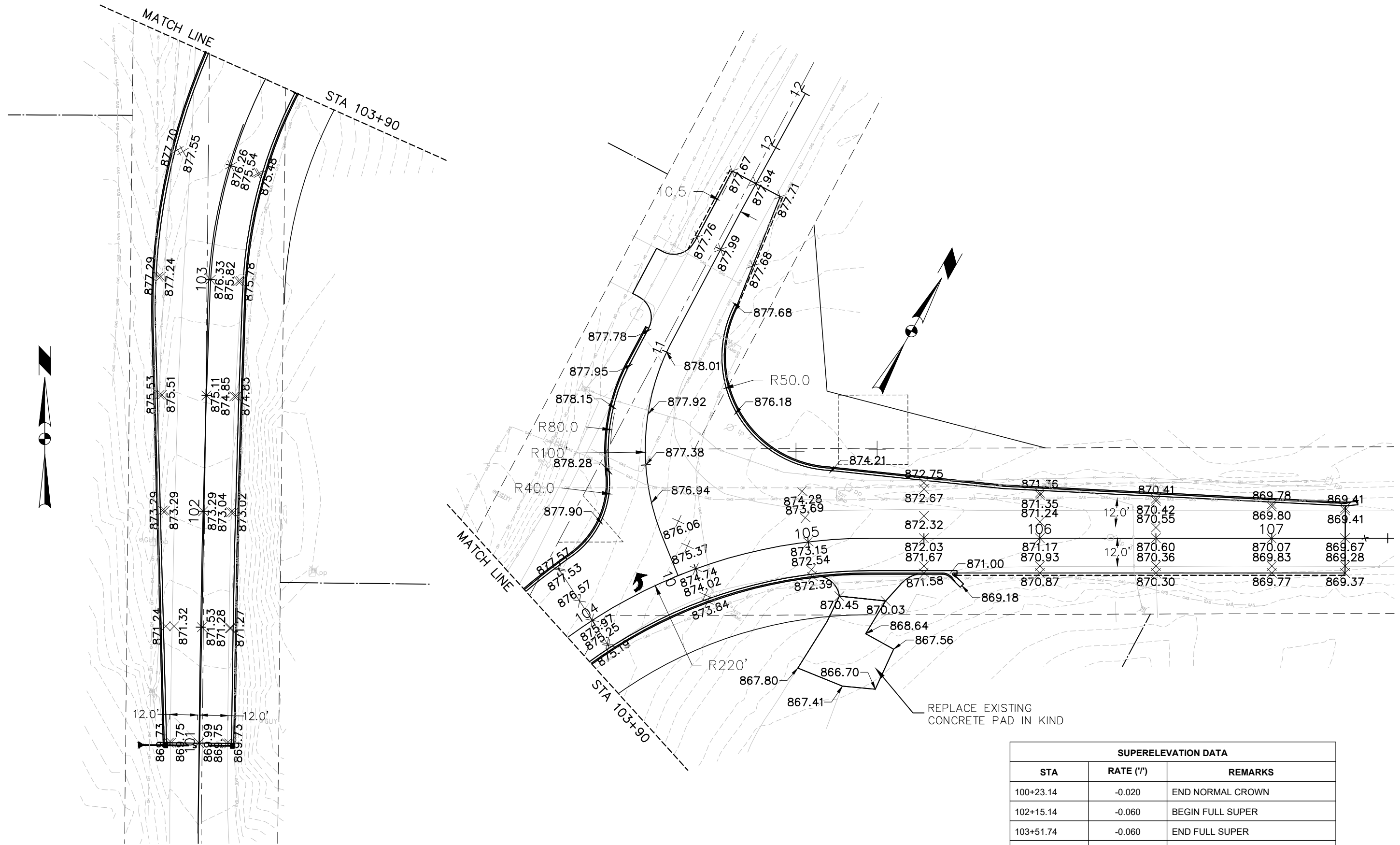


TYPICAL CROSS SECTION FOR PRIVATE DRIVE OR FIELD ENTRANCE





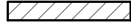

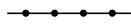



TYPICAL PLACEMENT OF RIPRAP AT PIPE DISCHARGE ENDS



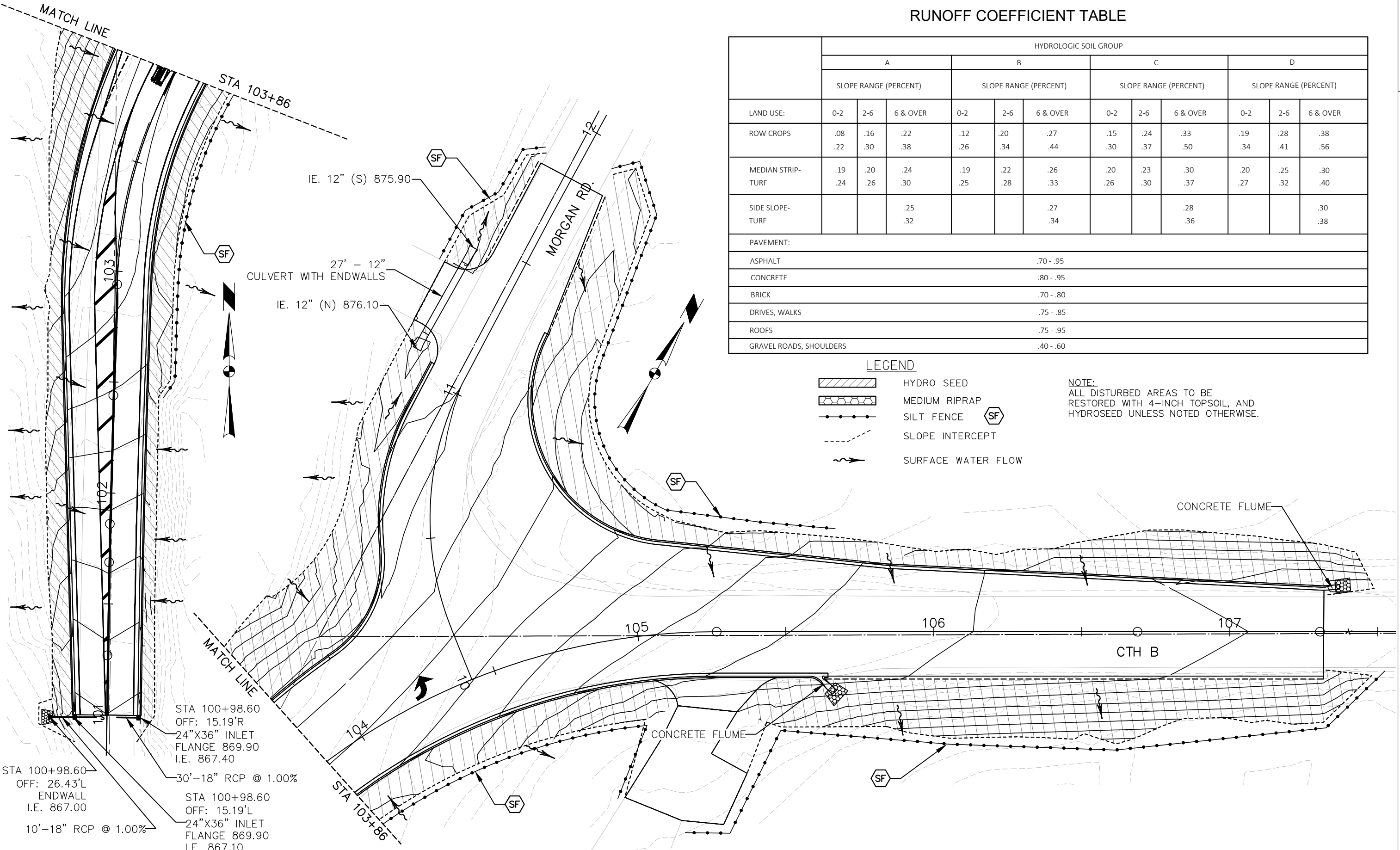
RUNOFF COEFFICIENT TABLE

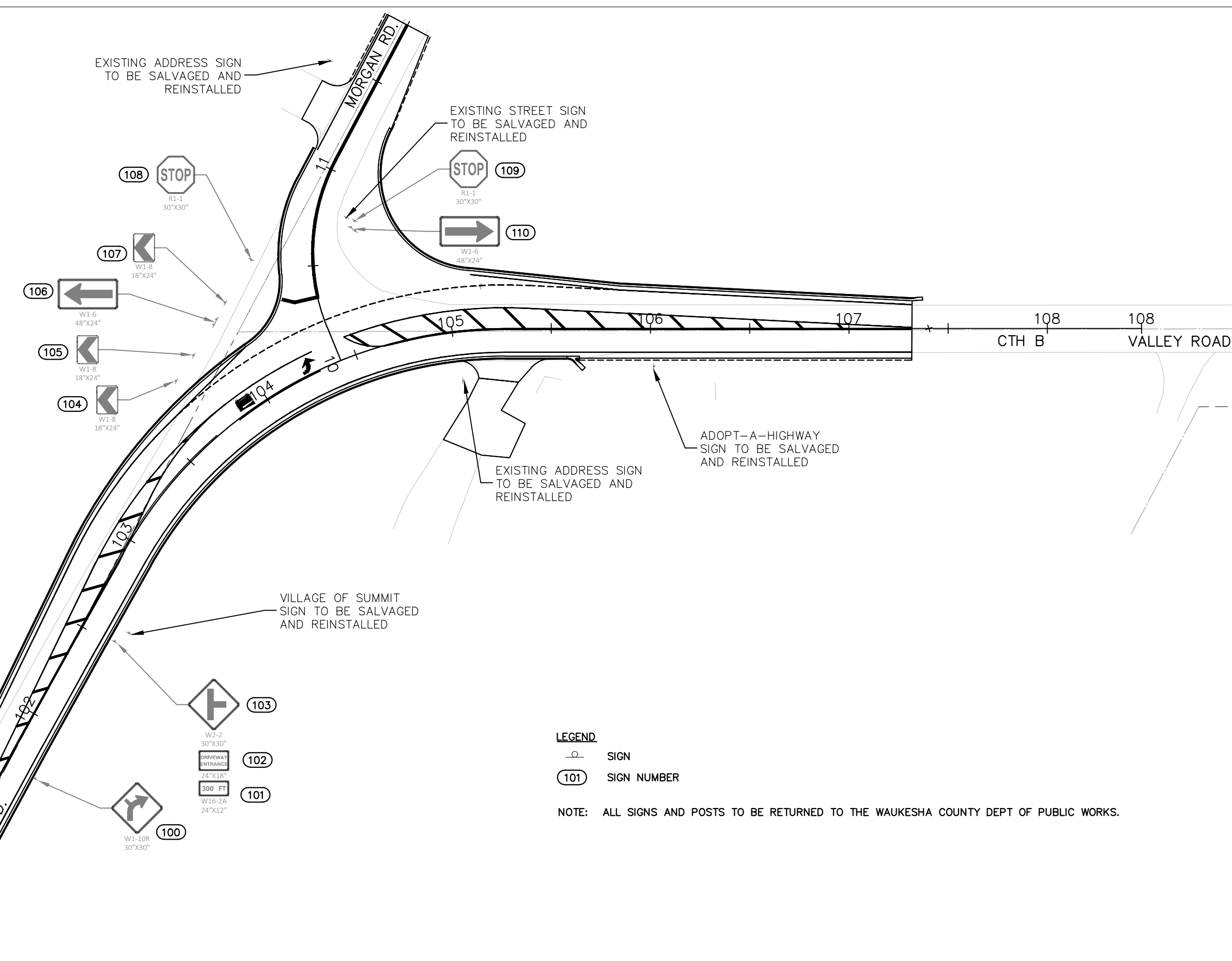
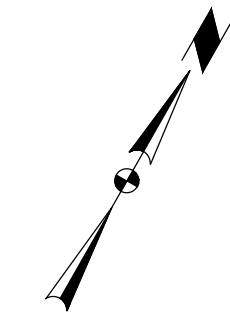
| | HYDROLOGIC SOIL GROUP | | | | | | | | | | | |
|-------------------------|-----------------------|-----|----------|-----------------------|-----|----------|-----------------------|-----|----------|-----------------------|-----|----------|
| | A | | | B | | | C | | | D | | |
| | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | |
| LAND USE: | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER |
| ROW CROPS | .08 | .16 | .22 | .12 | .20 | .27 | .15 | .24 | .33 | .19 | .28 | .38 |
| | .22 | .30 | .38 | .26 | .34 | .44 | .30 | .37 | .50 | .34 | .41 | .56 |
| MEDIAN STRIP-TURF | .19 | .20 | .24 | .19 | .22 | .26 | .20 | .23 | .30 | .20 | .25 | .30 |
| | .24 | .26 | .30 | .25 | .28 | .33 | .26 | .30 | .37 | .27 | .32 | .40 |
| SIDE SLOPE-TURF | | | .25 | | | .27 | | | .28 | | | .30 |
| | | | .32 | | | .34 | | | .36 | | | .38 |
| PAVEMENT: | | | | | | | | | | | | |
| ASPHALT | .70 - .95 | | | | | | | | | | | |
| CONCRETE | .80 - .95 | | | | | | | | | | | |
| BRICK | .70 - .80 | | | | | | | | | | | |
| DRIVES, WALKS | .75 - .85 | | | | | | | | | | | |
| ROOFS | .75 - .95 | | | | | | | | | | | |
| GRAVEL ROADS, SHOULDERS | .40 - .60 | | | | | | | | | | | |

LEGEND

-  HYDRO SEED
-  MEDIUM RIPRAP
-  SILT FENCE 
-  SLOPE INTERCEPT
-  SURFACE WATER FLOW

NOTE:
ALL DISTURBED AREAS TO BE RESTORED WITH 4-INCH TOPSOIL, AND HYDROSEED UNLESS NOTED OTHERWISE.



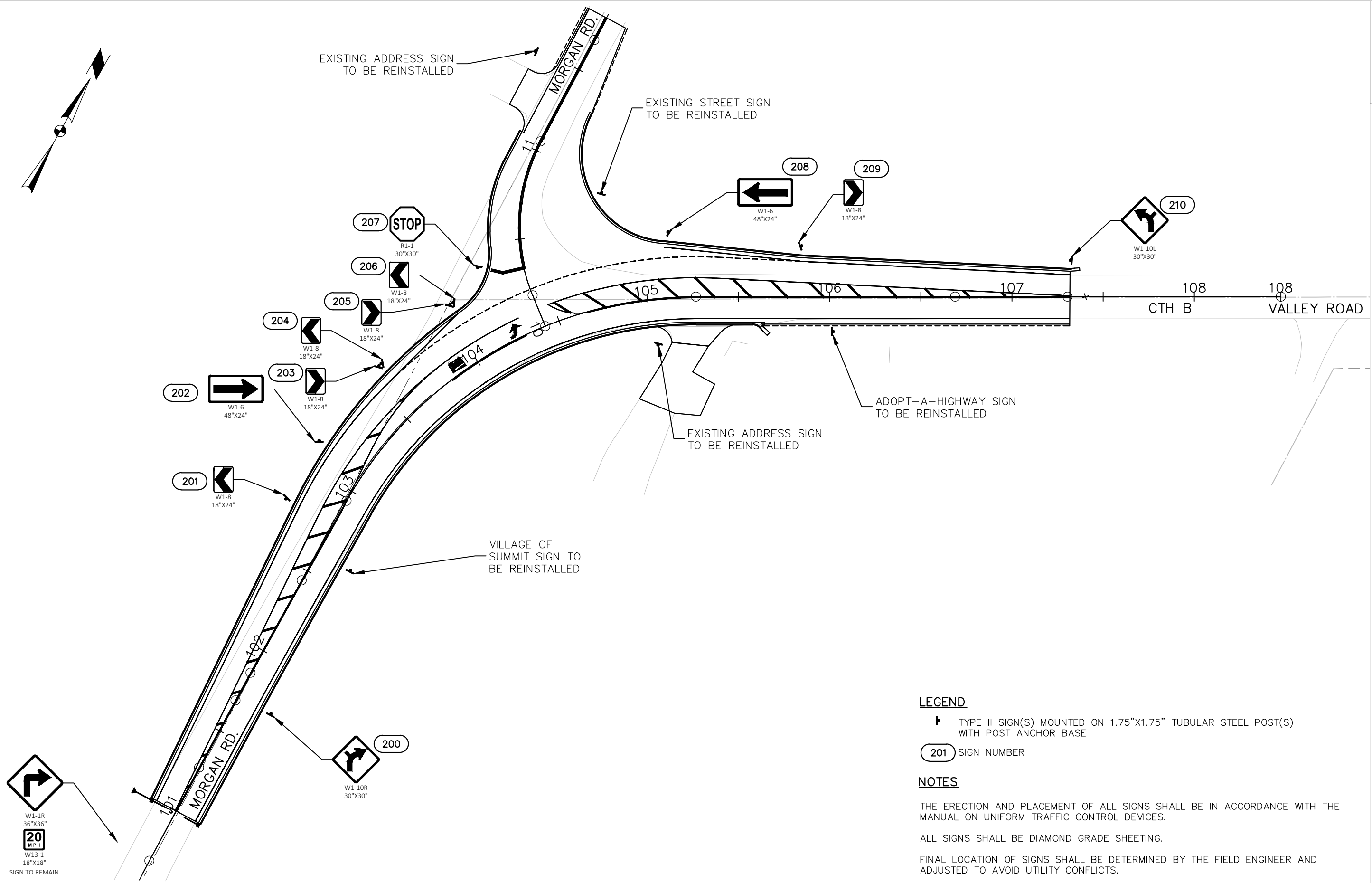


LEGEND

○ SIGN

① SIGN NUMBER

NOTE: ALL SIGNS AND POSTS TO BE RETURNED TO THE WAUKESHA COUNTY DEPT OF PUBLIC WORKS.



LEGEND

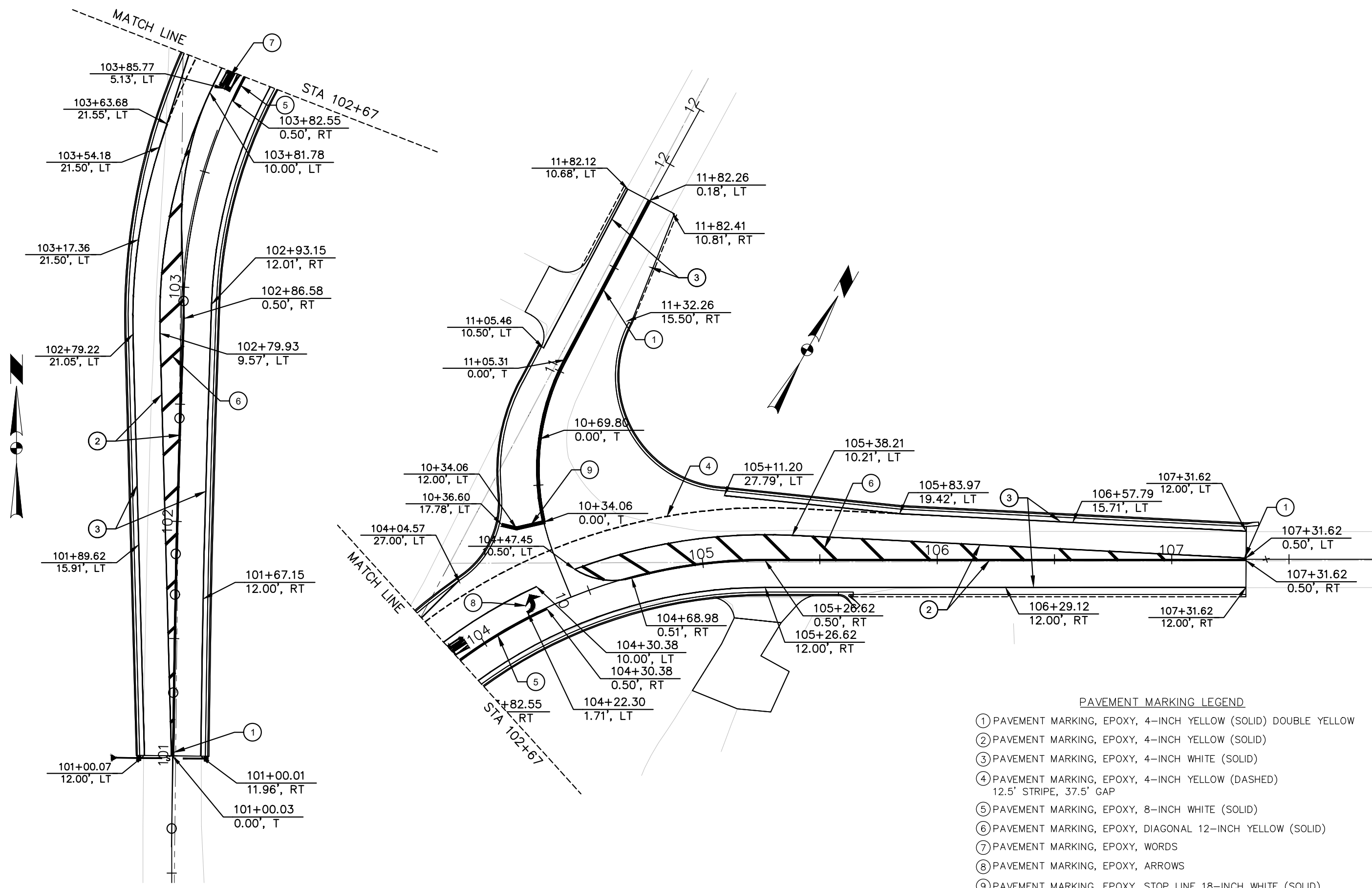
- ▬ TYPE II SIGN(S) MOUNTED ON 1.75"x1.75" TUBULAR STEEL POST(S) WITH POST ANCHOR BASE
- 201 SIGN NUMBER

NOTES

THE ERECTION AND PLACEMENT OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

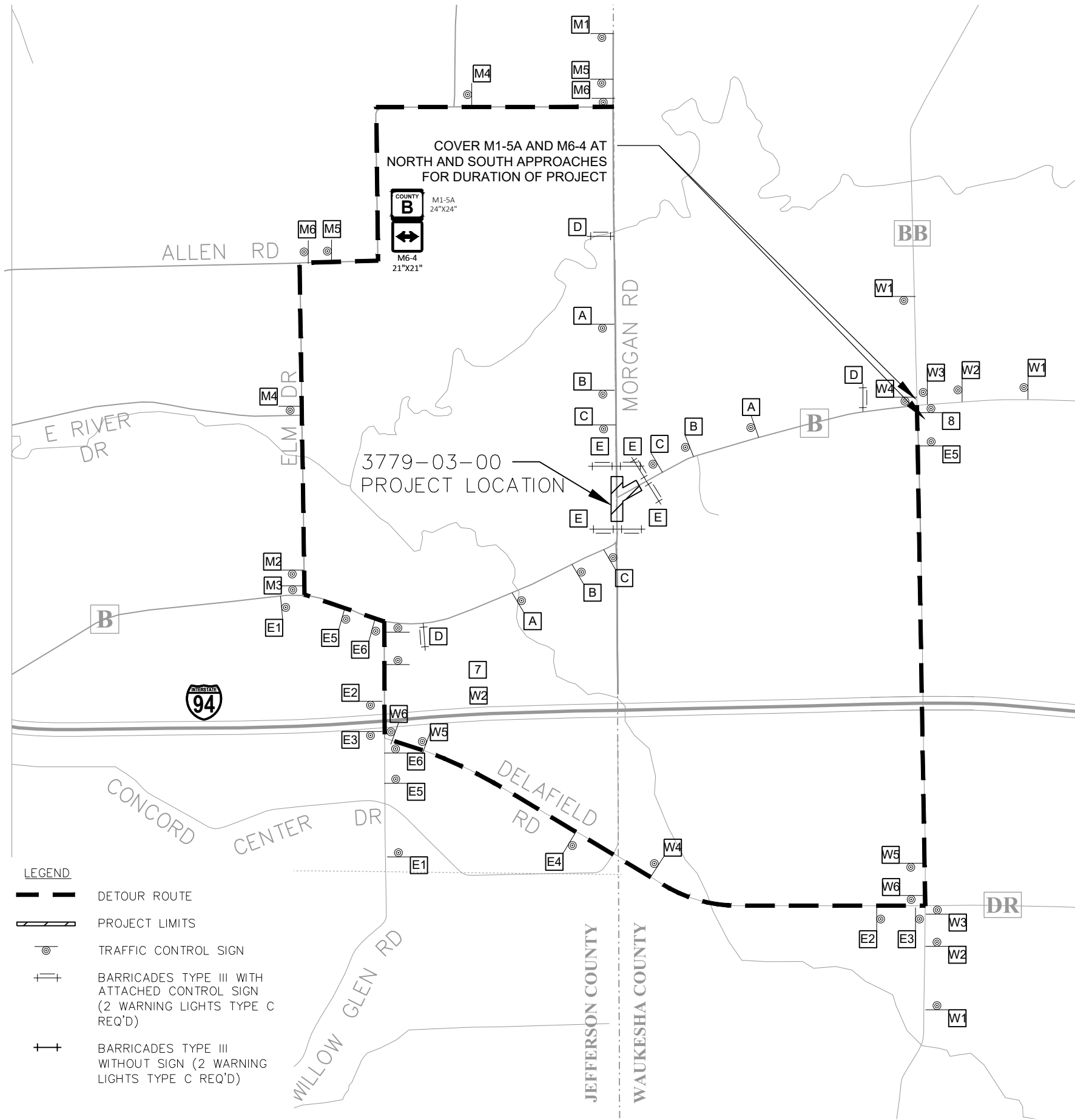
ALL SIGNS SHALL BE DIAMOND GRADE SHEETING.

FINAL LOCATION OF SIGNS SHALL BE DETERMINED BY THE FIELD ENGINEER AND ADJUSTED TO AVOID UTILITY CONFLICTS.



PAVEMENT MARKING LEGEND

- ① PAVEMENT MARKING, EPOXY, 4-INCH YELLOW (SOLID) DOUBLE YELLOW
- ② PAVEMENT MARKING, EPOXY, 4-INCH YELLOW (SOLID)
- ③ PAVEMENT MARKING, EPOXY, 4-INCH WHITE (SOLID)
- ④ PAVEMENT MARKING, EPOXY, 4-INCH YELLOW (DASHED) 12.5' STRIPE, 37.5' GAP
- ⑤ PAVEMENT MARKING, EPOXY, 8-INCH WHITE (SOLID)
- ⑥ PAVEMENT MARKING, EPOXY, DIAGONAL 12-INCH YELLOW (SOLID)
- ⑦ PAVEMENT MARKING, EPOXY, WORDS
- ⑧ PAVEMENT MARKING, EPOXY, ARROWS
- ⑨ PAVEMENT MARKING, EPOXY, STOP LINE 18-INCH WHITE (SOLID)



DETOUR PLAN NOTES :

PORTABLE CHANGEABLE MESSAGE BOARDS MUST BE PLACED ALONG EASTBOUND/WESTBOUND CTH B APPROACHES TO PROJECT AREA 7 DAYS PRIOR TO DETOUR. LOCATIONS AS DIRECTED BY ENGINEER. USE THE FOLLOWING MESSAGE.

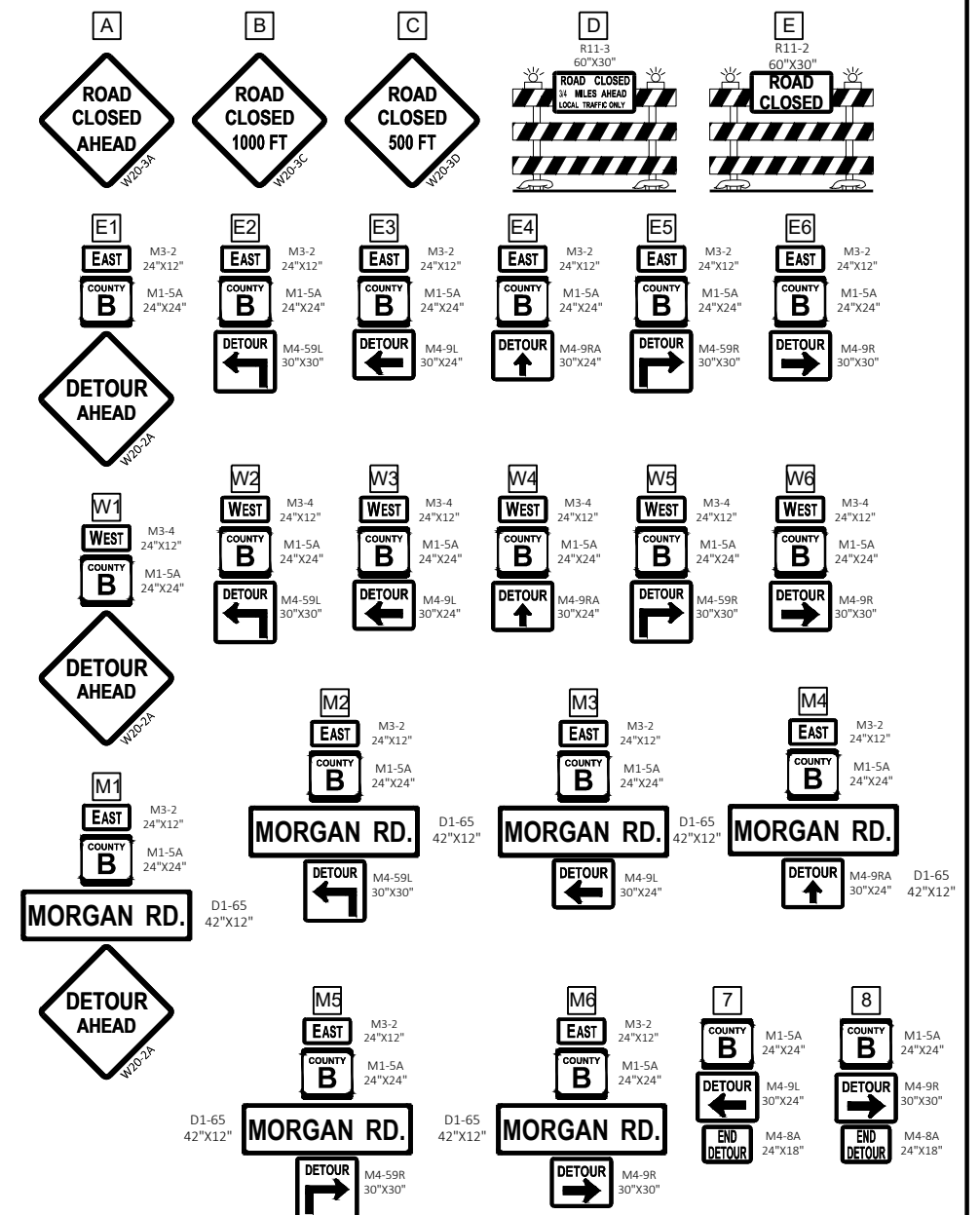
| | |
|---------------|------------------|
| 1ST FRAME | 2ND FRAME |
| ROAD TO CLOSE | BEGINNING <DATE> |
| | TO CLOSE |

DETOUR MARKER PLAN WILL BE ADJUSTED TO CORRESPOND WITH EXISTING ROUTE MARKERS. ANY SIGNS, TEMPORARY OR EXISTING, THAT CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE COVERED. THE COVERING OF POST MOUNTED SIGNS IS INCLUDED UNDER TRAFFIC CONTROL ITEM. THE CONTRACTOR MAY CHOOSE TO REMOVE AND REINSTALL EXISTING POST MOUNTED SIGNS IN LIEU OF COVERING THEM.

ALL "W" SERIES SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED

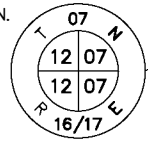
IMMEDIATELY RE-ESTABLISH "STOP" SIGNS THAT ARE REMOVED FOR A CONSTRUCTION OPERATION

SEE WISDOT S.D.D. "BARRICADES AND SIGNS FOR MAINLINE CLOSURES" & "DETOUR SIGNING FOR MAINLINE CLOSURES" FOR SIGN SPACING AND LOCATIONS





A CONCRETE MON.
WITH A SEWRPC
BRASS CAP
Y = 187915.912
X = 600841.493

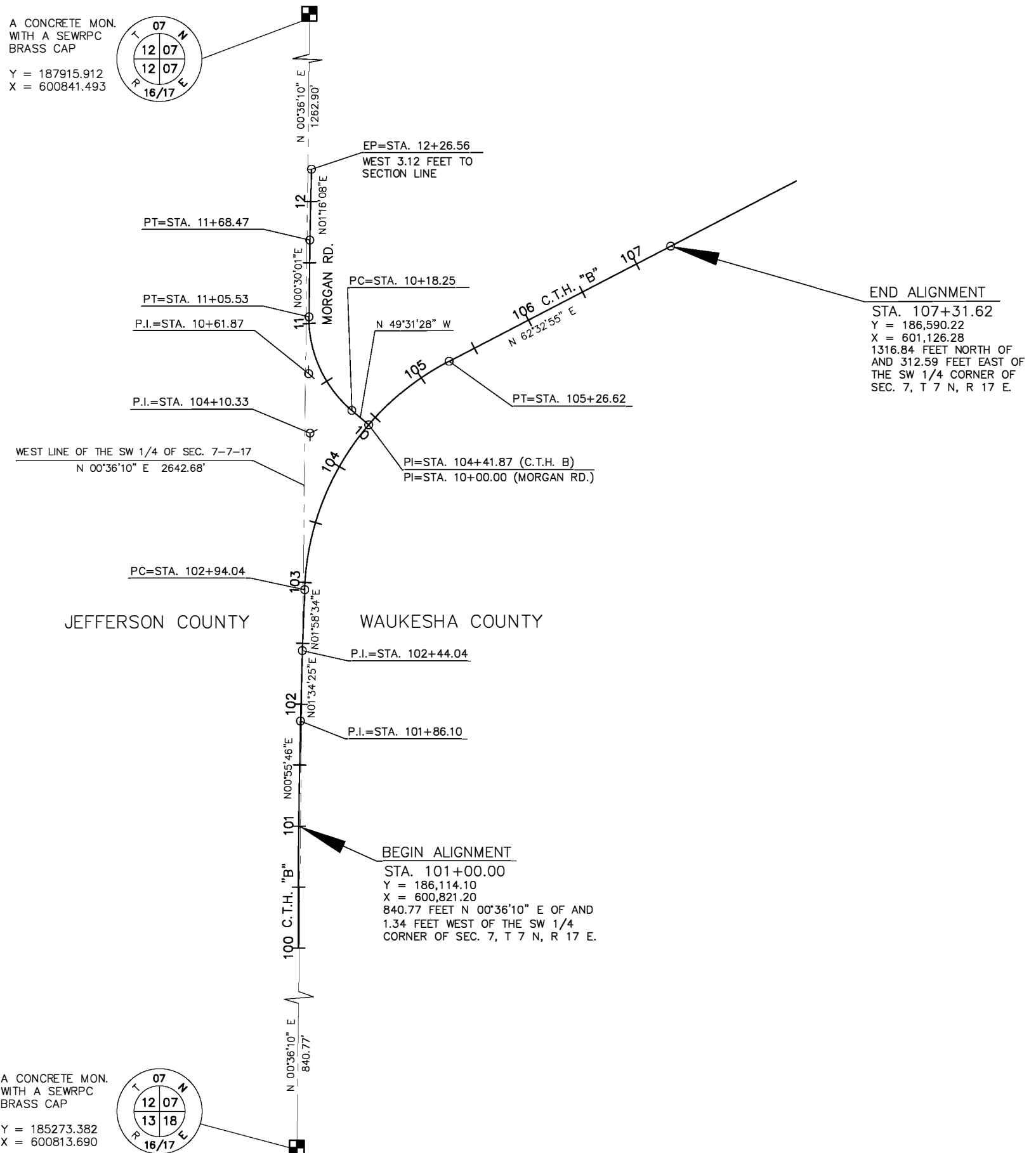


CURVE DATA (C/L "B")

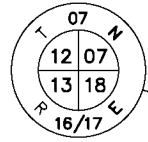
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Y = 186436.49
X = 600830.34
D = 26°02'37"
TAN = 128.52'
LCH = 221.90'
LCB = N 32°15'45" E
ARC L = 232.58'
R = 220.00'
PC = 101+67.15
PT = 103+99.73

CURVE DATA (C/L "MORGAN RD.")

PI = 10+61.87
Y = 186485.41
X = 600829.08
D = 52°17'45"
TAN = 46.62'
LCH = 84.53'
LCB = N 24°30'10" W
ARC L = 87.28'
R = 100.00'
PC = 10+18.25
PT = 11+05.53



A CONCRETE MON.
WITH A SEWRPC
BRASS CAP
Y = 185273.382
X = 600813.690



Estimate Of Quantities

3779-03-70

| Line | Item | Item Description | Unit | Total | Qty |
|------|------------|---|------|-----------|-----------|
| 0002 | 203.0100 | Removing Small Pipe Culverts | EACH | 1.000 | 1.000 |
| 0004 | 205.0100 | Excavation Common | CY | 3,500.000 | 3,500.000 |
| 0006 | 213.0100 | Finishing Roadway (project) 01. 3779-03-70 | EACH | 1.000 | 1.000 |
| 0008 | 305.0120 | Base Aggregate Dense 1 1/4-Inch | TON | 2,500.000 | 2,500.000 |
| 0010 | 305.0130 | Base Aggregate Dense 3-Inch | TON | 315.000 | 315.000 |
| 0012 | 305.0500 | Shaping Shoulders | STA | 3.000 | 3.000 |
| 0014 | 416.1010 | Concrete Surface Drains | CY | 2.000 | 2.000 |
| 0016 | 455.0605 | Tack Coat | GAL | 250.000 | 250.000 |
| 0018 | 460.2000 | Incentive Density HMA Pavement | DOL | 1,200.000 | 1,200.000 |
| 0020 | 460.5223 | HMA Pavement 3 LT 58-28 S | TON | 800.000 | 800.000 |
| 0022 | 460.5225 | HMA Pavement 5 LT 58-28 S | TON | 400.000 | 400.000 |
| 0024 | 465.0120 | Asphaltic Surface Driveways and Field Entrances | TON | 100.000 | 100.000 |
| 0026 | 465.0425 | Asphaltic Shoulder Rumble Strips 2-Lane Rural | LF | 1,200.000 | 1,200.000 |
| 0028 | 465.0475 | Asphalt Centerline Rumble Strips 2-Lane Rural | LF | 600.000 | 600.000 |
| 0030 | 520.1012 | Apron Endwalls for Culvert Pipe 12-Inch | EACH | 2.000 | 2.000 |
| 0032 | 521.3112 | Culvert Pipe Corrugated Steel 12-Inch | LF | 27.000 | 27.000 |
| 0034 | 522.1018 | Apron Endwalls for Culvert Pipe Reinforced Concrete 18-Inch | EACH | 1.000 | 1.000 |
| 0036 | 606.0200 | Riprap Medium | CY | 5.000 | 5.000 |
| 0038 | 608.0318 | Storm Sewer Pipe Reinforced Concrete Class III 18-Inch | LF | 40.000 | 40.000 |
| 0040 | 611.0642 | Inlet Covers Type MS | EACH | 2.000 | 2.000 |
| 0042 | 611.3901 | Inlets Median 1 Grate | EACH | 2.000 | 2.000 |
| 0044 | 616.0700.S | Fence Safety | LF | 300.000 | 300.000 |
| 0046 | 619.1000 | Mobilization | EACH | 1.000 | 1.000 |
| 0048 | 625.0100 | Topsoil | SY | 600.000 | 600.000 |
| 0050 | 625.0500 | Salvaged Topsoil | SY | 3,000.000 | 3,000.000 |
| 0052 | 627.0200 | Mulching | SY | 500.000 | 500.000 |
| 0054 | 628.1504 | Silt Fence | LF | 780.000 | 780.000 |
| 0056 | 628.1520 | Silt Fence Maintenance | LF | 780.000 | 780.000 |
| 0058 | 628.1905 | Mobilizations Erosion Control | EACH | 1.000 | 1.000 |
| 0060 | 628.1910 | Mobilizations Emergency Erosion Control | EACH | 2.000 | 2.000 |
| 0062 | 628.2027 | Erosion Mat Class II Type C | SY | 500.000 | 500.000 |
| 0064 | 628.7020 | Inlet Protection Type D | EACH | 2.000 | 2.000 |
| 0066 | 628.7555 | Culvert Pipe Checks | EACH | 1.000 | 1.000 |
| 0068 | 630.0200 | Seeding Temporary | LB | 100.000 | 100.000 |
| 0070 | 637.2230 | Signs Type II Reflective F | SF | 52.750 | 52.750 |
| 0072 | 642.5001 | Field Office Type B | EACH | 1.000 | 1.000 |
| 0074 | 643.0420 | Traffic Control Barricades Type III | DAY | 920.000 | 920.000 |
| 0076 | 643.0715 | Traffic Control Warning Lights Type C | DAY | 1,840.000 | 1,840.000 |
| 0078 | 643.0900 | Traffic Control Signs | DAY | 8,070.000 | 8,070.000 |
| 0080 | 643.0920 | Traffic Control Covering Signs Type II | EACH | 4.000 | 4.000 |
| 0082 | 643.1050 | Traffic Control Signs PCMS | DAY | 10.000 | 10.000 |
| 0084 | 643.5000 | Traffic Control | EACH | 1.000 | 1.000 |
| 0086 | 645.0120 | Geotextile Type HR | SY | 100.000 | 100.000 |
| 0088 | 646.1020 | Marking Line Epoxy 4-Inch | LF | 2,855.000 | 2,855.000 |
| 0090 | 646.3020 | Marking Line Epoxy 8-Inch | LF | 50.000 | 50.000 |
| 0092 | 646.5020 | Marking Arrow Epoxy | EACH | 1.000 | 1.000 |
| 0094 | 646.5120 | Marking Word Epoxy | EACH | 1.000 | 1.000 |
| 0096 | 646.6120 | Marking Stop Line Epoxy 18-Inch | LF | 20.000 | 20.000 |
| 0098 | 646.7120 | Marking Diagonal Epoxy 12-Inch | LF | 250.000 | 250.000 |

Estimate Of Quantities

3779-03-70

| Line | Item | Item Description | Unit | Total | Qty |
|------|----------|--|------|-----------|-----------|
| 0100 | 650.4000 | Construction Staking Storm Sewer | EACH | 40.000 | 40.000 |
| 0102 | 650.4500 | Construction Staking Subgrade | LF | 820.000 | 820.000 |
| 0104 | 650.5000 | Construction Staking Base | LF | 820.000 | 820.000 |
| 0106 | 650.5500 | Construction Staking Curb Gutter and Curb & Gutter | LF | 1,200.000 | 1,200.000 |
| 0108 | 650.6000 | Construction Staking Pipe Culverts | EACH | 1.000 | 1.000 |
| 0110 | 650.9910 | Construction Staking Supplemental Control (project) 01. 3779-03-70 | LS | 1.000 | 1.000 |
| 0112 | 690.0150 | Sawing Asphalt | LF | 125.000 | 125.000 |
| 0114 | ASP.1T0A | On-the-Job Training Apprentice at \$5.00/HR | HRS | 200.000 | 200.000 |
| 0116 | ASP.1T0G | On-the-Job Training Graduate at \$5.00/HR | HRS | 750.000 | 750.000 |
| 0118 | SPV.0060 | Special 01. Utility Line Opening | EACH | 4.000 | 4.000 |
| 0120 | SPV.0060 | Special 02. Post Tubular Steel 1 3/4" x 1 3/4" x 12' | EACH | 12.000 | 12.000 |
| 0122 | SPV.0060 | Special 03. Removing Roadway Signs | EACH | 1.000 | 1.000 |
| 0124 | SPV.0090 | Special 01. Concrete Curb & Gutter 18-Inch Type J | LF | 1,200.000 | 1,200.000 |
| 0126 | SPV.0180 | Special 01. Hydroseed | SY | 1,900.000 | 1,900.000 |
| 0128 | SPV.0180 | Special 02. High Friction Surface Treatment | SY | 3,300.000 | 3,300.000 |

3

3

| REMOVALS | | |
|-----------------------------------|--|--|
| | 203.0100 REMOVING SMALL PIPE CULVERTS EACH | |
| LOCATION | | |
| STA 11+26 | 1 | |
| TOTAL | 1 | |
| EXCAVATION COMMON | | |
| | 205.0100 CY | |
| LOCATION | | |
| STA 101+00 TO 107+31 | 3,500 | |
| TOTAL | 3,500 | |
| MOBILIZATION | | |
| | 619.1000 EACH | |
| LOCATION | | |
| 3779-03-70 | 1 | |
| TOTAL | 1 | |
| FIELD OFFICE | | |
| | 642.5001 EACH | |
| LOCATION | | |
| 3779-03-70 | 1 | |
| TOTAL | 1 | |
| UTILITY | | |
| | SPV.0060.01 UTILITY LINE OPENING EACH | |
| LOCATION | | |
| 3779-03-70 | 4 | |
| TOTAL | 4 | |
| CONCRETE CURB & GUTTER | | |
| | 416.1010 CONCRETE SURFACE DRAINS CY | SPV.0090.01 6-INCH SLOPED 18-INCH TYPE J LF |
| LOCATION | | |
| STA 105+63 R | 1 | -- |
| STA 107+33 L | 1 | -- |
| STA 101+00 TO 107+31 | -- | 1,200 |
| TOTAL | 2 | 1,200 |

| SAWING | |
|---------------|-------------------------------------|
| | 690.0150 SAWING ASPHALT LF |
| LOCATION | |
| STA 101+00 | 25 |
| STA 105+00 | 25 |
| STA 107+31 | 25 |
| STA 11+25 | 25 |
| STA 11+82 | 25 |
| TOTAL | 125 |

| PERMANENT SIGNAGE | | | |
|--------------------------|--|---|--|
| | 637.2230 SIGN TYPE II REFLECTIVE F SF | SPV.0060.02 POST TUBULAR STEEL 1 3/4"x1 3/4"x12' EACH | SPV.0105.01 REMOVING SIGNS EACH |
| LOCATION | | | |
| STA 101+72 R | 6.25 | 1 | -- |
| STA 102+78 L | 3.00 | 1 | -- |
| STA 103+13 L | 8.00 | 1 | -- |
| STA 103+59 L | 3.00 | 1 | -- |
| STA 103+60 L | 3.00 | 1 | -- |
| STA 104+04 L | 3.00 | 1 | -- |
| STA 104+05 L | 3.00 | 1 | -- |
| STA 104+23 L | 6.25 | 1 | -- |
| STA 104+86 L | -- | 1 | -- |
| STA 105+13 L | 8.00 | 1 | -- |
| STA 105+85 L | 3.00 | 1 | -- |
| STA 107+33 R | 6.25 | 1 | -- |
| PROJECT | -- | -- | 1 |
| TOTAL | 52.75 | 12 | 1 |

| BASE AGGREGATE DENSE | | | | |
|-----------------------------|--|---|---|-------------------------------------|
| | 213.0100 FINISHING ROADWAY (PROJECT) EACH | 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON | 305.0130 BASE AGGREGATE DENSE 3-INCH TON | 305.0500 SHAPING SHOULDER STA |
| LOCATION | | | | |
| STA 101+00 TO 107+31 | 1 | 2,500 | 315 | 3 |
| TOTAL | 1 | 2,500 | 315 | 3 |

| CONSTRUCTION STAKING | | | | | | |
|-----------------------------|-------------------------------|----------------------------|------------------------|---------------------------------|-----------------------------------|---|
| | 650.4000 STORM SEWER LF | 650.4500 SUBGRADE LF | 650.5000 BASE LF | 650.5500 CURB & GUTTER LF | 650.6000 PIPE CULVERTS EACH | 650.9910 SUPPLEMENTAL CONTROL EACH |
| LOCATION | | | | | | |
| STA 101+00 TO 107+31 | 40 | 820 | 820 | 1200 | 1 | 1 |
| TOTAL | 40 | 820 | 820 | 1,200 | 1 | 1 |

| MARKING | | | | | | | | | |
|----------------------|---|--------------|--|------------------|--|------------------------------------|---|---|------------|
| | 646.1020 LINE EPOXY 4-INCH YELLOW | | 646.3020 LINE EPOXY 8-INCH YELLOW | | 646.5020 ARROW EPOXY TYPE 2 EACH | 646.5120 ARROW EPOXY EACH | 646.6120 STOP LINE EPOXY 18-INCH LF | 646.7120 DIAGONAL EPOXY 12-INCH YELLOW LF | |
| LOCATION | LF | DOUBLE LF | SOLID LF | 12.5' LINE LF | LF | EACH | EACH | LF | LF |
| STA 101+00 TO 107+32 | 1200 | -- | 1160 | 65 | 50 | 1 | 1 | -- | -- |
| STA 10+20 TO 11+75 | -- | 150 | 130 | -- | -- | -- | -- | 20 | 250 |
| SUBTOTAL | 1,200 | 300 | 1,290 | 65 | 50 | 1 | 1 | 20 | 250 |
| TOTAL | 2,855 | | | | 50 | 1 | 1 | 20 | 250 |

3

3

ASPHALT

| LOCATION | 455.0605 | 460.2000 | 460.5223 | 460.5225 | 465.0120 | 465.0425 | 465.0475 | SPV.180.02 |
|----------------------|---------------------|---|--|----------|--|---|---|--|
| | TACK COAT GAL | INCENTIVE DENSITY HMA PAVEMENT DOL | HMA PAVEMENT 3 LT 58-28 S 5 LT 58-28 S TON TON | | ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON | ASPHALTIC SHOULDER RUMBLE STRIPS 2-LINE RURAL LF | ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LINE RURAL LF | HIGH FRICTION SURFACE TREATMENT SY |
| STA 101+00 TO 107+31 | 250 | 1,200 | 800 | 400 | 100 | 1,200 | 600 | 3,300 |
| TOTAL | 250 | 1,200 | 800 | 400 | 100 | 1,200 | 600 | 3,300 |

TRAFFIC CONTROL

| LOCATION | 643.0420 | | 643.0715 | | 643.0900 | | 643.0920 | | 643.1050 | | SPV.0060.01 | 643.5000 |
|----------------------|---------------------|-----|-----------------------|-------|----------|-------|--|-------|------------|-----|--|-------------------------|
| | BARRICADES TYPE III | | WARNING LIGHTS TYPE C | | SIGNS | | TRAFFIC CONTROL COVERING SIGNS TYPE II | | SIGNS PCMS | | POST TUBULAR STEEL 1 3/4"x1 3/4"x12' | TRAFFIC CONTROL EACH |
| | EACH | DAY | EACH | DAY | EACH | DAY | EACH | CYCLE | EACH | DAY | EACH | EACH |
| STA 101+00 TO 107+31 | 12 | 720 | 24 | 1,440 | 129 | 7,870 | 4 | 1 | 2 | 10 | 150 | 1 |
| UNDISTRIBUTED | -- | 200 | -- | 400 | -- | 200 | -- | -- | -- | -- | -- | -- |
| TOTAL | | 920 | | 1,840 | | 8,070 | | 4 | | 10 | 150 | 1 |

EROSION CONTROL

| LOCATION | 616.0700.S | 625.0100 | 625.0500 | 627.0200 | 628.1504 | 628.1520 | 628.1905 | | 628.1910 | 628.2027 | 628.7020 | 628.7555 | 630.0200 | 645.0120 | SPV.0180.01 |
|----------------------|-----------------------|---------------|---------------------------|----------------|---------------------|---------------------------------|--|----|--------------------------------------|---|---------------------------------------|--------------------------------|----------------------------|-----------------------------|-----------------|
| | FENCE SAFETY LF | TOPSOIL SY | SALVAGED TOPSOIL SY | MULCHING SY | SILT FENCE LF | SILT FENCE MAINTENANCE LF | MOBILIZATIONS EROSION CONTROL EACH | | EMERGENCY EROSION CONTROL EACH | EROSION MAT CLASS II TYPE C SY | INLET PROTECTION TYPE D EACH | CULVERT PIPE CHECKS EACH | SEEDING TEMPORARY LB | GEOTEXTILE TYPE HR SY | HYDROSEED SY |
| STA 101+00 TO 107+31 | -- | 500 | 2,500 | -- | 780 | 780 | 1 | 2 | -- | 2 | -- | -- | -- | -- | 1,500 |
| STA 11+20 | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | 1 | -- | -- | -- | -- |
| UNDISTRIBUTED | 300 | 100 | 500 | 500 | -- | -- | -- | -- | 500 | -- | -- | 100 | 100 | 400 | |
| TOTAL | 300 | 600 | 3,000 | 500 | 780 | 780 | 1 | 2 | 500 | 2 | 1 | 100 | 100 | 1,900 | |

STORM SEWER

| LOCATION | 520.1012 | 521.3112 | 522.1018 | 606.0200 | 608.0318 | 611.0642 | 611.3901 |
|---------------|---|---|--|------------------------|--|---------------------------------|---------------------------------|
| | APRON ENDWALLS FOR CULVERT PIPE 12-INCH EACH | CULVERT PIPE CORRUGATED STEEL 12-INCH LF | APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 18-INCH LF | RIPRAP MEDIUM CY | STORM SEWER PIPE REINFORCED CONCRETE CLASS III 18-INCH LF | INLET COVERS TYPE MS EACH | INLET MEDIAN 1 GRATE EACH |
| STA 11+13 | 1 | -- | -- | -- | -- | -- | -- |
| STA 11+40 | 1 | -- | -- | -- | -- | -- | -- |
| STA 11+27 | -- | 27 | -- | -- | -- | -- | -- |
| STA 100+98.60 | -- | -- | 1 | 1.66 | 40 | 2 | 2 |
| STA 105+67.50 | -- | -- | -- | 1.66 | -- | -- | -- |
| STA 107+38.70 | -- | -- | -- | 1.66 | -- | -- | -- |
| TOTAL | 2 | 27 | 1 | 5.0 | 40 | 2 | 2 |

CONVENTIONAL SYMBOLS

| | | | | | |
|---|-------|--|--|--------------------------|-----------|
| SECTION LINE | ----- | SECTION CORNER | | R/W MONUMENT | ● |
| QUARTER LINE | ----- | NOTATION FOR COMBUSTIBLE FLUIDS | | NON-MONUMENTED R/W POINT | ○ |
| SIXTEENTH LINE | ----- | NOTATION FOR HIGH VOLTAGE TRANSMISSION LINES | | FOUND IRON PIN | IP |
| NEW REFERENCE LINE | ----- | ELECTRIC POLE | | VALVE (GAS, WATER, ETC.) | ⊙ (TYPED) |
| NEW R/W LINE | ----- | TELEPHONE POLE | | SIGN | |
| EXISTING R/W LINE | ----- | PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.) | | OFF-PREMISE SIGN | |
| PROPERTY LINE | ----- | ACCESS CONTROLLED BY ACQUISITION | | COMPENSABLE | |
| LOT, TIE & OTHER MINOR LINES | ----- | NO ACCESS (BY STATUTORY AUTHORITY) | | NON-COMPENSABLE | |
| CORPORATE LIMITS | ----- | ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL) | | | |
| UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.) | | | | | |
| FEE ACQUISITION AREA (HATCHING VARIES BY OWNER) | | | | | |
| TEMPORARY LIMITED EASEMENT AREA | | | | | |
| EASEMENT AREA (HIGHWAY, PERMANENT LIMITED, OR RESTRICTED DEVELOPMENT) | | | | | |
| TRANSMISSION STRUCTURES | | | | | |
| BUILDING | | BRIDGE | | PARCEL NUMBER | |
| NATIONAL GEODETIC SURVEY MONUMENT | | | | UTILITY NUMBER | |
| SIXTEENTH CORNER MONUMENT | | | | | |

CONVENTIONAL ABBREVIATIONS

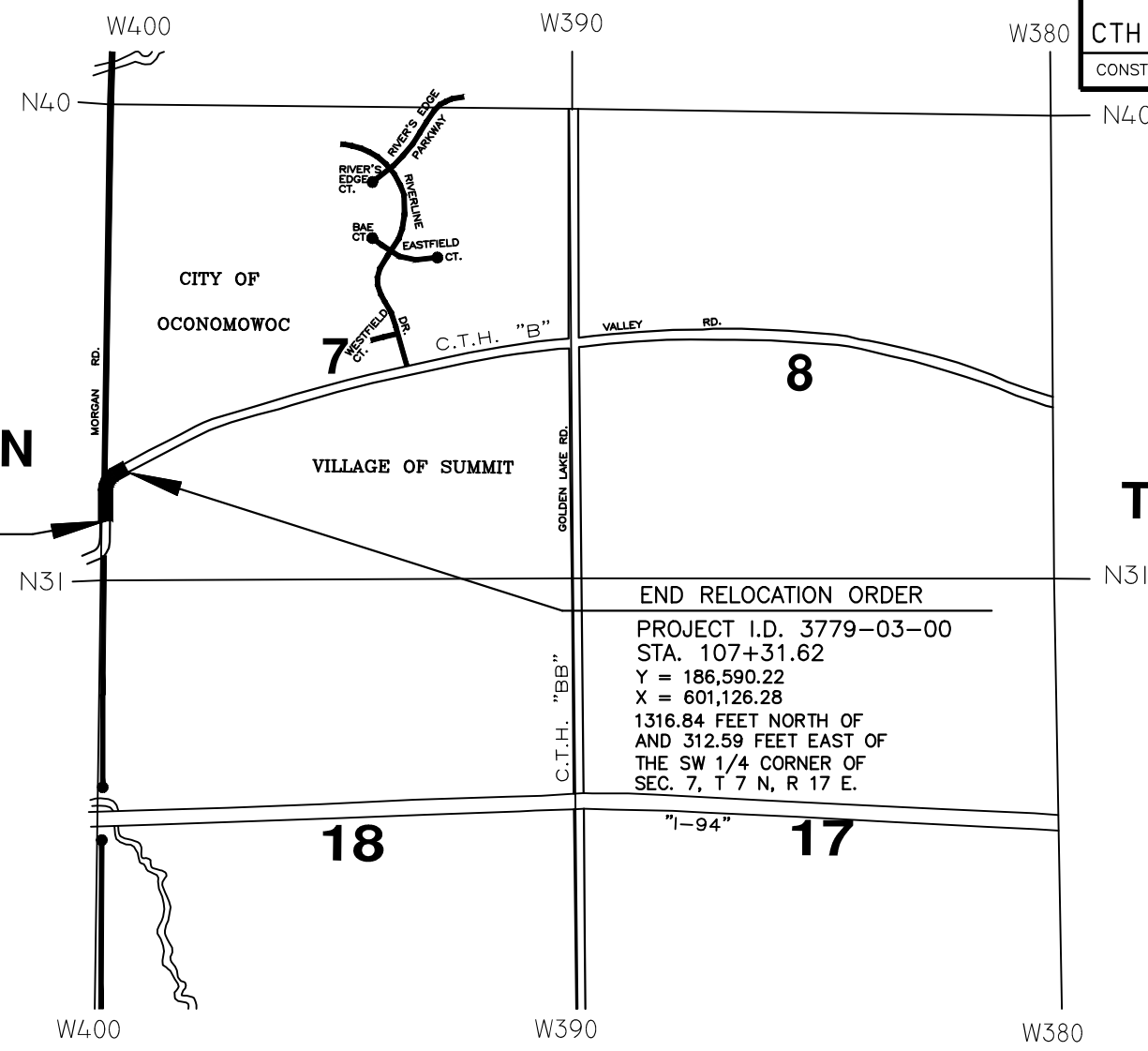
| | | | |
|----------------------------|-------|----------------------------|--------|
| ACCESS RIGHTS | AR | POINT OF COMPOUND CURVE | PCC |
| ACRES | AC | POINT OF INTERSECTION | PI |
| AHEAD | AH | PROPERTY LINE | PL |
| ALUMINUM | ALUM | RECORDED AS | (100') |
| AND OTHERS | ET AL | REFERENCE LINE | R/L |
| BACK | BK | REMAINING | REM |
| BLOCK | BLK | RIGHT | RT |
| CENTERLINE | C/L | RIGHT OF WAY | R/W |
| CERTIFIED SURVEY MAP | CSM | SECTION | SEC |
| CONCRETE | CONC | SEPTIC VENT | SEPV |
| COUNTY | CO | SQUARE FEET | SF |
| COUNTY TRUNK HIGHWAY | CTH | STATE TRUNK HIGHWAY | STH |
| DISTANCE | DIST | STATION | STA |
| CORNER | COR | SUBDIVISION | SUBD |
| DOCUMENT NUMBER | DOC | TANGENT | TAN |
| EASEMENT | EASE | TELEPHONE PEDESTAL | TP |
| EXISTING | EX | TEMPORARY LIMITED EASEMENT | TLE |
| GAS VALVE | GV | TRANSPORTATION PROJECT | TPP |
| GRID NORTH | GN | PLAT | PLAT |
| HIGHWAY EASEMENT | HE | UNITED STATES HIGHWAY | USH |
| IDENTIFICATION | ID | VOLUME | V |
| LAND CONTRACT | LC | | |
| LEFT | LT | | |
| MONUMENT | MON | | |
| NATIONAL GEODETIC SURVEY | NGS | | |
| NUMBER | NO | | |
| OUTLOT | OL | | |
| PAGE | P | | |
| POINT OF TANGENCY | PT | | |
| PERMANENT LIMITED EASEMENT | PLE | | |
| POINT OF BEGINNING | POB | | |
| POINT OF CURVATURE | PC | | |

CURVE DATA

| | |
|------------------------|-----|
| LONG CHORD | LC |
| LONG CHORD BEARING | LCB |
| RADIUS | R |
| DEGREE OF CURVE | D |
| CENTRAL ANGLE OR DELTA | Δ |
| LENGTH OF CURVE | L |
| TANGENT | T |
| DIRECTION AHEAD | DA |
| DIRECTION BACK | DB |

CONVENTIONAL UTILITY SYMBOLS

| | |
|--------------------|--|
| WATER | |
| GAS | |
| TELEPHONE | |
| OVERHEAD | |
| TRANSMISSION LINES | |
| ELECTRIC | |
| CABLE TELEVISION | |
| FIBER OPTIC | |
| SANITARY SEWER | |
| STORM SEWER | |



BEGIN RELOCATION ORDER
 PROJECT I.D. 3779-03-00
 STA. 101+00.00
 Y = 186,114.10
 X = 600,821.20
 840.77 FEET N 00°36'10" E
 OF AND 1.34 FEET WEST OF
 THE SW 1/4 CORNER OF
 SEC. 7, T 7 N, R 17 E.

END RELOCATION ORDER
 PROJECT I.D. 3779-03-00
 STA. 107+31.62
 Y = 186,590.22
 X = 601,126.28
 1316.84 FEET NORTH OF
 AND 312.59 FEET EAST OF
 THE SW 1/4 CORNER OF
 SEC. 7, T 7 N, R 17 E.

| | | | | | |
|--|------------|--------------|-----------------|--------------|---|
| R/W PROJECT NUMBER | 3779-03-00 | SHEET NUMBER | 4.1 | TOTAL SHEETS | 3 |
| PLAT OF RIGHT OF WAY REQUIRED FOR CTH B MORGAN ROAD INTERSECTION | | | | | |
| CTH B | | | WAUKESHA COUNTY | | |
| CONSTRUCTION PROJECT NUMBER | 3779-03-70 | | | | |



ORIGINAL PLAT PREPARED BY
 WAUKESHA COUNTY D.P.W.
 515 W. MORELAND BLVD. RM 220
 WAUKESHA, WI 53188

DATE: _____ (Signature)

APPROVED FOR
 WAUKESHA COUNTY
 DEPARTMENT OF PUBLIC WORKS

DATE: _____ DIRECTOR

DATE: _____ ENGINEERING SERVICES MANAGER

| | |
|---------------|--|
| REVISION DATE | STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION |
| | APPROVED FOR THE DEPARTMENT |
| | DATE: _____ N/A (Signature) |

NOTES
 HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, WAUKESHA COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT OF WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER SURVEYS OF PUBLIC RECORD

THE EXISTING HIGHWAY RIGHT OF WAY SHOWN HEREON IS BASED ON EXISTING CERTIFIED SURVEY MAPS, SUBDIVISION PLATS, AND OTHER SURVEYS OF PUBLIC RECORD.

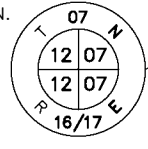
DIMENSIONING TO THE NEW RIGHT OF WAY IS MEASURED ALONG AND PERPENDICULAR TO THE CENTERLINE OF CONSTRUCTION.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN. INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE. INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. ALL TLE'S EXPIRES AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.



TOTAL NET LENGTH OF CENTERLINE = 0.114 MI. (RURAL)

A CONCRETE MON.
WITH A SEWRPC
BRASS CAP
Y = 187915.912
X = 600841.493



BLACK HAT PROPERTIES
DOC. NO. 1336552
TAX KEY NO. 006.0716.1244.000

CURVE DATA (C/L "B")
PI = 102+83.44
Y = 186436.49
X = 600830.34
D = 26°02'37"
TAN = 128.52'
LCH = 221.90'
LGB = N 32°15'45" E
ARC L = 232.58'
R = 220.00'
PC = 101+67.15
PT = 103+99.73

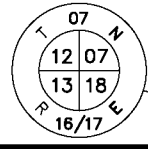
CURVE DATA (C/L "MORGAN RD.")
DERRIKE & JENNA RETZLAFF
DOC. NO. 1385142
TAX KEY NO. 006.0716.1244.001

PI = 10+61.87
Y = 186485.74
X = 600829.08
D = 52°17'45"
TAN = 46.62'
LCH = 84.53'
LCB = N 24°30'10" W
ARC L = 87.28'
R = 100.00'
PC = 10+18.25
PT = 11+05.53

WEST LINE OF THE SW 1/4 OF SEC. 7-7-17
N 00°36'10" E 2642.68'

MICHAEL SCHAEFER & BARBARA BLEECKER
DOC. NO. 1001382
TAX KEY NO. 006.0716.1241.000

A CONCRETE MON.
WITH A SEWRPC
BRASS CAP
Y = 185273.382
X = 600813.690



OUTLOT 1, MORGAN STATION SUBDIVISION

SLOPE LIMITS

DOC. NO. 2246190
30'X30' AMERITECH EASE.

END ALIGNMENT

STA. 107+31.62
Y = 186,590.22
X = 601,126.28
1316.84 FEET NORTH OF
AND 312.59 FEET EAST OF
THE SW 1/4 CORNER OF
SEC. 7, T 7 N, R 17 E.

DAVID & JENNIFER KRAFT
DOC. NO. 3735768
TAX KEY NO. SUMT.0603.999.001

CURVE DATA

C-1
ARC LEN. = 197.70'
CH. LEN. = 188.62'
CH. BRG. = S 32°15'45" W
RADIUS = 187.00'

LINE TABLE FOR PARCEL 1

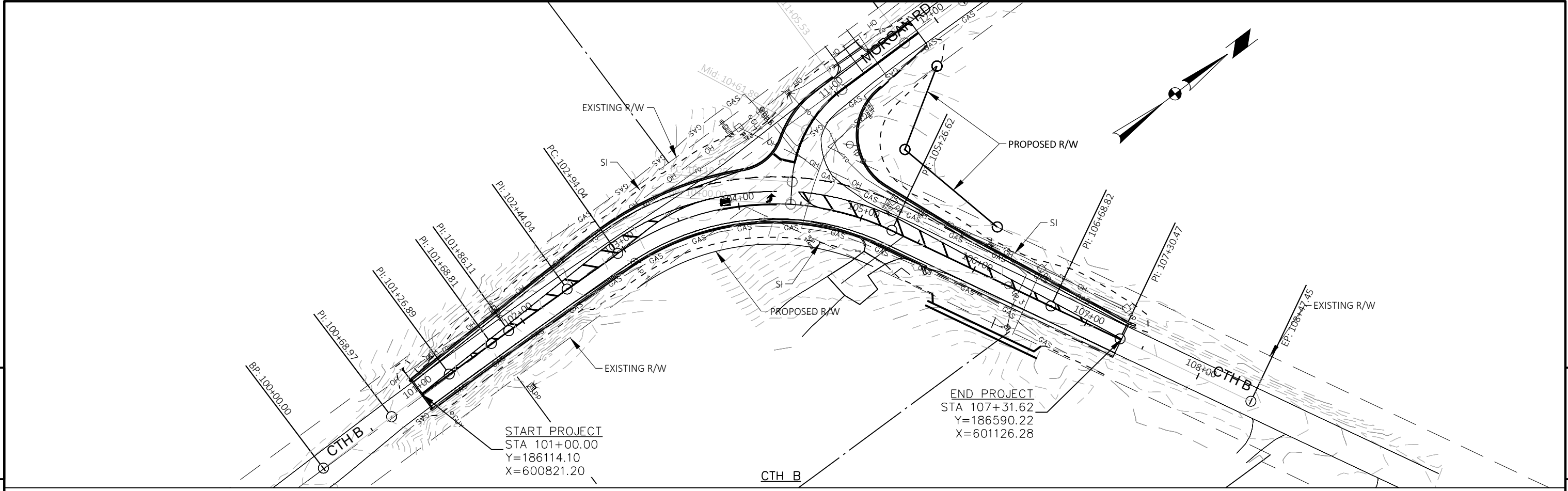
| | | | |
|-----|---|---------------|---------|
| L-1 | = | N 00°36'10" E | 910.33' |
| L-2 | = | N 00°36'10" E | 250.54' |
| L-3 | = | N 62°32'55" E | 271.95' |
| L-4 | = | S 00°36'10" W | 37.39' |
| L-5 | = | S 62°32'55" W | 120.88' |
| L-6 | = | S 01°58'34" W | 50.00' |
| L-7 | = | N 88°01'26" W | 0.12' |
| L-8 | = | S 00°36'10" W | 73.66' |
| L-9 | = | N 89°23'50" W | 33.00' |

BEGIN ALIGNMENT

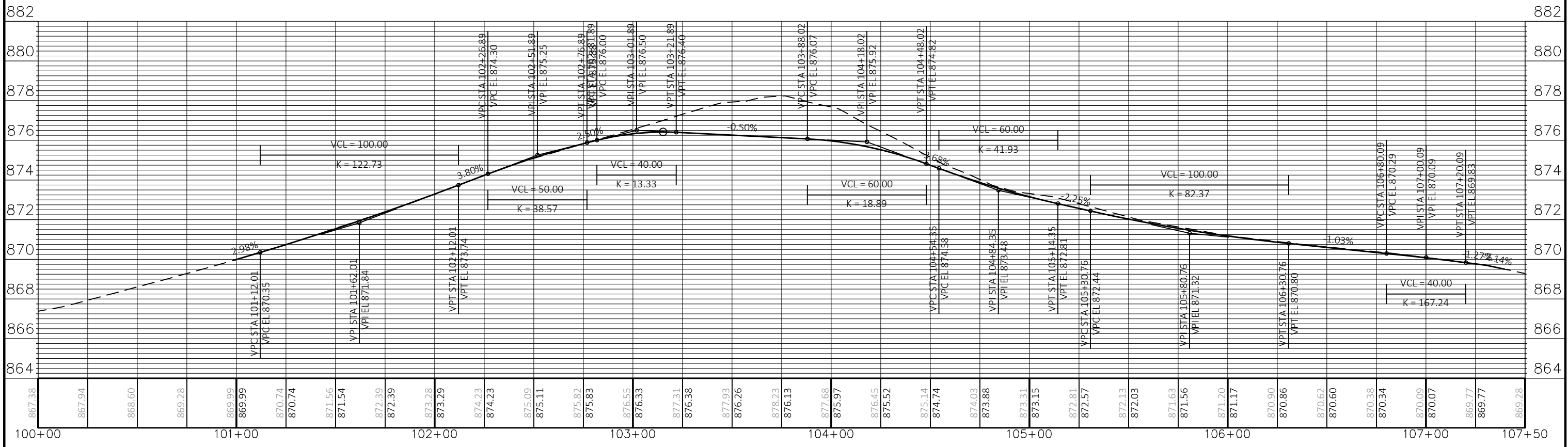
STEVEN SAWEL
DOC. NO. 2444991
TAX KEY NO. 188M14903.995
X = 600,821.20
840.77 FEET N 00°36'10" E OF AND
1.34 FEET WEST OF THE SW 1/4
CORNER OF SEC. 7, T 7 N, R 17 E.

| | | | |
|---------------|-----------------|------------------|--------------------------------------|
| REVISION DATE | DATE: 2/15/2022 | HWY: B | CONSTRUCTION PROJECT NO.: 3779-03-70 |
| | | COUNTY: WAUKESHA | R/W PROJECT NO.: 3779-03-00 |
| | | | PLAT SHEET NO. 4.3 |

5



5



| | | | | | |
|------------------------|------------|------------------|----------------|----------|---|
| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA | PLAN & PROFILE | SHEET 24 | E |
|------------------------|------------|------------------|----------------|----------|---|

5

REMOVE EXISTING CULVERT
(30' - 15" CMP)

EXISTING R/W

PROPOSED R/W

MORGAN RD.

END PROJECT
STA 11+82.26
Y=186608.79
X=600830.34

MORGAN ROAD

Stationing: 10+00, 11+00, 12+00

Utility lines: GAS, SV9, OH, HO, SI, HC, FO

Vertical curve data: PVI: 11+05.53, VPI EL 878.03, VPT STA 10+80.00, VPT EL 878.02

Other points: STA 10+00.00 EL 875.03, STA 10+17.20 VPC EL 876.22, STA 11+66.39 EL 877.99, STA 11+91.88 EL 877.92

5

882

880

878

876

874

872

9+75 10+00 11+00 12+00

Grades: -0.03%, 0.29%

Vertical Curve Data:
VCL = 60.00
K = 9.92

| Station | Elevation |
|------------------|-----------|
| STA 10+00.00 | 875.03 |
| STA 10+17.20 | 876.22 |
| VPI | 878.03 |
| VPT STA 10+80.00 | 878.02 |
| STA 11+66.39 | 877.99 |
| STA 11+91.88 | 877.92 |

PROJECT NO: 3779-03-70

HWY: CTH B

COUNTY: WAUKESHA

PLAN & PROFILE

SHEET 25

E

FILE NAME : N:\DPW\ENGINEER\PROJECTS\B AT MORGAN RD HSIP 3779-03-00 & -70\ACAD\DESIGN\MODEL\CRDR_B & MORGAN.DWG
LAYOUT NAME - Plan (2)

PLOT DATE : 5/23/2022 12:47 PM

PLOT BY : NATHAN BETH

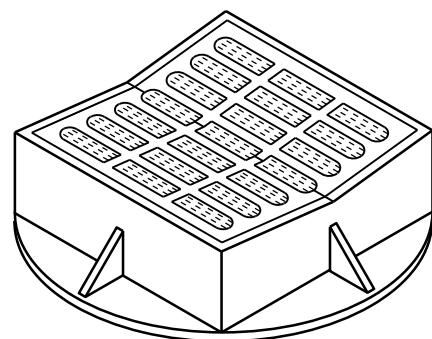
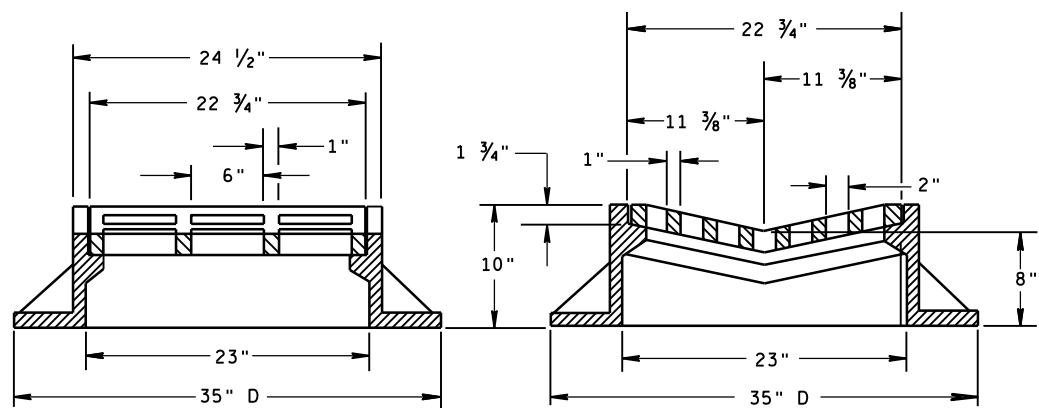
PLOT NAME :

PLOT SCALE : 1" = 50'

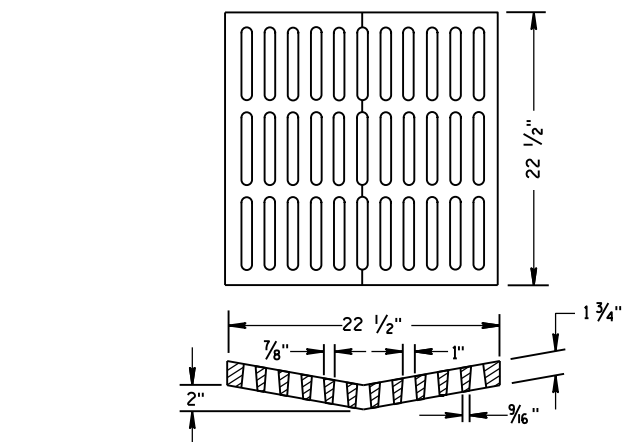
WISDOT/CADD SHEET 44

Standard Detail Drawing List

| | |
|-----------|---|
| 08A05-19B | INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM |
| 08C08-02 | INLETS MEDIAN 1 AND 2 GRATE |
| 08D01-22A | CONCRETE CURB & GUTTER |
| 08D01-22B | CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS |
| 08D04-06 | CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES |
| 08D22-01 | DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL |
| 08E09-06 | SILT FENCE |
| 08E10-02 | INLET PROTECTION TYPE A, B, C AND D |
| 08E15-01 | CULVERT PIPE CHECK |
| 08F01-11 | APRON ENDWALLS FOR CULVERT PIPE |
| 09A01-13A | AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE |
| 13A10-02A | 2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING |
| 13A10-02B | 2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING |
| 14B29-01 | SAFETY EDGE |
| 15C02-08A | BARRICADES AND SIGNS FOR MAINLINE CLOSURES |
| 15C02-08C | DETOUR SIGNING FOR MAINLINE CLOSURES |
| 15C07-15B | PAVEMENT MARKING WORDS |
| 15C07-15C | PAVEMENT MARKING ARROWS |
| 15C08-20A | LONGITUDINAL MARKING (MAINLINE) |
| 15C08-20B | PAVEMENT MARKING (TURN LANES) |
| 15C08-20C | PAVEMENT MARKING (TURN LANES) |
| 15C33-04 | STOP LINE AND CROSSWALK PAVEMENT MARKING |
| 15C35-04A | PAVEMENT MARKING (INTERSECTIONS) |
| 15D42-01 | TRAFFIC CONTROL, TWO LANE FULL FREEWAY CLOSURE |

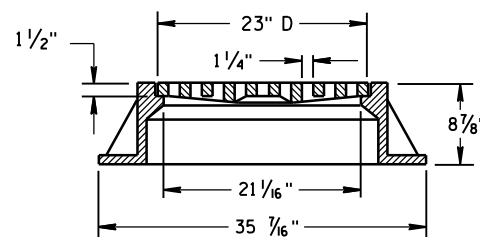
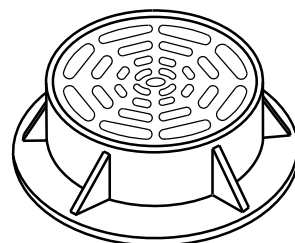
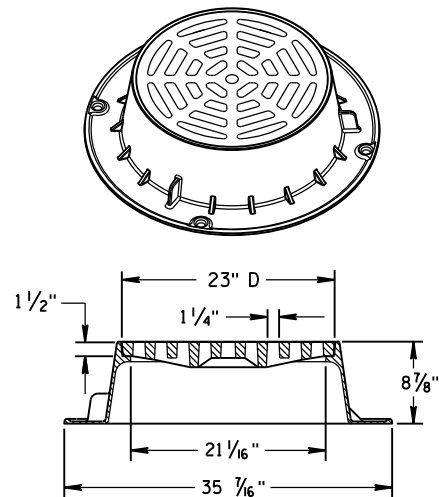


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

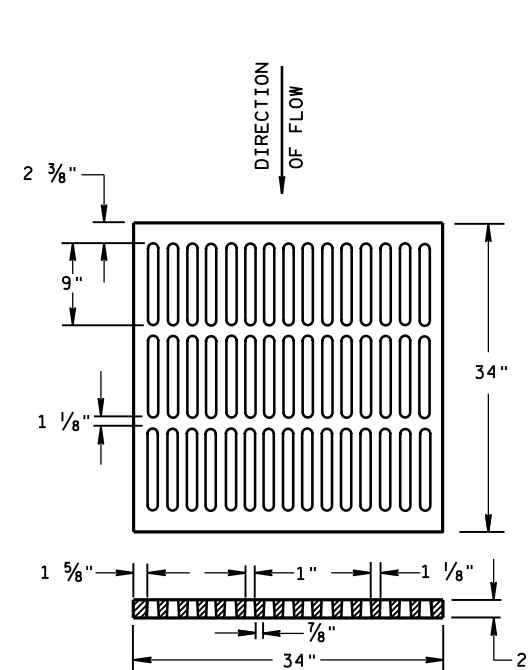
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

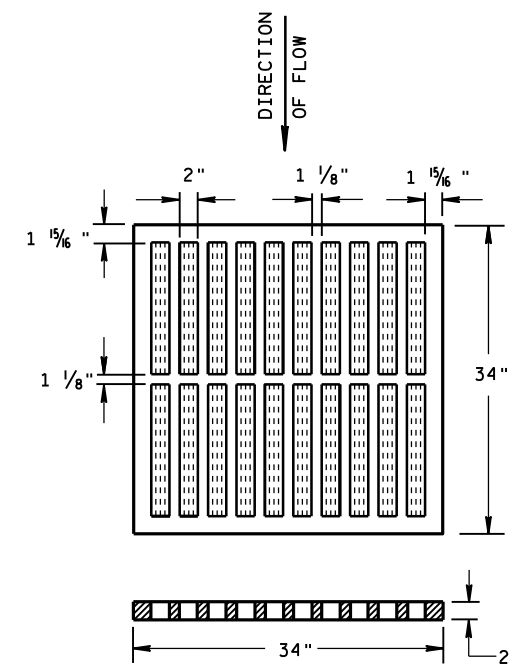
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



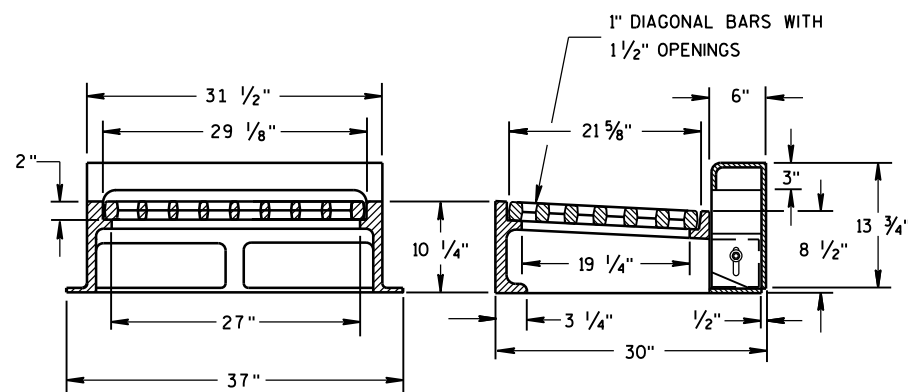
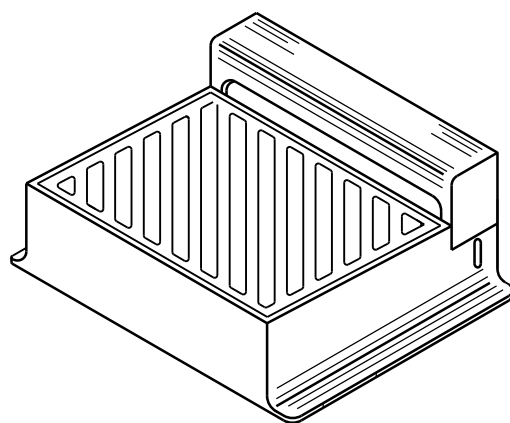
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE



TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

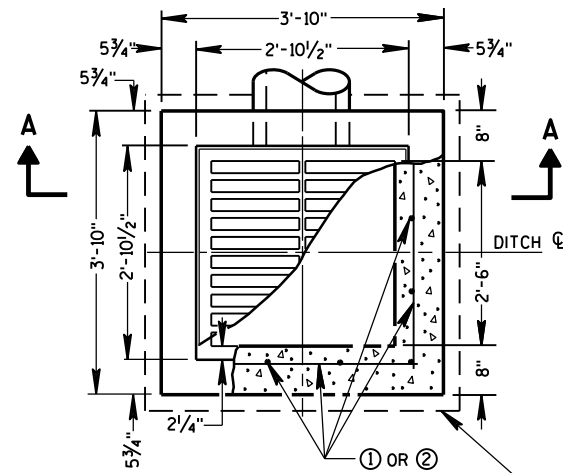
DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.

DIRECTION OF FLOW

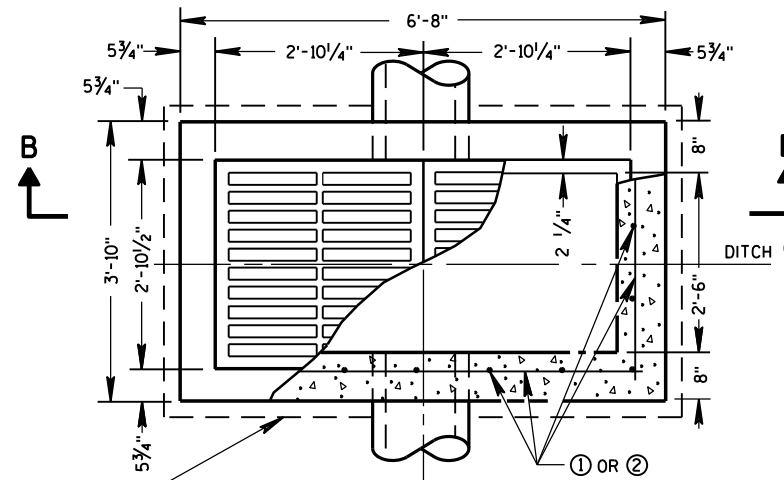
INLET COVERS
TYPE B, B-A, C,
MS, MS-A, & WM

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE 11/27/2013 /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

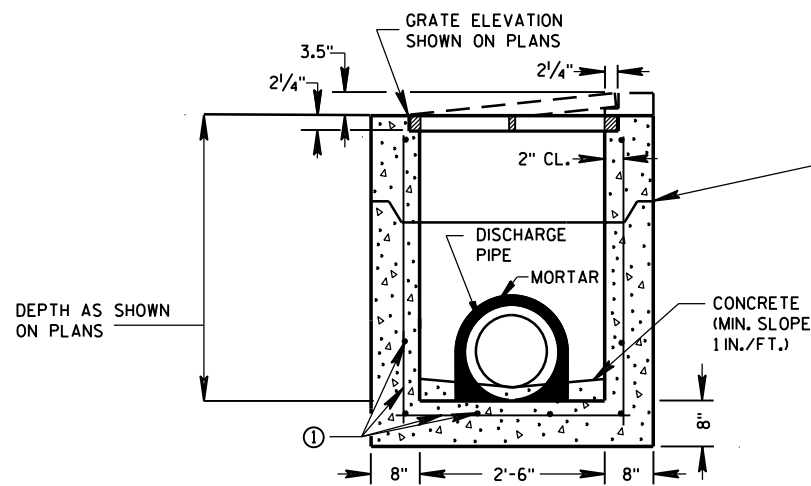


PLAN VIEW

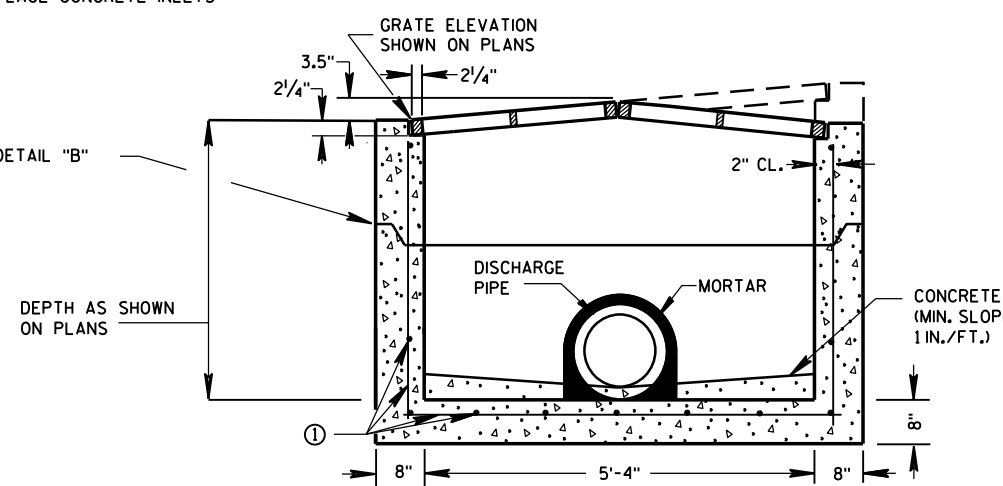


PLAN VIEW

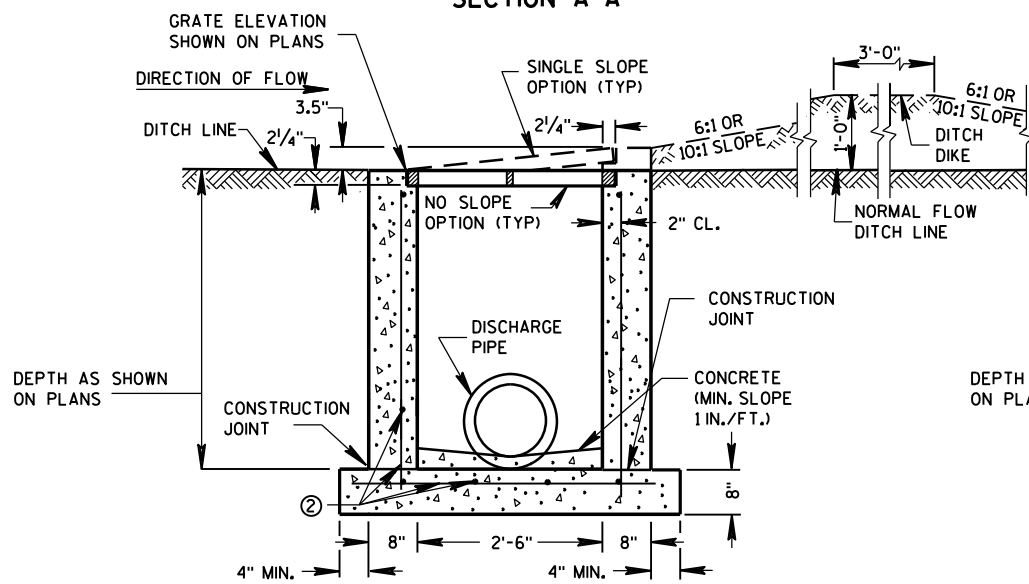
4" OVERHANGING BASE ON REINFORCED CAST-IN-PLACE CONCRETE INLETS



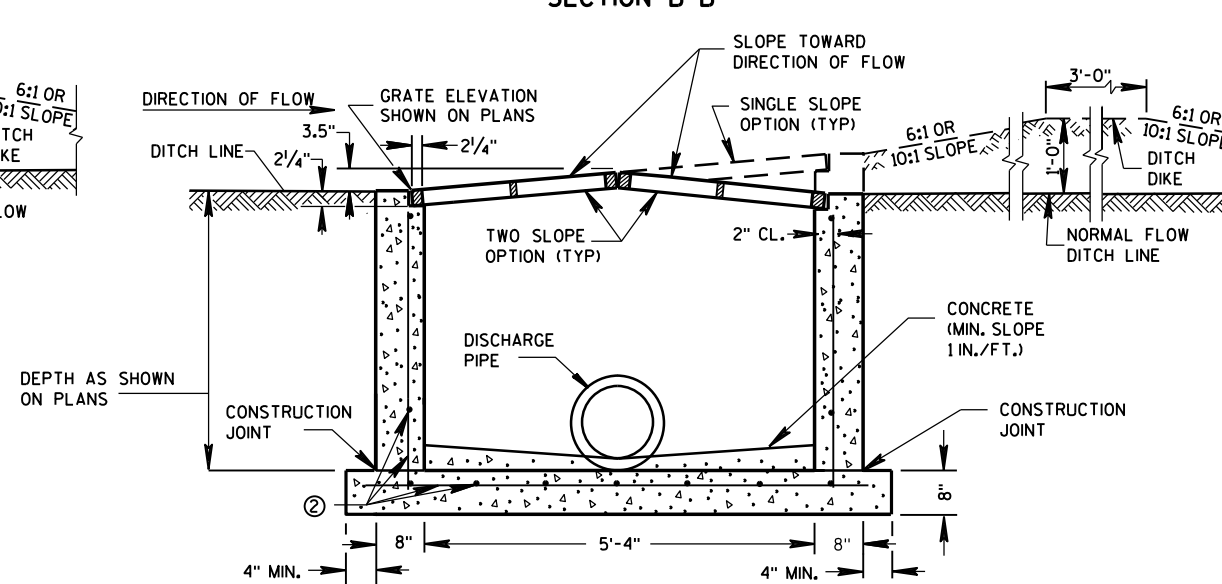
PRECAST REINFORCED CONCRETE SECTION A-A



PRECAST REINFORCED CONCRETE SECTION B-B



REINFORCED CAST-IN-PLACE CONCRETE SECTION A-A



REINFORCED CAST-IN-PLACE CONCRETE SECTION B-B

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLETS WHICH MAY INCLUDE PRECAST REINFORCED CONCRETE INLETS, SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL MEDIAN INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, IG-MS", ETC. THE FIRST NUMBER AND LETTER DESIGNATE THE TYPE OF STRUCTURE, AND THE FOLLOWING LETTERS DESIGNATE THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT. BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

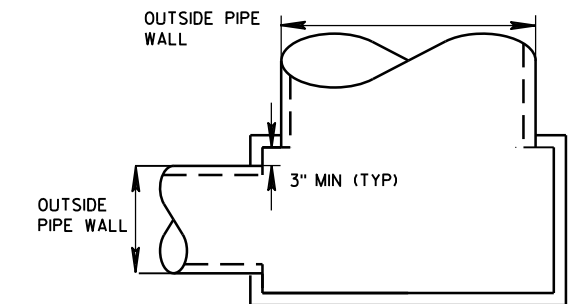
ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

- ① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

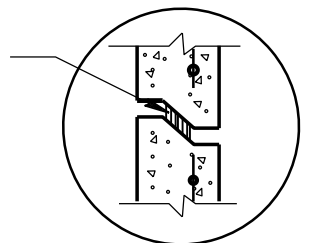
PIPE MATRIX

| INLET SIZE | MAXIMUM INSIDE PIPE DIAMETER | |
|------------|------------------------------|-------------|
| | WIDTH (IN) | LENGTH (IN) |
| 1 GRATE | 18 | 18 |
| 2 GRATE | 18 | 42 |



DETAIL "A"

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)

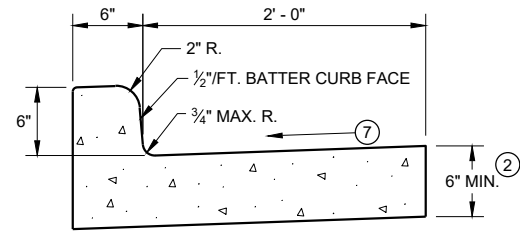


DETAIL "B"

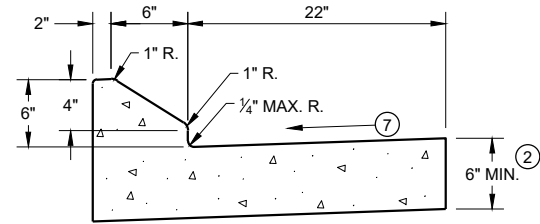
INLETS MEDIAN 1 AND 2 GRATE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

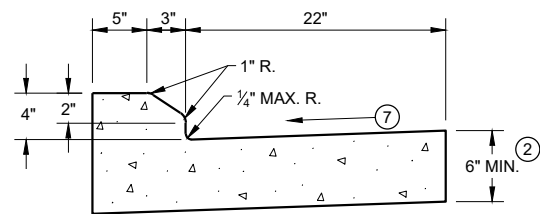
APPROVED
Sept., 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



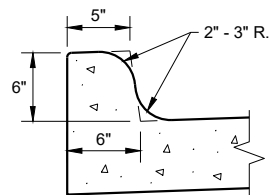
TYPES A¹ & D



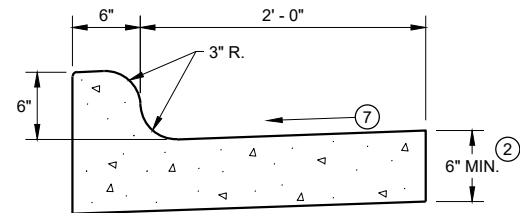
6" SLOPED CURB TYPES G¹ & J



4" SLOPED CURB TYPES G¹ & J

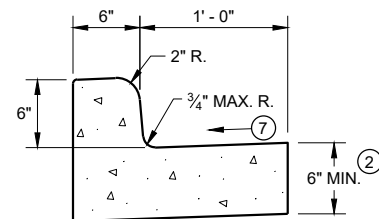


TYPES K¹ & L
(OPTIONAL CURB SHAPE)



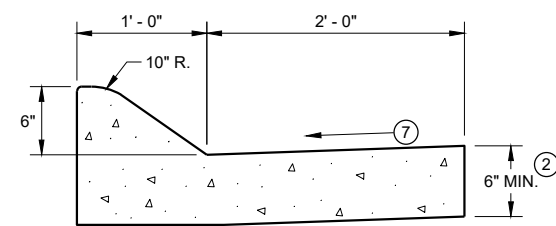
TYPES K¹ & L

CONCRETE CURB AND GUTTER 30"

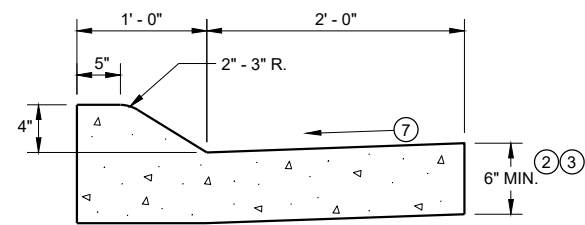


TYPES A¹ & D

CONCRETE CURB AND GUTTER 18"

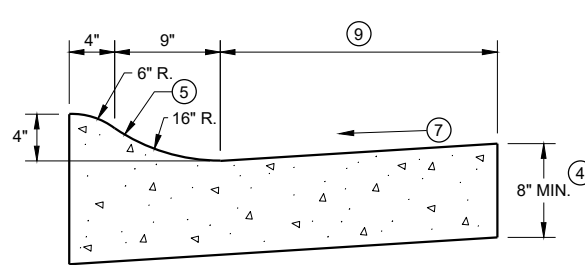


6" SLOPED CURB TYPES A¹ & D



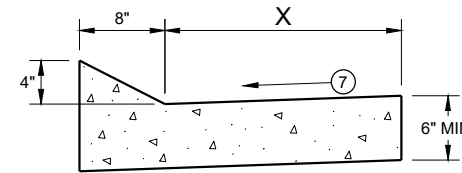
4" SLOPED CURB TYPES A¹ & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R¹ & T

| | |
|------------|-----|
| TBT & TBTT | X |
| 30" | 22" |
| 36" | 28" |

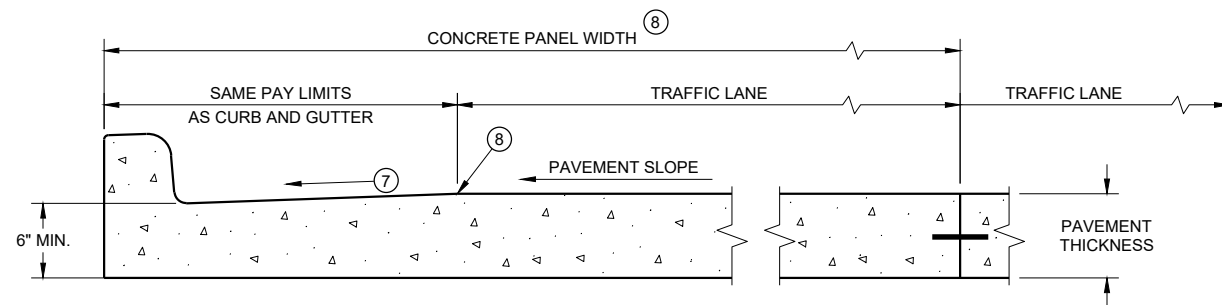


TYPES TBT & TBTT¹

CONCRETE CURB AND GUTTER

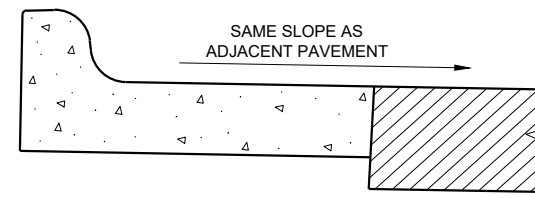
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

| PAVEMENT THICKNESS | MAXIMUM PANEL WIDTH |
|--------------------|---------------------|
| LESS THAN 10" | 12' |
| 10" & ABOVE | 15' |



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁶
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

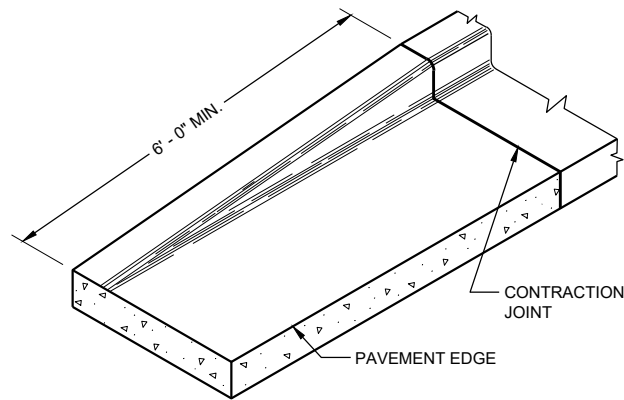
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

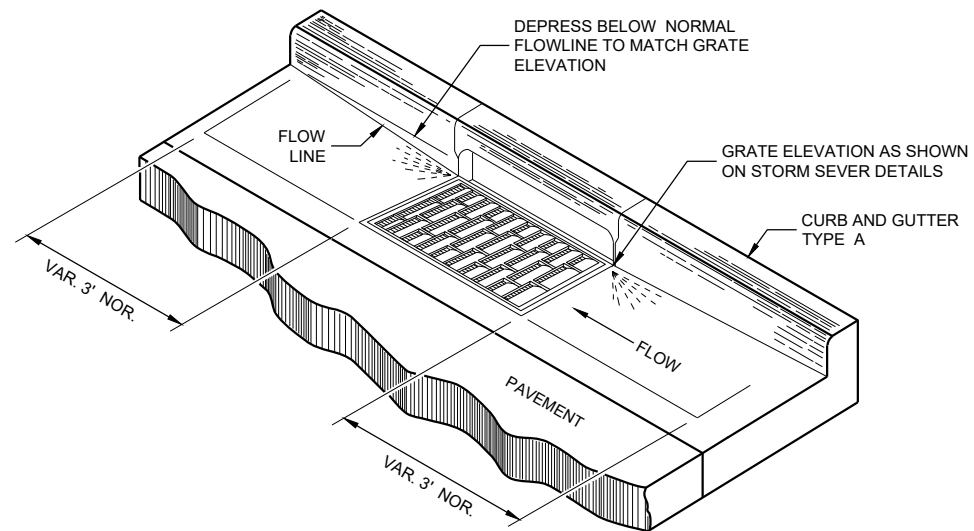
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

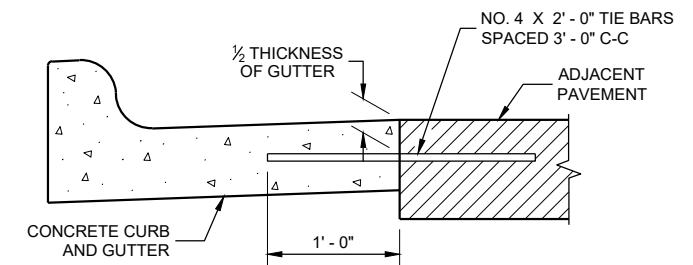
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

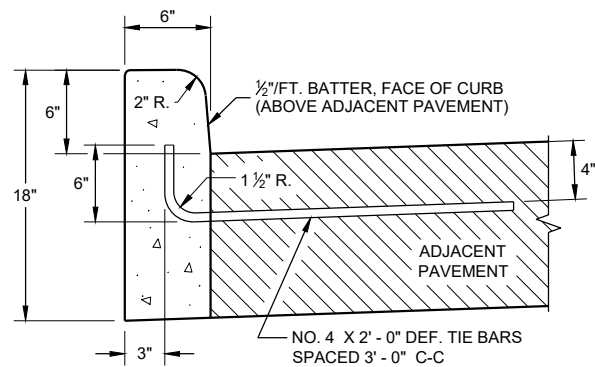
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

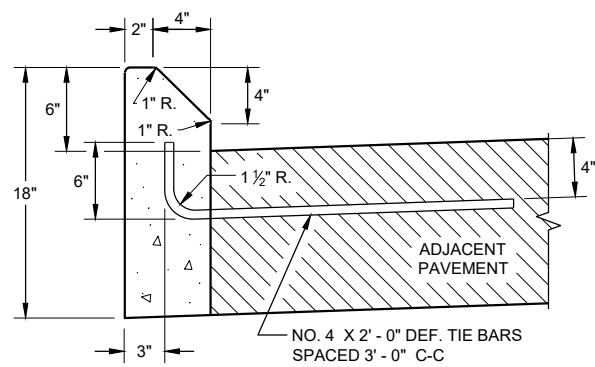
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑨ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION ①

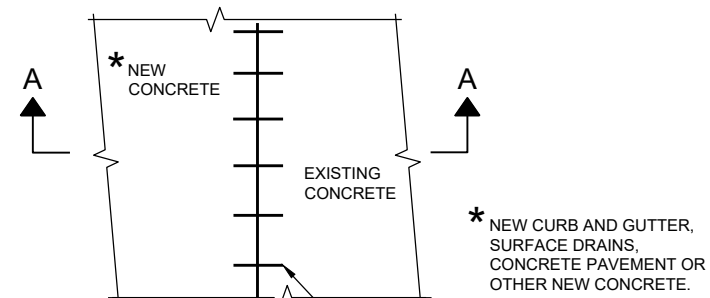


TYPES A ① & D

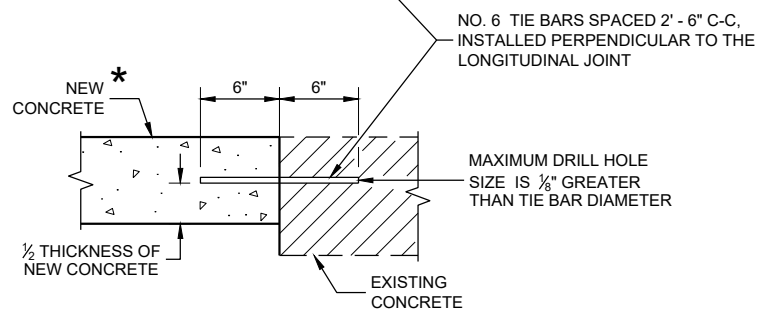


TYPES G ① & J

CONCRETE CURB

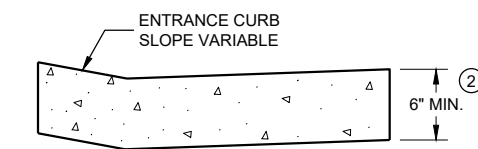


PLAN VIEW



SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



DRIVEWAY ENTRANCE CURB ⑨
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

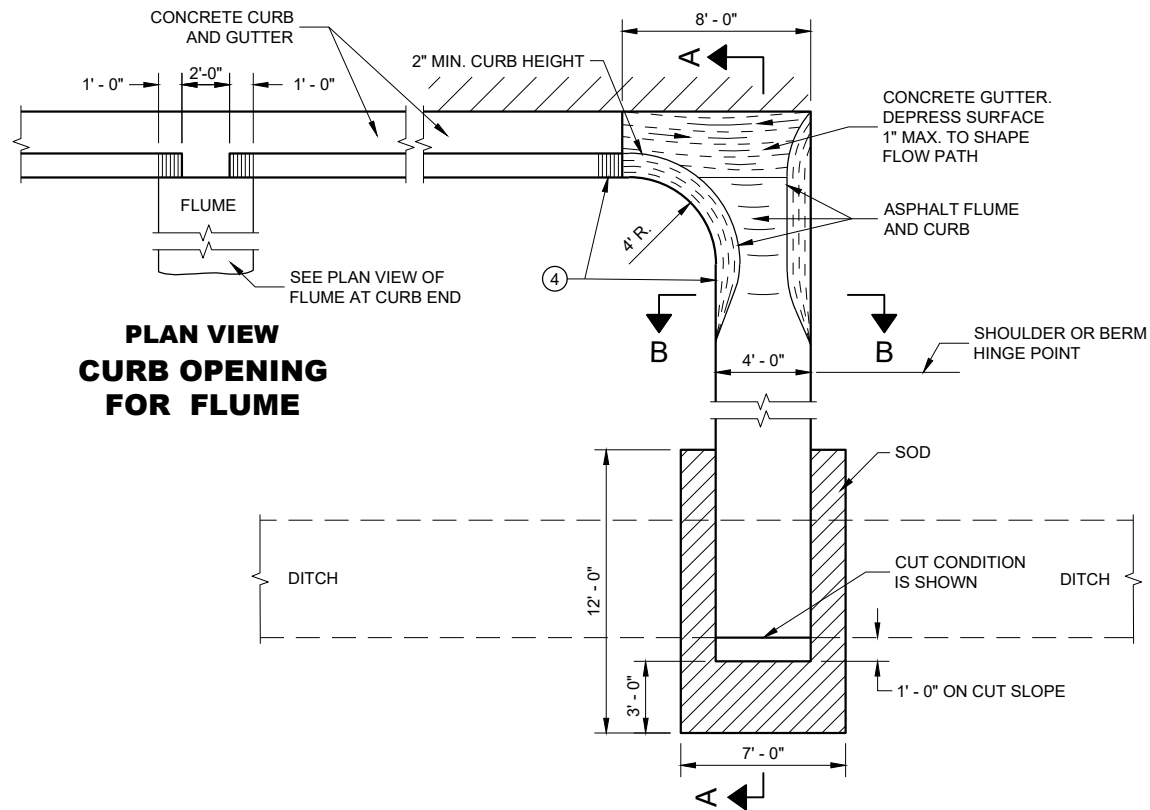
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA

NOTE: TAPER CURB ENDS TO GUTTER IN 1' - 0"

ASPHALTIC FLUME



**PLAN VIEW
CURB OPENING
FOR FLUME**

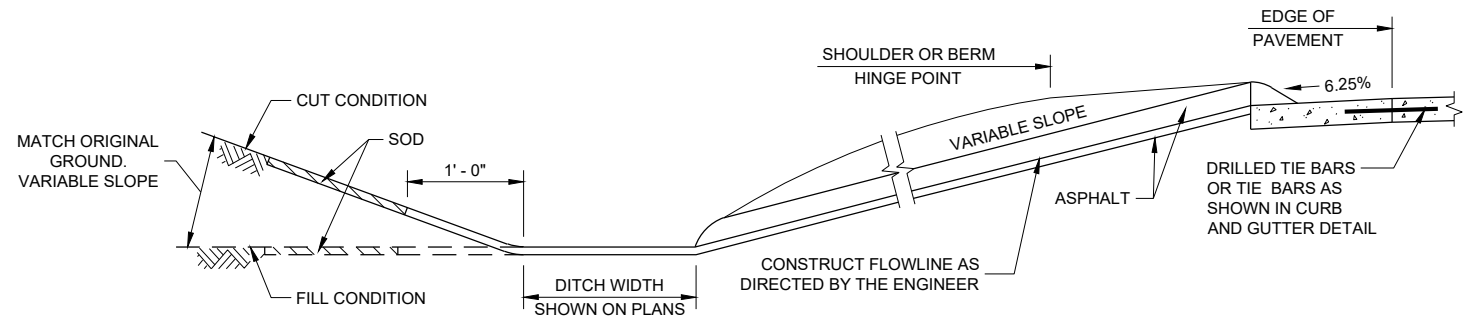
**PLAN VIEW
FLUME AT CURB END**

GENERAL NOTES

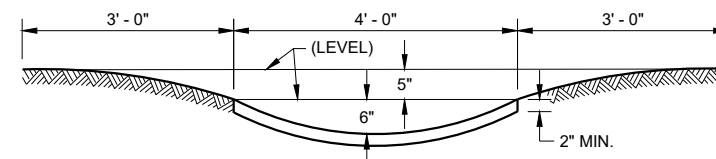
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

4" X 4" - W3.0 X W3.0 CONCRETE REINFORCEMENT SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

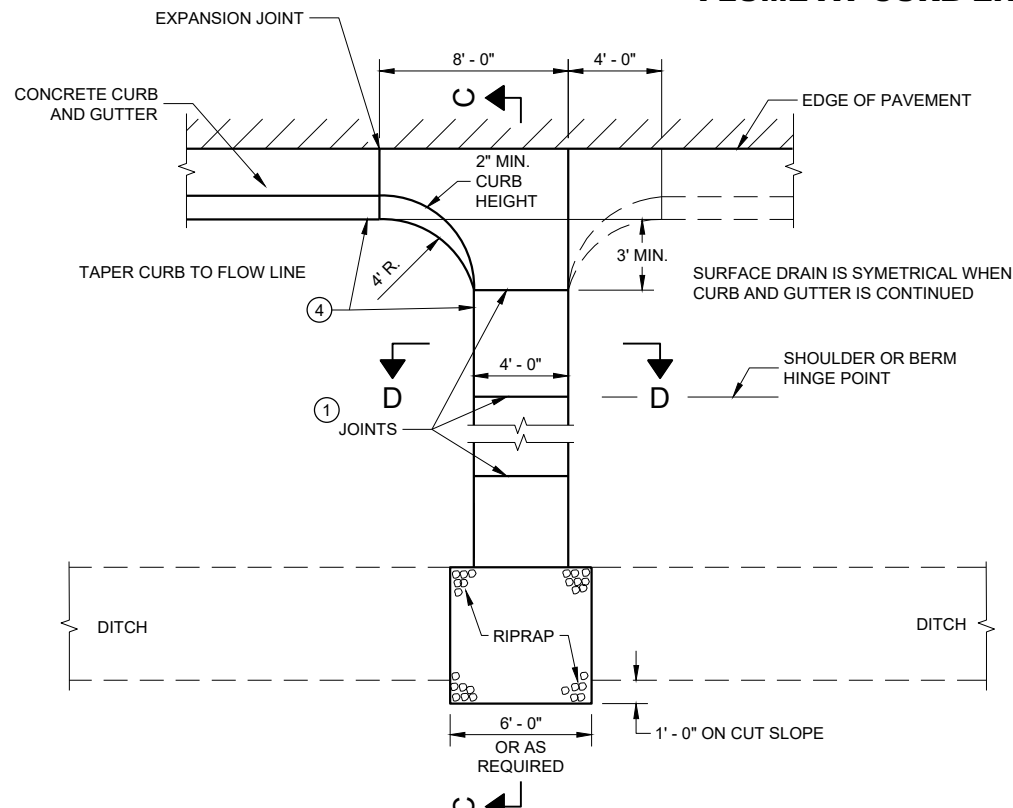
- ① JOINTS SHALL BE 1/8" TO 1/4" WIDE BY 1 1/2" DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
- ② GEOTEXTILE TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
- ③ CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED.
- ④ ANGLE OF FLUME IN RELATION TO BACK OF CURB TO BE CONSTRUCTED PER THE PLAN DETAILS OR AS DIRECTED BY THE ENGINEER. ANGLE OF FLUME MAY BE OTHER THAN 90 DEGREES AS SHOWN.



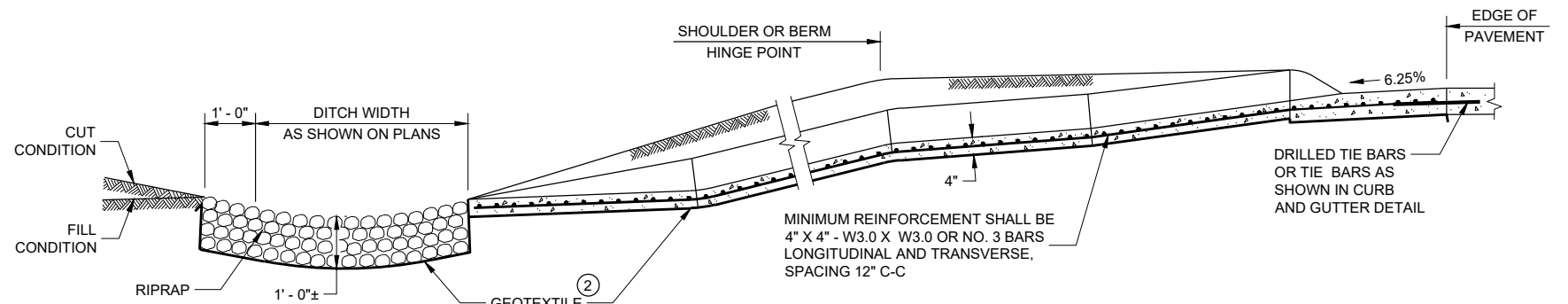
SECTION A - A



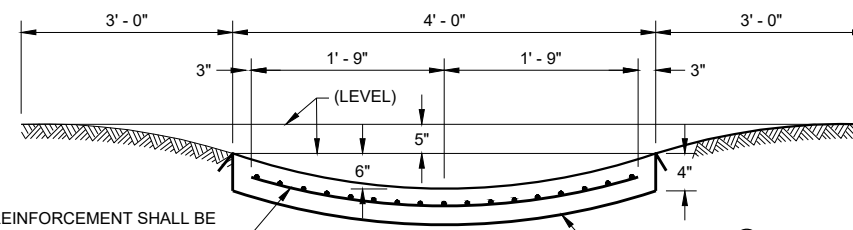
SECTION B - B



**PLAN VIEW
CONCRETE SURFACE DRAIN**



SECTION C - C



SECTION D - D

MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE, SPACING 12" C-C

MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE, SPACING 12" C-C

CONCRETE SURFACE DRAINS AND ASPHALTIC FLUMES

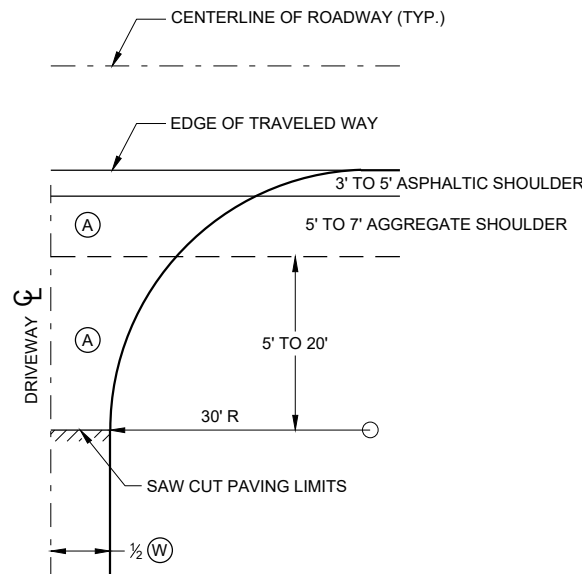
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA

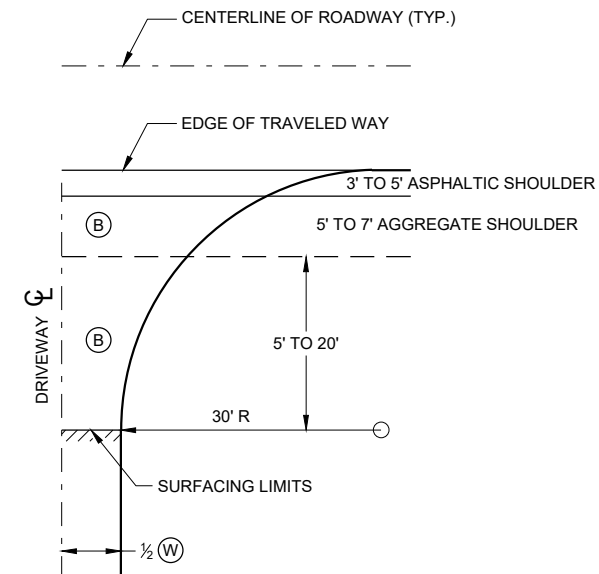
GENERAL NOTES

- ① DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

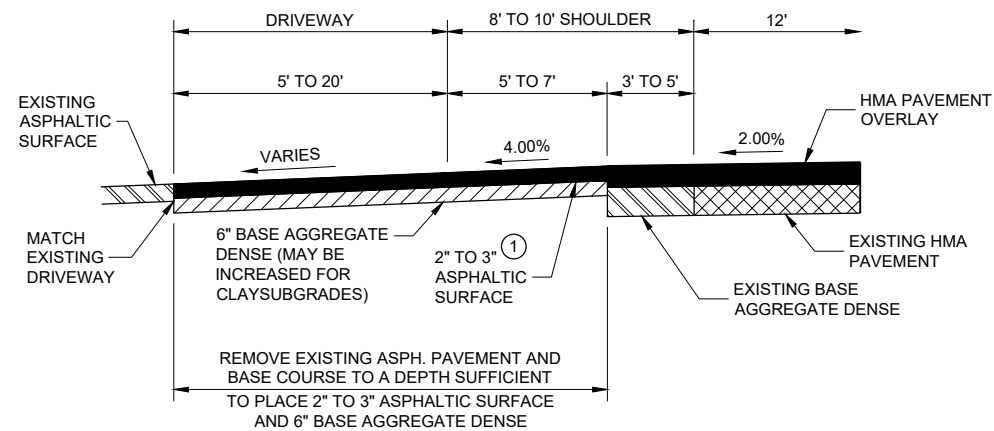


- (A) : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)
- (B) : PAID FOR AS BASE AGGREGATE DENSE 1 1/4" (TON)
- (W) : DRIVEWAY WIDTH 16' MIN. - 24' MAX.

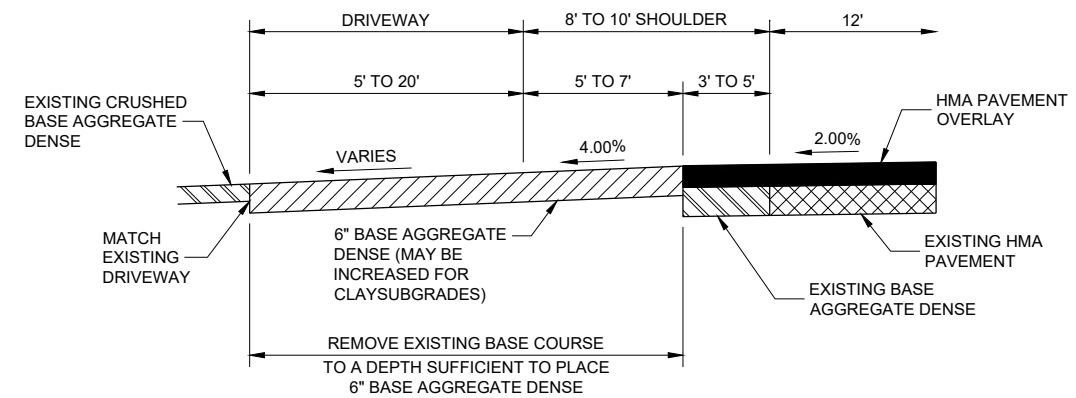
**PLAN VIEW
HALF SECTION**



**PLAN VIEW
HALF SECTION**



**PROFILE VIEW
RURAL ENTRANCE
WITH ASPHALTIC SURFACE
RESURFACING PROJECTS**



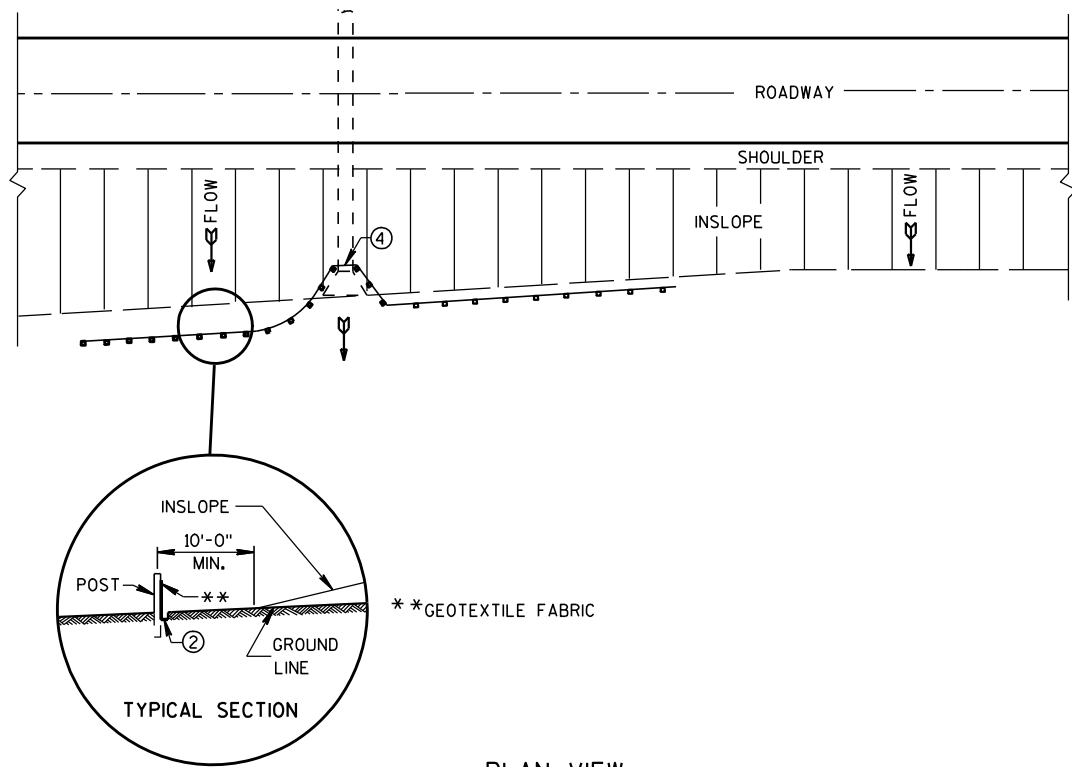
**PROFILE VIEW
RURAL ENTRANCE
WITH AGGREGATE SURFACE
6" BASE AGGREGATE DENSE
RESURFACING PROJECTS**

**DRIVEWAYS WITHOUT CURB
AND GUTTER RESURFACING
PROJECTS RURAL**

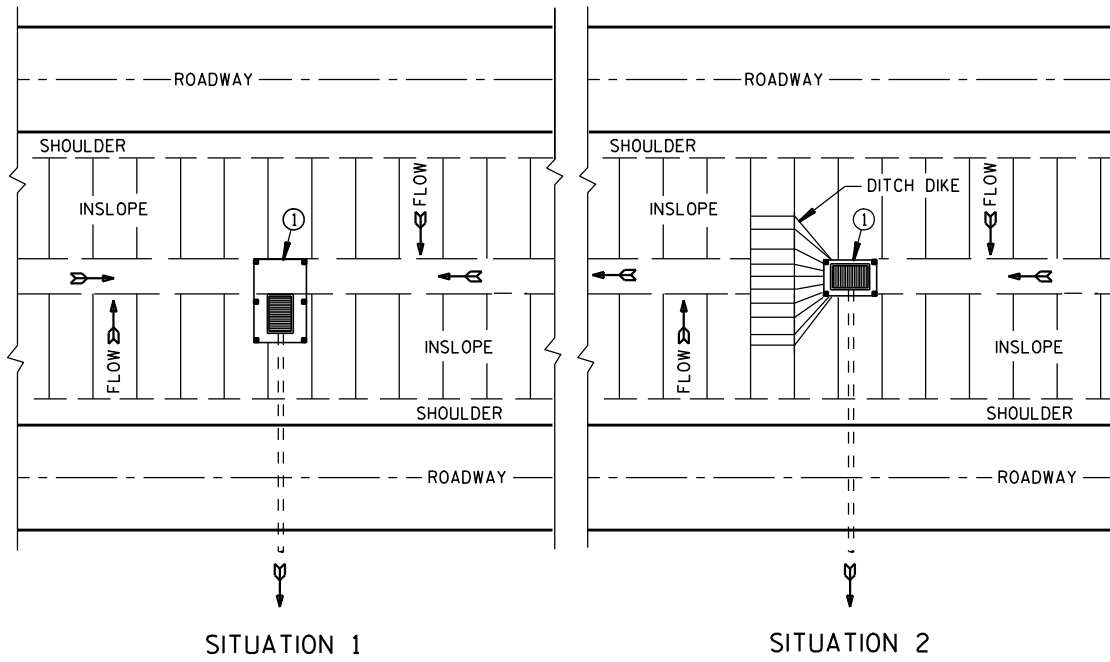
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

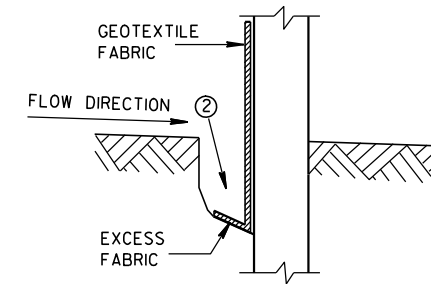


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

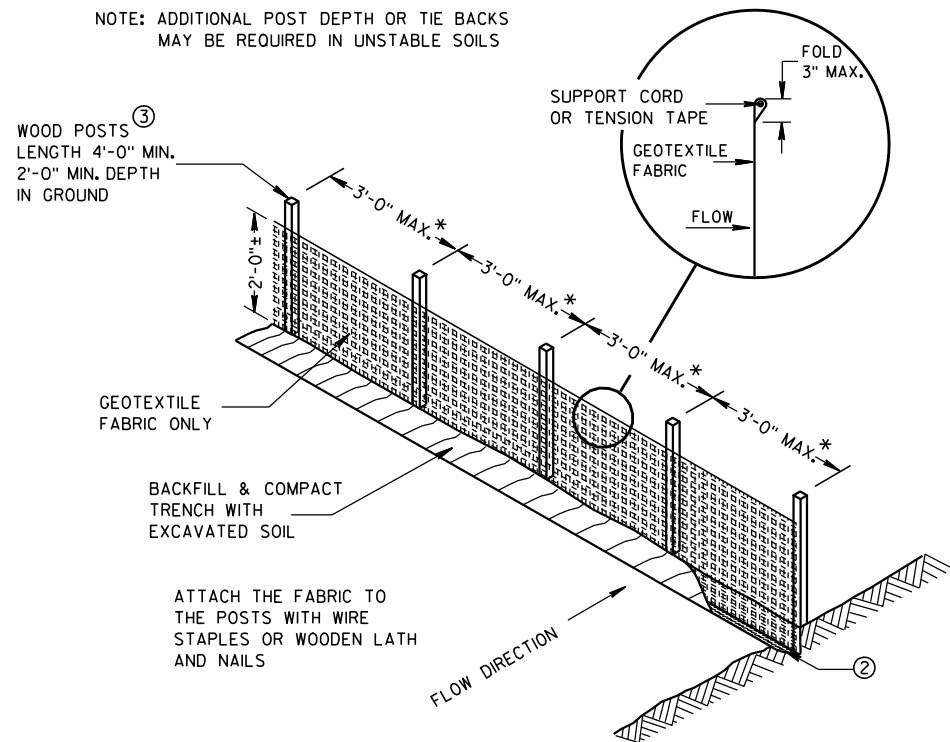
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



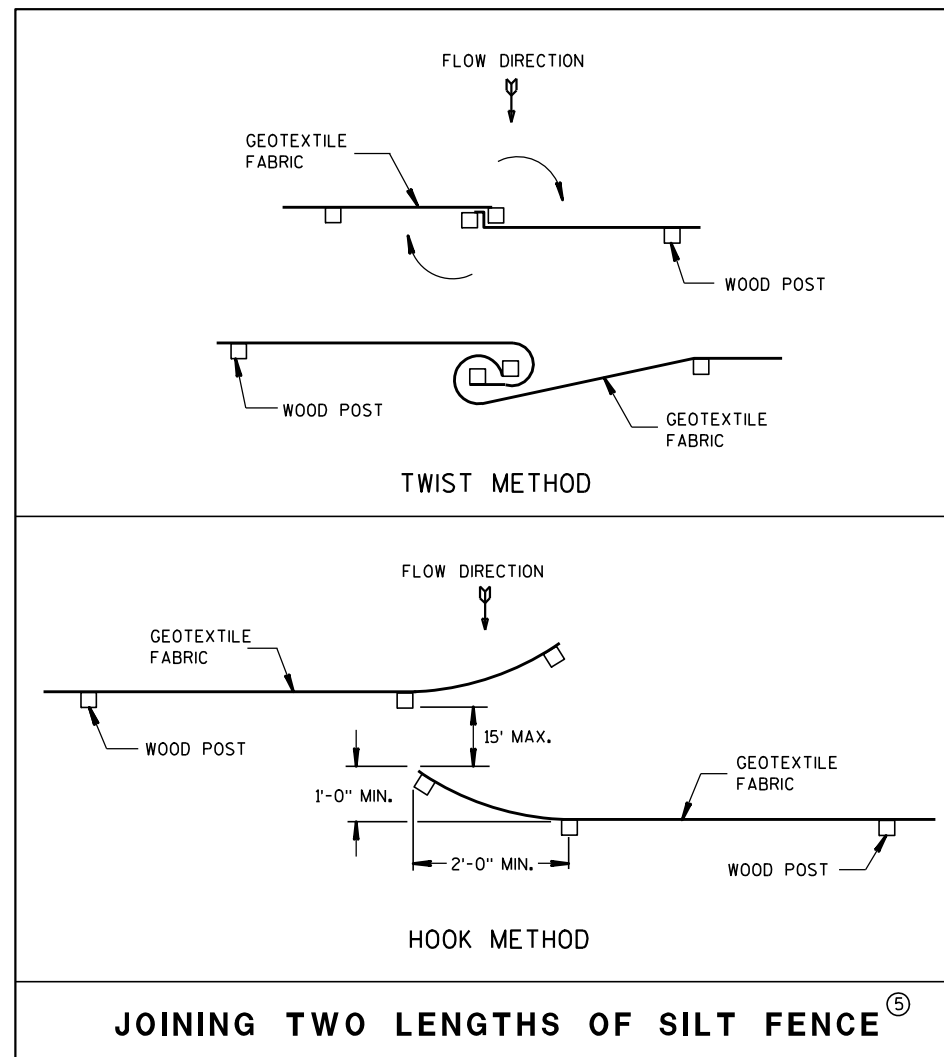
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

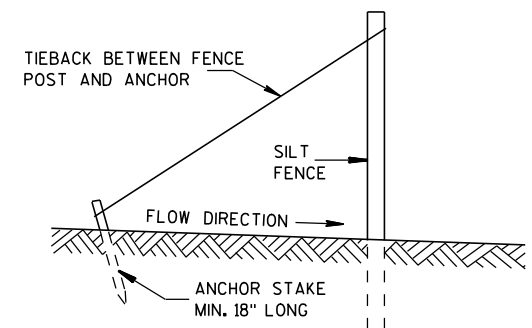


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

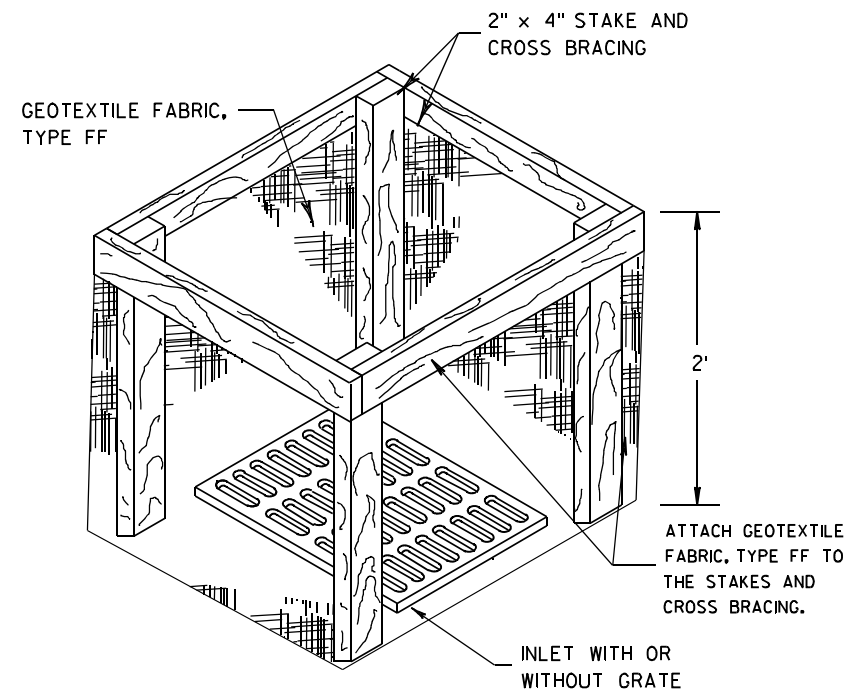
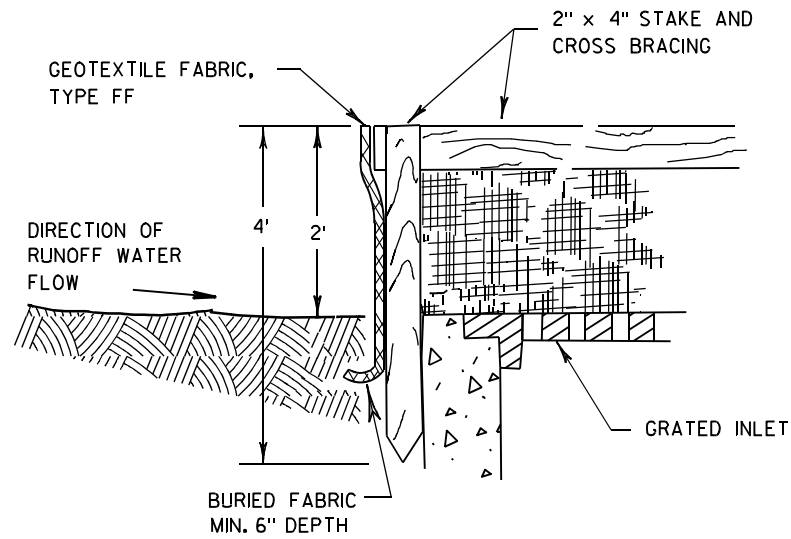
APPROVED

4-29-05

DATE

FHWA

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER



INLET PROTECTION, TYPE A

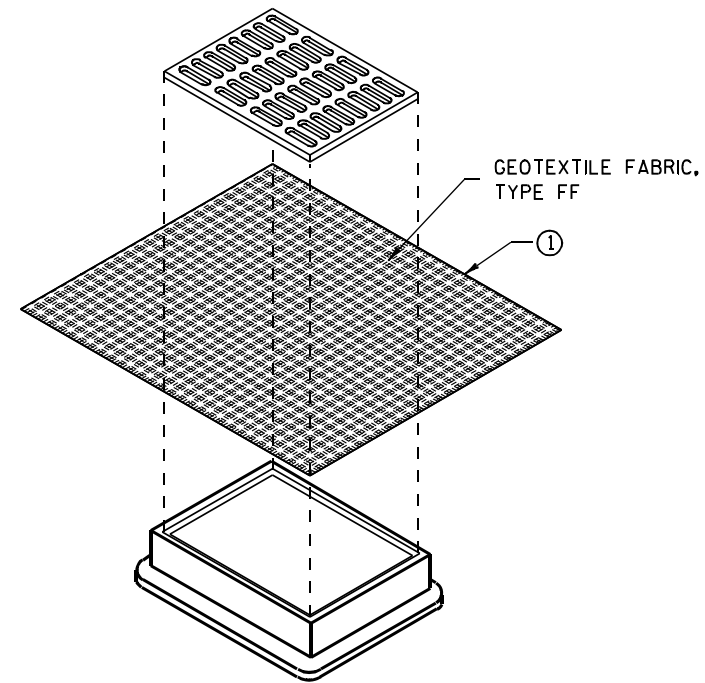
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

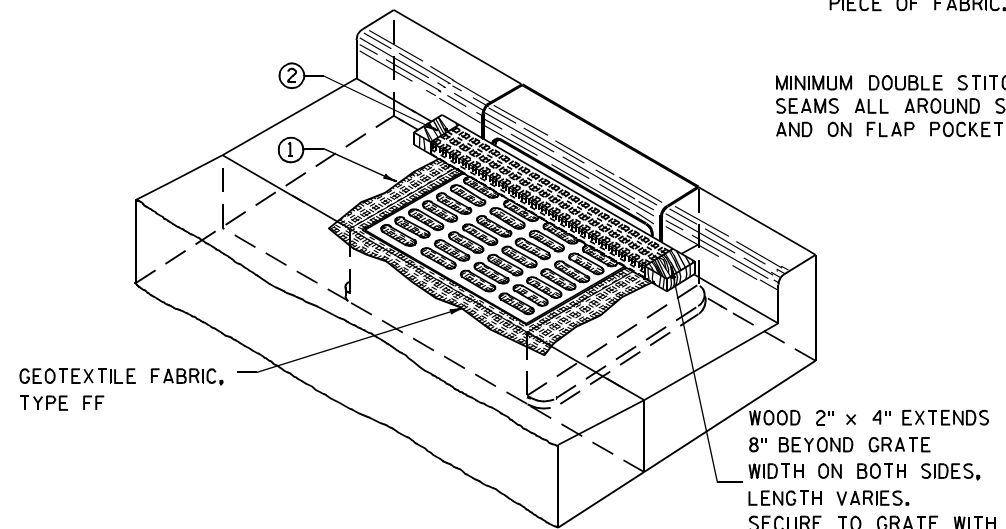
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

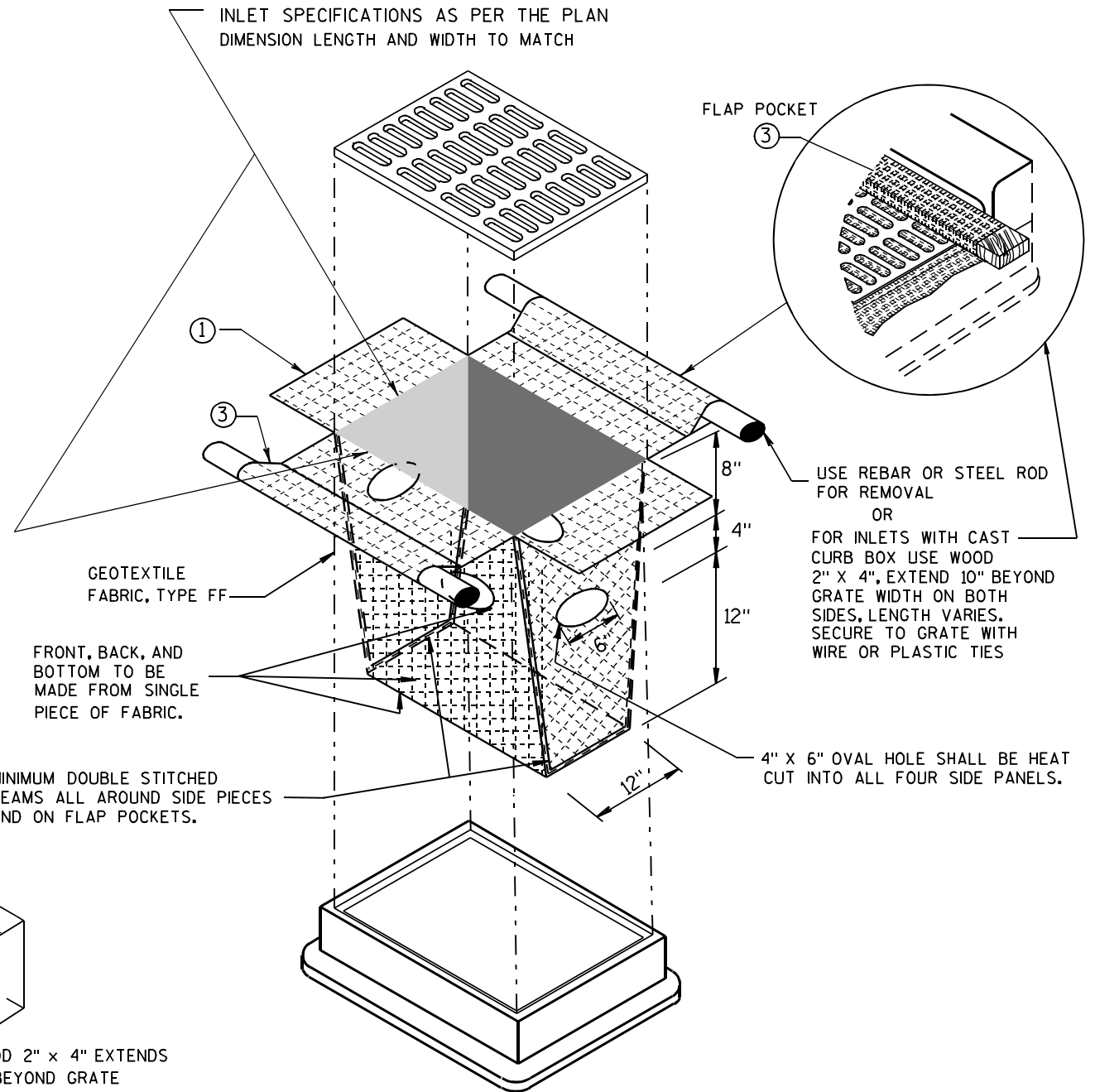
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

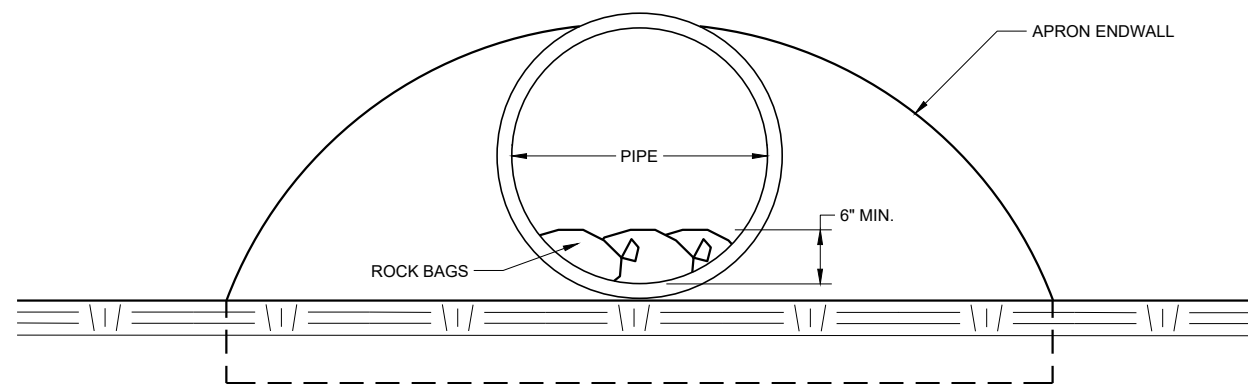
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



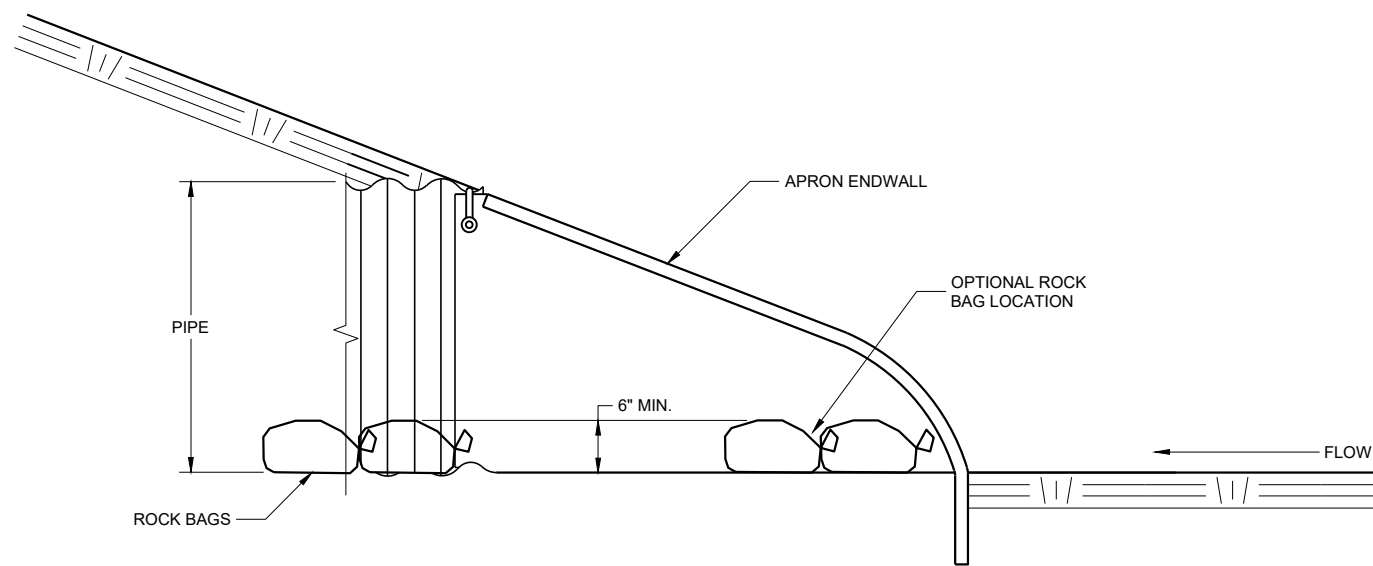
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

| | |
|--|--|
| INLET PROTECTION TYPE A, B, C, AND D | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED 10/16/02 DATE | /S/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER |
| FHWA | |



END VIEW



SIDE VIEW

CULVERT PIPE CHECK
 (INSTALL ON INLET END ONLY)

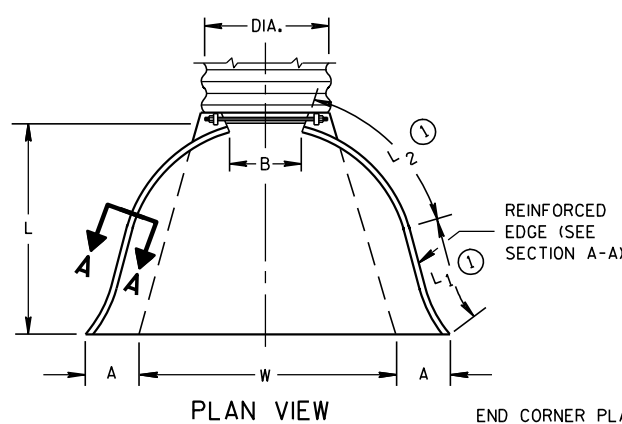
| | |
|--|---|
| CULVERT PIPE CHECK | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED May 2019 DATE | /S/ Daniel Schave EROSION CONTROL ENGINEER |
| <small>FHWA</small> | |

| METAL APRON ENDWALLS | | | | | | | | | | | |
|----------------------|----------------------|-------|---------------------|----------|---------|-------------|----|--------|---------|---------------|-------|
| PIPE DIA. (IN.) | MIN. THICK. (Inches) | | DIMENSIONS (Inches) | | | | | | | APPROX. SLOPE | BODY |
| | STEEL | ALUM. | A (±1") | B (MAX.) | H (±1") | L (±1 1/2") | L1 | L2 | W (±2") | | |
| 12 | .064 | .060 | 6 | 6 | 6 | 21 | 12 | 17 1/2 | 24 | 2 1/2 to 1 | 1 Pc. |
| 15 | .064 | .060 | 7 | 8 | 6 | 26 | 14 | 21 3/4 | 30 | 2 1/2 to 1 | 1 Pc. |
| 18 | .064 | .060 | 8 | 10 | 6 | 31 | 15 | 28 1/4 | 36 | 2 1/2 to 1 | 1 Pc. |
| 21 | .064 | .060 | 9 | 12 | 6 | 36 | 18 | 29 5/8 | 42 | 2 1/2 to 1 | 1 Pc. |
| 24 | .064 | .075 | 10 | 13 | 6 | 41 | 18 | 37 1/4 | 48 | 2 1/2 to 1 | 1 Pc. |
| 30 | .079 | .075 | 12 | 16 | 8 | 51 | 18 | 52 1/4 | 60 | 2 1/2 to 1 | 1 Pc. |
| 36 | .079 | .105 | 14 | 19 | 9 | 60 | 24 | 59 3/4 | 72 | 2 1/2 to 1 | 2 Pc. |
| 42 | .109 | .105 | 16 | 22 | 11 | 69 | 24 | 75 5/8 | 84 | 2 1/2 to 1 | 2 Pc. |
| 48 | .109 | .105 | 18 | 27 | 12 | 78 | 24 | 81 | 90 | 2 1/4 to 1 | 3 Pc. |
| 54 | .109 | .105 | 18 | 30 | 12 | 84 | 30 | 85 1/2 | 102 | 2 1/4 to 1 | 3 Pc. |
| 60 | .109x | .105x | 18 | 33 | 12 | 87 | — | — | 114 | 2 to 1 | 3 Pc. |
| 66 | .109x | .105x | 18 | 36 | 12 | 87 | — | — | 120 | 2 to 1 | 3 Pc. |
| 72 | .109x | .105x | 18 | 39 | 12 | 87 | — | — | 126 | 2 to 1 | 3 Pc. |
| 78 | .109x | .105x | 18 | 42 | 12 | 87 | — | — | 132 | 1 1/2 to 1 | 3 Pc. |
| 84 | .109x | .105x | 18 | 45 | 12 | 87 | — | — | 138 | 1 1/2 to 1 | 3 Pc. |
| 90 | .109x | .105x | 18 | 37 | 12 | 87 | — | — | 144 | 1 1/2 to 1 | 3 Pc. |
| 96 | .109x | .105x | 18 | 35 | 12 | 87 | — | — | 150 | 1 1/2 to 1 | 3 Pc. |

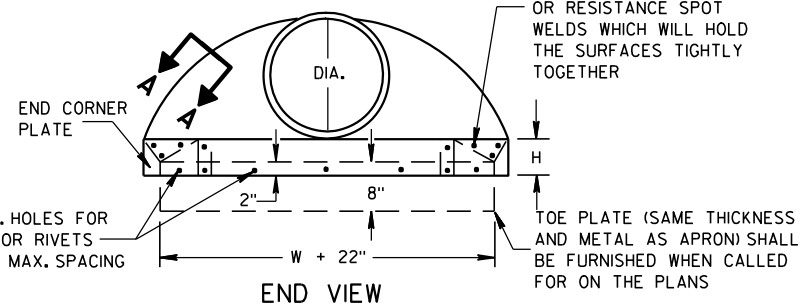
* EXCEPT CENTER PANEL SEE GENERAL NOTES

| REINFORCED CONCRETE APRON ENDWALLS | | | | | | | | | |
|------------------------------------|---------------------|--------|--------|-----------|------------|-----|-------|---------------|--|
| PIPE DIA. (IN.) | DIMENSIONS (Inches) | | | | | | | APPROX. SLOPE | |
| | T | A | B | C | D | E | G | | |
| 12 | 2 | 4 | 24 | 48 1/8 | 72 1/8 | 24 | 2 | 3 to 1 | |
| 15 | 2 1/4 | 6 | 27 | 46 | 73 | 30 | 2 1/4 | 3 to 1 | |
| 18 | 2 1/2 | 9 | 27 | 46 | 73 | 36 | 2 1/2 | 3 to 1 | |
| 21 | 2 3/4 | 9 | 36 | 37 1/2 | 73 1/2 | 42 | 2 3/4 | 3 to 1 | |
| 24 | 3 | 9 1/2 | 43 1/2 | 30 | 73 1/2 | 48 | 3 | 3 to 1 | |
| 27 | 3 1/4 | 10 1/2 | 49 1/2 | 24 | 73 1/2 | 54 | 3 1/4 | 3 to 1 | |
| 30 | 3 1/2 | 12 | 54 | 19 3/4 | 73 1/2 | 60 | 3 1/2 | 3 to 1 | |
| 36 | 4 | 15 | 63 | 34 3/4 | 97 3/4 | 72 | 4 | 3 to 1 | |
| 42 | 4 1/2 | 21 | 63 | 35 | 98 | 78 | 4 1/2 | 3 to 1 | |
| 48 | 5 | 24 | 72 | 26 | 98 | 84 | 5 | 3 to 1 | |
| 54 | 5 1/2 | 27 | 65 | 33 1/4-35 | 98 1/4-100 | 90 | 5 1/2 | 2 1/2 to 1 | |
| 60 | 6 | 30-35 | 60 | 39 | 99 | 96 | 5 | 2 to 1 | |
| 66 | 6 1/2 | 30-35 | 72-78 | 21-27 | 99 | 102 | 5 1/2 | 2 to 1 | |
| 72 | 7 | 30-35 | 78 | 21 | 99 | 108 | 6 | 2 to 1 | |
| 78 | 7 1/2 | 30-35 | 78 | 21 | 99 | 114 | 6 1/2 | 2 to 1 | |
| 84 | 8 | 36 | 90 1/2 | 21 | 111 1/2 | 120 | 6 1/2 | 1 1/2 to 1 | |
| 90 | 8 1/2 | 41 | 87 1/2 | 24 | 111 1/2 | 132 | 6 1/2 | 1 1/2 to 1 | |

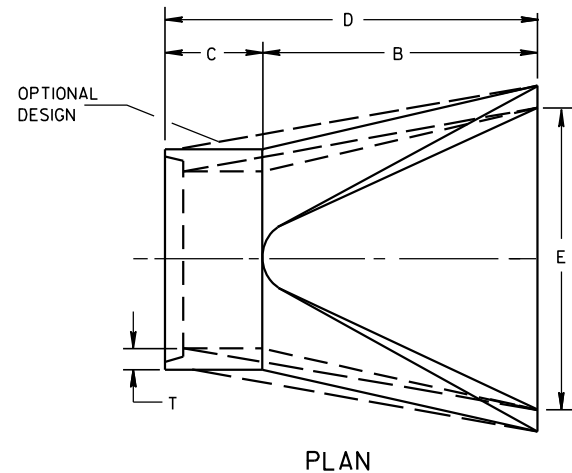
* MINIMUM
** MAXIMUM



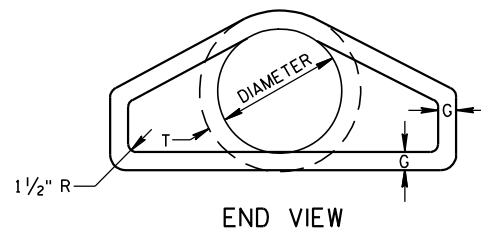
END CORNER PLATES MAY BE FASTENED TO APRON PROPER BY BOLTS, RIVETS, OR RESISTANCE SPOT WELDS WHICH WILL HOLD THE SURFACES TIGHTLY TOGETHER



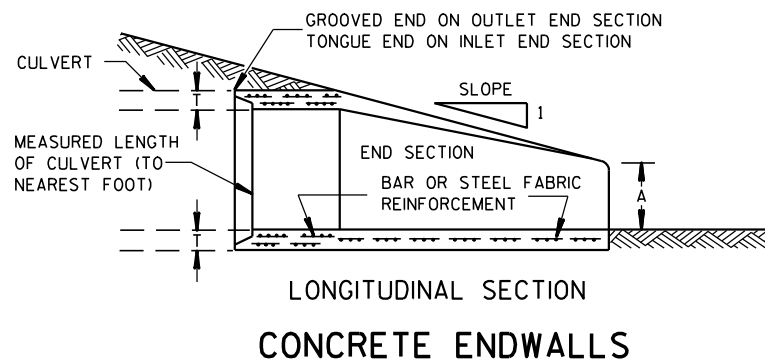
SIDE ELEVATION
METAL ENDWALLS



PLAN

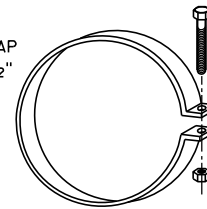


END VIEW

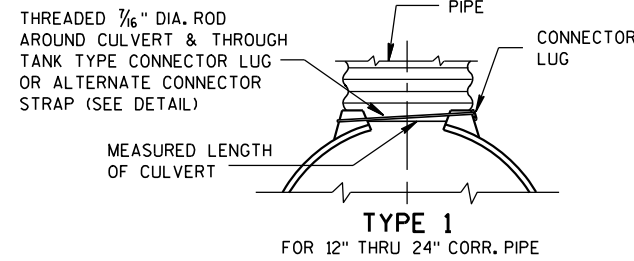


LONGITUDINAL SECTION
CONCRETE ENDWALLS

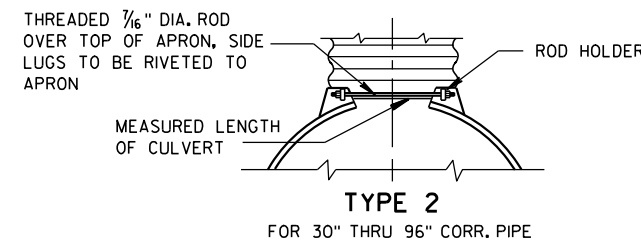
1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



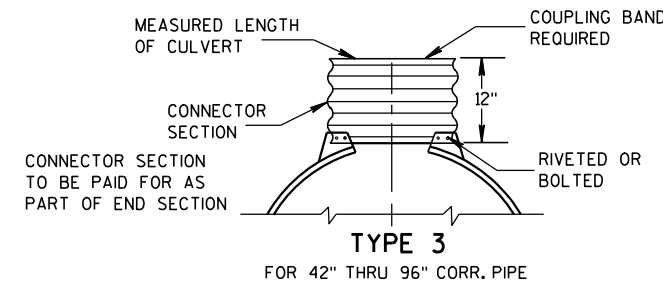
ALTERNATE FOR TYPE 1 CONNECTION
END SECTION CONNECTOR STRAP



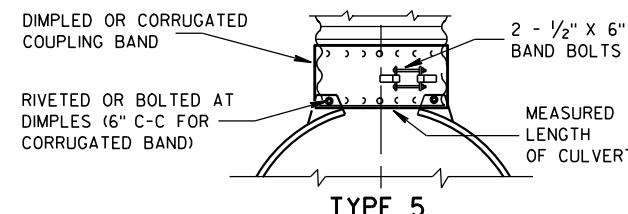
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

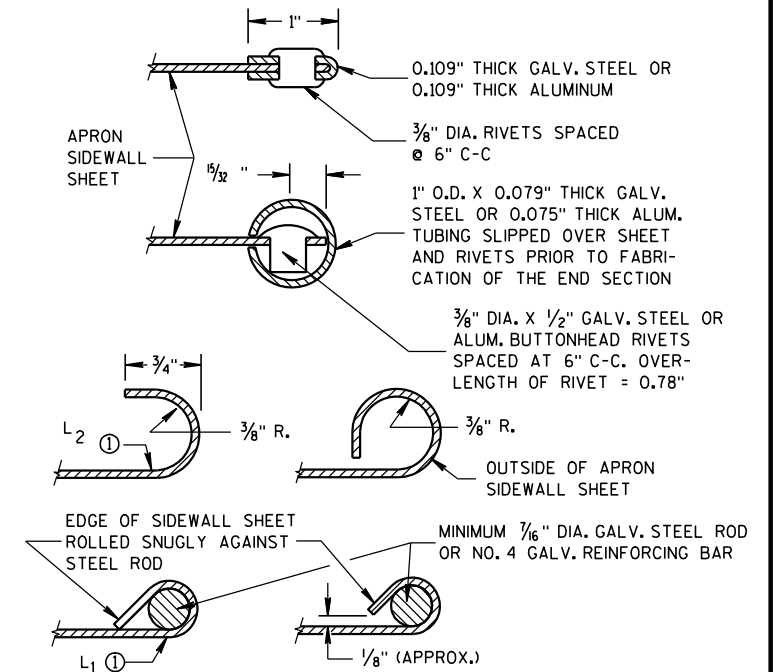
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VICE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

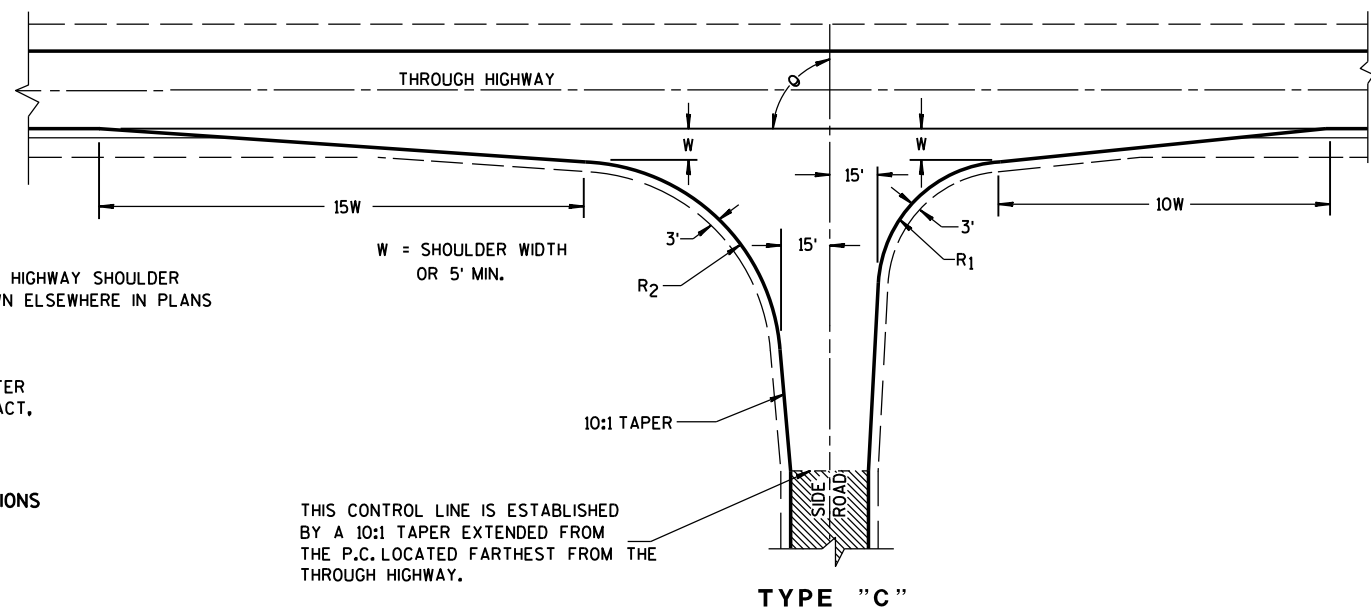
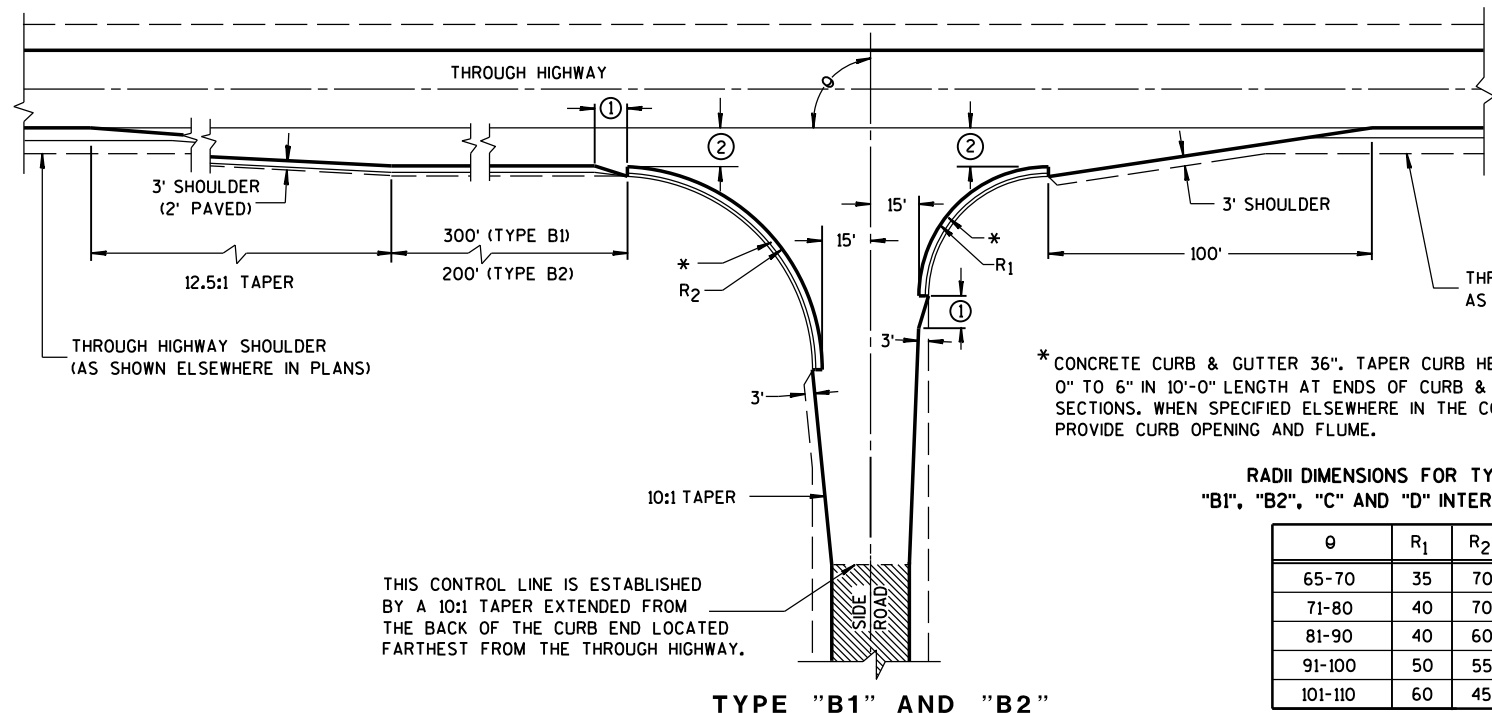
WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR
CULVERT PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 DATE /S/ Rory L. Rhinesmith
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



GENERAL NOTES

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

SIDE ROAD SURFACING NOTE

WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

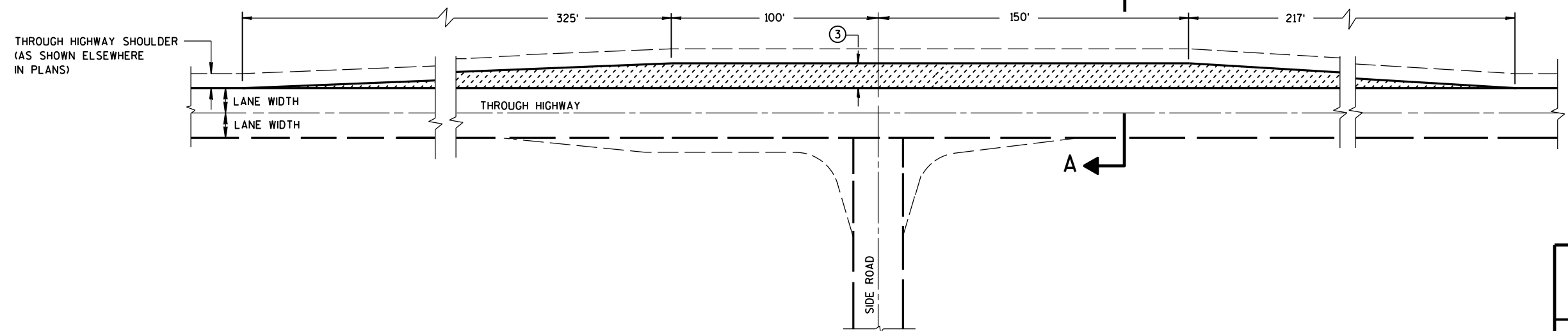
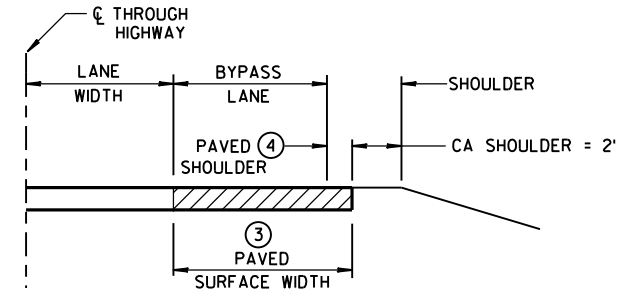
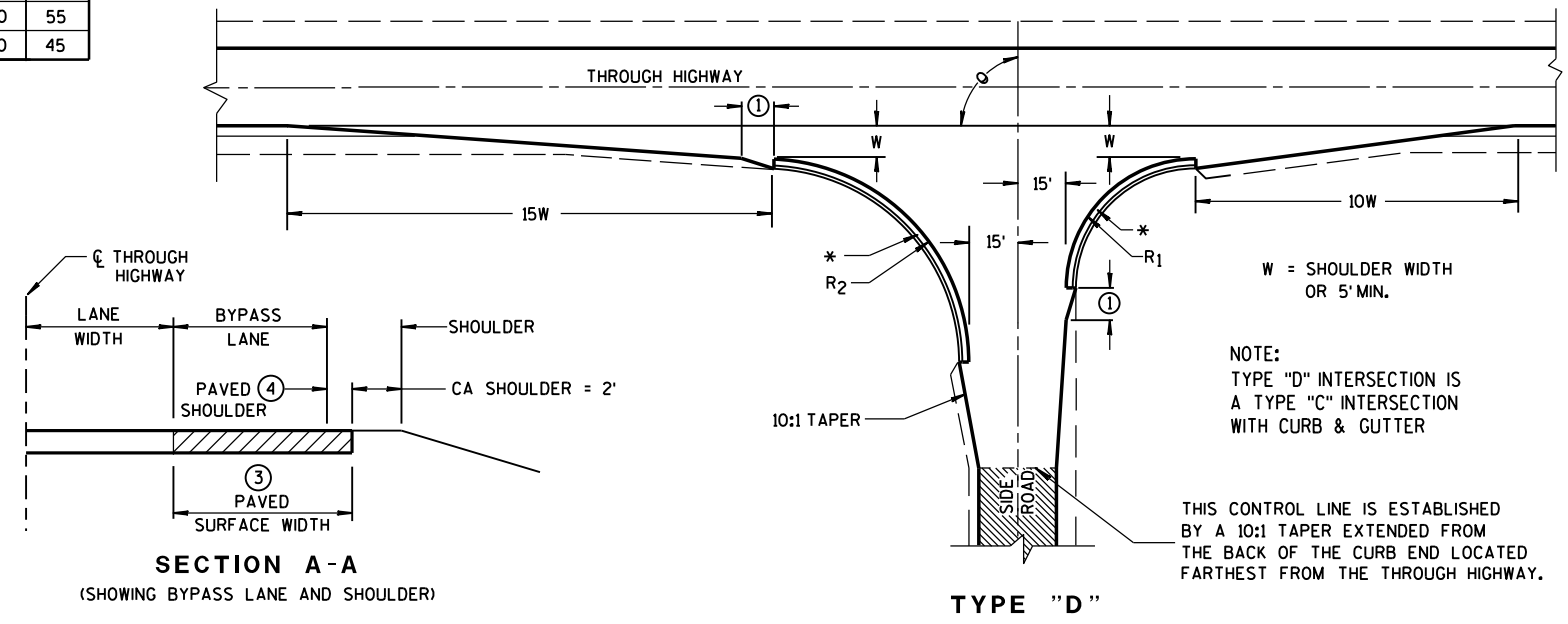
WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

EXISTING PAVED SURFACE

BYPASS LANE

- ① 10-FT TYPICAL.
- ② 12-FT** PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.
- **10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.
- ③ BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE
-ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH.
-PC CPNCRETE = 13-FT PLUS PAVED SHOULDER WIDTH.
- ④ BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.



TEE INTERSECTION BYPASS LANE DETAIL

AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND "D" AND TEE INTERSECTION BYPASS LANE

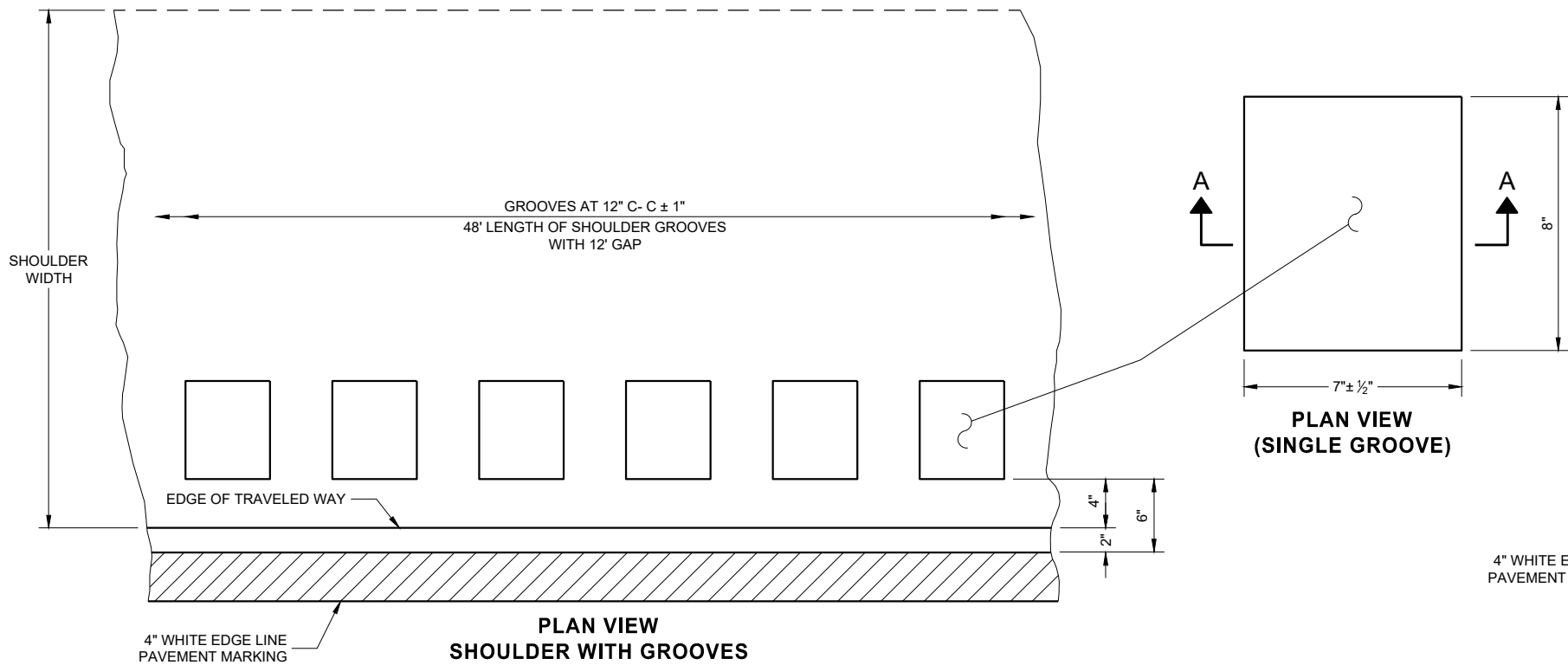
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

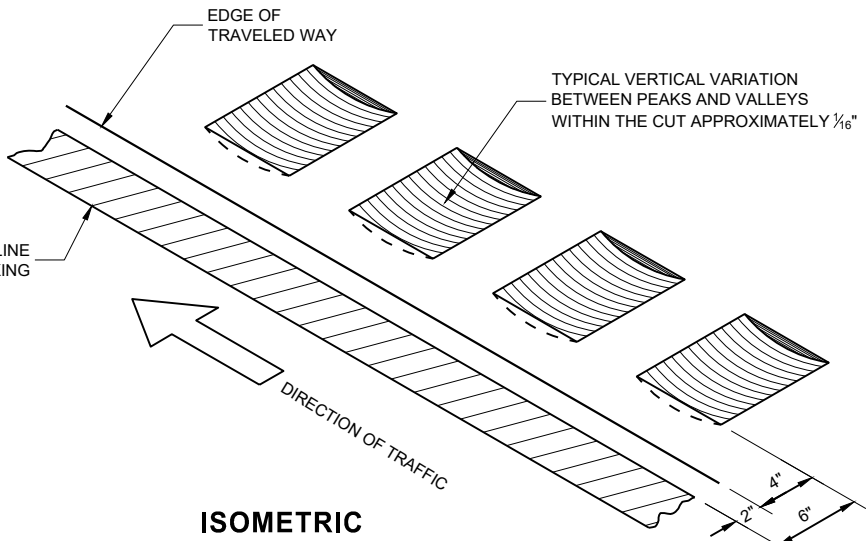
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL SHOULDER GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

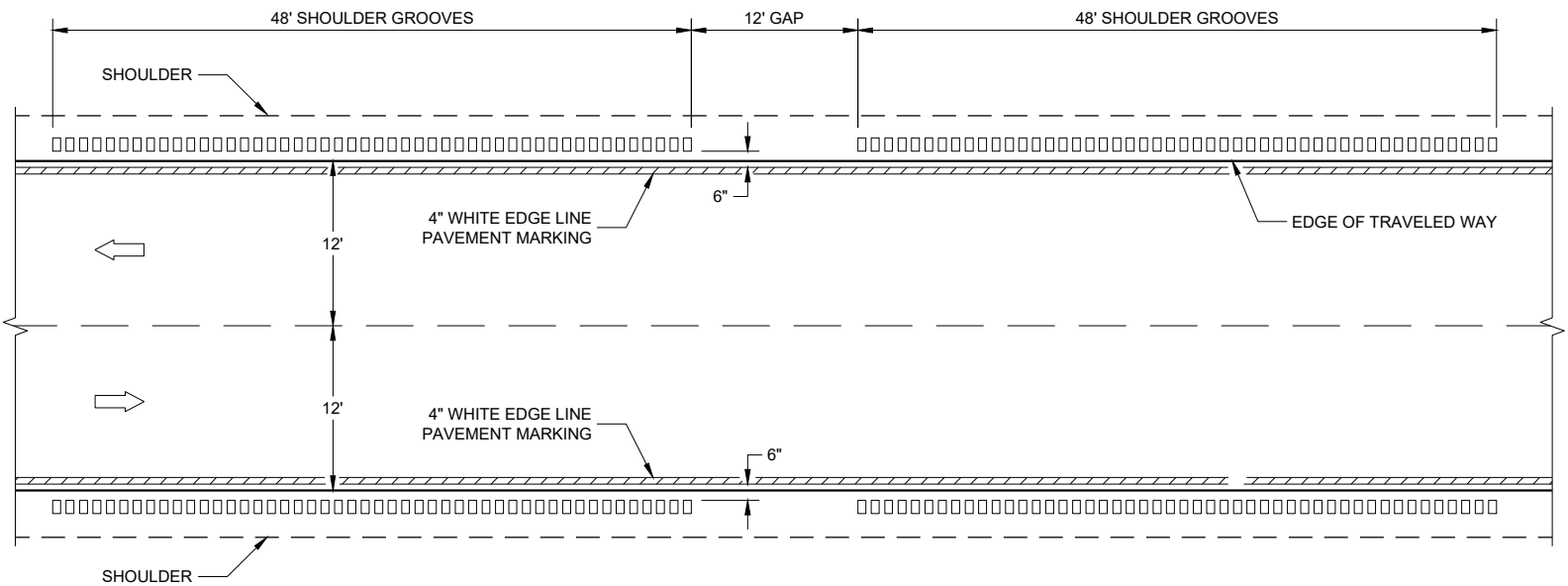
- ① SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



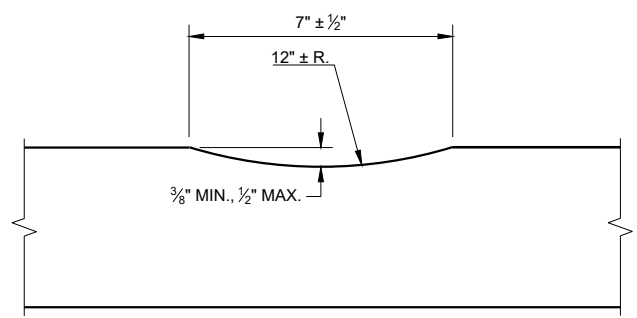
PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP



ISOMETRIC

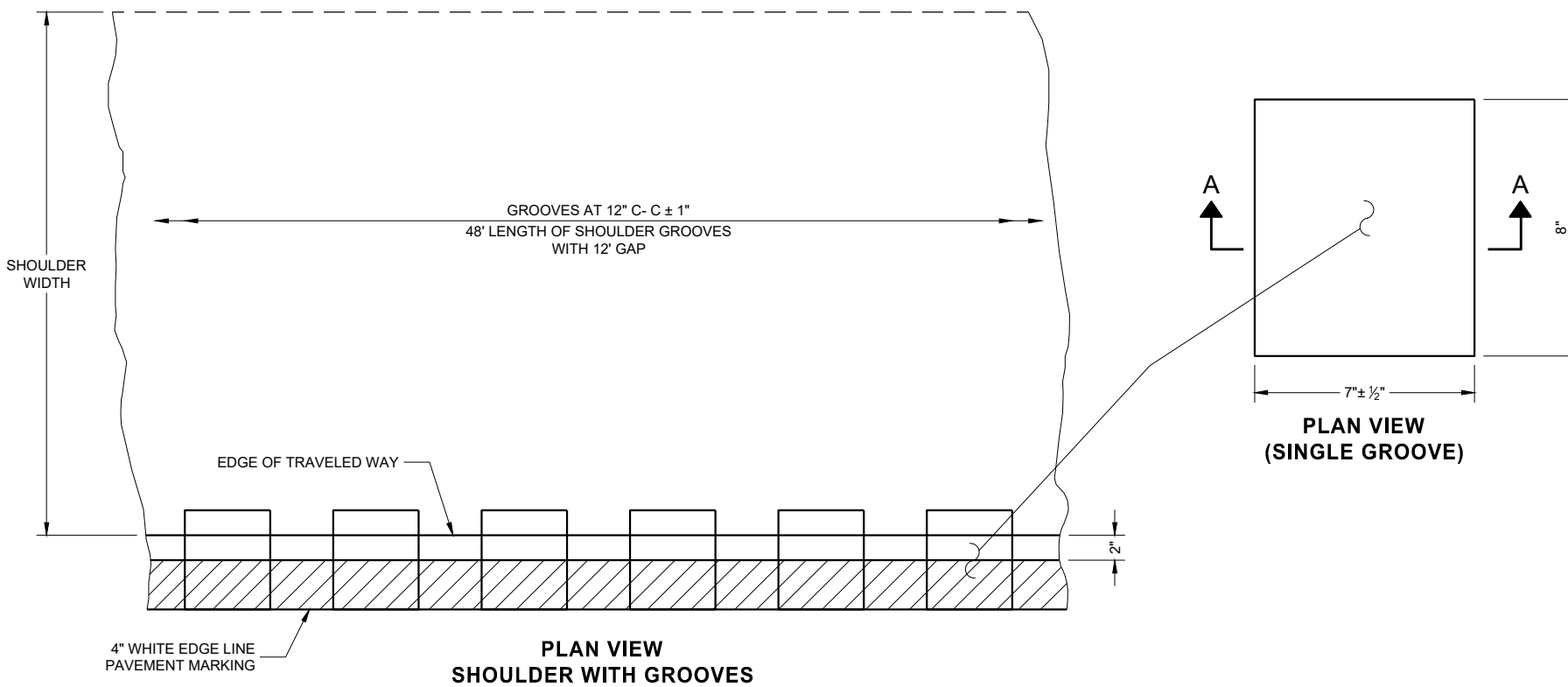


**TYPE 1
2 - LANE SHOULDER RUMBLE STRIP**



SECTION A - A

| |
|--|
| 2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION |



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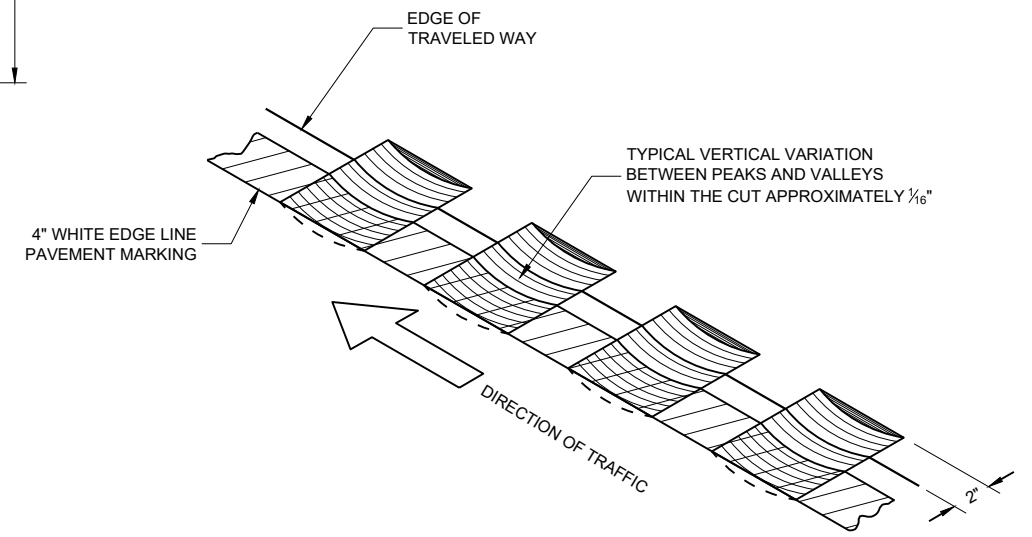
PLACEMENT DETAIL FOR TYPE 2 MILLED RUMBLE STRIP

GENERAL NOTES

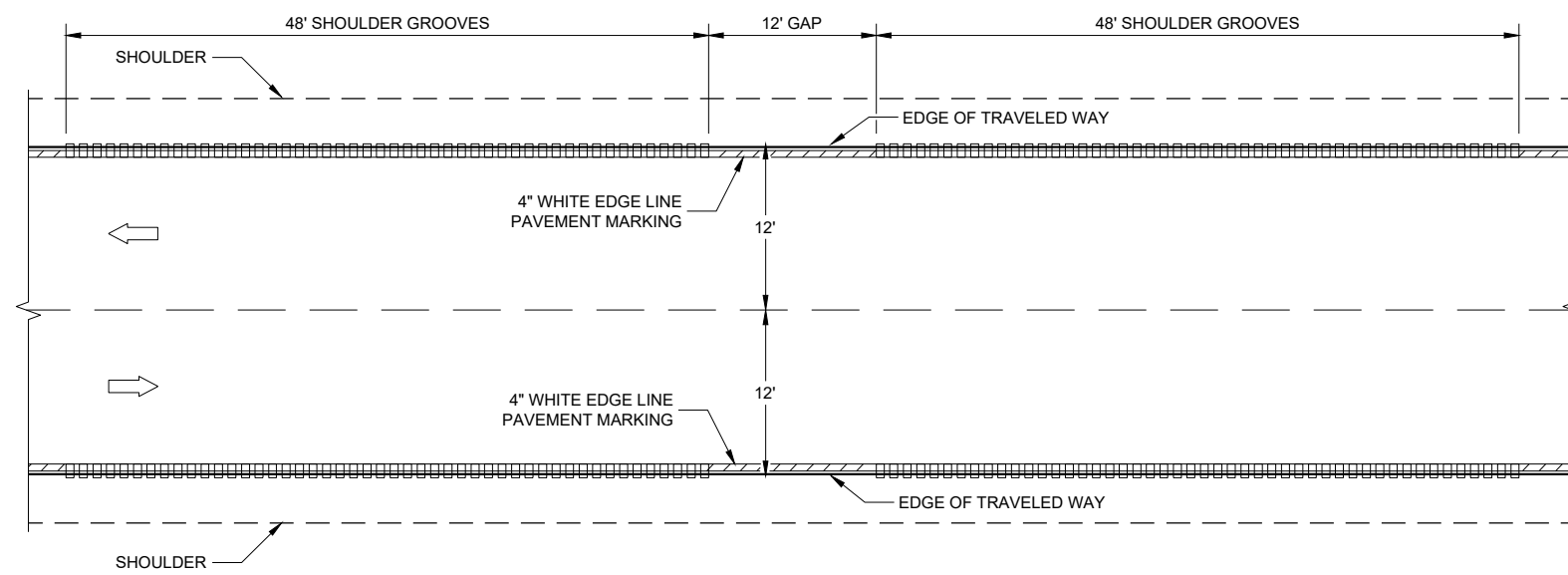
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL SHOULDER GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

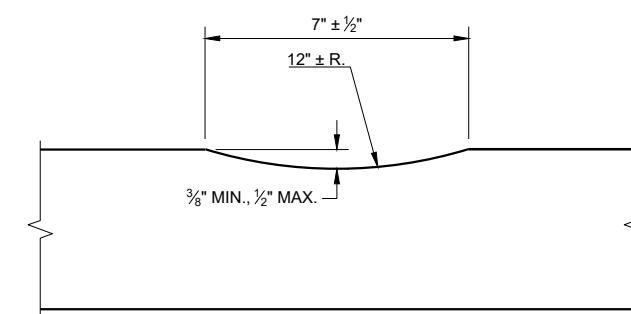
- ① SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



ISOMETRIC



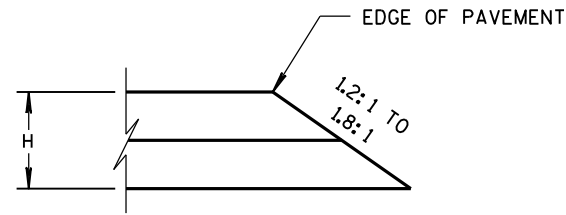
TYPE 2
2 - LANE SHOULDER RUMBLE STRIP



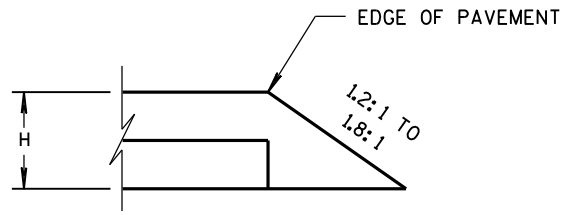
SECTION A - A

**2-LANE RURAL SHOULDER
RUMBLE STRIP, MILLING**

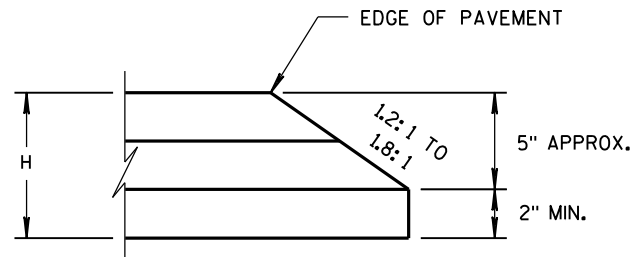
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



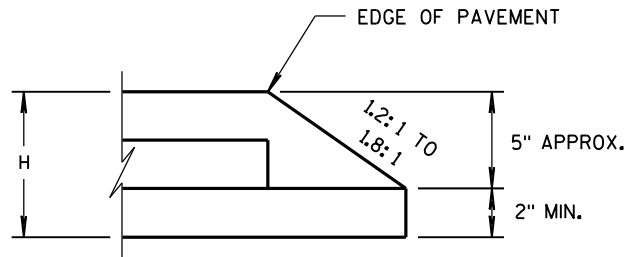
CONSTRUCTED WITH FINAL TWO LAYERS
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER
FOR H 5" OR LESS

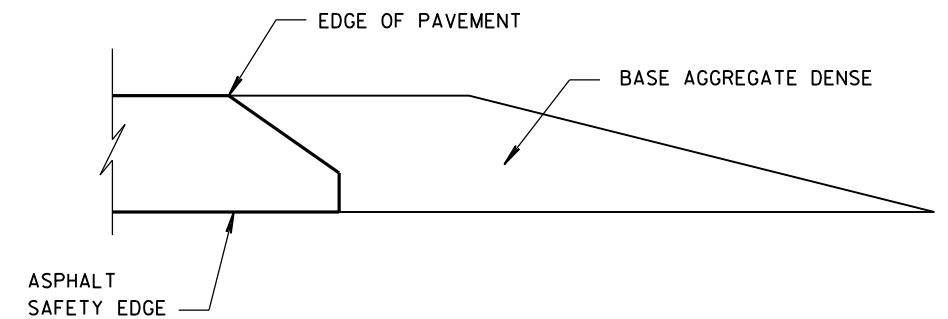


CONSTRUCTED WITH FINAL TWO LAYERS
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER
FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

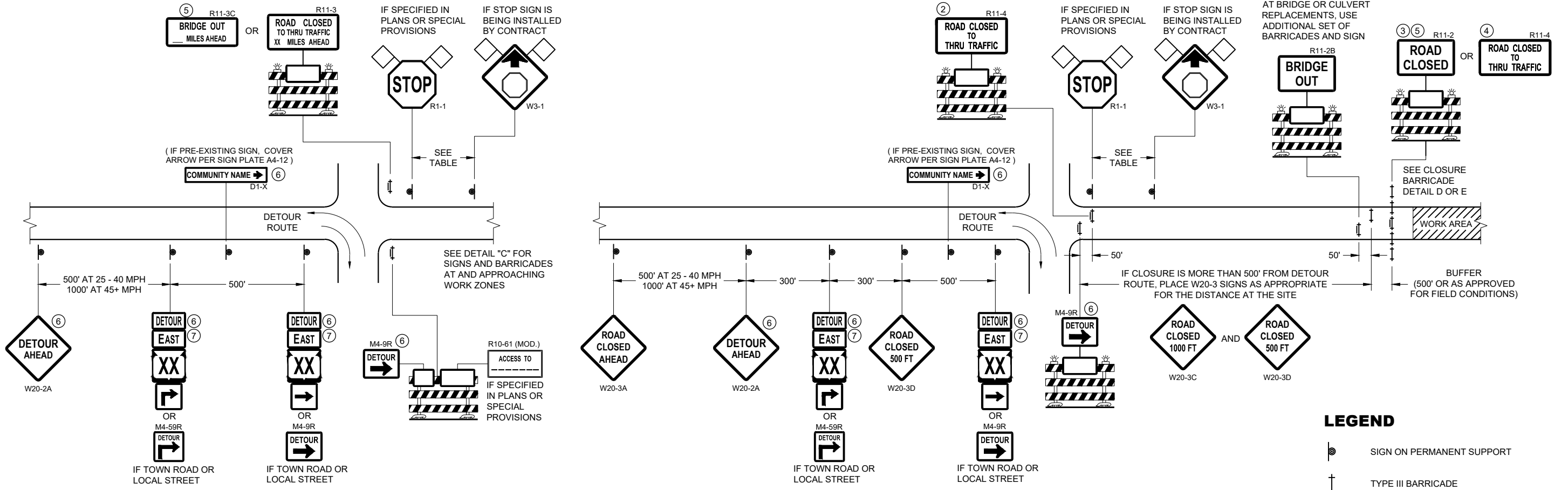
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S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

| | |
|--|--|
| SAFETY EDGE _{SM} | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED DATE | /s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER |
| FHWA | |



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

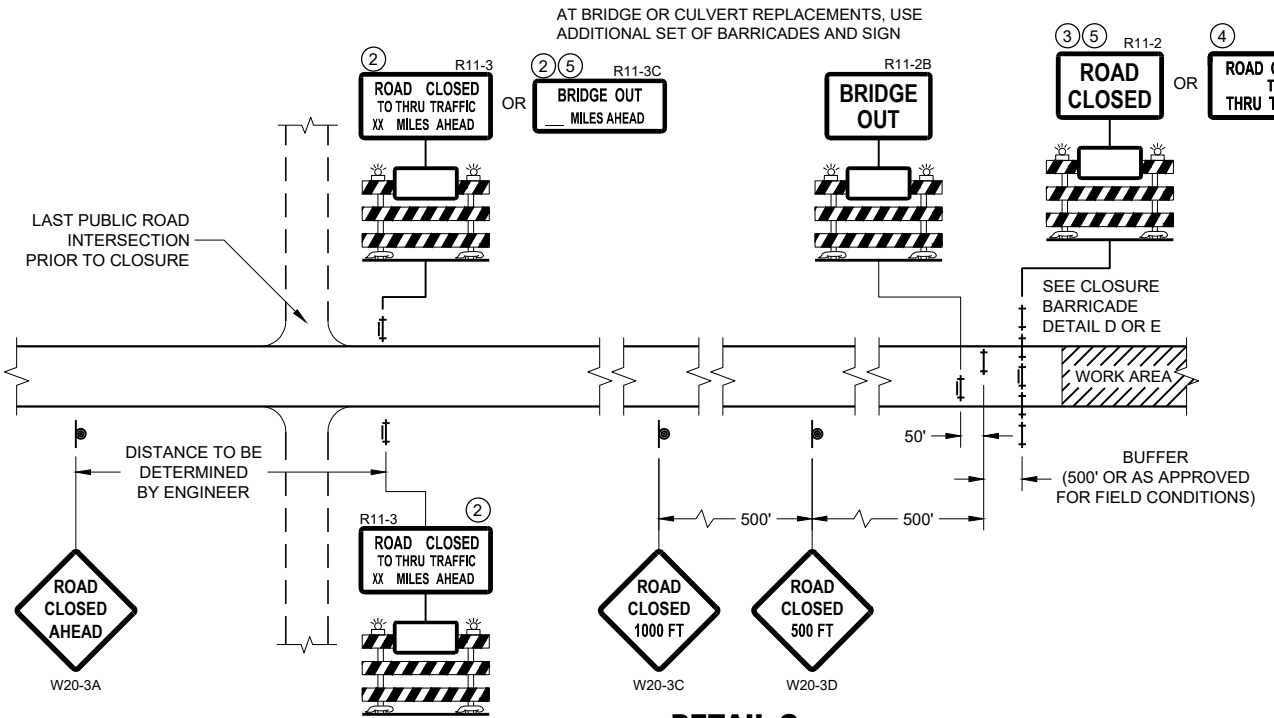
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

| SPEED LIMIT (MPH) | "STOP AHEAD" ADVANCE WARNING DISTANCE (FT) |
|-------------------|--|
| 25 | 200 |
| 30 | 200 |
| 35 | 350 |
| 40 | 350 |
| 45 | 500 |
| 50 | 550 |
| 55 | 750 |

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



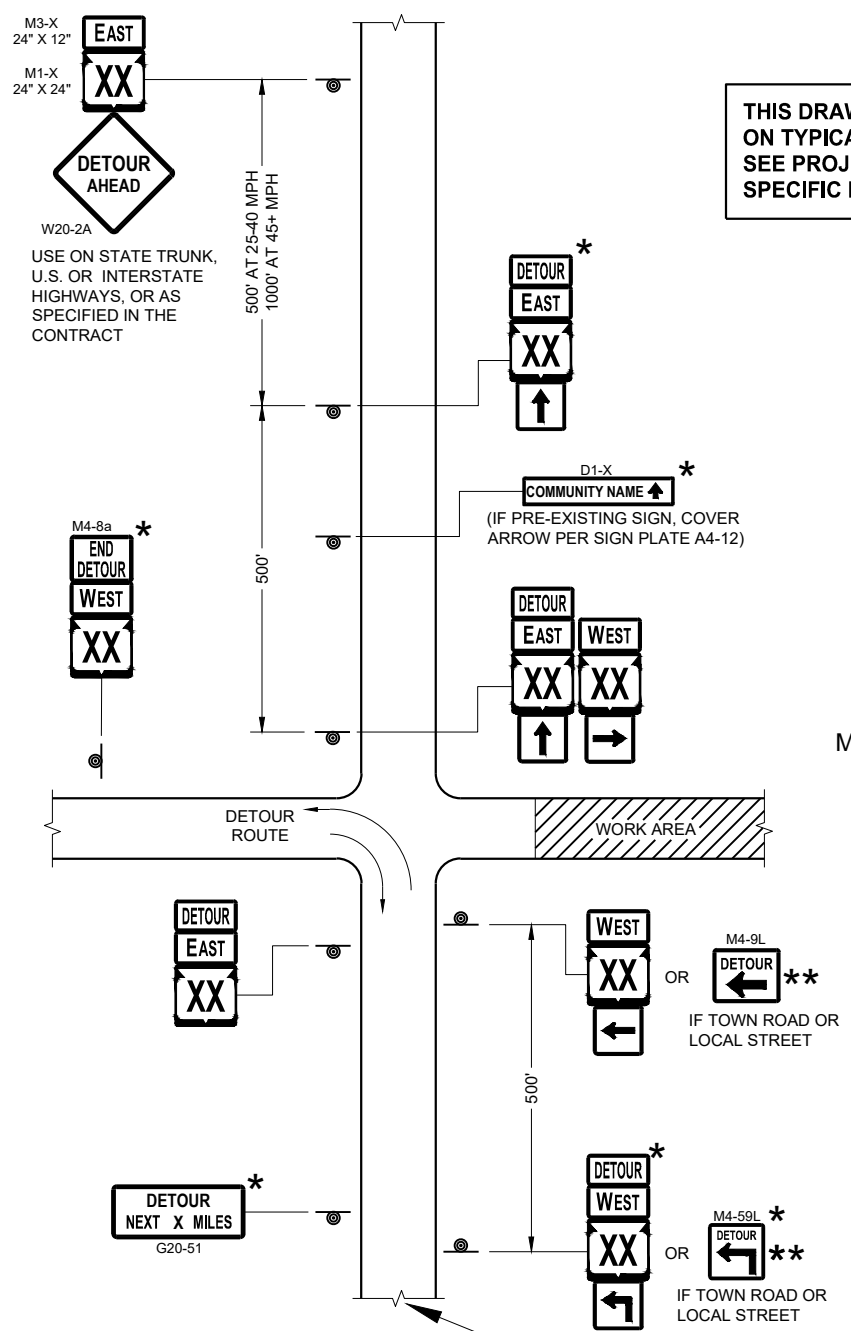
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

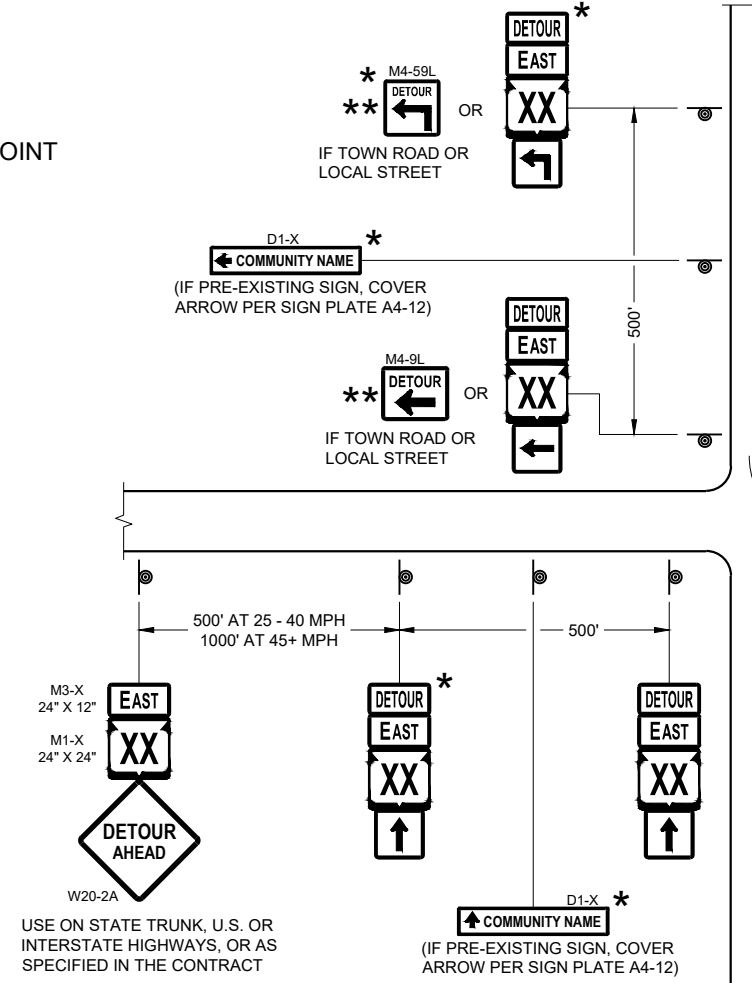
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

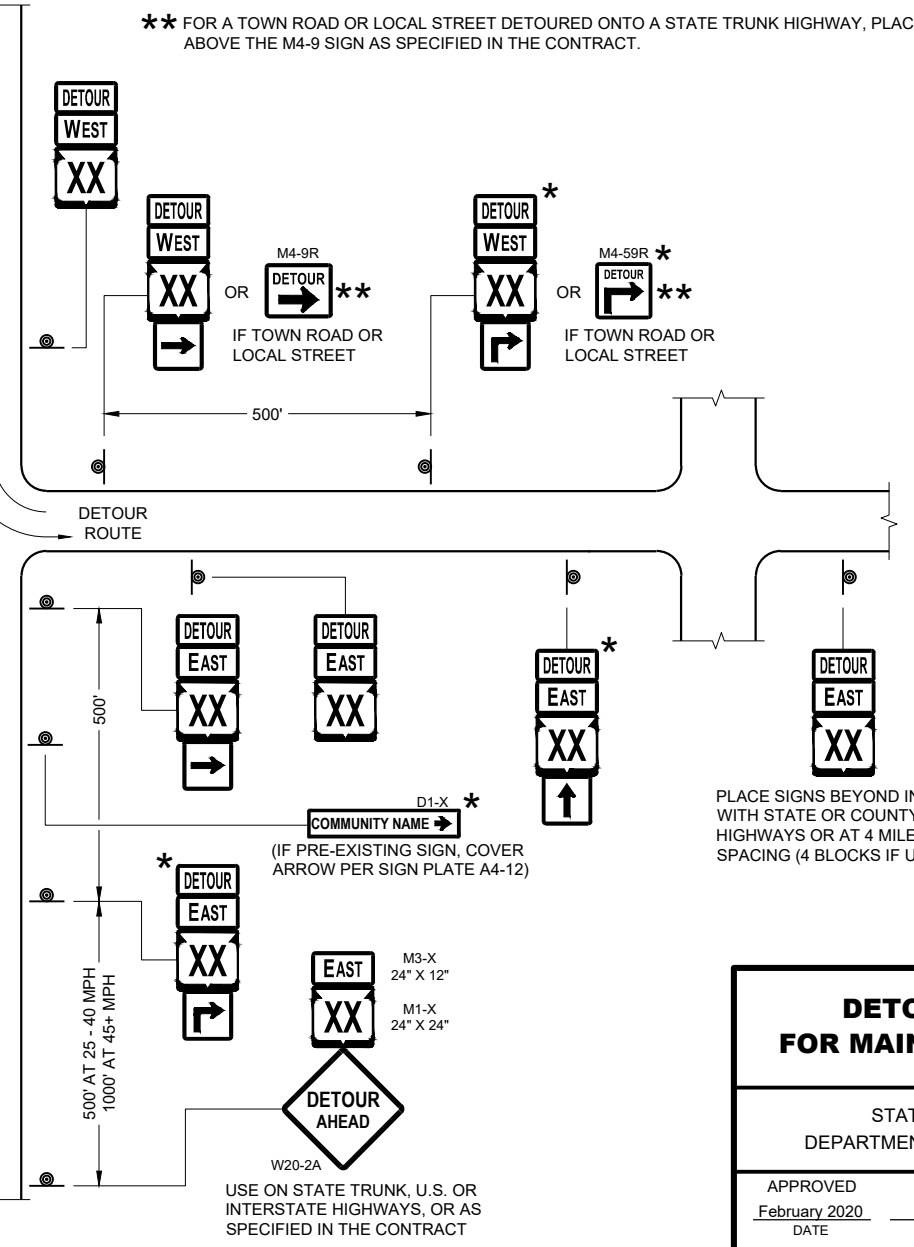
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



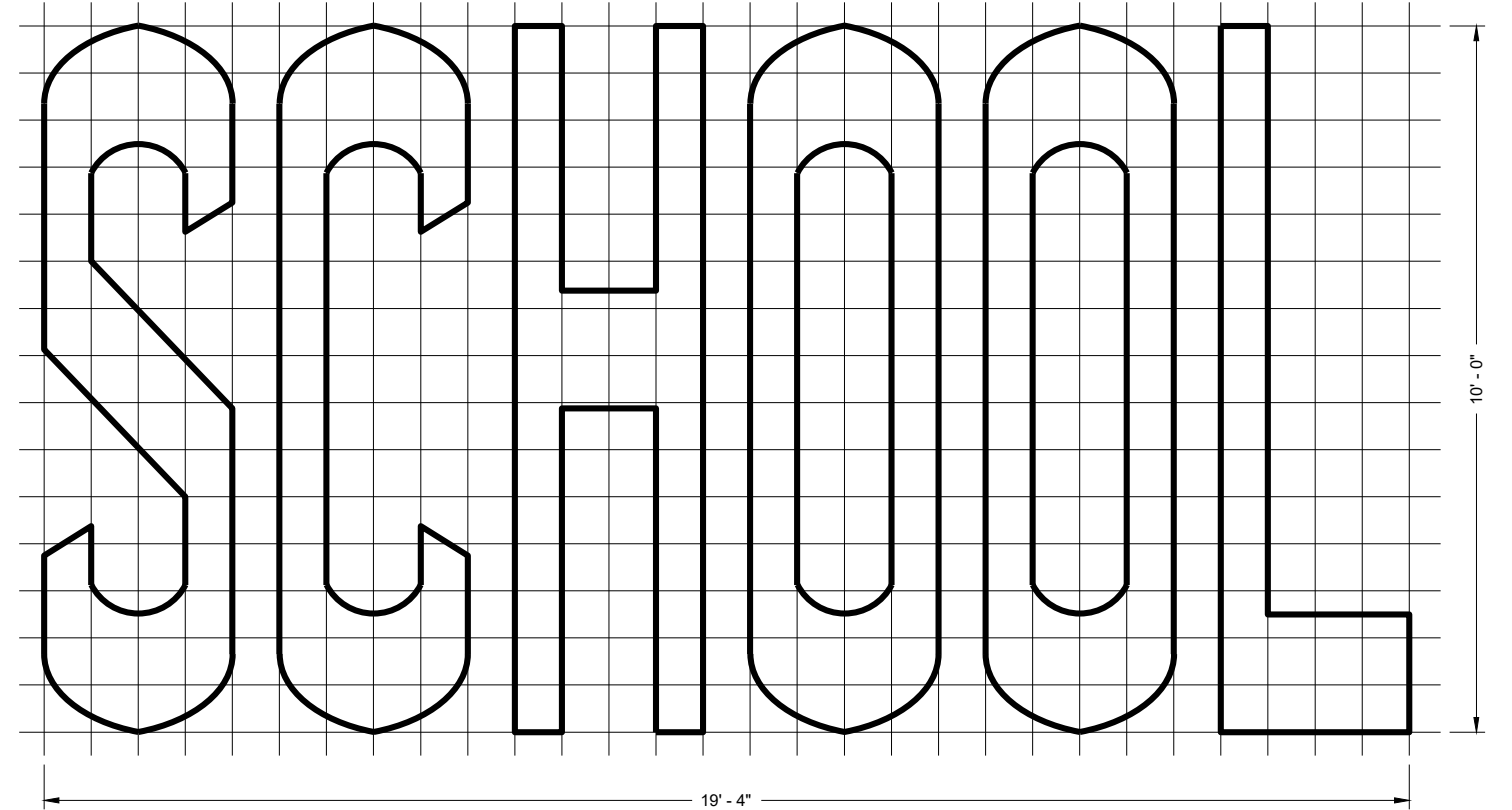
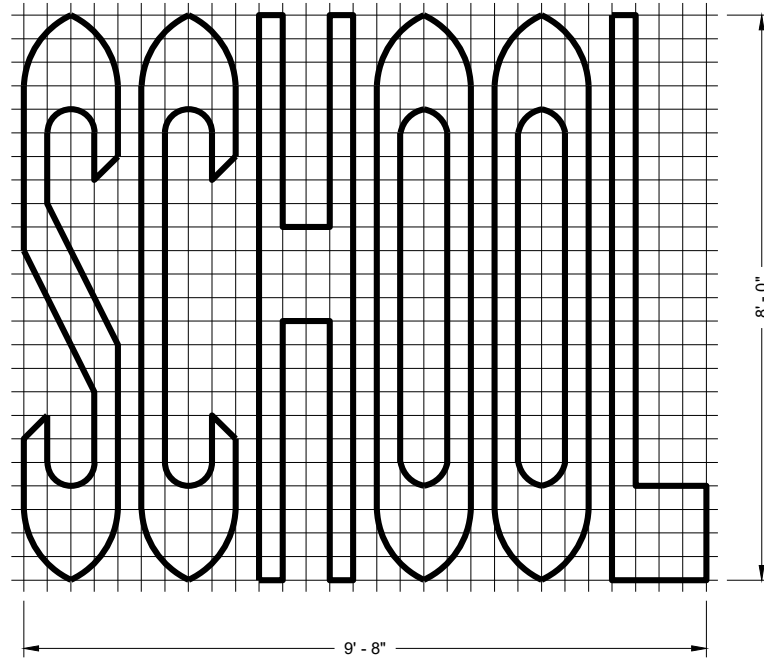
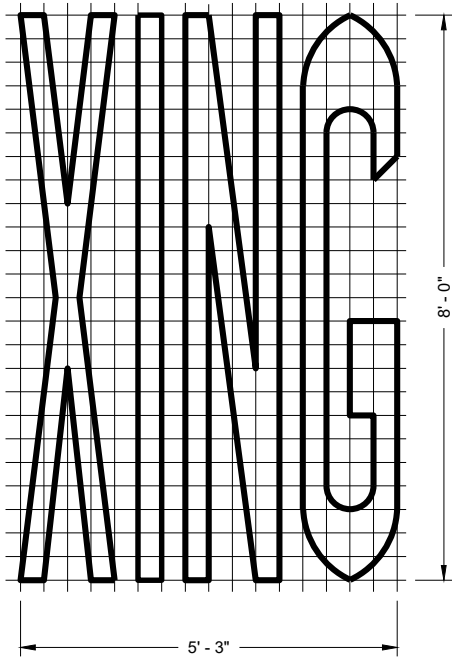
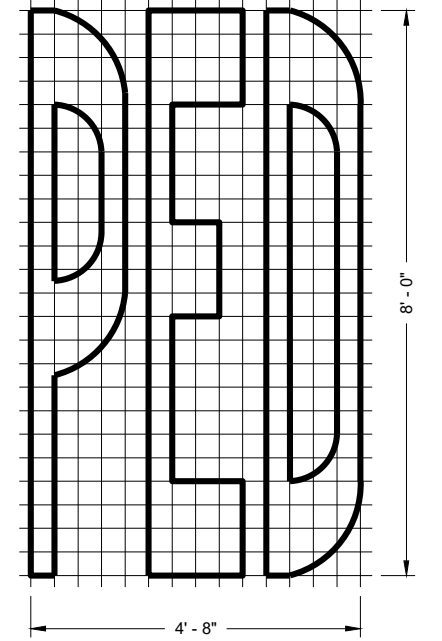
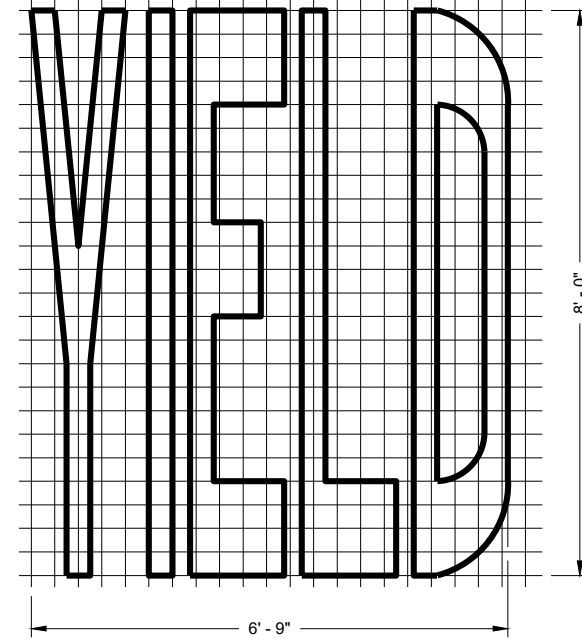
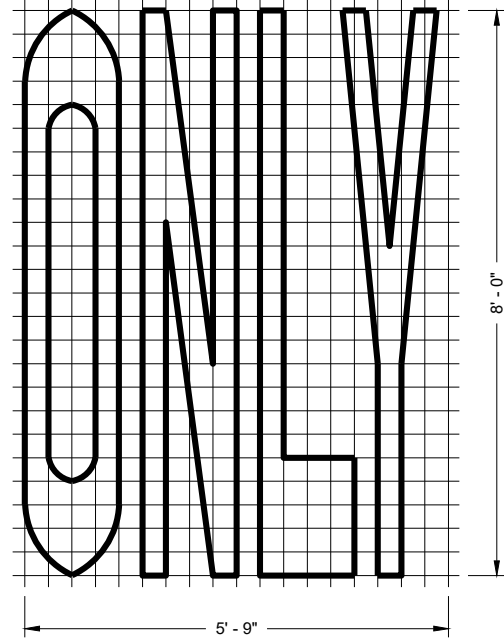
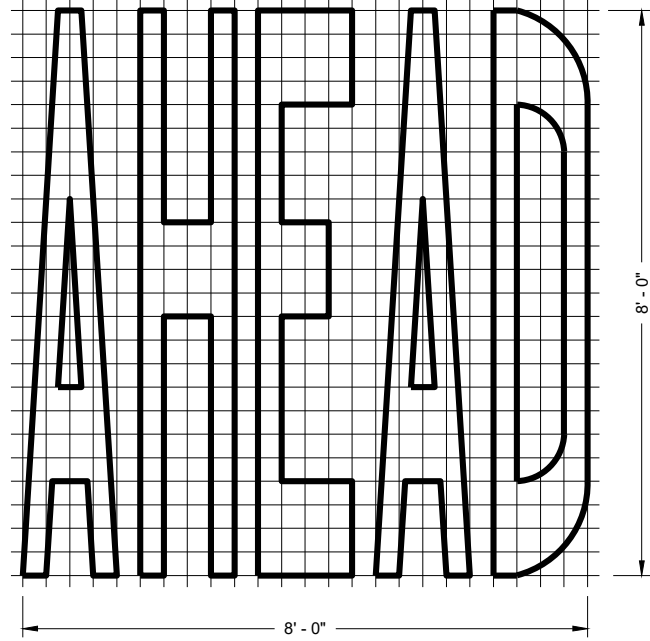
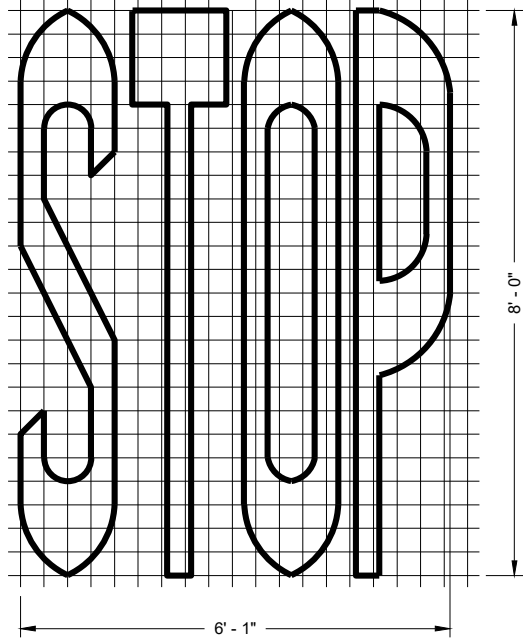
**DETAIL F
DETOUR SIGNING**



PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

| DETOUR SIGNING FOR MAINLINE CLOSURES | |
|--|--|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED February 2020 DATE | /S/ Andrew Heidtke WORK ZONE ENGINEER |
| FHWA | |



SINGLE LANE

TWO - LANE

GENERAL NOTES

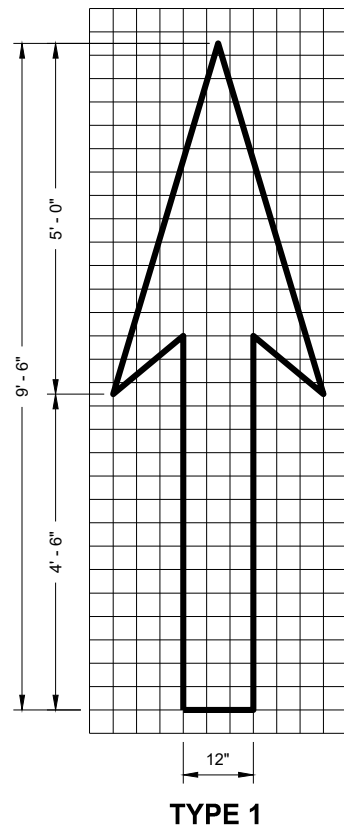
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING WORDS

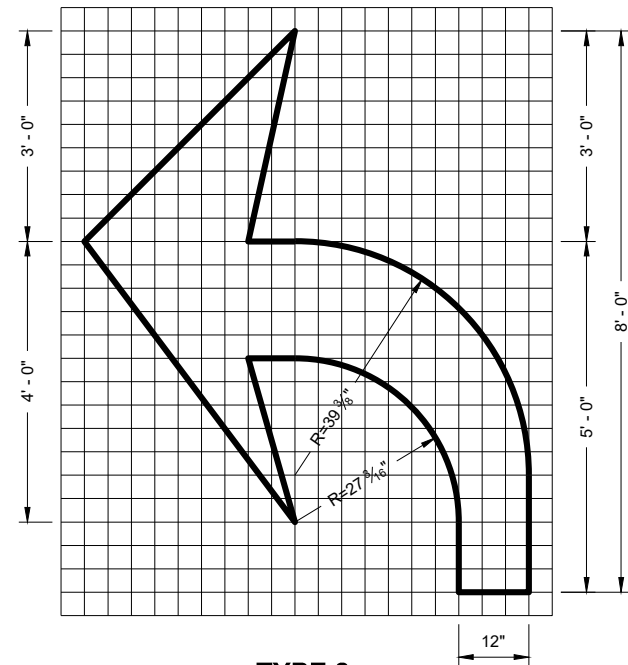
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

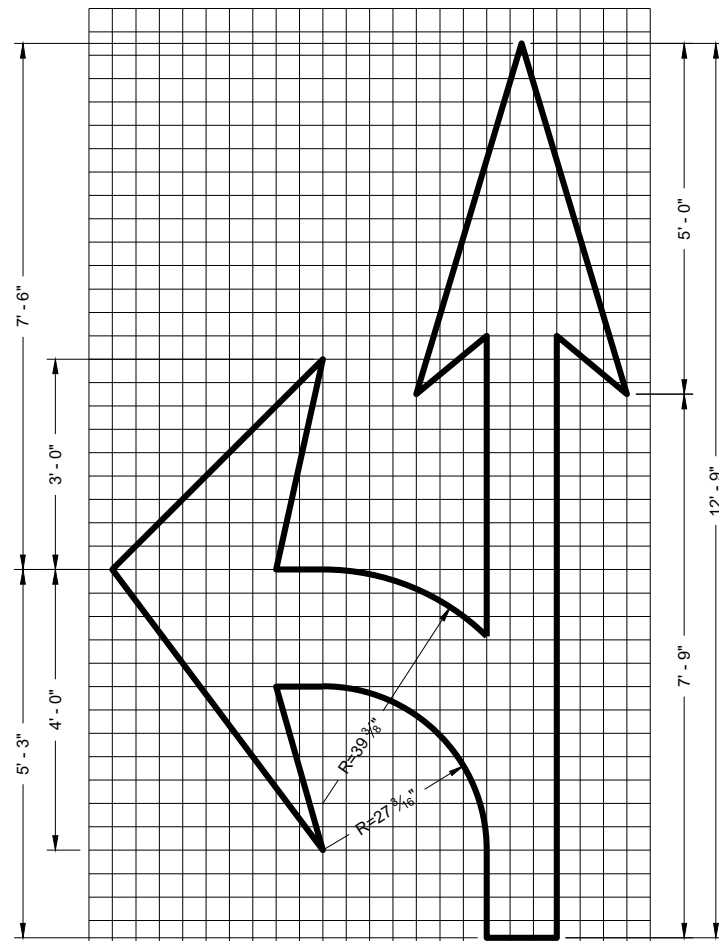
FHWA



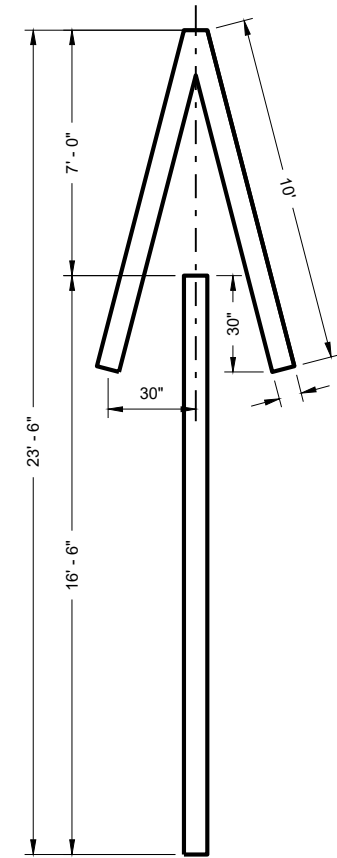
TYPE 1



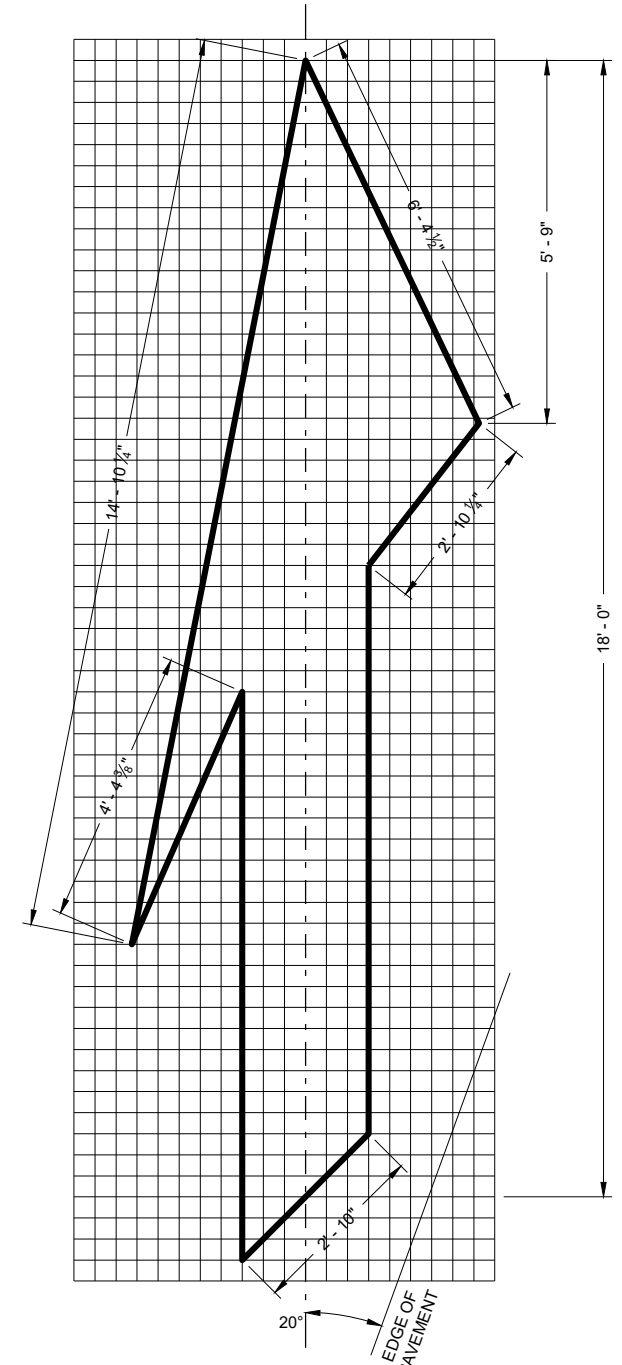
TYPE 2



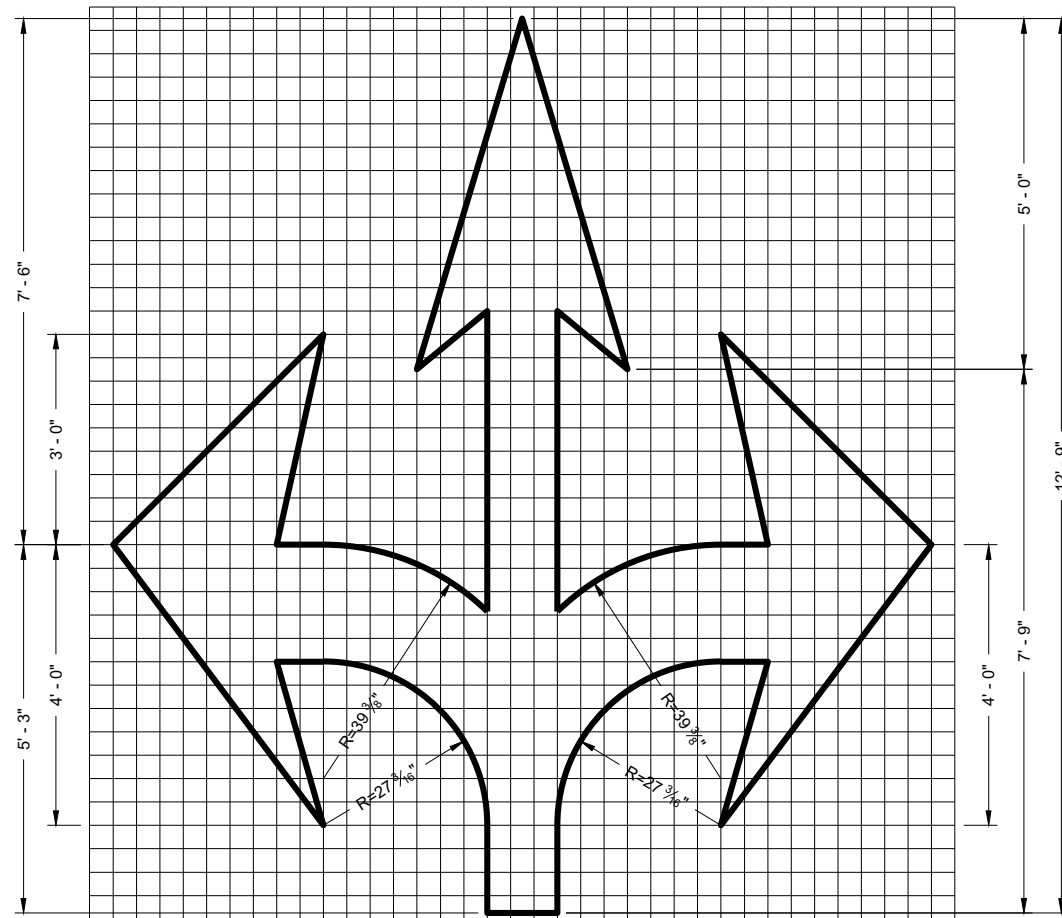
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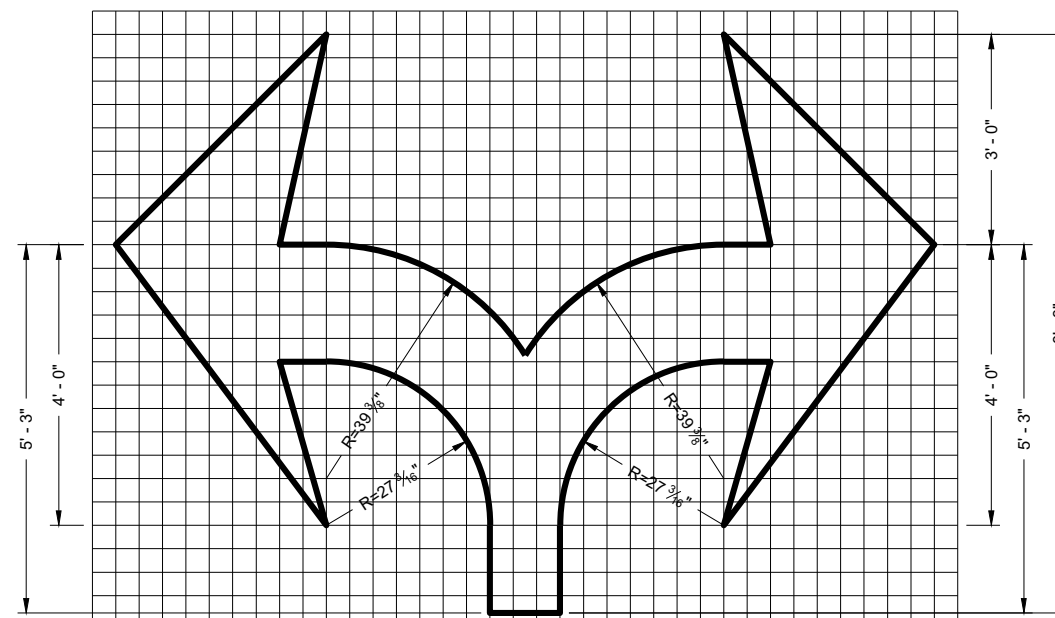
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

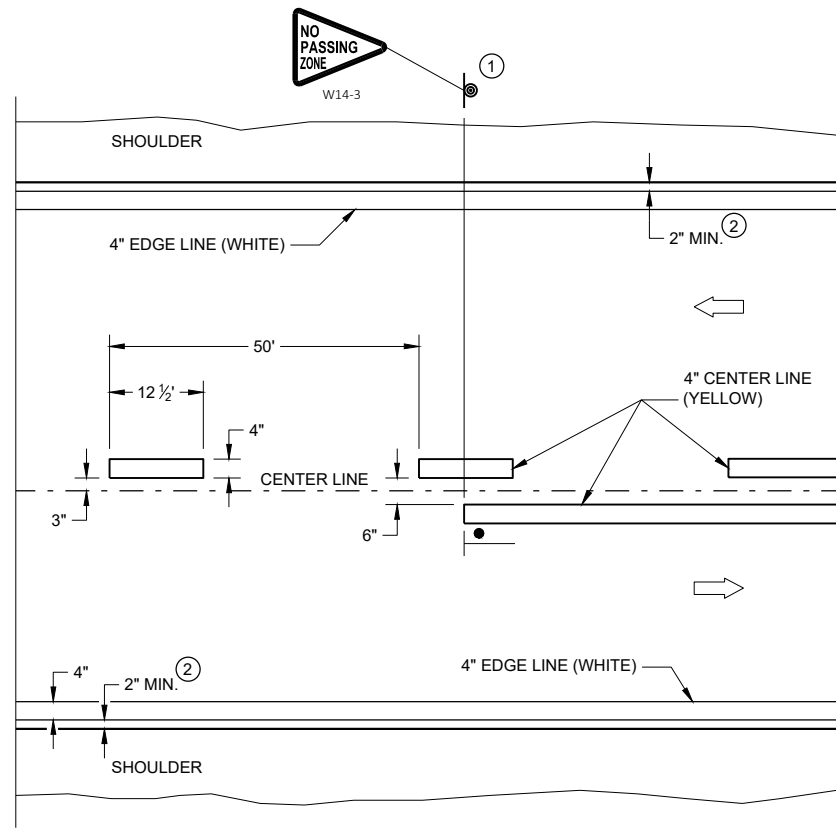
PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

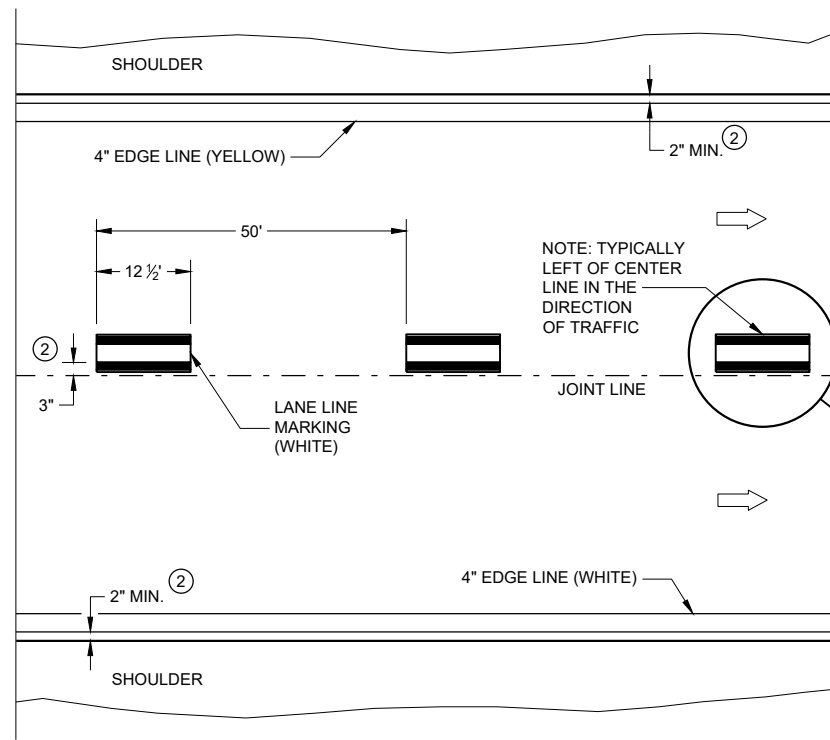
APPROVED
November 2019
DATE

/s/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

FHWA

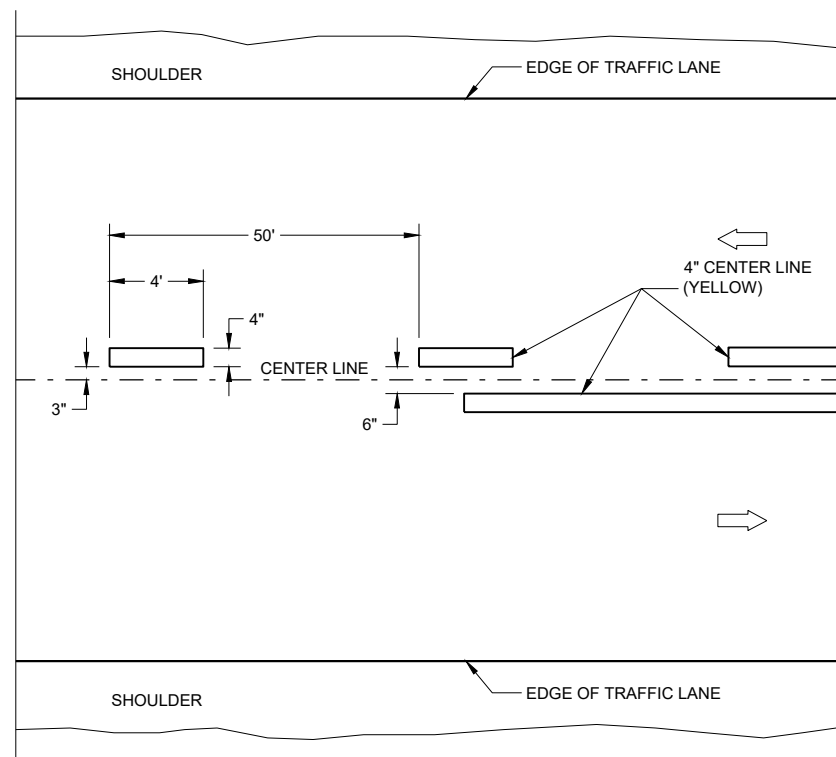


TWO WAY TRAFFIC

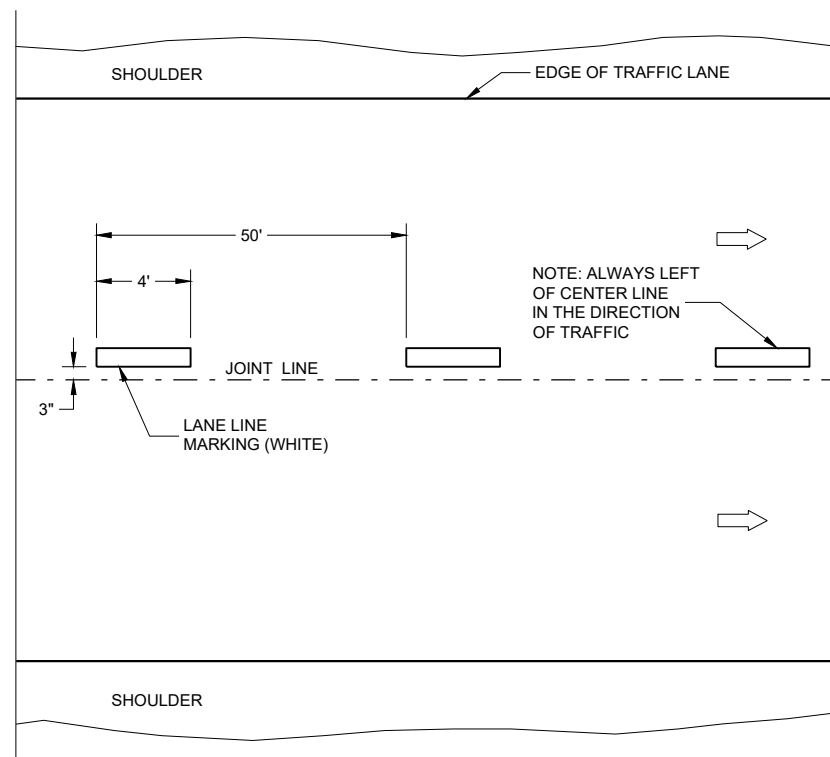


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

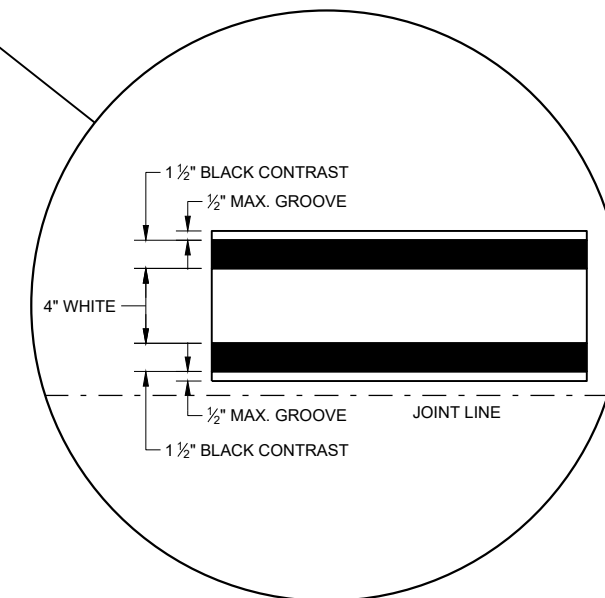
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

- |• "T" MARKING
- ⊙ SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC



**LONGITUDINAL MARKING
(MAINLINE)**

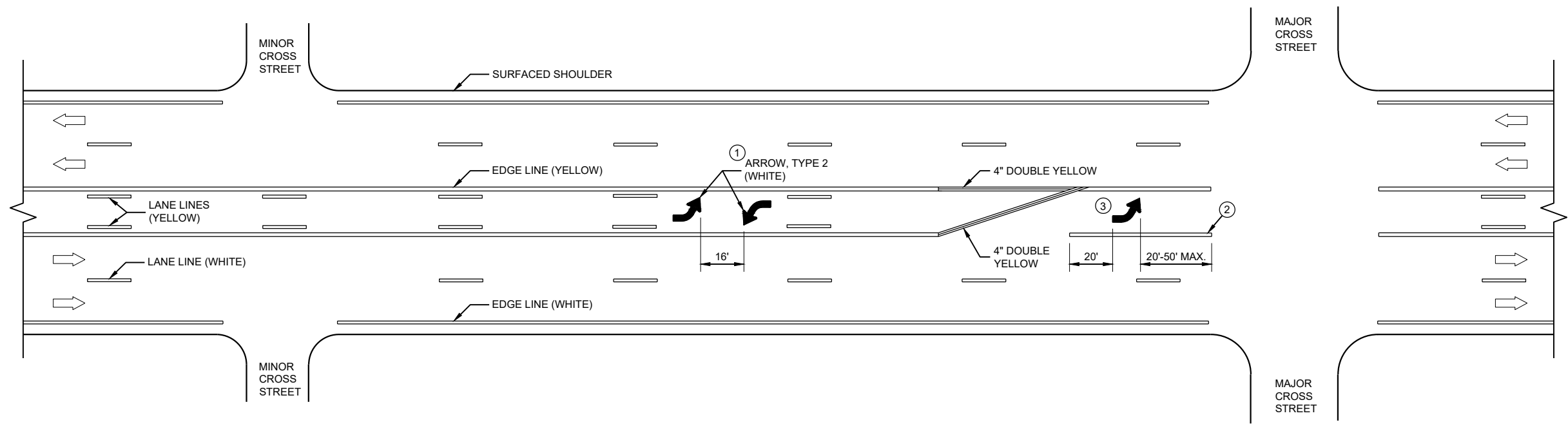
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Matthew Rauch
DATE STATEWIDE SIGNING AND MARKING
ENGINEER

GENERAL NOTES

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 8" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC



TWO WAY LEFT TURN LANE

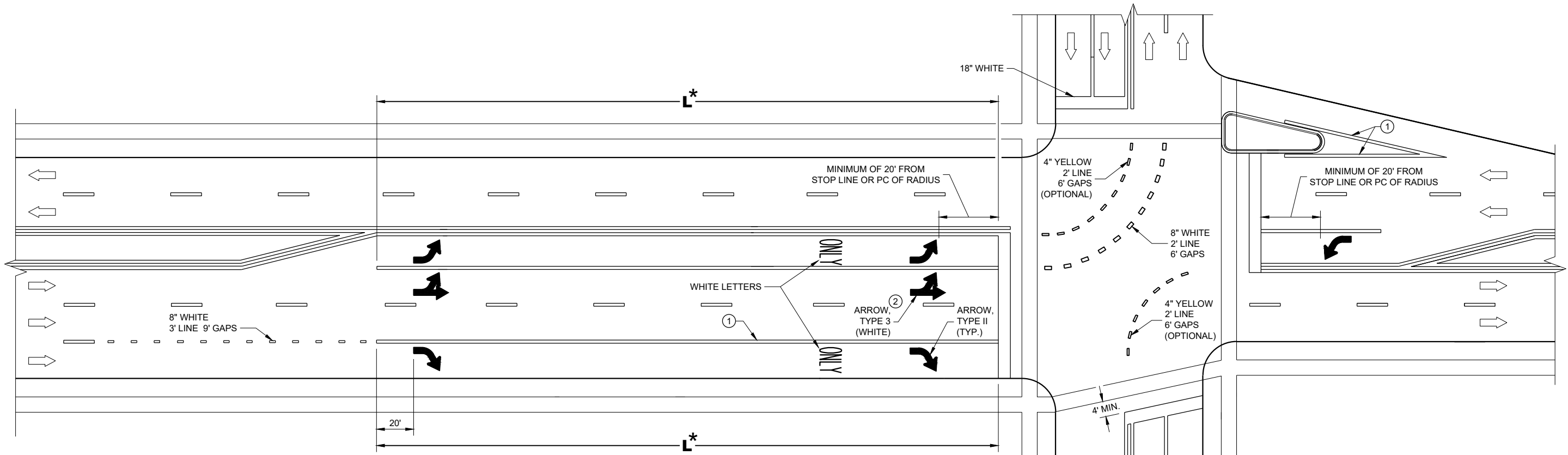
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SDD 15C08 - 20b

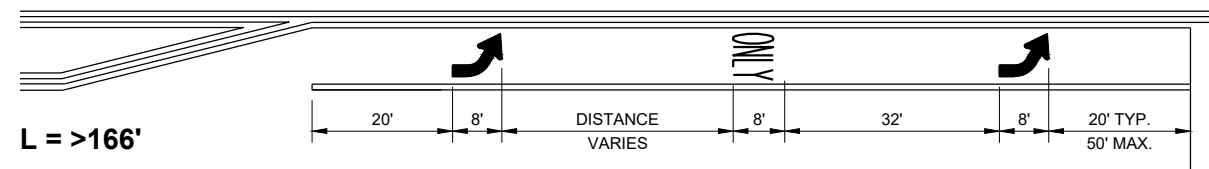
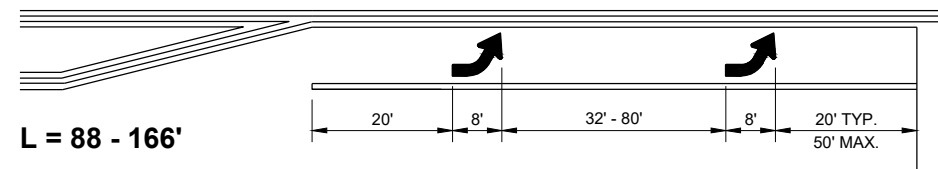
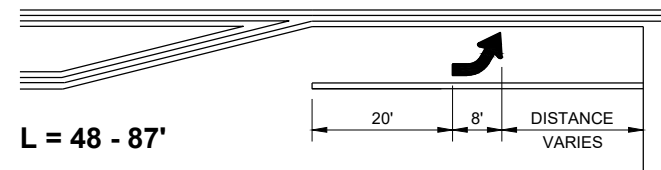
SDD 15C08 - 20b

| |
|--|
| PAVEMENT MARKING (TURN LANES) |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION |



TURN LANE OPTIONS

LENGTH OF TURN BAY (L) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

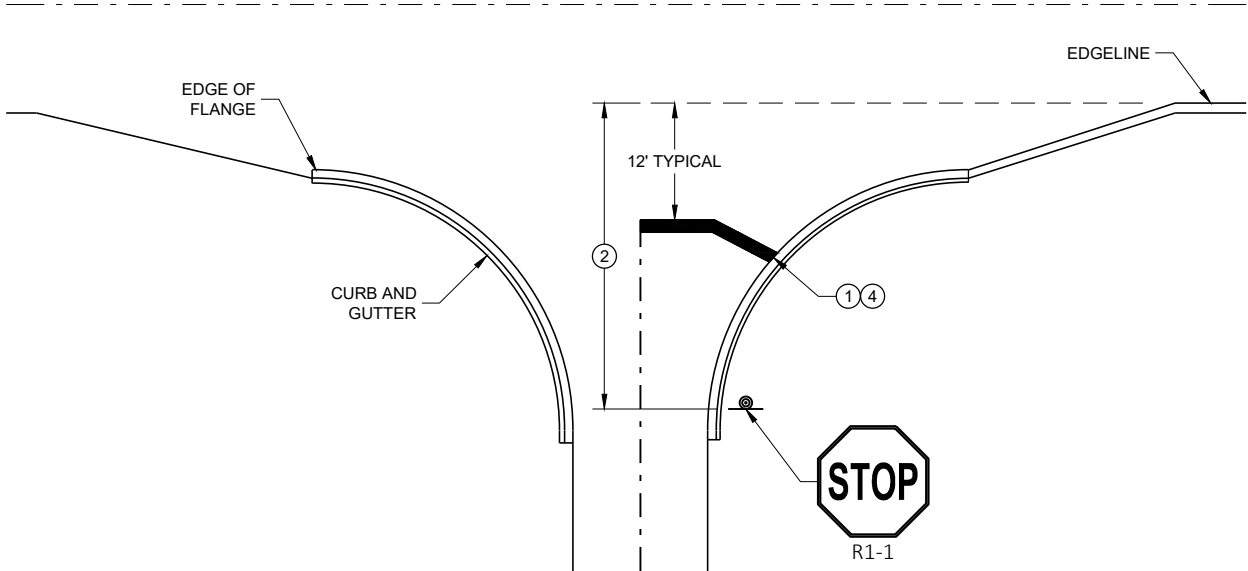
**PAVEMENT MARKING
(TURN LANES)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

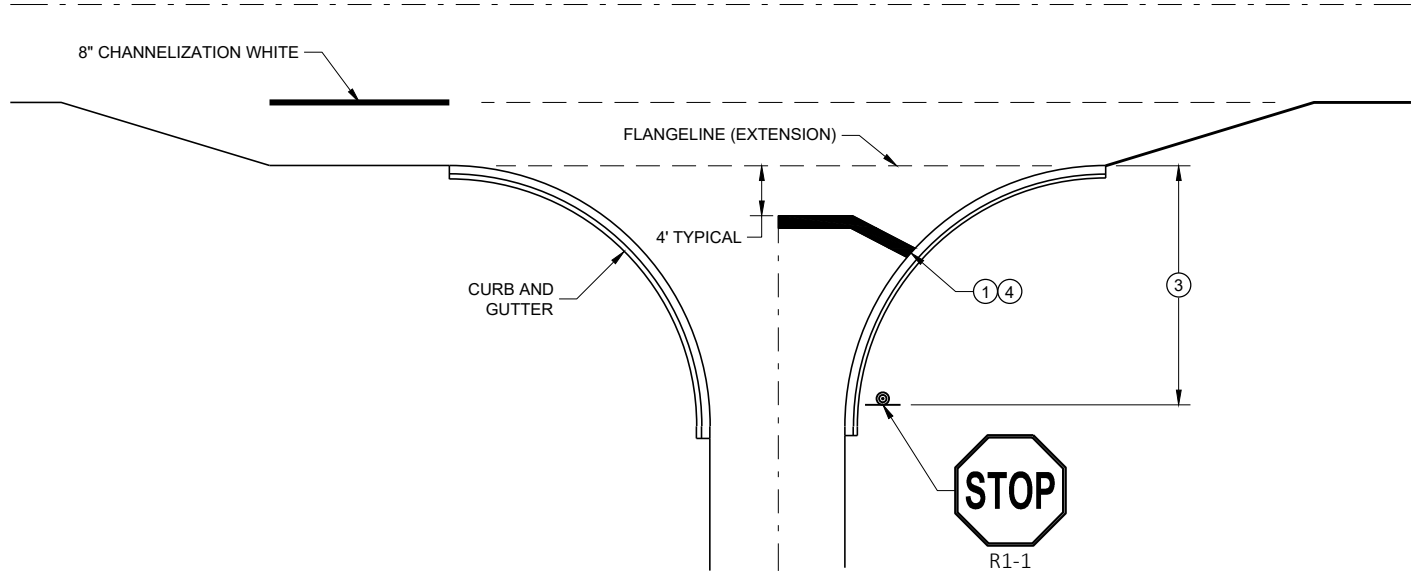
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

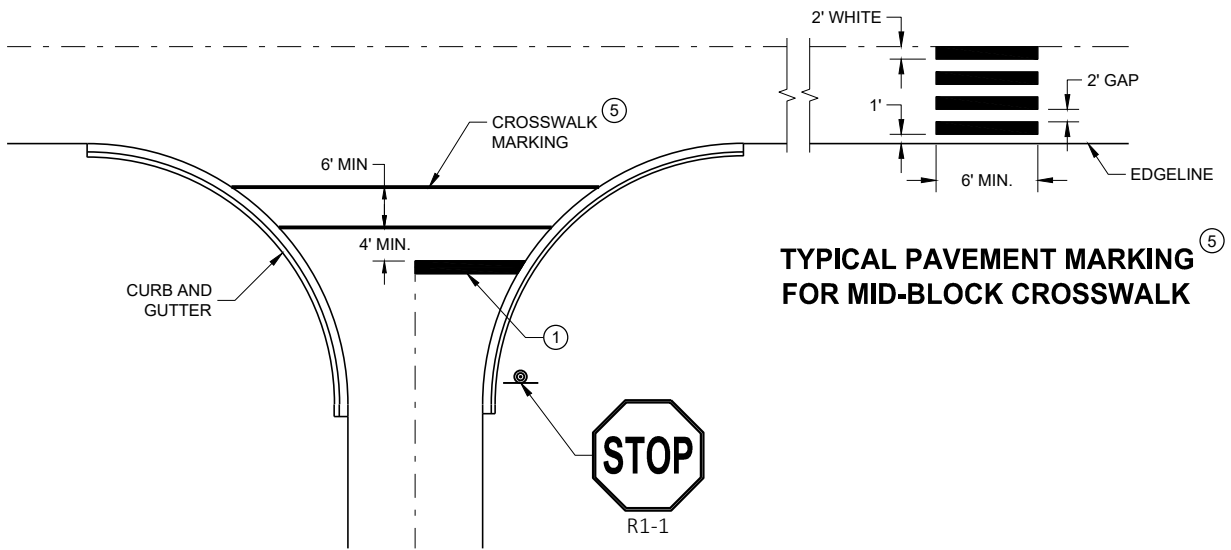
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

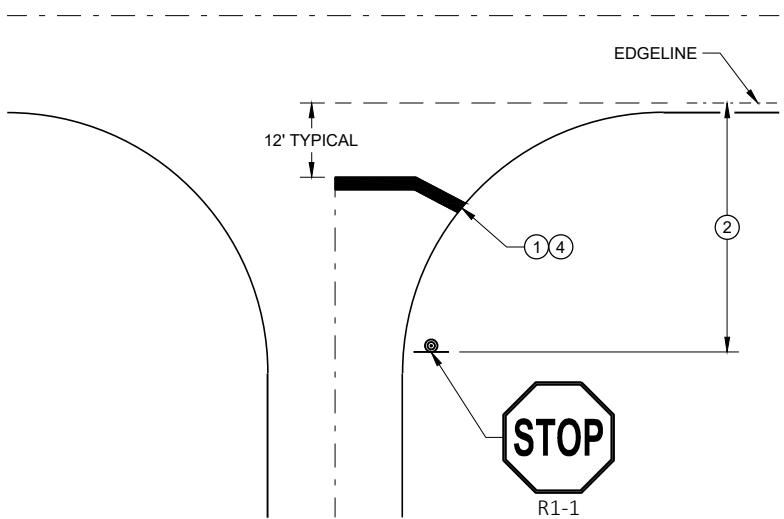


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

| STOP LINE AND CROSSWALK PAVEMENT MARKING | |
|--|--|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED November 2019 DATE | /S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER |
| FHWA | |

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SDD 15C33 - 04

SDD 15C33 - 04

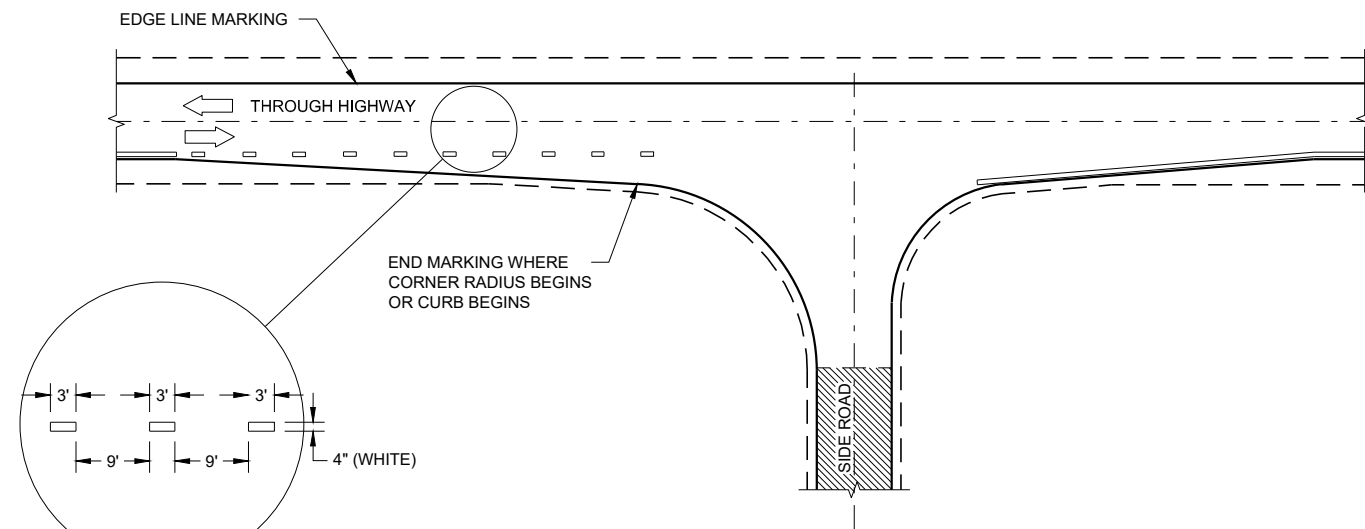
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

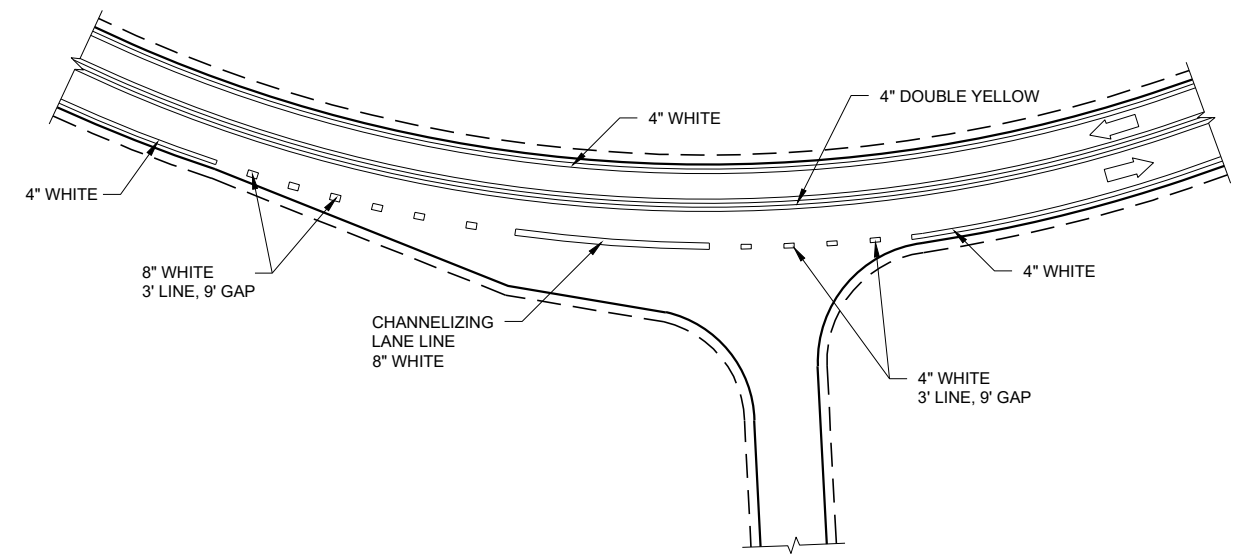
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

LEGEND

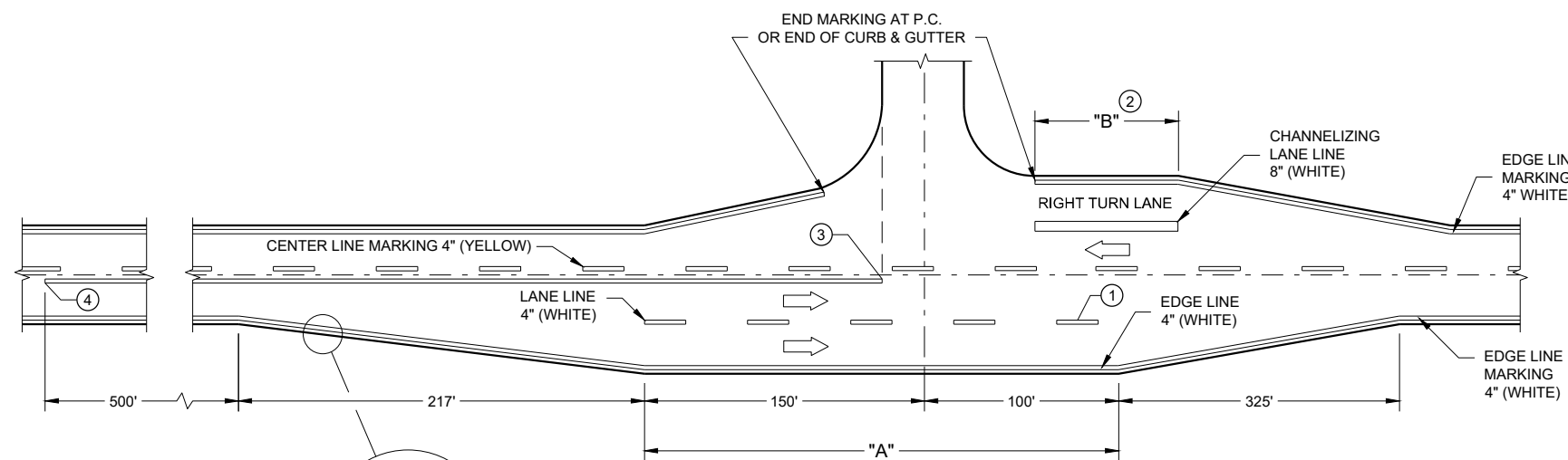
➡ DIRECTION OF TRAVEL



MINOR INTERSECTION



INTERSECTION ON OUTSIDE OF CURVE

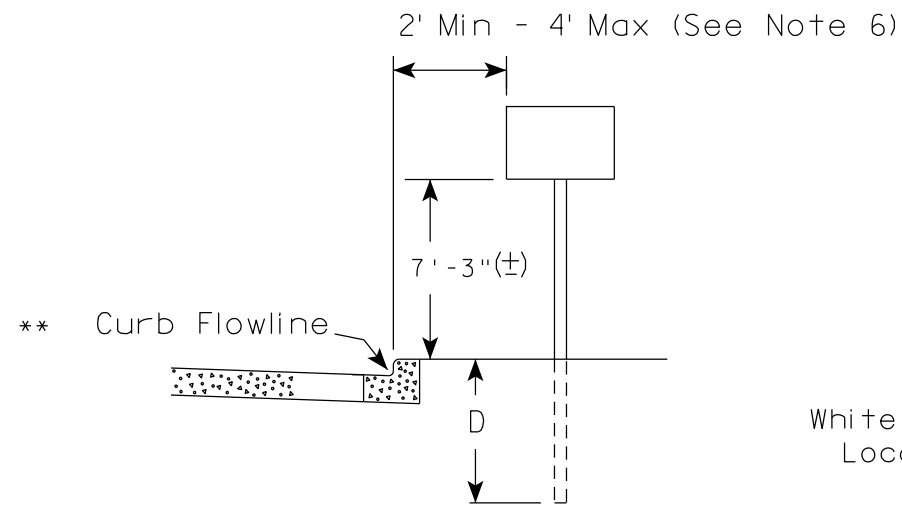


**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

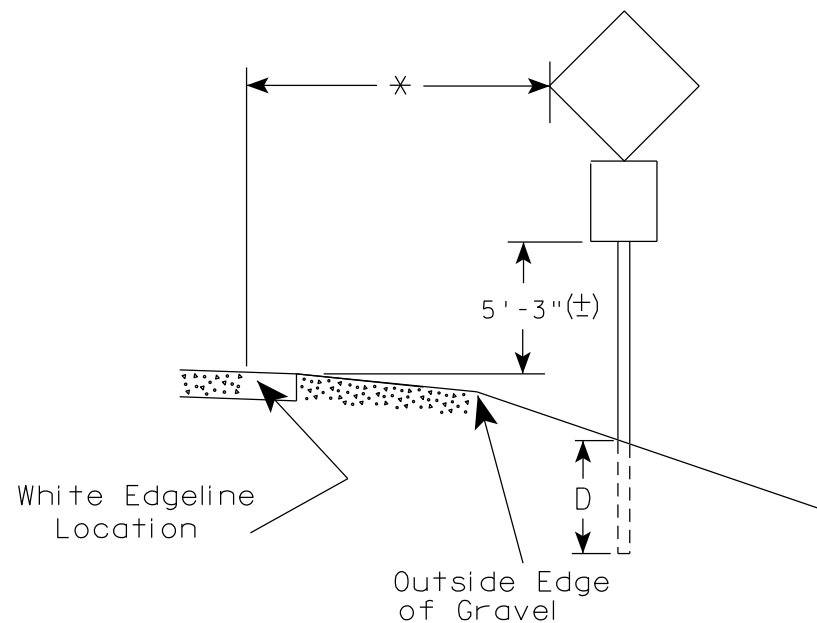
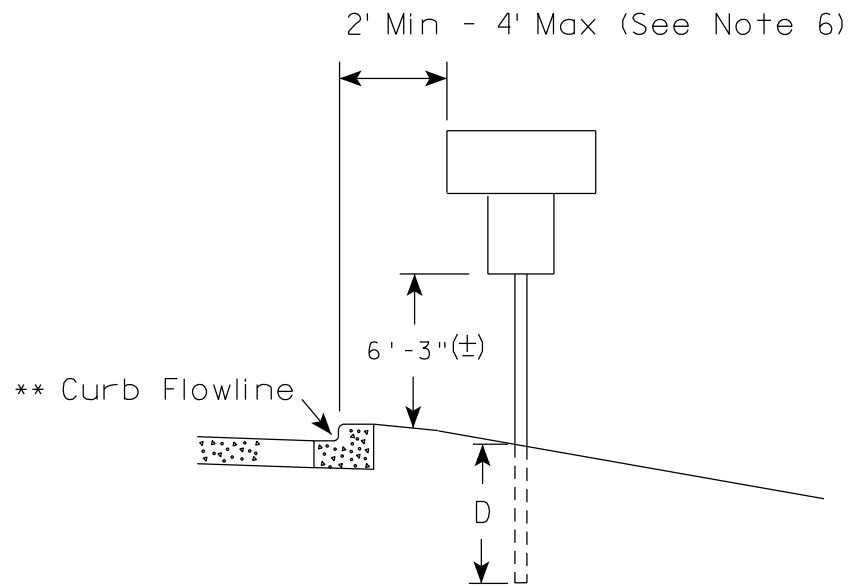
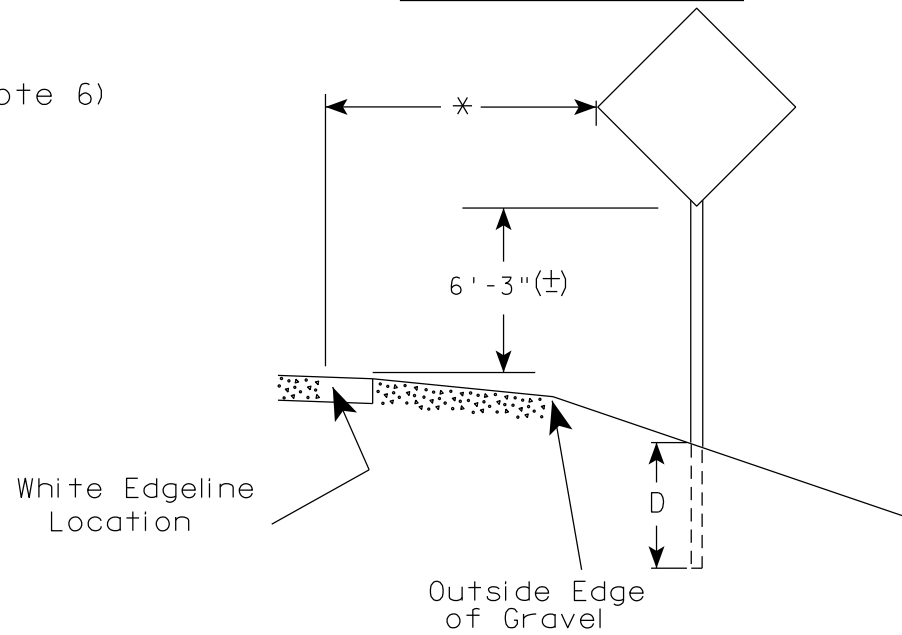
**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|---------------------------------------|-----------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

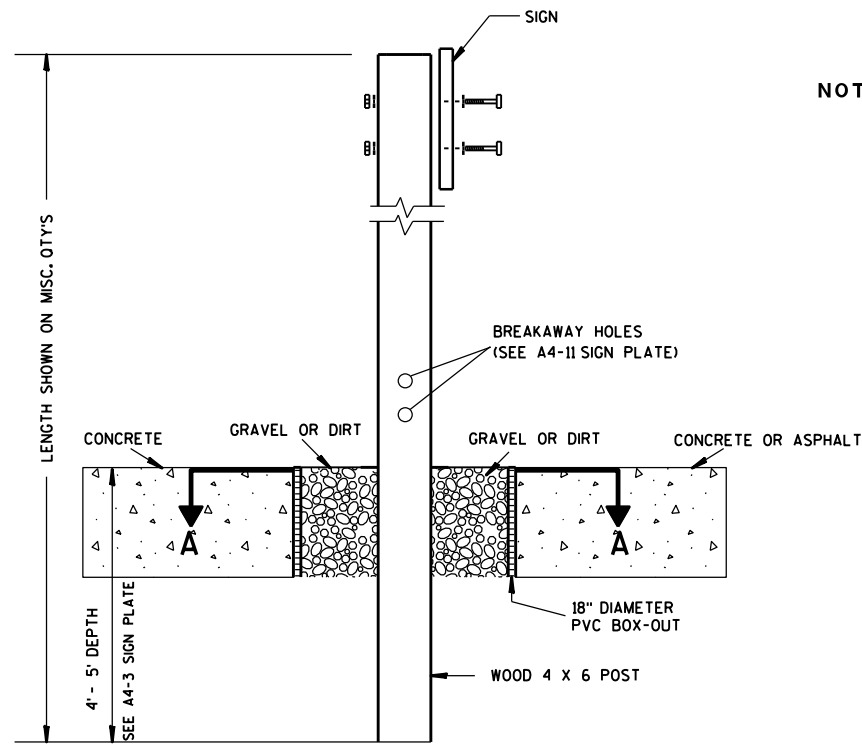
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

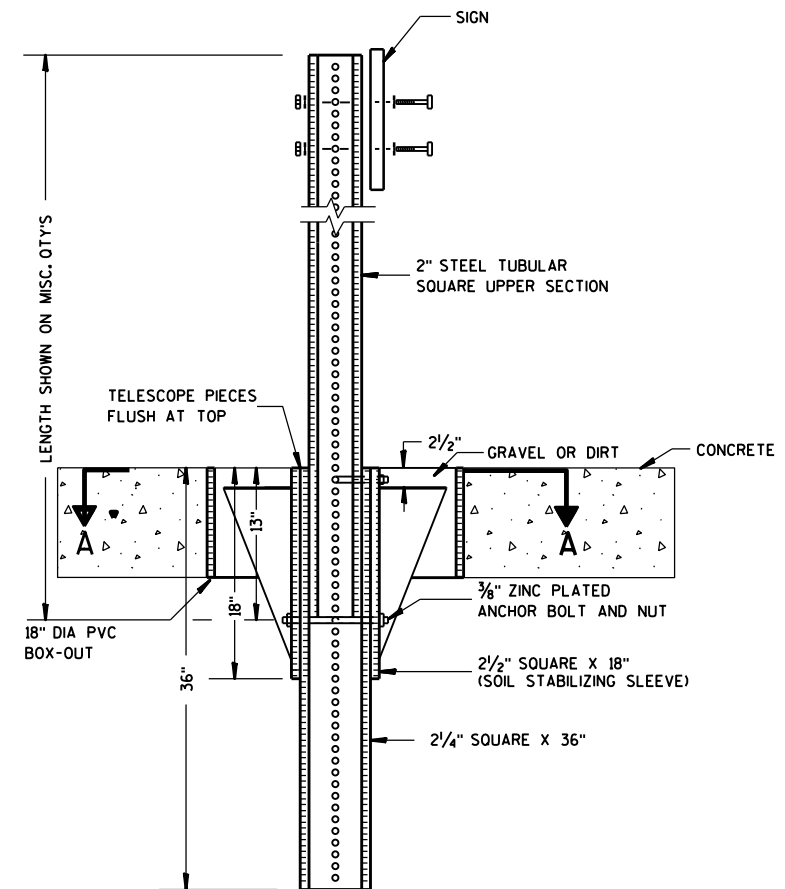
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

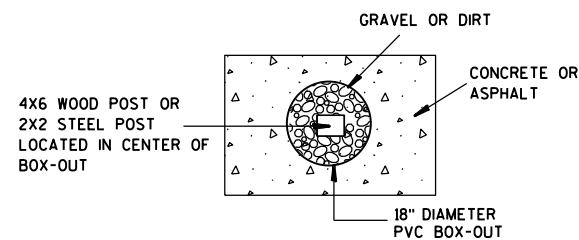
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

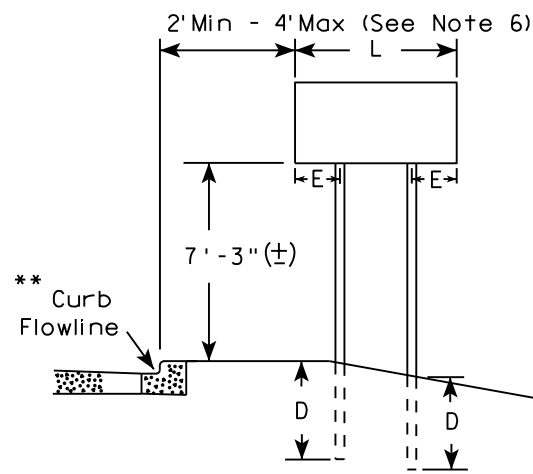
FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

| | |
|---|----------------------------------|
| SIGN POST BOX-OUTS A4-3B | |
| <small>WISCONSIN DEPT OF TRANSPORTATION</small> | |
| APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small> | |
| <small>DATE 1/27/14</small> | <small>PLATE NO. A4-3B.1</small> |

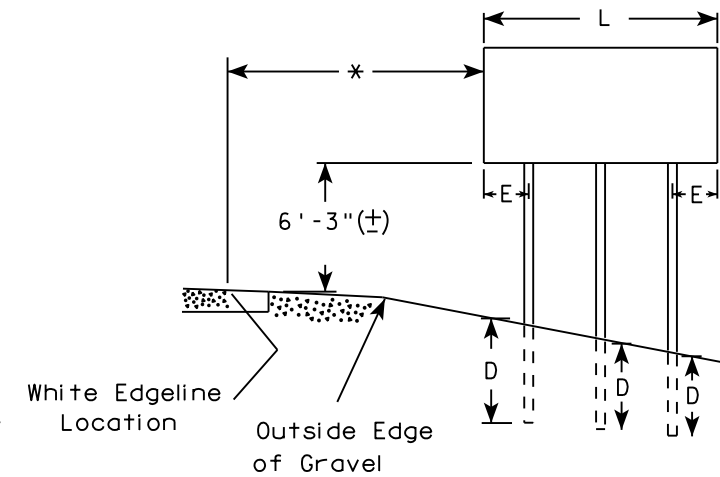
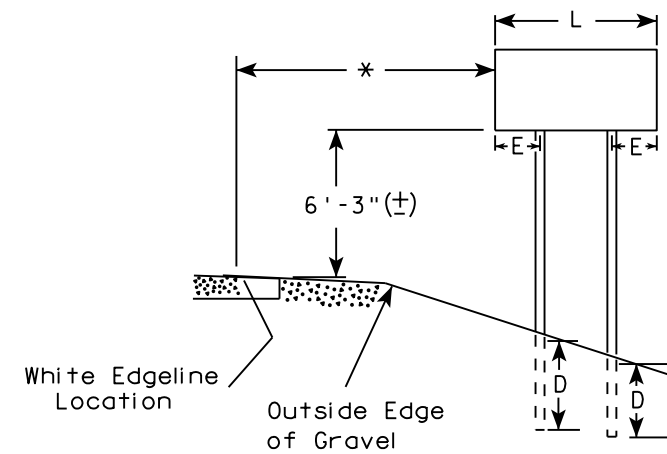
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

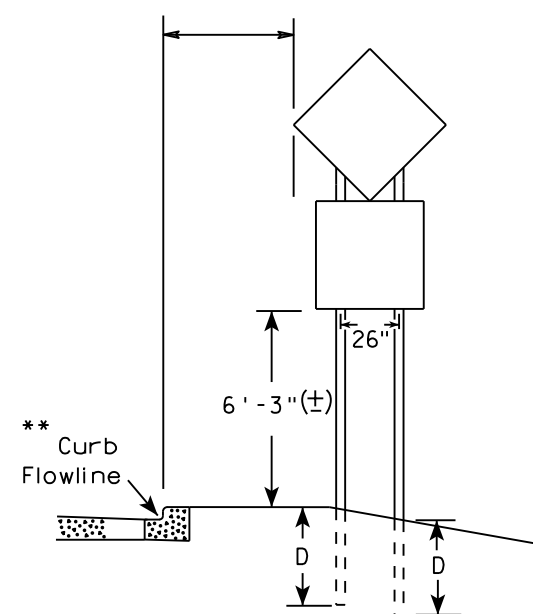
URBAN AREA



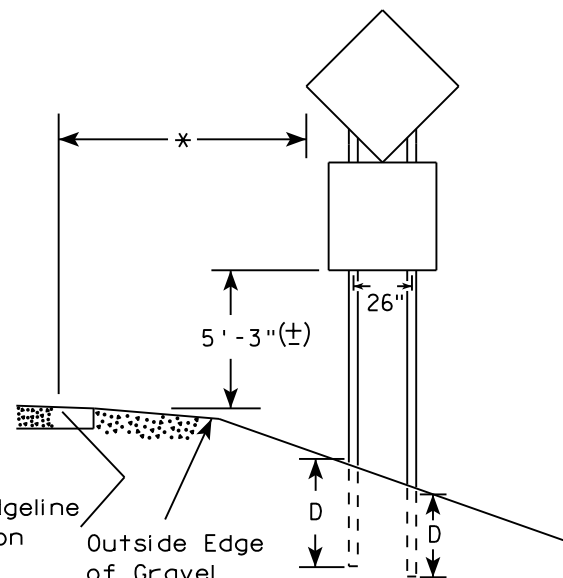
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

| SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED) | |
|--|-----|
| L | E |
| Greater than 48" Less than 60" | 12" |
| 60" to 108" | L/5 |

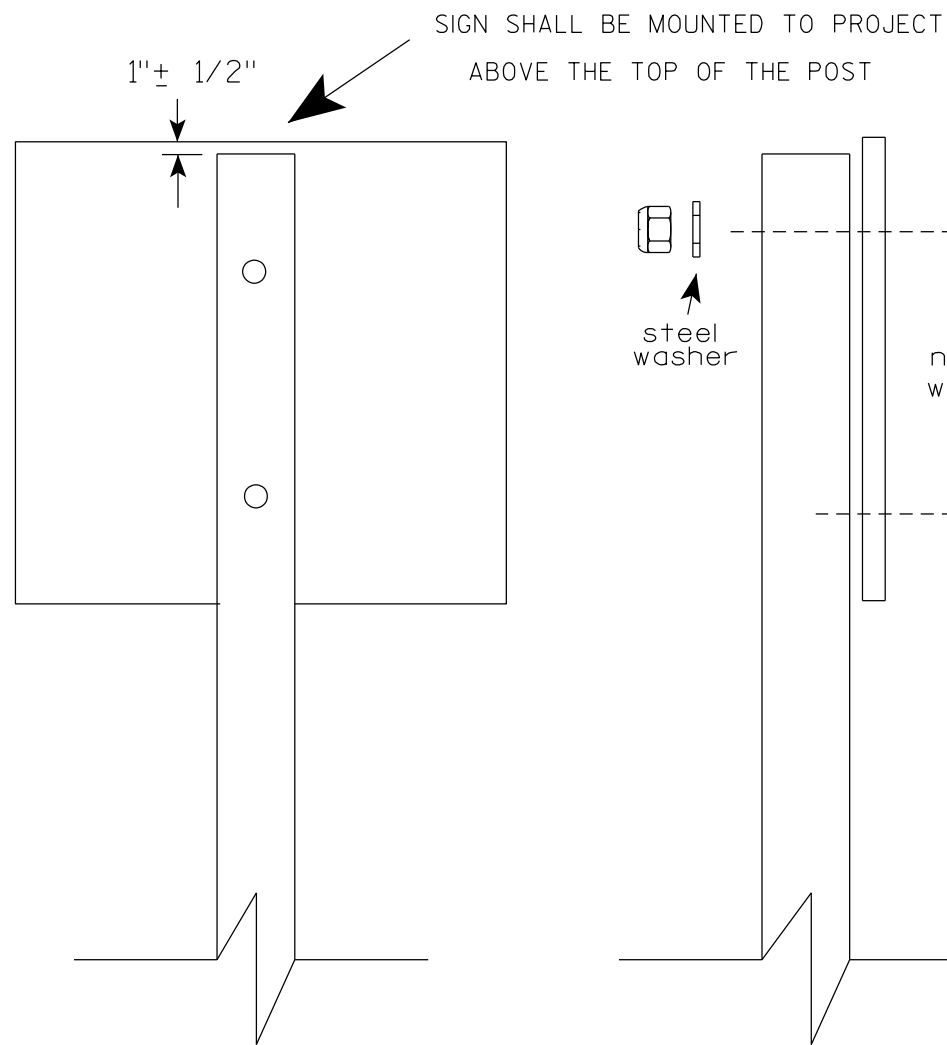
| SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED) | |
|--|-----|
| L | E |
| Greater than 108" to 144" | 12" |

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|---------------------------------------|-----------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL

1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

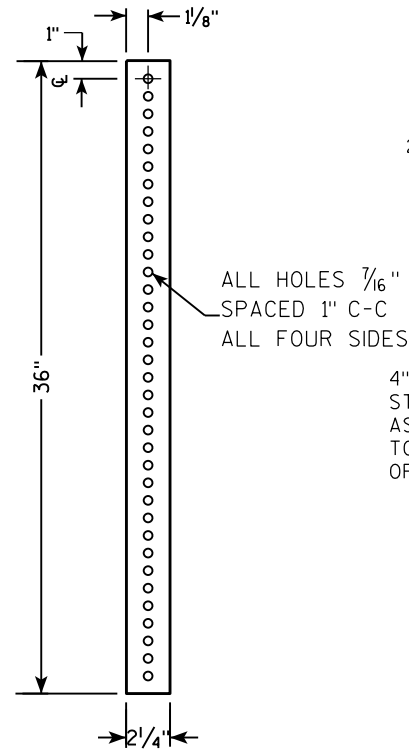
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

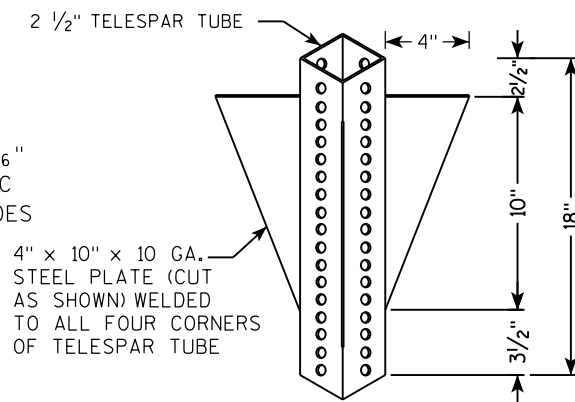
DATE 4/1/2020 PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

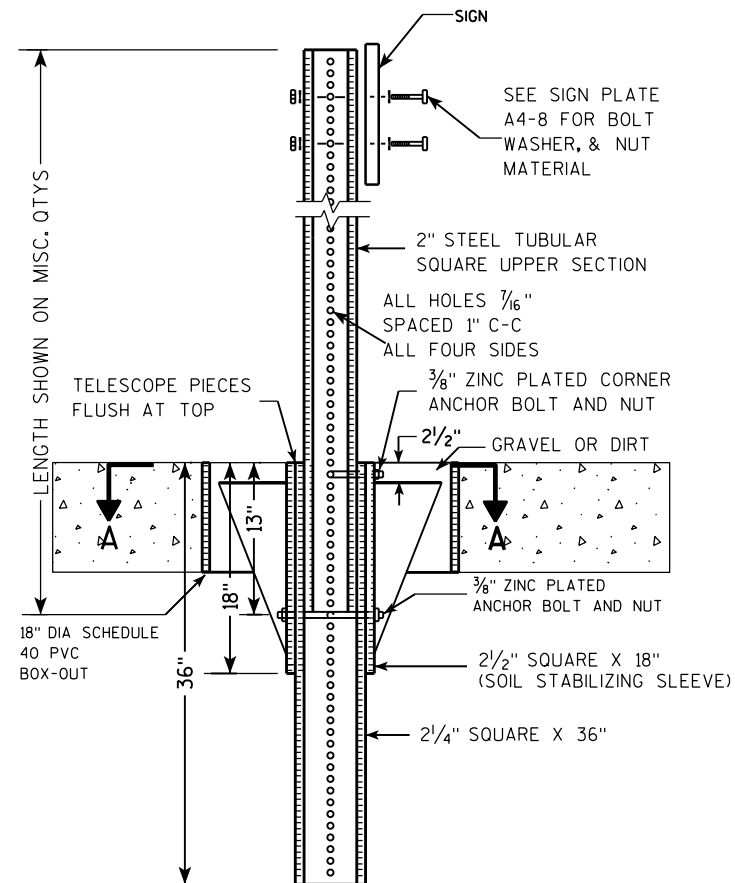
**2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



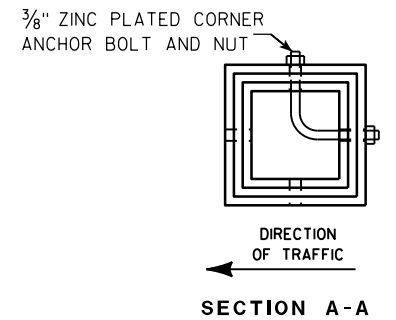
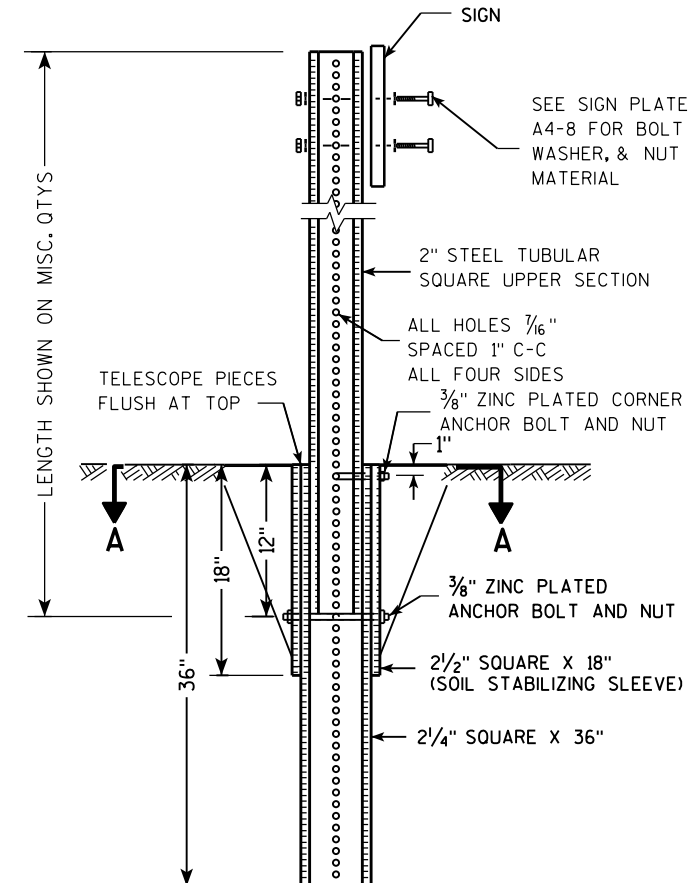
**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



| Area of Sign Installation (Sq. Ft.) | Number of Required Posts |
|--|--------------------------|
| 9 or less | 1 |
| Greater than 9 less than or equal to 18 | 2 |
| Greater than 18 less than or equal to 27 | 3 |

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

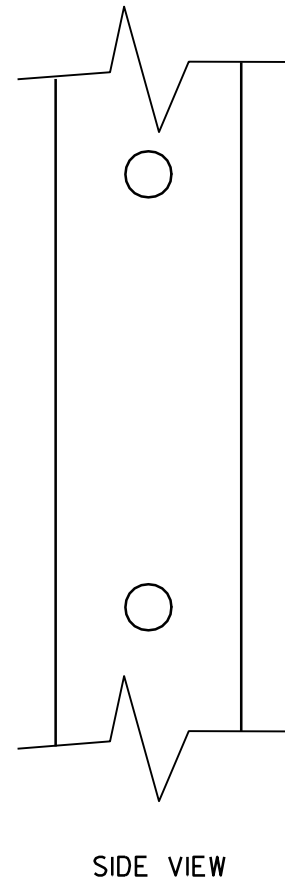
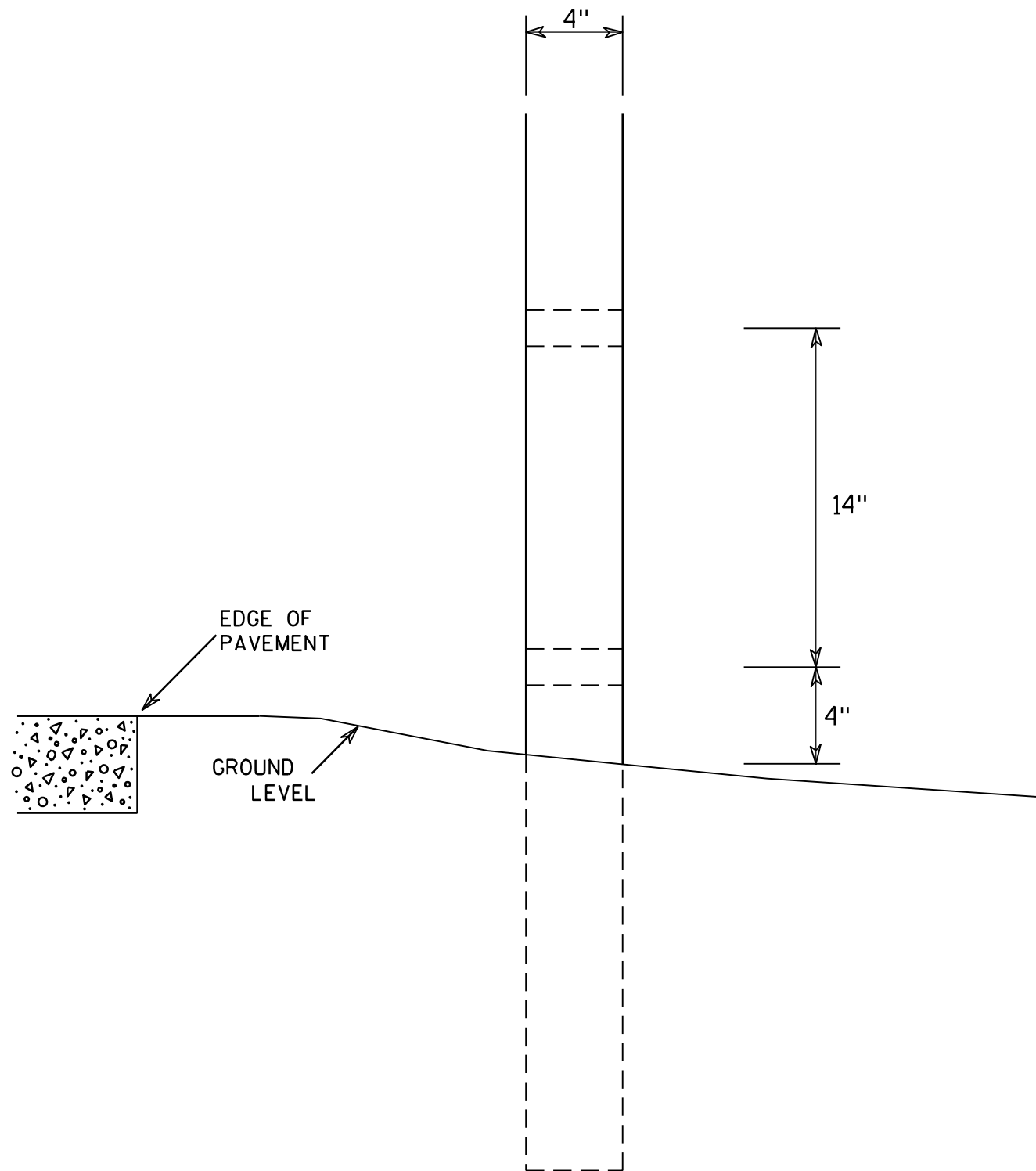
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

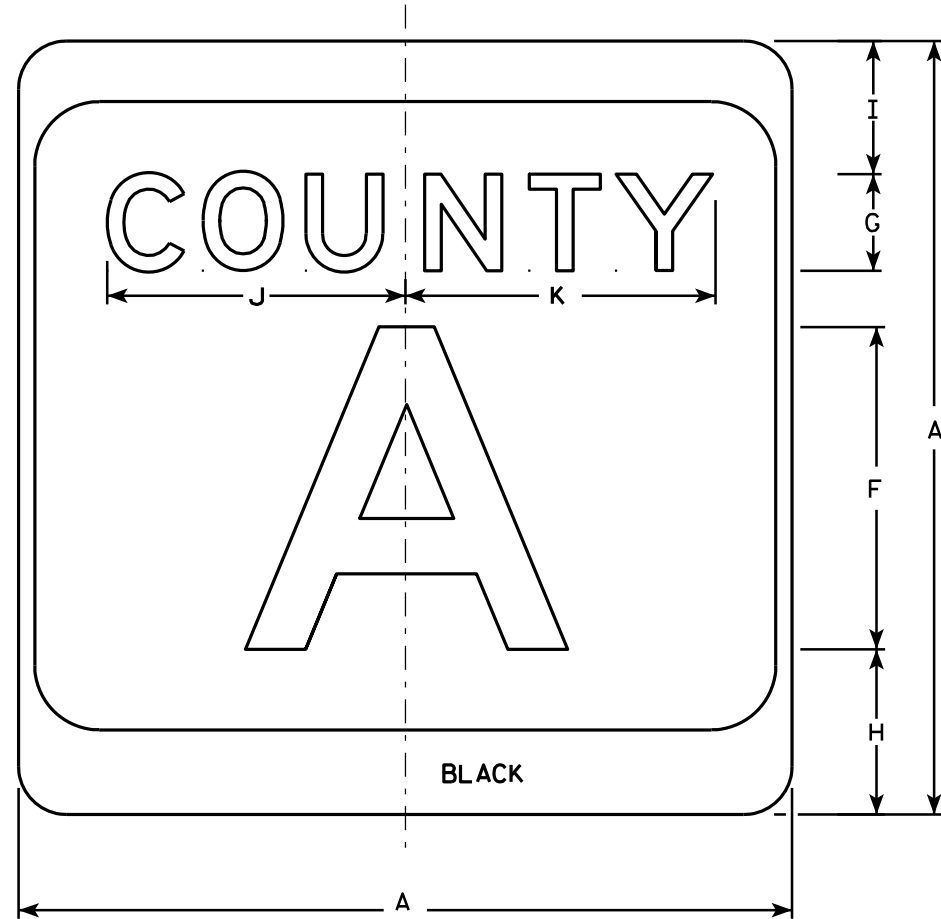
7

7

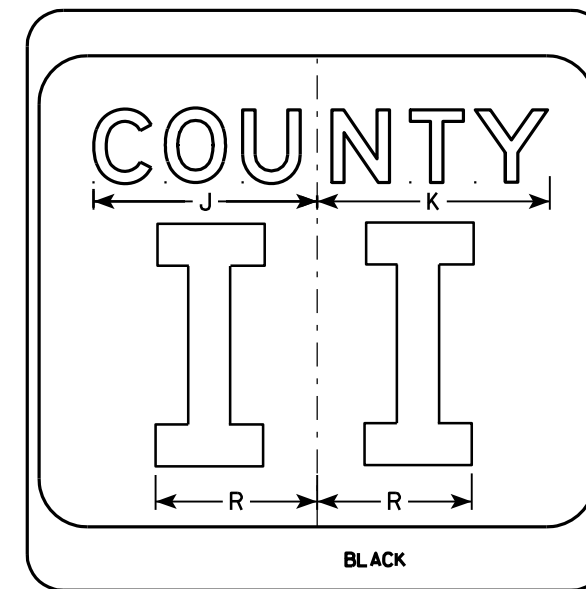
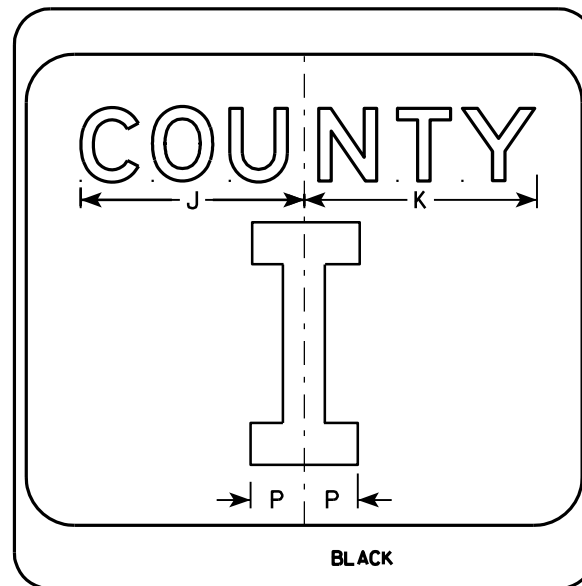
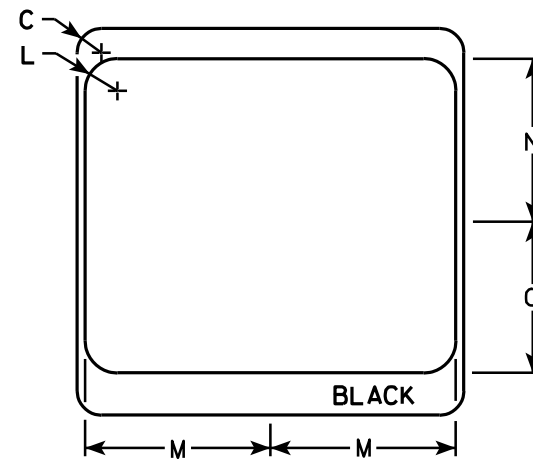
| | |
|---|--|
| 4 X 6 WOOD POST MODIFICATIONS | |
| <i>WISCONSIN DEPT OF TRANSPORTATION</i> | |
| APPROVED | <i>Chester J Spang</i> for State Traffic Engineer |
| DATE <u>3/27/97</u> | PLATE NO. <u>A4-11.2</u> |

NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 7
Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
Message Series D for 2 letters unless message is too big then Series C.
Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective



M1-5A



7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|---|---|----|---|-------|-------|--------|--------|---|--------|--------|-------|-------|---|-------|---|---|---|---|---|---|---|---|--------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 24 | | 1 1/2 | | | 10 | 3 | 5 1/8 | 4 1/8 | 9 1/4 | 9 5/8 | 2 | 11 1/2 | 10 1/8 | 9 3/8 | 2 1/4 | | 6 5/8 | | | | | | | | | 4.0 |
| 3 | 36 | | 2 1/4 | | | 16 | 4 | 7 5/8 | 5 5/8 | 12 1/4 | 12 7/8 | 3 | 17 1/8 | 15 1/4 | 14 | 3 3/8 | | 10 | | | | | | | | | 9.0 |
| 4 | 36 | | 2 1/4 | | | 16 | 4 | 7 5/8 | 5 5/8 | 12 1/4 | 12 7/8 | 3 | 17 1/8 | 15 1/4 | 14 | 3 3/8 | | 10 | | | | | | | | | 9.0 |
| 5 | 36 | | 2 1/4 | | | 16 | 4 | 7 5/8 | 5 5/8 | 12 1/4 | 12 7/8 | 3 | 17 1/8 | 15 1/4 | 14 | 3 3/8 | | 10 | | | | | | | | | 9.0 |

CTH MARKER
M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

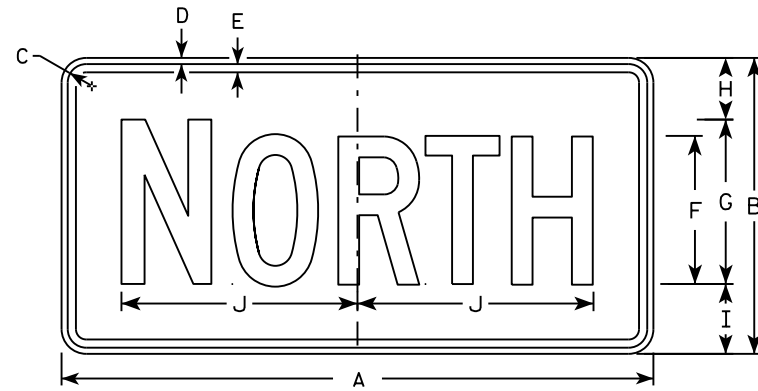
APPROVED *Matthew R. Raub*
For State Traffic Engineer

DATE 9/27/11 PLATE NO. MI-5A.8

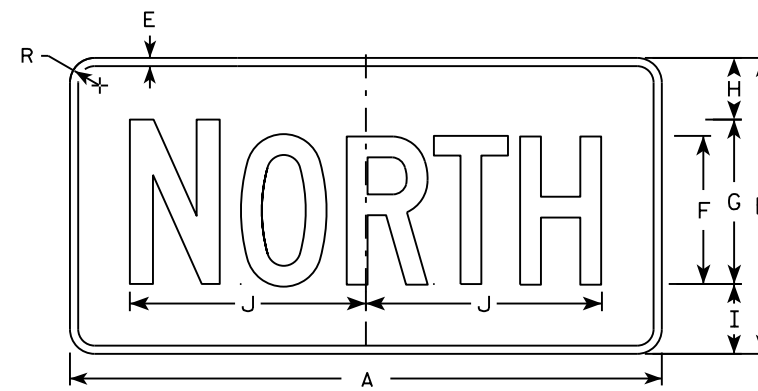
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

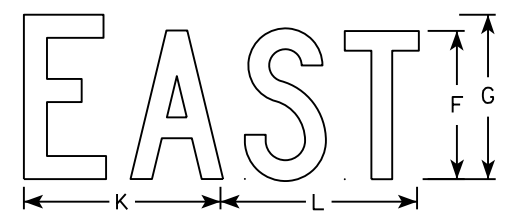
- All Signs Type II - Type H
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
 Message - Black
 MB3-1 thru MB3-4 Background - Blue
 Message - White
 MK3-1 thru MK3-4 Background - Green
 Message - White
 MM3-1 thru MM3-4 Background - White
 Message - Green
 MN3-1 thru MN3-4 Background - Brown
 Message - White
 MP3-1 thru MP3-4 Background - White
 Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.



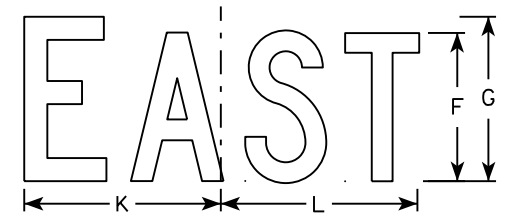
M3-1
MM3-1
MP3-1



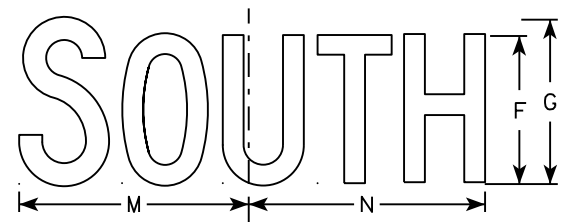
MB3-1
MK3-1
MN3-1



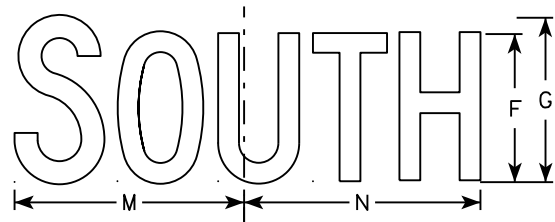
M3-2
MM3-2
MP3-2



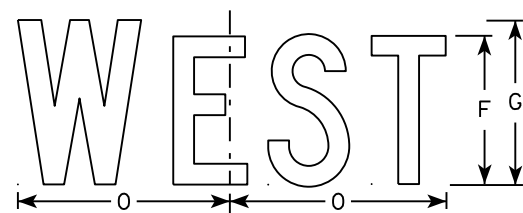
MB3-2
MK3-2
MN3-2



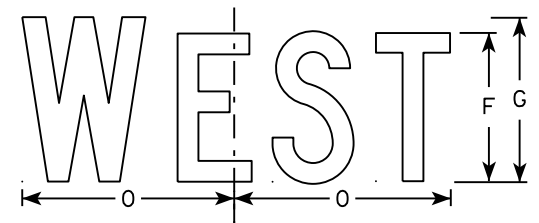
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|----|-------|-------|--------|-------|--------|--------|--------|-------|---|---|-------|---|---|---|---|---|---|---|---|--------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 24 | 12 | 1 1/8 | 3/8 | 3/8 | 6 | 7 | 2 1/4 | 2 3/4 | 10 1/4 | 7 7/8 | 8 3/8 | 10 1/4 | 9 3/4 | 8 3/4 | | | 1 1/2 | | | | | | | | | 2.00 |
| 3 | 36 | 18 | 1 1/8 | 3/8 | 1/2 | 9 | 10 | 3 3/4 | 4 1/4 | 14 3/8 | 12 | 12 1/8 | 14 | 14 1/8 | 13 | | | 1 1/2 | | | | | | | | | 4.5 |
| 4 | 36 | 18 | 1 1/8 | 3/8 | 1/2 | 9 | 10 | 3 3/4 | 4 1/4 | 14 3/8 | 12 | 12 1/8 | 14 | 14 1/8 | 13 | | | 1 1/2 | | | | | | | | | 4.5 |
| 5 | 36 | 18 | 1 1/8 | 3/8 | 1/2 | 9 | 10 | 3 3/4 | 4 1/4 | 14 3/8 | 12 | 12 1/8 | 14 | 14 1/8 | 13 | | | 1 1/2 | | | | | | | | | 4.5 |

STANDARD SIGNS
M3-1 thru M3-4
SERIES

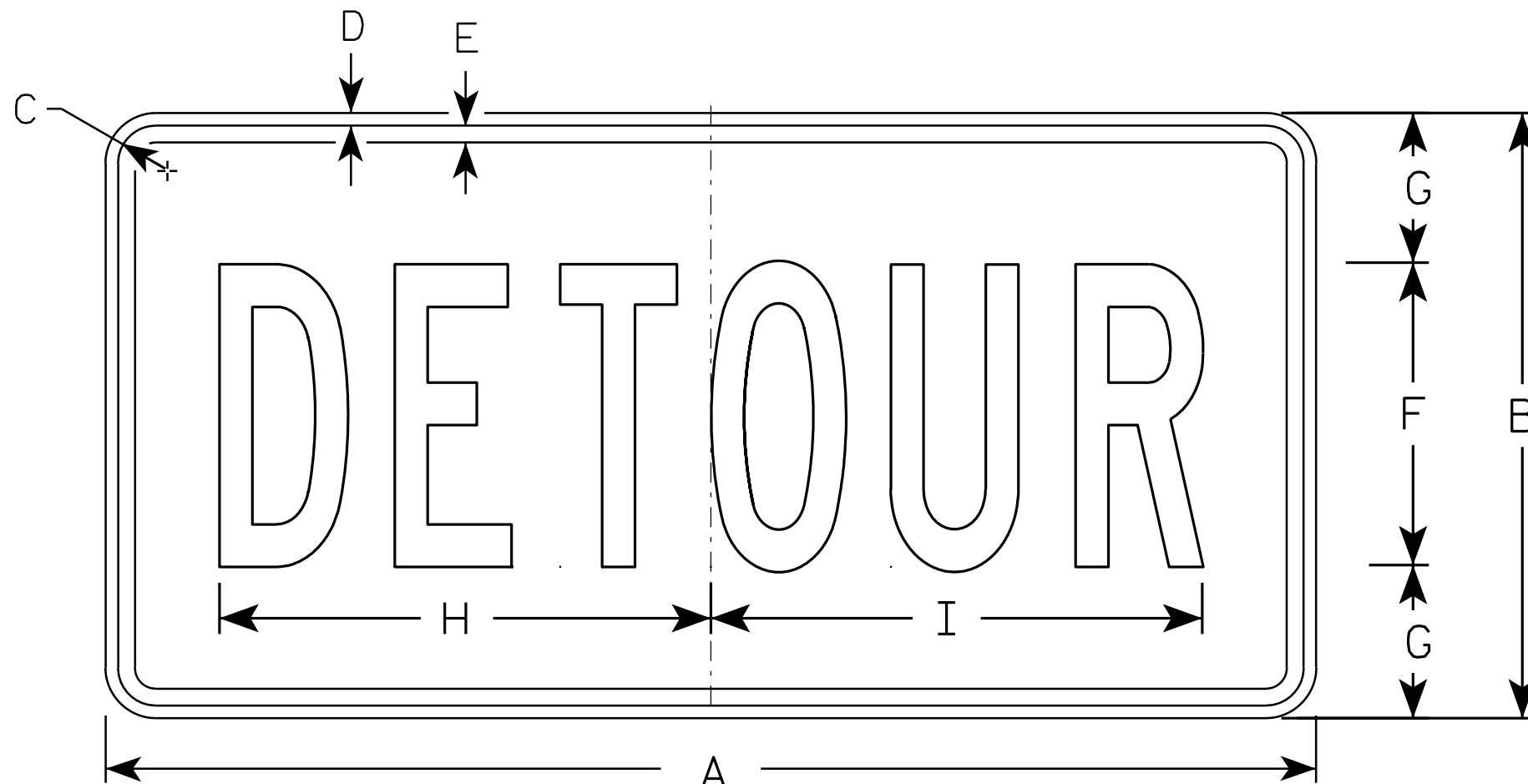
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|--------|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|--------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 24 | 12 | 1 1/8 | 3/8 | 3/8 | 6 | 3 | 10 | 10 1/4 | | | | | | | | | | | | | | | | | | 2.0 |
| 3 | 36 | 18 | 1 1/8 | 3/8 | 1/2 | 9 | 4 1/2 | 14 5/8 | 14 1/2 | | | | | | | | | | | | | | | | | | 4.5 |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

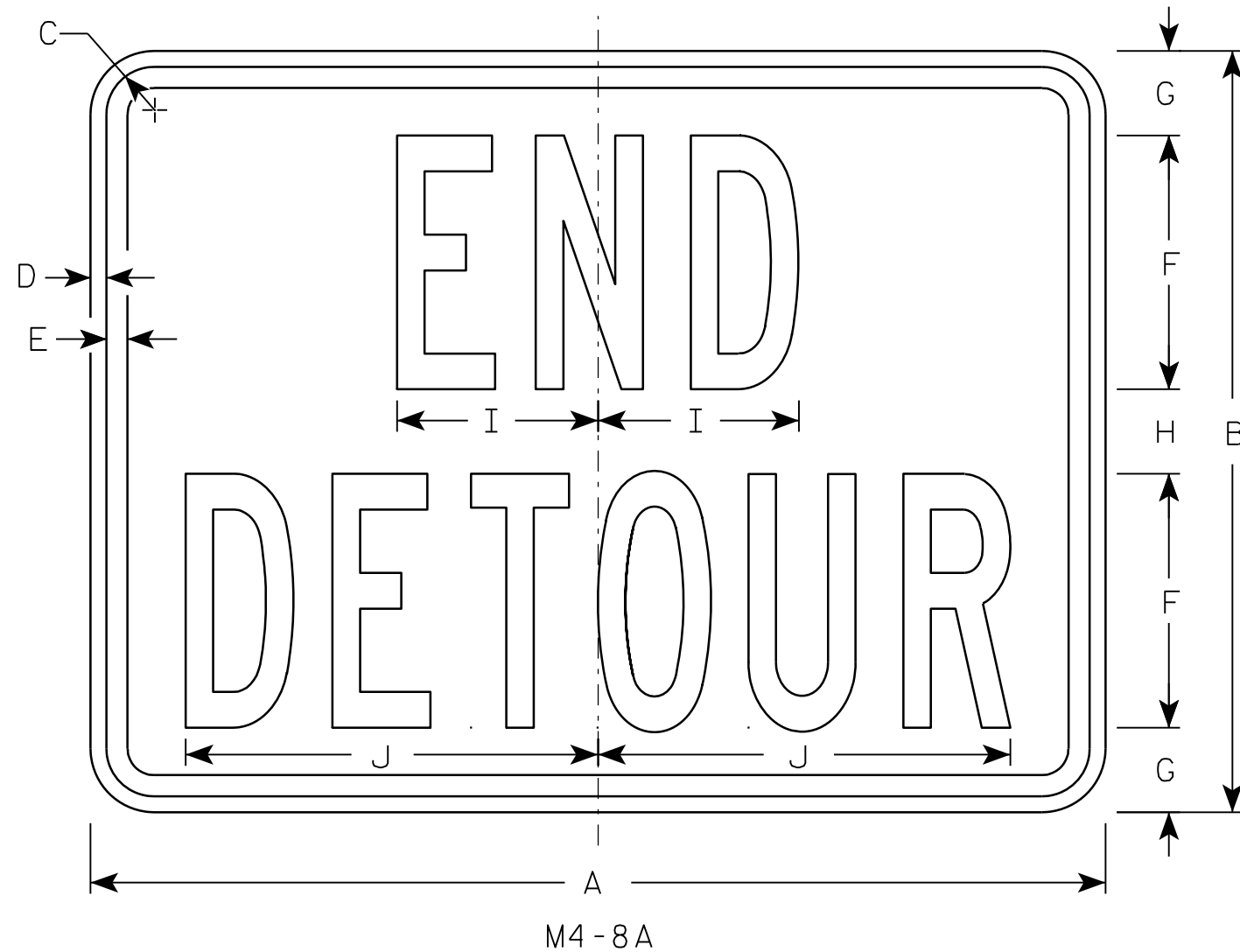
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|---|-------|-------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 24 | 18 | 1 1/8 | 3/8 | 1/2 | 6 | 2 | 2 | 4 3/4 | 9 3/4 | | | | | | | | | | | | | | | | | 3.0 |
| 3 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 8 | 2 1/2 | 3 | 6 3/4 | 13 | | | | | | | | | | | | | | | | | 5.0 |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

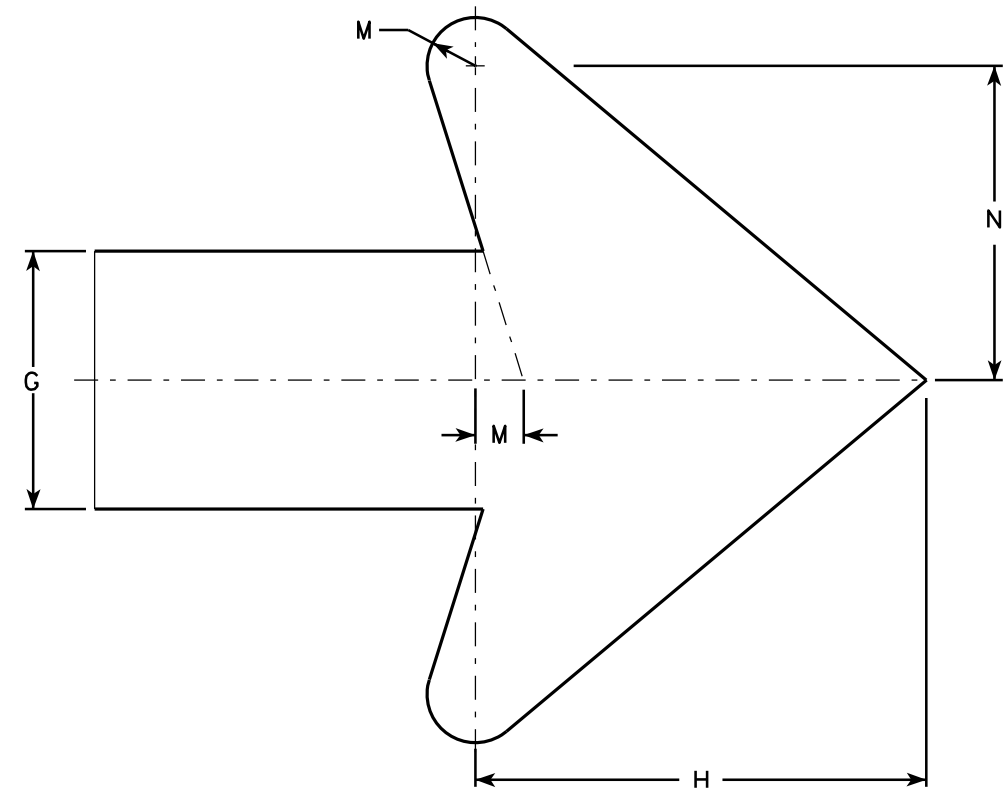
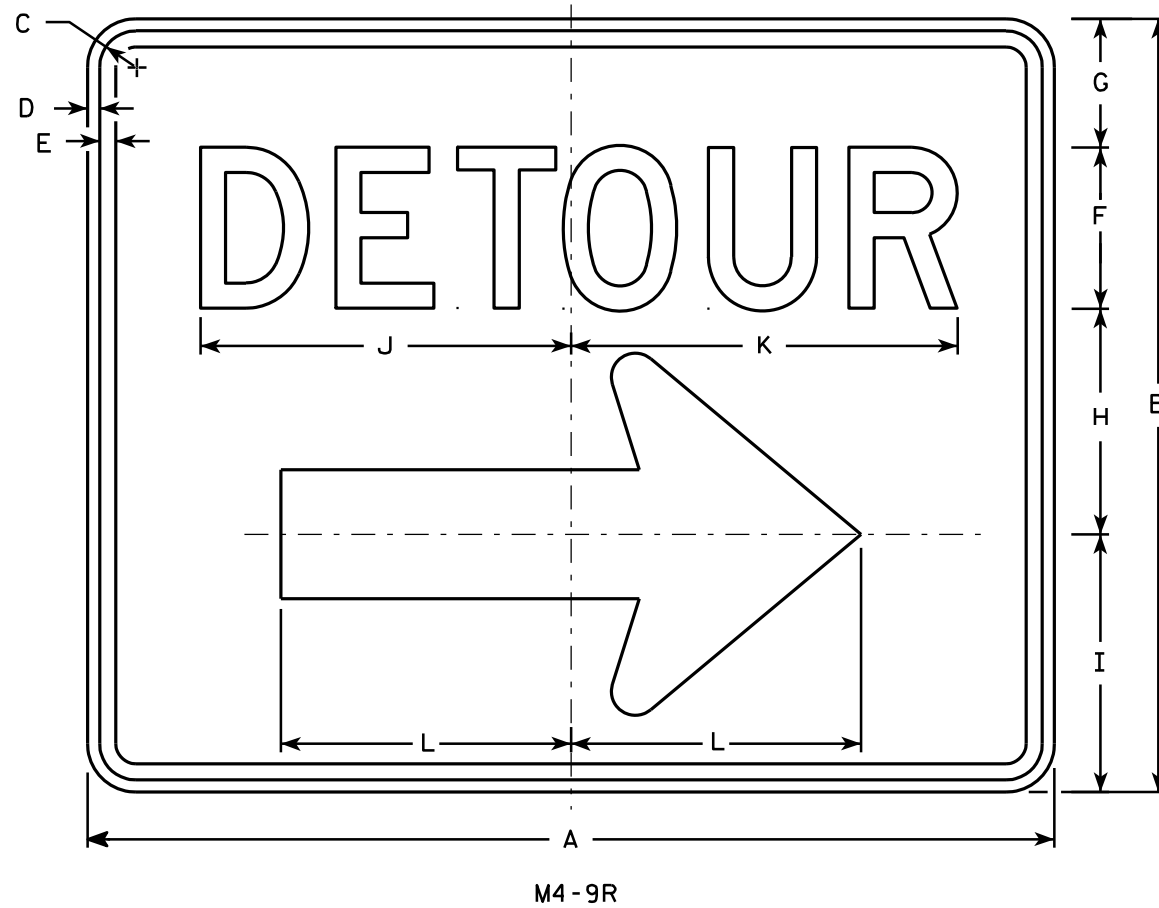
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|--------|--------|--------|--------|--------|-------|-------|---|---|---|---|---|---|---|---|---|---|---|---|--------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 5 | 4 | 7 | 8 | 11 1/2 | 12 | 9 | 3/4 | 4 7/8 | | | | | | | | | | | | | 5.00 |
| 3 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 5 | 4 | 7 | 8 | 11 1/2 | 12 | 9 | 3/4 | 4 7/8 | | | | | | | | | | | | | 5.00 |
| 4 | 48 | 36 | 1 3/8 | 1/2 | 5/8 | 8 | 6 | 10 1/2 | 11 5/8 | 20 5/8 | 20 1/2 | 13 1/4 | 1 1/8 | 6 7/8 | | | | | | | | | | | | | 12.0 |
| 5 | 48 | 36 | 1 3/8 | 1/2 | 5/8 | 8 | 6 | 10 1/2 | 11 5/8 | 20 5/8 | 20 1/2 | 13 1/4 | 1 1/8 | 6 7/8 | | | | | | | | | | | | | 12.0 |

STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

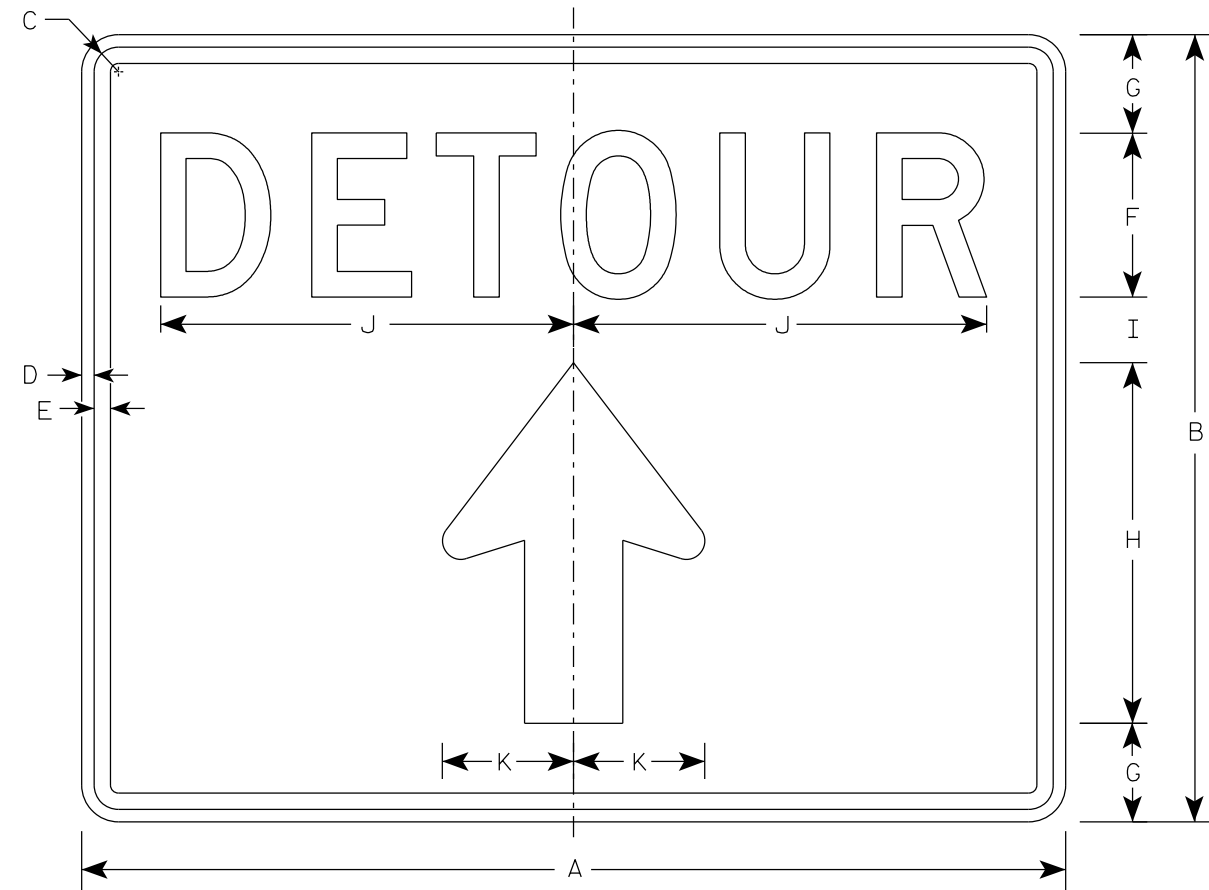
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

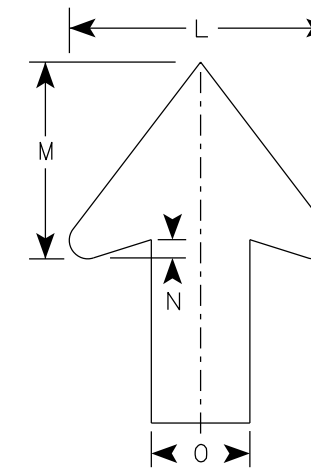
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D



M4-9RA



Arrow Detail

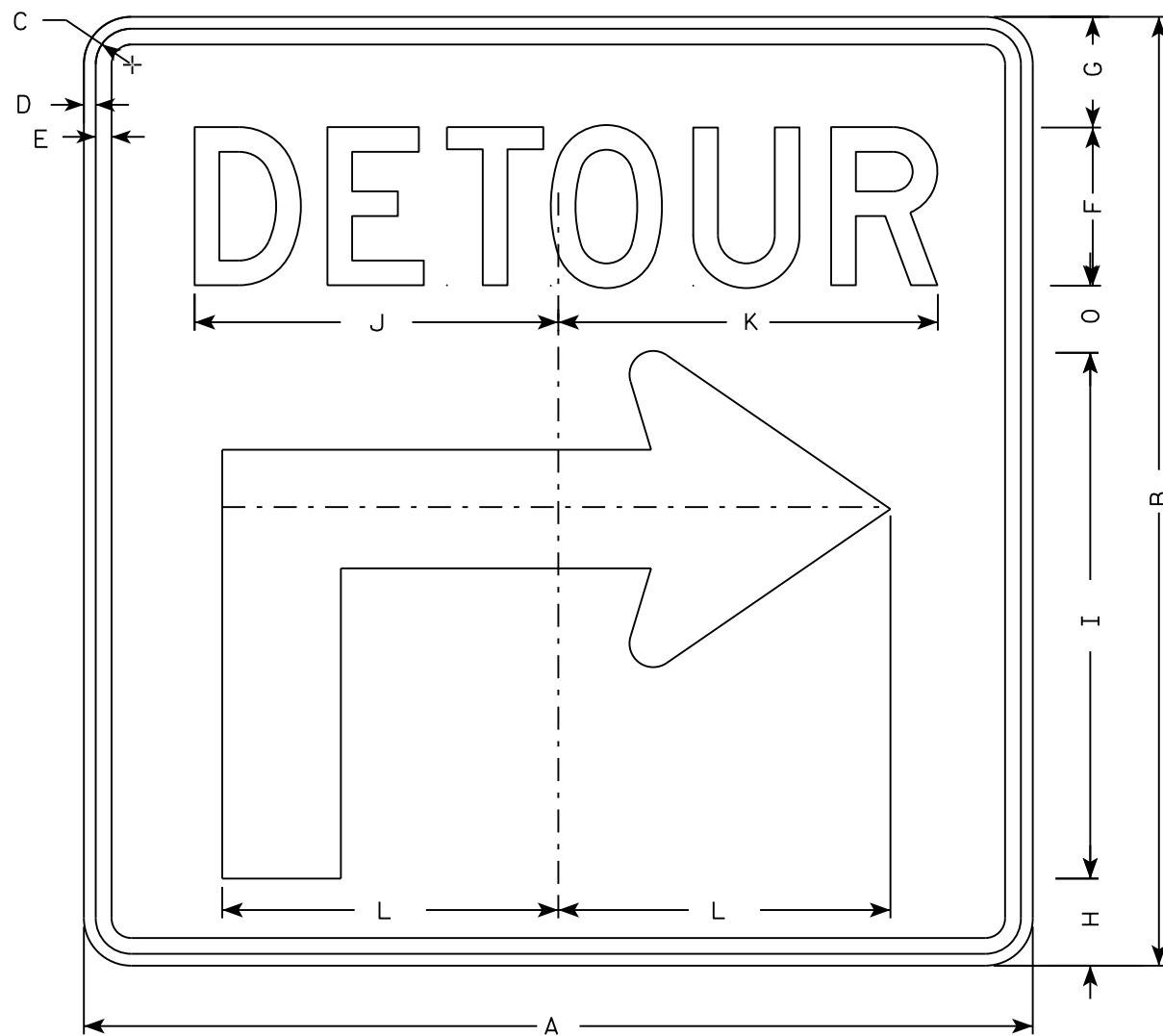
| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|----|---|--------|---|---|---|-----|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 5 | 3 | 11 | 2 | 12 5/8 | 4 | 8 | 6 | 1/2 | 3 | | | | | | | | | | | | 5.00 |
| 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
M4-9RA

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

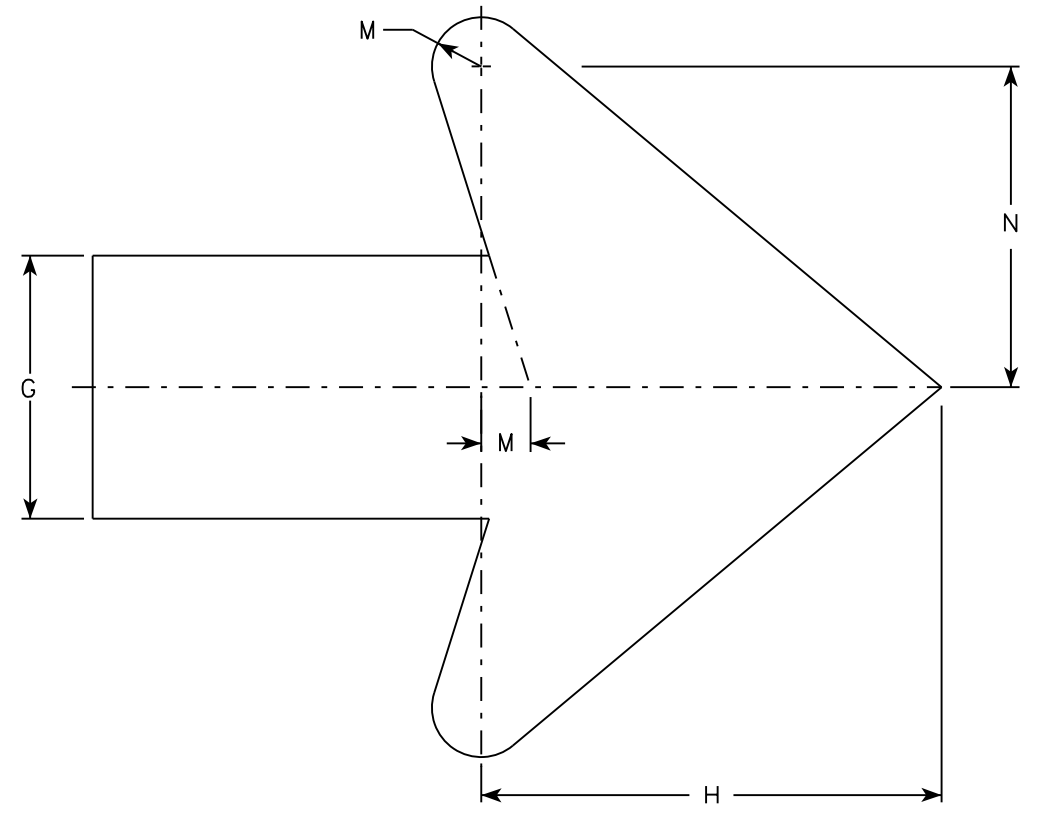
DATE 12/10/2020 PLATE NO. M4-9RA.1



M4-59R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown when base material is metal.
5. M4-59L is the same as M4-59R except the arrow is reversed.



Arrow Detail

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|-------|--------|--------|--------|--------|-------|-------|-------|---|---|---|---|---|---|---|---|---|---|---|--------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 30 | 30 | 1 1/8 | 3/8 | 1/2 | 5 | 3 1/2 | 2 3/4 | 16 5/8 | 11 1/2 | 12 | 10 1/2 | 3/4 | 4 7/8 | 2 1/8 | | | | | | | | | | | | 6.25 |
| 3 | 30 | 30 | 1 1/8 | 3/8 | 1/2 | 5 | 3 1/2 | 2 3/4 | 16 5/8 | 11 1/2 | 12 | 10 1/2 | 3/4 | 4 7/8 | 2 1/8 | | | | | | | | | | | | 6.25 |
| 4 | 48 | 48 | 1 3/8 | 1/2 | 5/8 | 8 | 5 5/8 | 4 3/8 | 26 5/8 | 20 5/8 | 20 1/2 | 17 | 1 1/8 | 6 7/8 | 3 3/8 | | | | | | | | | | | | 16.0 |
| 5 | 48 | 48 | 1 3/8 | 1/2 | 5/8 | 8 | 5 5/8 | 4 3/8 | 26 5/8 | 20 5/8 | 20 1/2 | 17 | 1 1/8 | 6 7/8 | 3 3/8 | | | | | | | | | | | | 16.0 |

STANDARD SIGN
M4-59 L&R

WISCONSIN DEPT OF TRANSPORTATION

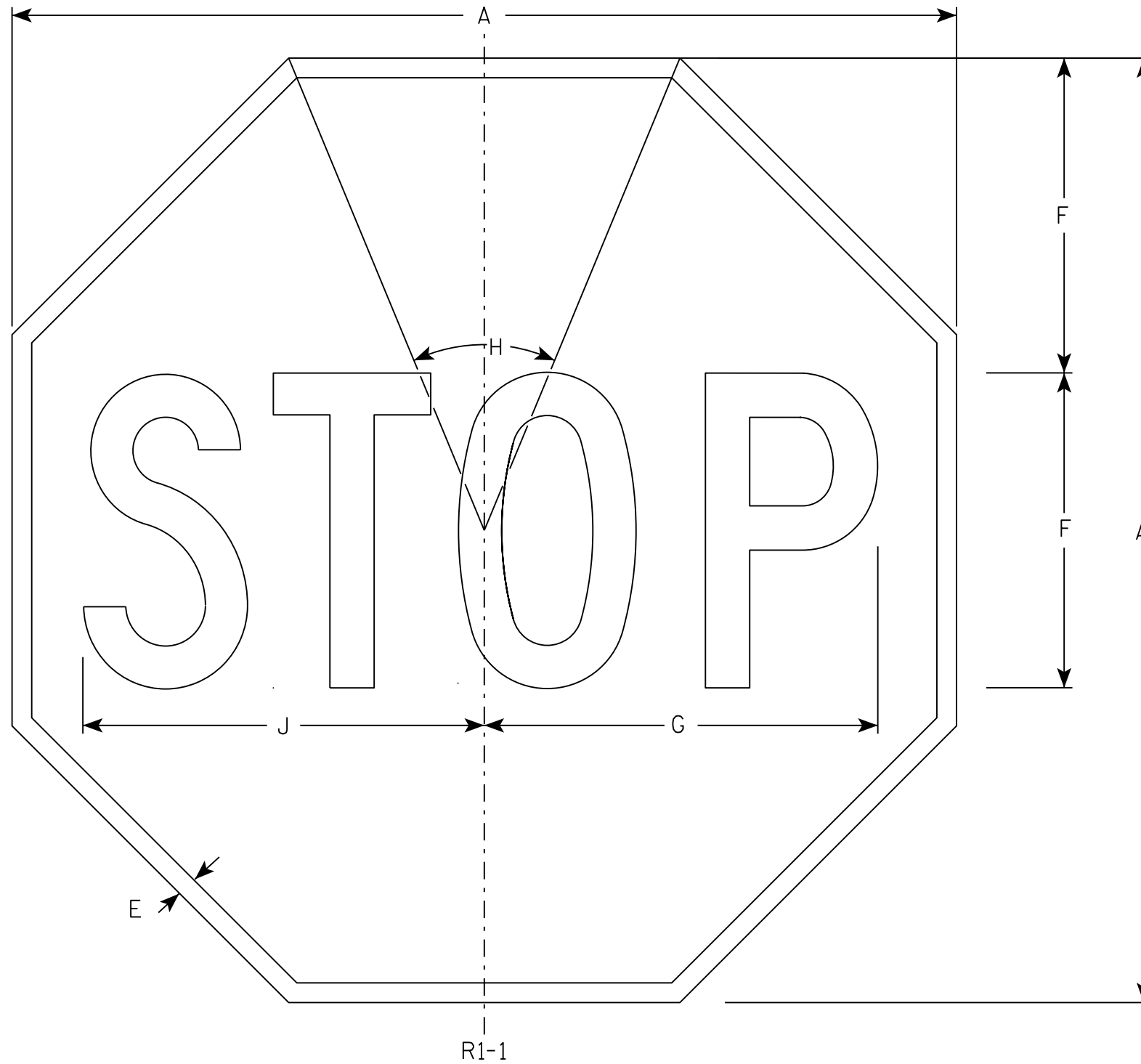
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/15 PLATE NO. M4-59.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C



R1-1

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|---|---|-----|----|--------|-----|---|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | 30 | | | | 5/8 | 10 | 12 1/2 | 45° | | 12 3/4 | | | | | | | | | | | | | | | | | 5.18 |
| 2S | 30 | | | | 5/8 | 10 | 12 1/2 | 45° | | 12 3/4 | | | | | | | | | | | | | | | | | 5.18 |
| 2M | 36 | | | | 3/4 | 12 | 15 | 45° | | 15 3/8 | | | | | | | | | | | | | | | | | 7.46 |
| 3 | 36 | | | | 3/4 | 12 | 15 | 45° | | 15 3/8 | | | | | | | | | | | | | | | | | 7.46 |
| 4 | 48 | | | | 1 | 16 | 20 | 45° | | 20 1/2 | | | | | | | | | | | | | | | | | 13.25 |
| 5 | 48 | | | | 1 | 16 | 20 | 45° | | 20 1/2 | | | | | | | | | | | | | | | | | 13.25 |
| 6 | 18 | | | | 3/8 | 6 | 7 3/4 | 45° | | 7 3/4 | | | | | | | | | | | | | | | | | 1.86 |
| 7 | 12 | | | | 1/4 | 4 | 5 | 45° | | 5 1/8 | | | | | | | | | | | | | | | | | 0.78 |

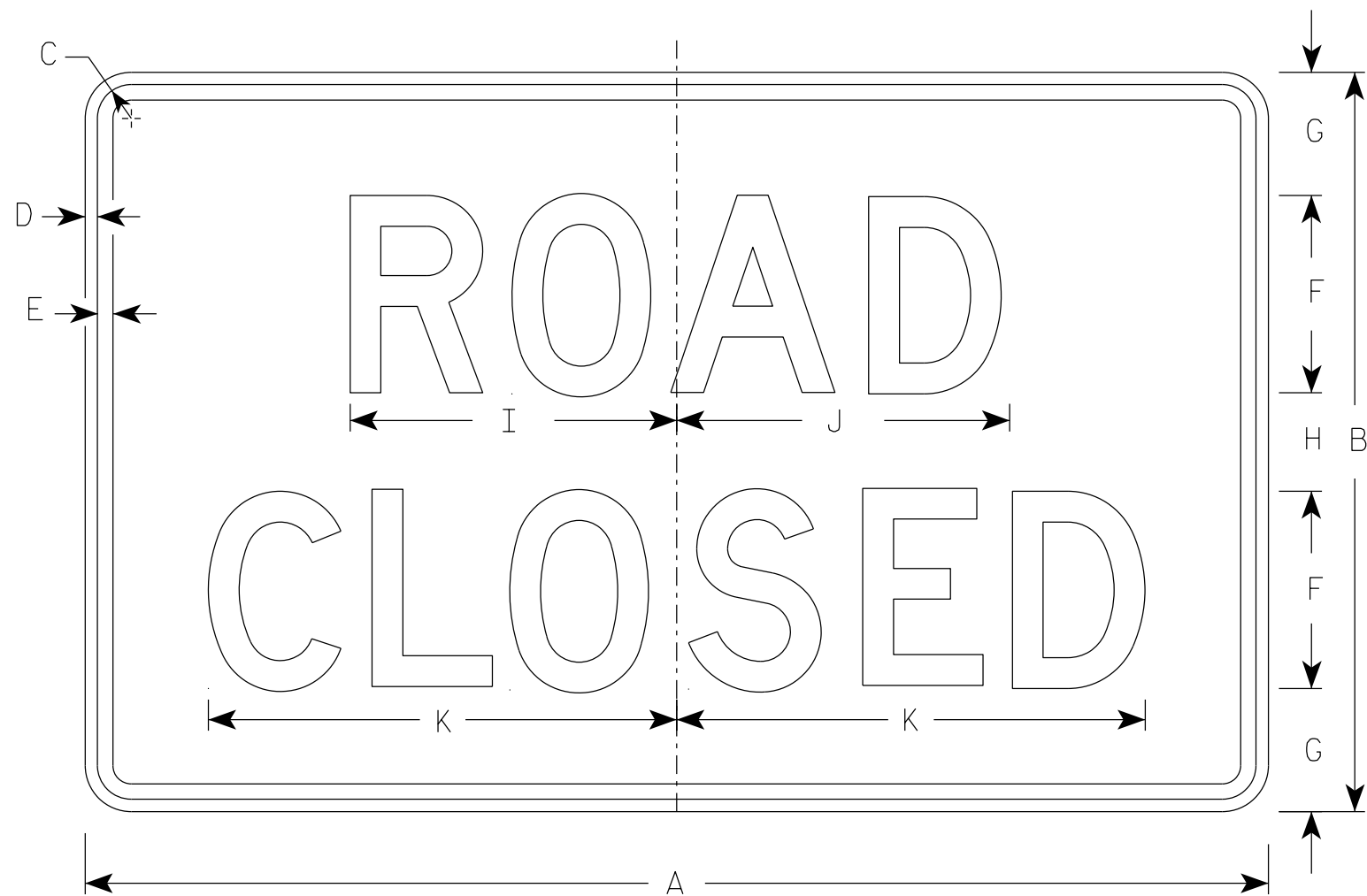
STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



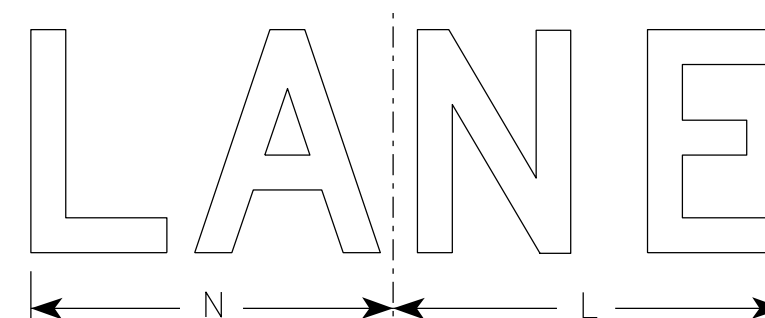
R11-2



R11-2R



R11-2T



R11-2L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|---|--------|--------|----|----|----|----|--------|---|---|---|---|---|---|---|---|---|---|---|--------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 48 | 30 | 1 3/8 | 1/2 | 5/8 | 8 | 5 | 4 | 13 1/4 | 13 1/2 | 19 | 14 | 15 | 13 | 15 5/8 | | | | | | | | | | | | 10.0 |
| 2M | 48 | 30 | 1 3/8 | 1/2 | 5/8 | 8 | 5 | 4 | 13 1/4 | 13 1/2 | 19 | 14 | 15 | 13 | 15 5/8 | | | | | | | | | | | | 10.0 |
| 3 | 48 | 30 | 1 3/8 | 1/2 | 5/8 | 8 | 5 | 4 | 13 1/4 | 13 1/2 | 19 | 14 | 15 | 13 | 15 5/8 | | | | | | | | | | | | 10.0 |
| 4 | 48 | 30 | 1 3/8 | 1/2 | 5/8 | 8 | 5 | 4 | 13 1/4 | 13 1/2 | 19 | 14 | 15 | 13 | 15 5/8 | | | | | | | | | | | | 10.0 |
| 5 | 48 | 30 | 1 3/8 | 1/2 | 5/8 | 8 | 5 | 4 | 13 1/4 | 13 1/2 | 19 | 14 | 15 | 13 | 15 5/8 | | | | | | | | | | | | 10.0 |

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

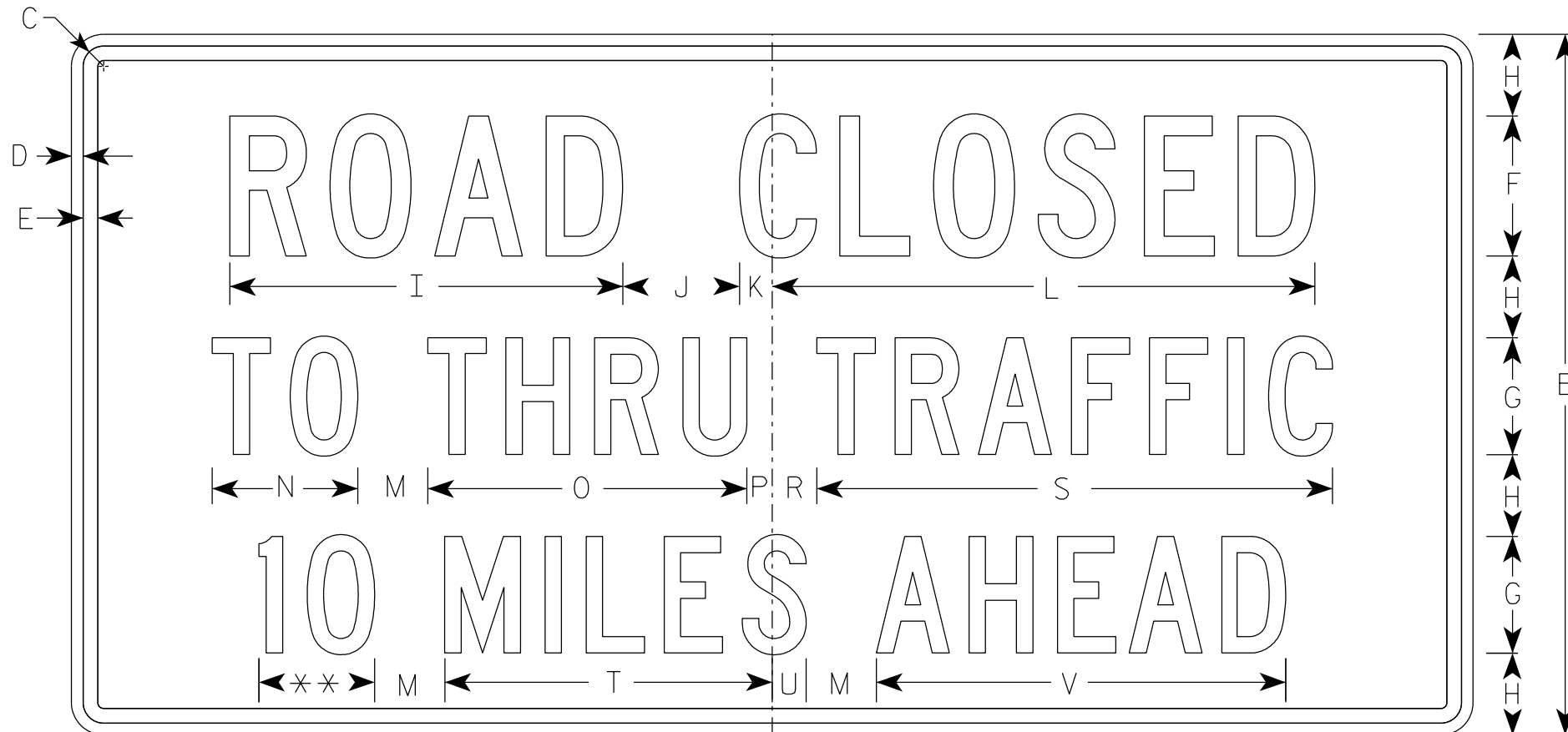
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 3/29/2021 PLATE NO. R11-2.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

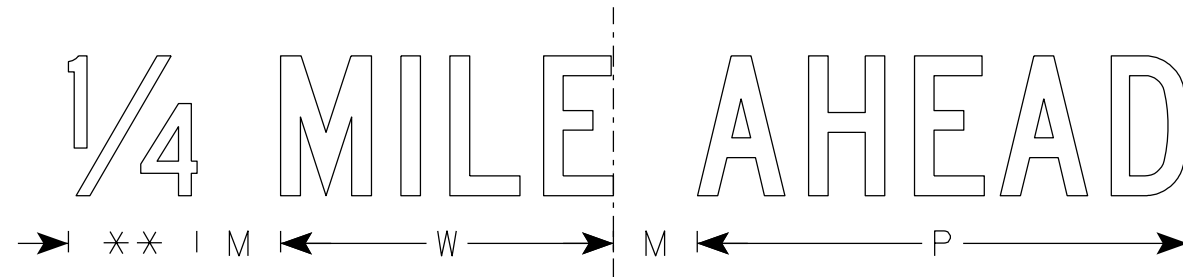
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | v | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|-------|--------|---|-------|--------|---|-------|--------|-------|---|-------|--------|-------|-------|--------|--------|---|---|------|--------------|
| 1 | 36 | 18 | 1 1/4 | 3/8 | 3/8 | 4 | 3 | 2 | 11 1/4 | 3 | 1 1/8 | 15 3/8 | 2 | 3 3/4 | 8 1/4 | 5/8 | | 1 3/8 | 13 1/4 | 8 3/8 | 7/8 | 10 1/2 | 7 1/8 | | | 4.5 | |
| 2S | 60 | 30 | 1 3/8 | 1/2 | 5/8 | 6 | 5 | 3 1/2 | 16 7/8 | 5 | 1 3/8 | 23 1/4 | 3 | 6 1/4 | 13 5/8 | 1 1/8 | | 1 7/8 | 22 1/8 | 14 | 1 1/2 | 17 1/2 | 11 7/8 | | | 12.5 | |
| 2M | 60 | 30 | 1 3/8 | 1/2 | 5/8 | 6 | 5 | 3 1/2 | 16 7/8 | 5 | 1 3/8 | 23 1/4 | 3 | 6 1/4 | 13 5/8 | 1 1/8 | | 1 7/8 | 22 1/8 | 14 | 1 1/2 | 17 1/2 | 11 7/8 | | | 12.5 | |
| 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
R11-3

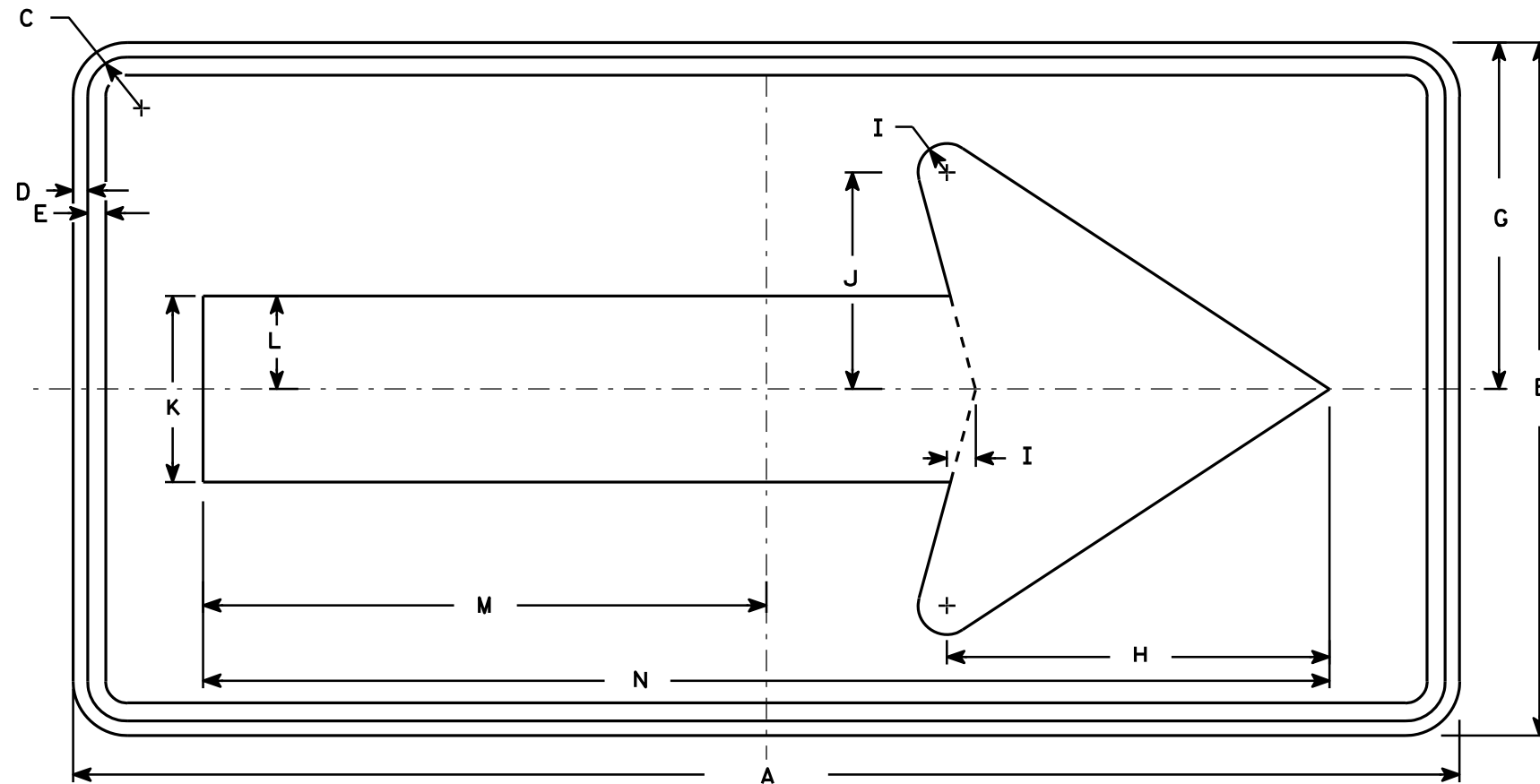
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/14/2021 PLATE NO. R11-3.9

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W1-6

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|----|--------|-------|-------|-------|-------|--------|--------|---|---|---|---|---|---|---|---|---|---|---|---|--------------|
| 1 | 36 | 18 | 1 1/8 | 3/8 | 3/8 | | 9 | 10 | 3/4 | 5 5/8 | 4 3/4 | 2 3/8 | 14 5/8 | 29 1/4 | | | | | | | | | | | | | 4.5 |
| 2S | 48 | 24 | 1 3/8 | 1/2 | 5/8 | | 12 | 13 1/4 | 1 | 7 1/2 | 6 1/2 | 3 1/4 | 19 1/2 | 39 | | | | | | | | | | | | | 8.0 |
| 2M | 48 | 24 | 1 3/8 | 1/2 | 5/8 | | 12 | 13 1/4 | 1 | 7 1/2 | 6 1/2 | 3 1/4 | 19 1/2 | 39 | | | | | | | | | | | | | 8.0 |
| 3 | 60 | 30 | 1 3/8 | 1/2 | 5/8 | | 15 | 16 1/4 | 1 1/4 | 9 1/4 | 8 | 4 | 24 3/8 | 48 3/4 | | | | | | | | | | | | | 12.5 |
| 4 | 60 | 30 | 1 3/8 | 1/2 | 5/8 | | 15 | 16 1/4 | 1 1/4 | 9 1/4 | 8 | 4 | 24 3/8 | 48 3/4 | | | | | | | | | | | | | 12.5 |
| 5 | 96 | 48 | 2 1/4 | 3/4 | 1 | | 24 | 26 1/2 | 2 | 15 | 13 | 6 1/2 | 39 | 78 | | | | | | | | | | | | | 32.0 |

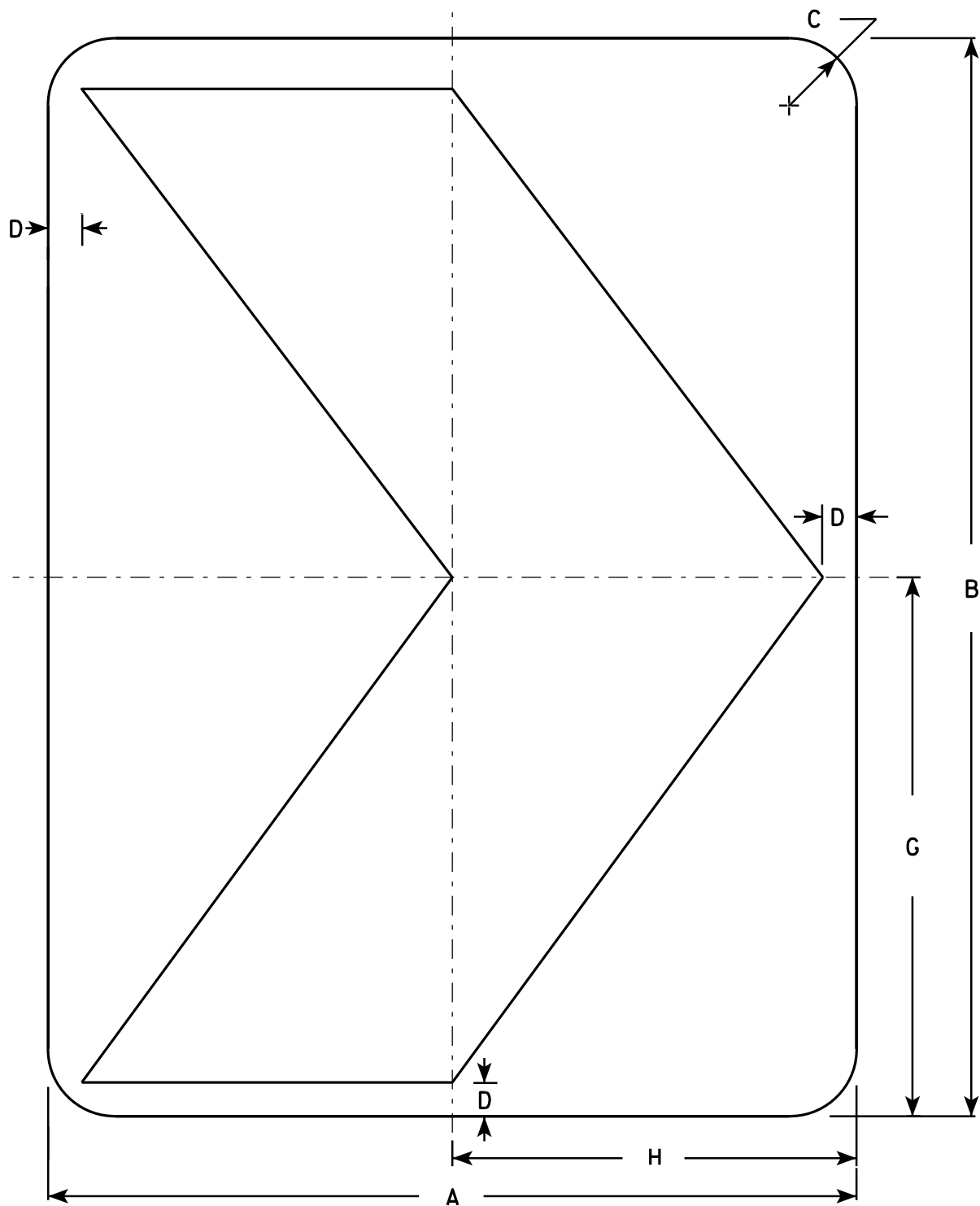
STANDARD SIGN
W1-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-6.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

W1-8

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-------|---|---|----|----|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|--------------|
| 1 | 12 | 18 | 1 1/2 | 1/2 | | | 9 | 6 | | | | | | | | | | | | | | | | | | | 1.5 |
| 2S | 18 | 24 | 1 1/2 | 3/4 | | | 12 | 9 | | | | | | | | | | | | | | | | | | | 3.0 |
| 2M | 18 | 24 | 1 1/2 | 3/4 | | | 12 | 9 | | | | | | | | | | | | | | | | | | | 3.0 |
| 3 | 24 | 30 | 1 1/2 | 1 | | | 15 | 12 | | | | | | | | | | | | | | | | | | | 5.0 |
| 4 | 30 | 36 | 1 7/8 | 1 1/4 | | | 18 | 15 | | | | | | | | | | | | | | | | | | | 7.5 |
| 5 | 36 | 48 | 2 1/4 | 1 1/2 | | | 24 | 18 | | | | | | | | | | | | | | | | | | | 12.0 |

STANDARD SIGN
W1-8

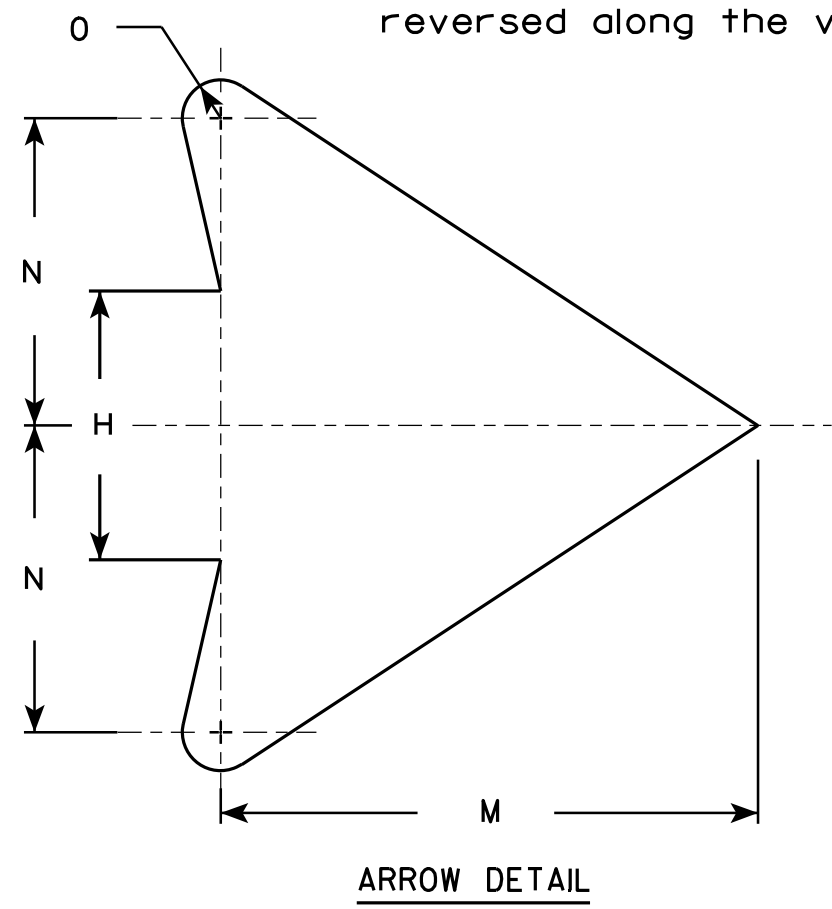
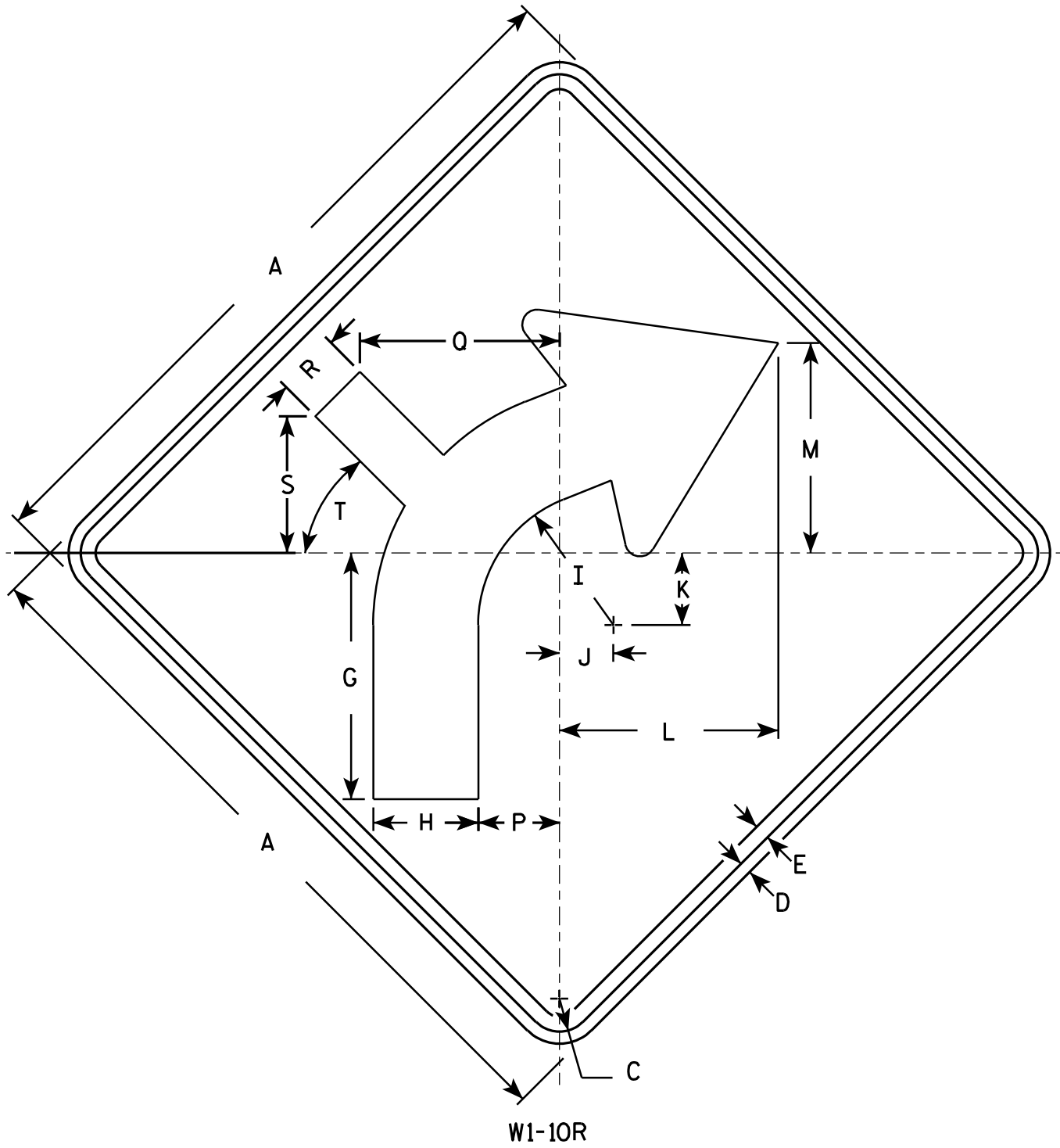
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-8.6

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-10L is the same as W1-10R except the arrow is reversed along the vertical centerline.



7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|--------|-------|-------|-------|-------|--------|--------|---|-----|-------|--------|-------|-------|-----|---|---|---|---|---|---|--------------|
| 1 | 24 | | 1 1/8 | 3/8 | 1/2 | | 8 1/4 | 3 1/2 | 4 1/2 | 1 3/4 | 2 3/8 | 7 1/4 | 7 | 4 | 1/2 | 2 5/8 | 6 5/8 | 2 1/8 | 4 1/2 | 45° | | | | | | | 4.0 |
| 2S | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 1/4 | 4 3/8 | 5 5/8 | 2 1/4 | 3 | 9 1/8 | 8 3/4 | 5 | 5/8 | 3 3/8 | 8 3/8 | 2 5/8 | 5 3/4 | 45° | | | | | | | 6.25 |
| 2M | 36 | | 1 5/8 | 5/8 | 3/4 | | 12 3/8 | 5 1/4 | 6 3/4 | 2 5/8 | 3 1/2 | 10 7/8 | 10 1/2 | 6 | 3/4 | 4 | 10 | 3 1/4 | 6 7/8 | 45° | | | | | | | 9.0 |
| 3 | 36 | | 1 5/8 | 5/8 | 3/4 | | 12 3/8 | 5 1/4 | 6 3/4 | 2 5/8 | 3 1/2 | 10 7/8 | 10 1/2 | 6 | 3/4 | 4 | 10 | 3 1/4 | 6 7/8 | 45° | | | | | | | 9.0 |
| 4 | 36 | | 1 5/8 | 5/8 | 3/4 | | 12 3/8 | 5 1/4 | 6 3/4 | 2 5/8 | 3 1/2 | 10 7/8 | 10 1/2 | 6 | 3/4 | 4 | 10 | 3 1/4 | 6 7/8 | 45° | | | | | | | 9.0 |
| 5 | 48 | | 2 1/4 | 3/4 | 1 | | 16 1/2 | 7 | 9 | 3 1/2 | 4 5/8 | 14 1/2 | 14 | 8 | 1 | 5 3/8 | 13 1/4 | 4 1/4 | 9 1/8 | 45° | | | | | | | 16.0 |

STANDARD SIGN
W1-10

WISCONSIN DEPT OF TRANSPORTATION

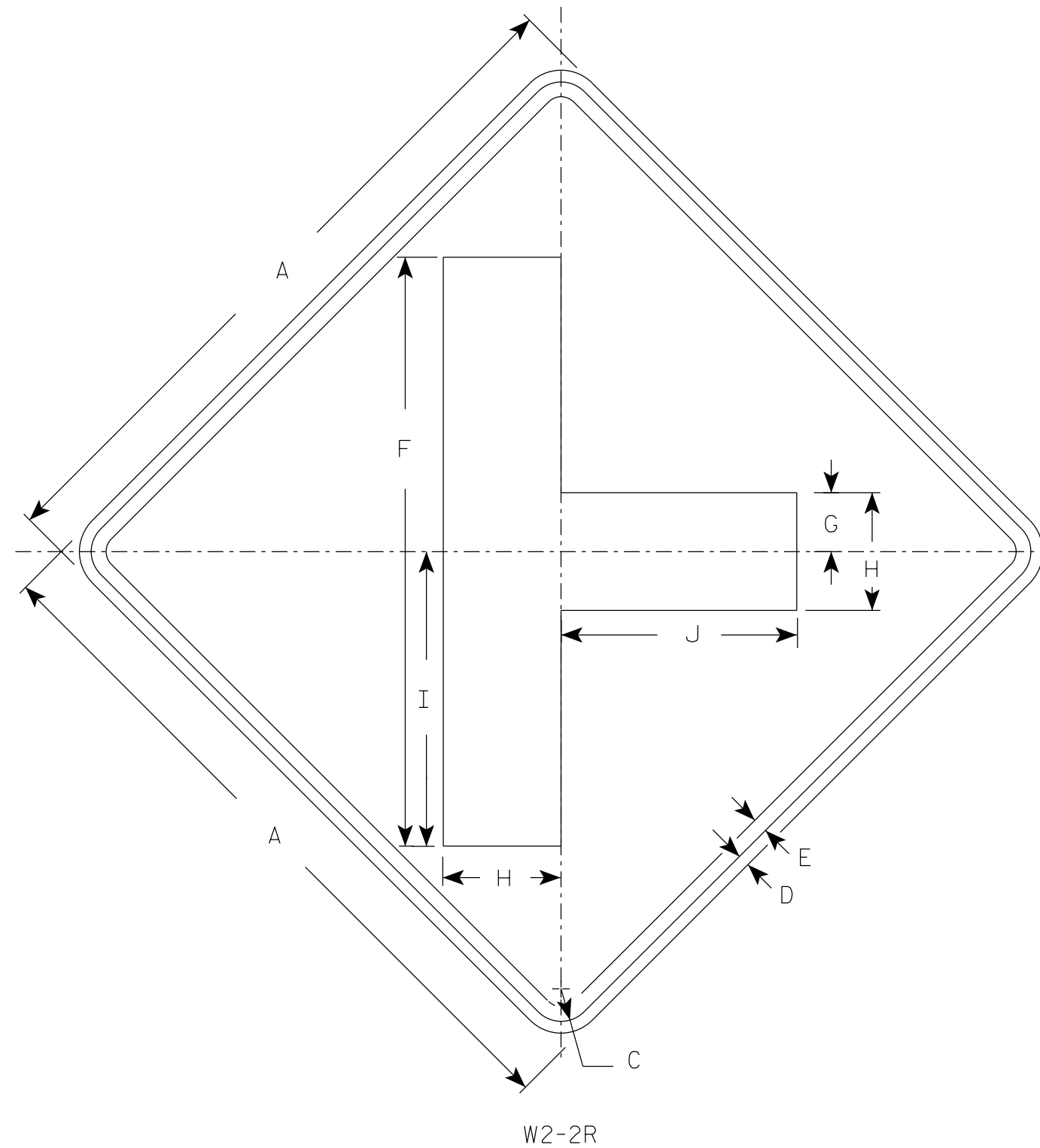
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/17/12 PLATE NO. W1-10.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. W2-2L same as W2-2R but is rotated 180° when mounted.



W2-2R

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|----|-------|---|--------|----|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | 24 | | 1 1/8 | 3/8 | 1/2 | 20 | 2 | 4 | 10 | 8 | | | | | | | | | | | | | | | | | 4.0 |
| 2S | 30 | | 1 3/8 | 1/2 | 5/8 | 25 | 2 1/2 | 5 | 12 1/2 | 10 | | | | | | | | | | | | | | | | | 6.25 |
| 2M | 30 | | 1 3/8 | 1/2 | 5/8 | 25 | 2 1/2 | 5 | 12 1/2 | 10 | | | | | | | | | | | | | | | | | 6.25 |
| 3 | 36 | | 1 5/8 | 5/8 | 3/4 | 30 | 3 | 6 | 15 | 12 | | | | | | | | | | | | | | | | | 9.0 |
| 4 | 48 | | 2 1/4 | 3/4 | 1 | 40 | 4 | 8 | 20 | 16 | | | | | | | | | | | | | | | | | 16.0 |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

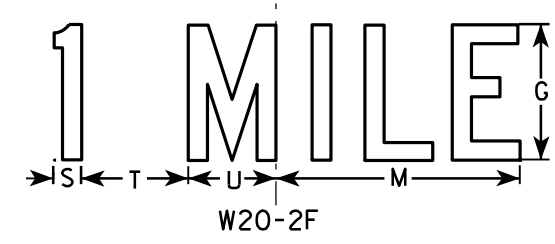
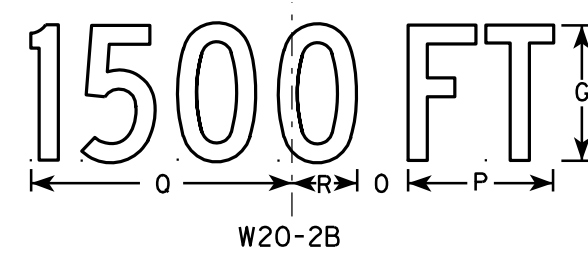
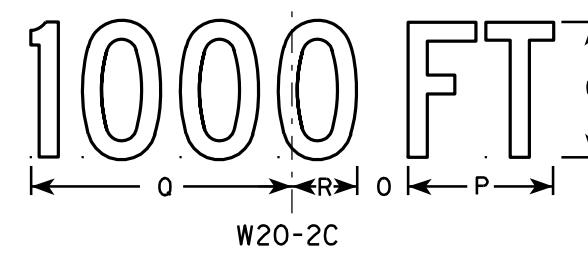
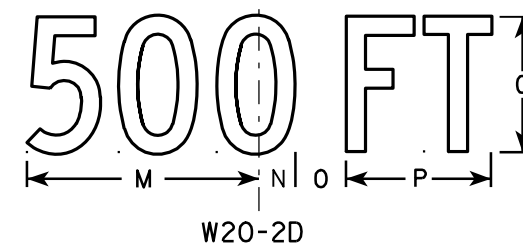
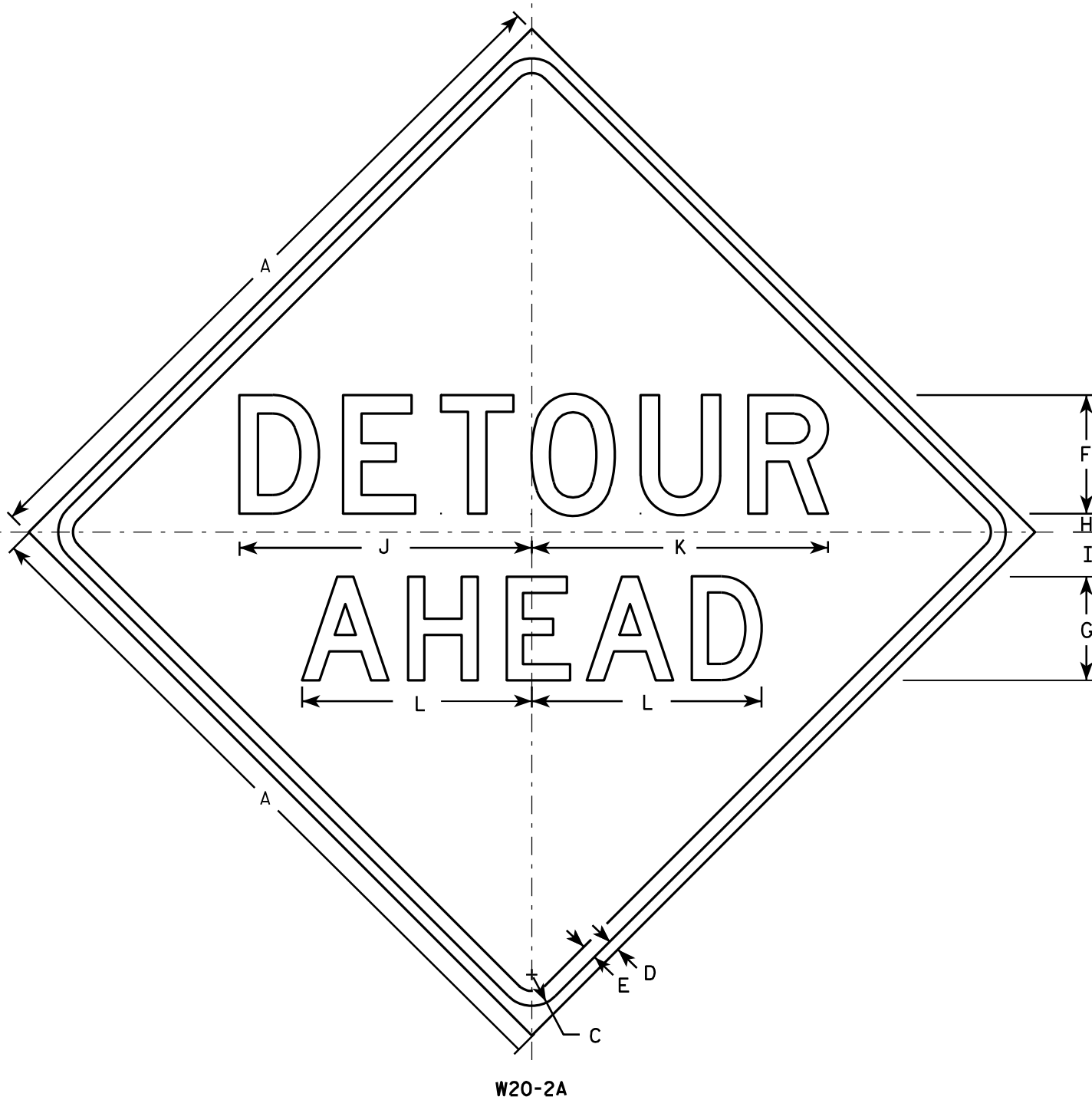
STANDARD SIGN
W2-2 L&R

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R Rauch*
 for State Traffic Engineer
 DATE 11/18/2021 PLATE NO. W2-2.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

7

7



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|---|-------|-------|--------|----|--------|----|-------|-------|-------|--------|-------|-------|-------|-------|--------|-------|--------|---|---|--------------|
| 1 | 36 | | 1 5/8 | 5/8 | 3/4 | 6 | 5 | 1 | 2 1/4 | 14 3/4 | 15 | 11 5/8 | 9 | 1 3/8 | 1 7/8 | 5 5/8 | 10 1/8 | 2 1/2 | 1 1/8 | 4 1/2 | 3 1/2 | 8 | 1 3/4 | 10 3/4 | | | 9.0 |
| 2S | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 2M | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 3 | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 4 | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 5 | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |

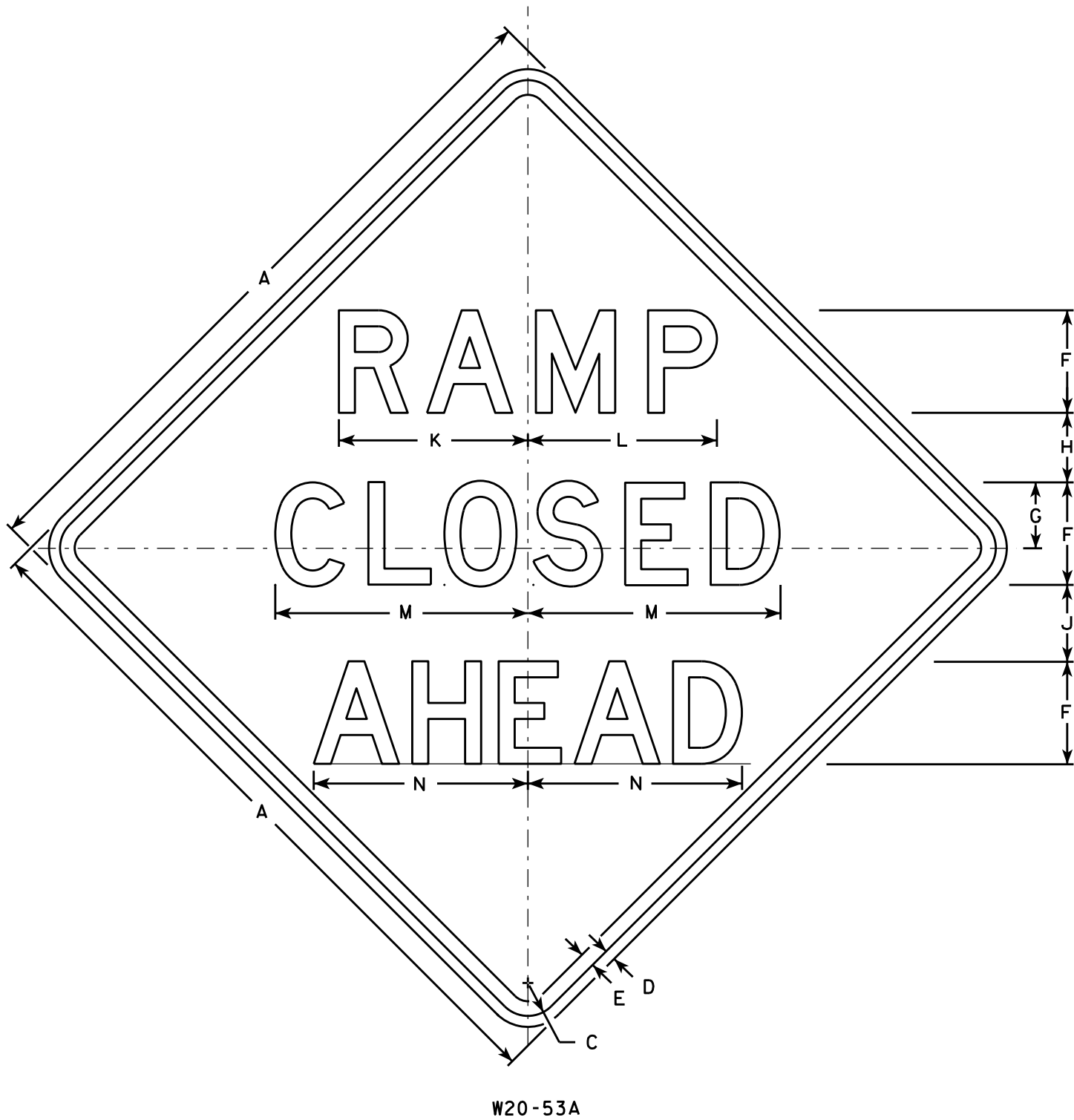
STANDARD SIGN
W20-2A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

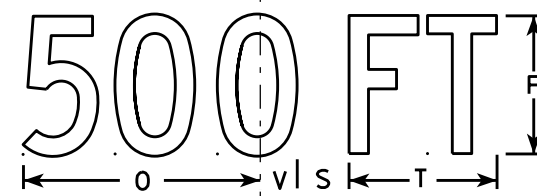
APPROVED *Matthew R. Raub*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

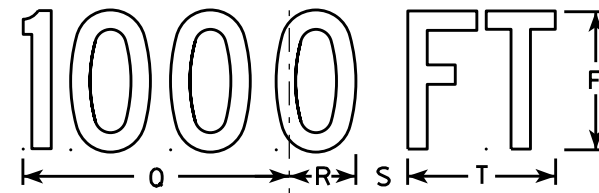
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



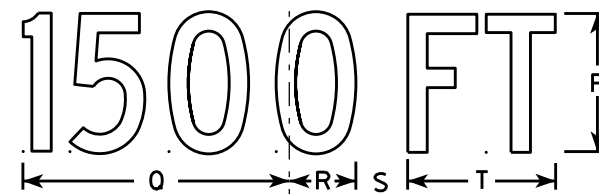
W20-53A



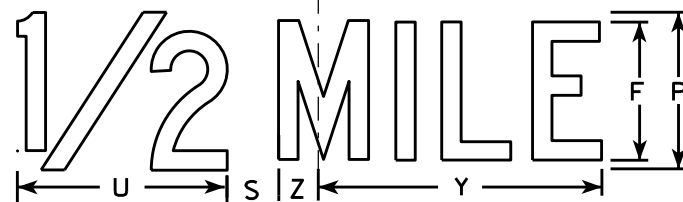
W20-53D



W20-53C



W20-53B



W20-53G



W20-53F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | O | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|-------|-------|-------|-------|--------|--------|--------|--------|----|---|--------|-------|-------|-------|--------|-------|-------|-------|--------|-------|--------------|
| 1 | 36 | | 1 5/8 | 5/8 | 3/4 | 5 | 3 3/8 | 3 1/2 | 1 1/8 | 4 | 9 1/4 | 9 1/4 | 12 1/2 | 11 | 9 | 6 | 10 1/8 | 2 1/2 | 1 7/8 | 5 5/8 | 8 | 1 3/8 | 4 1/2 | 3 1/2 | 10 3/4 | 1 3/4 | 9.0 |
| 2S | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 12 7/8 | 12 7/8 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 2M | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 12 7/8 | 12 7/8 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 3 | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 12 7/8 | 12 7/8 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 4 | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 12 7/8 | 12 7/8 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 5 | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 12 7/8 | 12 7/8 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |

STANDARD SIGN
W20-53A,B,C,D,F,G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/27/15 PLATE NO. W20-53.1

Volume Report

Project: C:\Wisdot\Std\temp\Crdr_B & Morgan_1_20321_f9328603.sv\$

Alignment: Alignment - CTH B

Sample Line Group: SLG-7

Start Sta: 100+75.000

End Sta: 107+50.000

| Station | Cut Area (Sq.ft.) | Cut Volume (Cu.yd.) | Reusable Volume (Cu.yd.) | Fill Area (Sq.ft.) | Fill Volume (Cu.yd.) | Cum. Cut Vol. (Cu.yd.) | Cum. Reusable Vol. (Cu.yd.) | Cum. Fill Vol. (Cu.yd.) | Cum. Net Vol. (Cu.yd.) |
|------------|-------------------|---------------------|--------------------------|--------------------|----------------------|------------------------|-----------------------------|-------------------------|------------------------|
| 100+75.000 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 |
| 101+00.000 | 2.76 | 1.28 | 1.28 | 3.83 | 1.77 | 1.28 | 1.28 | 1.77 | -0.50 |
| 101+25.000 | 5.93 | 4.02 | 4.02 | 1.32 | 2.38 | 5.30 | 5.30 | 4.16 | 1.14 |
| 101+50.000 | 11.52 | 8.08 | 8.08 | 0.03 | 0.63 | 13.38 | 13.38 | 4.78 | 8.60 |
| 101+75.000 | 14.57 | 12.08 | 12.08 | 1.71 | 0.80 | 25.46 | 25.46 | 5.59 | 19.87 |
| 102+00.000 | 17.23 | 14.72 | 14.72 | 3.29 | 2.31 | 40.18 | 40.18 | 7.90 | 32.28 |
| 102+25.000 | 19.24 | 16.88 | 16.88 | 5.49 | 4.07 | 57.06 | 57.06 | 11.97 | 45.10 |
| 102+50.000 | 16.57 | 16.58 | 16.58 | 8.40 | 6.43 | 73.64 | 73.64 | 18.40 | 55.24 |
| 102+75.000 | 15.93 | 15.04 | 15.04 | 7.07 | 7.16 | 88.69 | 88.69 | 25.56 | 63.13 |
| 103+00.000 | 21.91 | 17.52 | 17.52 | 3.04 | 4.68 | 106.20 | 106.20 | 30.24 | 75.96 |
| 103+25.000 | 62.73 | 39.18 | 39.18 | 0.00 | 1.41 | 145.39 | 145.39 | 31.65 | 113.74 |
| 103+50.000 | 106.33 | 78.27 | 78.27 | 0.00 | 0.00 | 223.65 | 223.65 | 31.65 | 192.00 |
| 103+75.000 | 132.44 | 110.54 | 110.54 | 0.00 | 0.00 | 334.19 | 334.19 | 31.65 | 302.54 |
| 104+00.000 | 92.84 | 104.29 | 104.29 | 8.97 | 4.15 | 438.49 | 438.49 | 35.80 | 402.68 |
| 104+25.000 | 42.89 | 62.84 | 62.84 | 0.00 | 4.15 | 501.32 | 501.32 | 39.96 | 461.37 |
| 104+50.000 | 16.52 | 27.51 | 27.51 | 0.89 | 0.41 | 528.83 | 528.83 | 40.37 | 488.46 |
| 104+75.000 | 4.19 | 9.59 | 9.59 | 8.87 | 4.52 | 538.42 | 538.42 | 44.89 | 493.53 |
| 105+00.000 | 3.17 | 3.41 | 3.41 | 21.65 | 14.13 | 541.83 | 541.83 | 59.02 | 482.81 |
| 105+25.000 | 2.91 | 2.82 | 2.82 | 10.02 | 14.66 | 544.64 | 544.64 | 73.68 | 470.96 |
| 105+50.000 | 5.54 | 3.92 | 3.92 | 15.91 | 12.01 | 548.56 | 548.56 | 85.68 | 462.87 |
| 105+75.000 | 4.88 | 4.82 | 4.82 | 22.47 | 17.77 | 553.38 | 553.38 | 103.46 | 449.93 |
| 106+00.000 | 0.49 | 2.49 | 2.49 | 34.49 | 26.37 | 555.87 | 555.87 | 129.83 | 426.04 |
| 106+25.000 | 1.30 | 0.83 | 0.83 | 38.23 | 33.67 | 556.70 | 556.70 | 163.50 | 393.20 |
| 106+50.000 | 7.91 | 4.26 | 4.26 | 37.59 | 35.10 | 560.96 | 560.96 | 198.60 | 362.36 |
| 106+75.000 | 18.54 | 12.24 | 12.24 | 19.18 | 26.28 | 573.20 | 573.20 | 224.89 | 348.32 |
| 107+00.000 | 20.06 | 17.87 | 17.87 | 17.32 | 16.90 | 591.07 | 591.07 | 241.79 | 349.28 |
| 107+25.000 | 11.49 | 14.60 | 14.60 | 22.69 | 18.52 | 605.68 | 605.68 | 260.31 | 345.37 |
| 107+31.622 | 8.13 | 2.41 | 2.41 | 19.18 | 5.13 | 608.08 | 608.08 | 265.44 | 342.64 |
| 107+50.000 | 0.00 | 2.77 | 2.77 | 0.00 | 6.53 | 610.85 | 610.85 | 271.97 | 338.87 |

Volume Report

Project: C:\Wisdot\Std\temp\Crdr_B & Morgan_1_20321_f9328603.sv\$

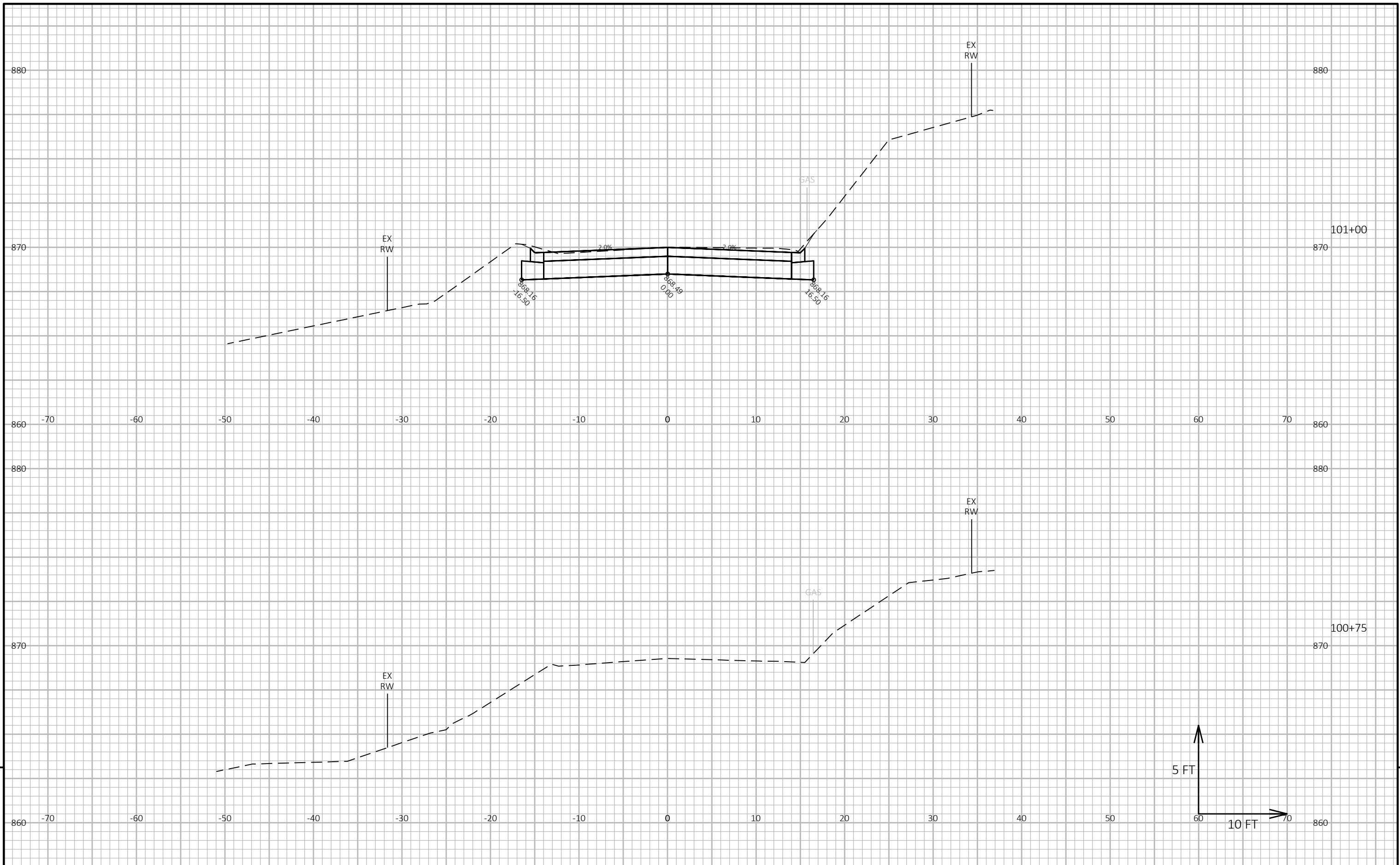
Alignment: Alignment - Morgan 2

Sample Line Group: SLG- Morgan

Start Sta: 10+25.000

End Sta: 12+00.000

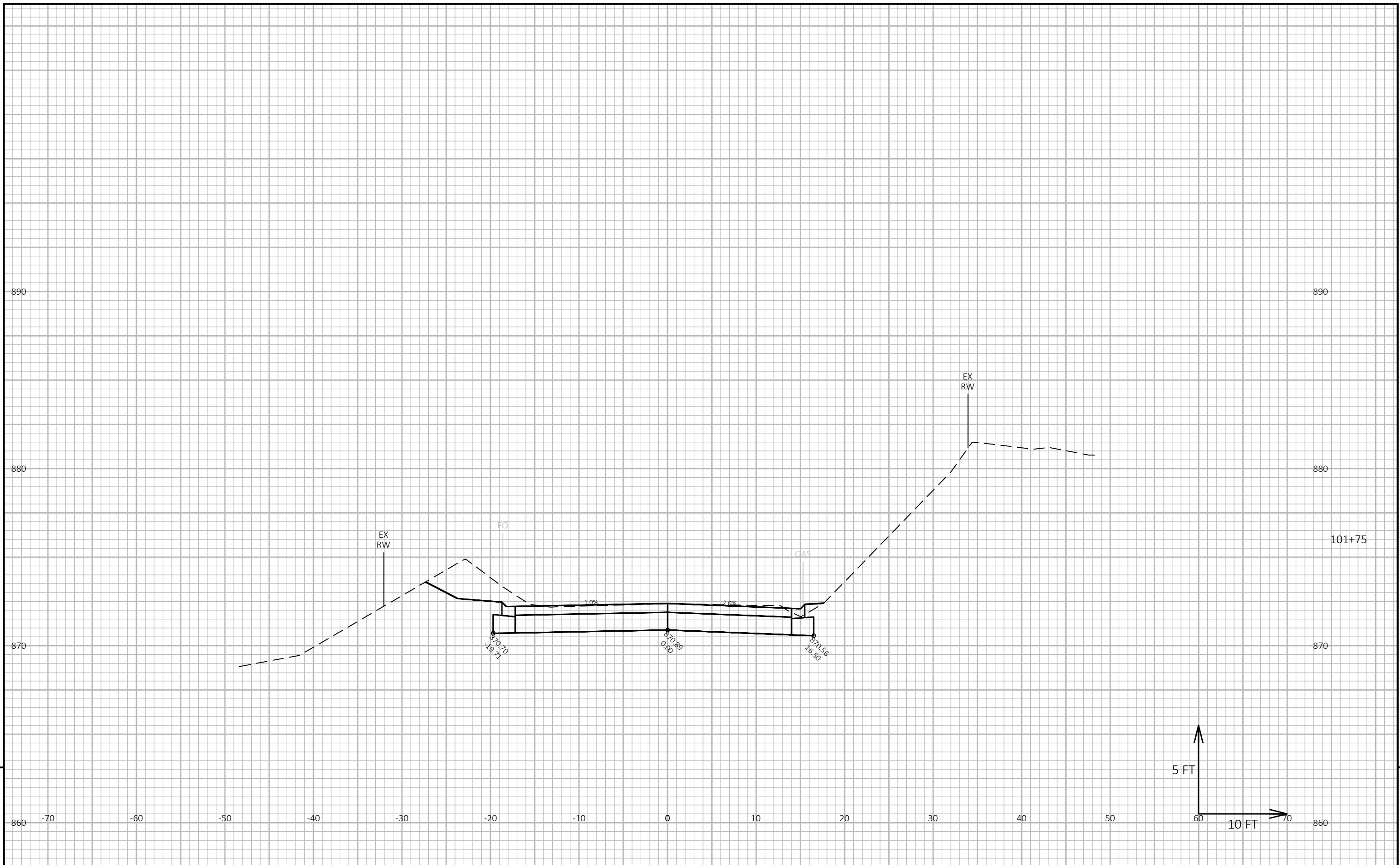
| Station | Cut Area (Sq.ft.) | Cut Volume (Cu.yd.) | Reusable Volume (Cu.yd.) | Fill Area (Sq.ft.) | Fill Volume (Cu.yd.) | Cum. Cut Vol. (Cu.yd.) | Cum. Reusable Vol. (Cu.yd.) | Cum. Fill Vol. (Cu.yd.) | Cum. Net Vol. (Cu.yd.) |
|-----------|-------------------|---------------------|--------------------------|--------------------|----------------------|------------------------|-----------------------------|-------------------------|------------------------|
| 10+25.000 | 34.64 | 0.00 | 0.00 | 20.73 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 |
| 10+50.000 | 8.65 | 20.04 | 20.04 | 77.80 | 45.62 | 20.04 | 20.04 | 45.62 | -25.57 |
| 10+75.000 | 3.65 | 5.69 | 5.69 | 71.29 | 69.02 | 25.73 | 25.73 | 114.64 | -88.90 |
| 11+00.000 | 4.65 | 3.84 | 3.84 | 34.98 | 49.20 | 29.58 | 29.58 | 163.84 | -134.26 |
| 11+25.000 | 0.76 | 2.51 | 2.51 | 10.96 | 21.27 | 32.08 | 32.08 | 185.11 | -153.03 |
| 11+50.000 | 9.76 | 4.87 | 4.87 | 0.74 | 5.42 | 36.96 | 36.96 | 190.53 | -153.57 |
| 11+75.000 | 13.50 | 10.77 | 10.77 | 1.23 | 0.91 | 47.73 | 47.73 | 191.44 | -143.71 |
| 12+00.000 | 0.00 | 6.25 | 6.25 | 0.00 | 0.57 | 53.98 | 53.98 | 192.02 | -138.04 |



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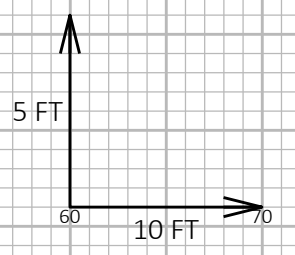
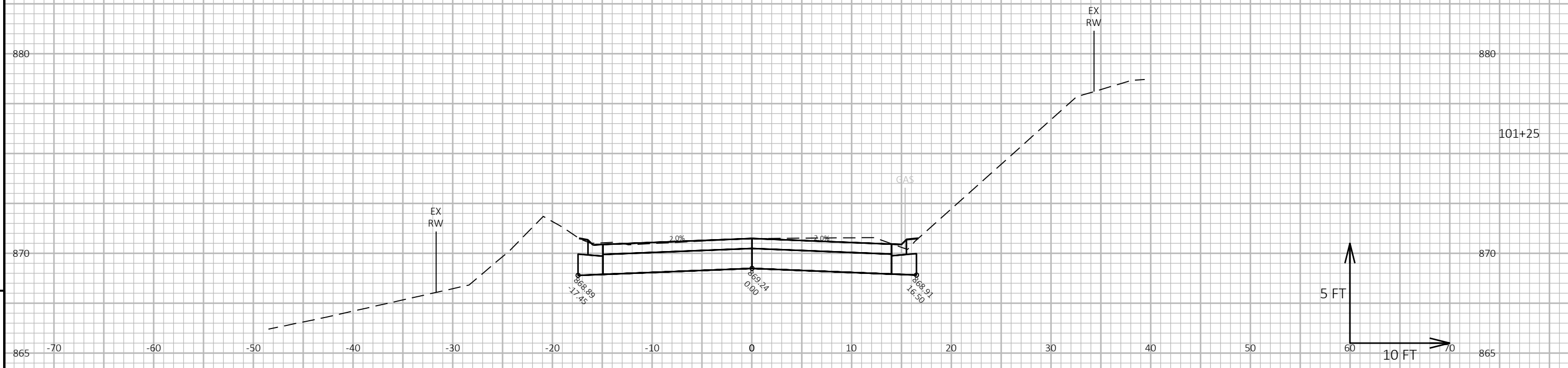
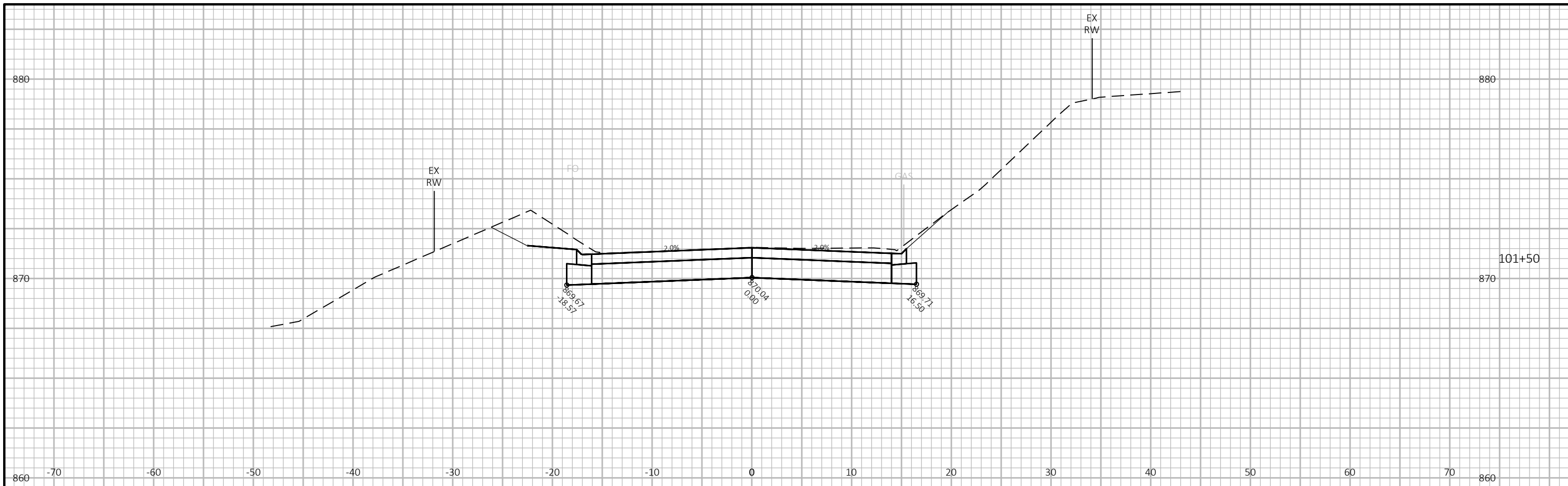
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|------------------------|------------|-------------------------|-----------------------|----------|---|
| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 27 | E |
|------------------------|------------|-------------------------|-----------------------|----------|---|



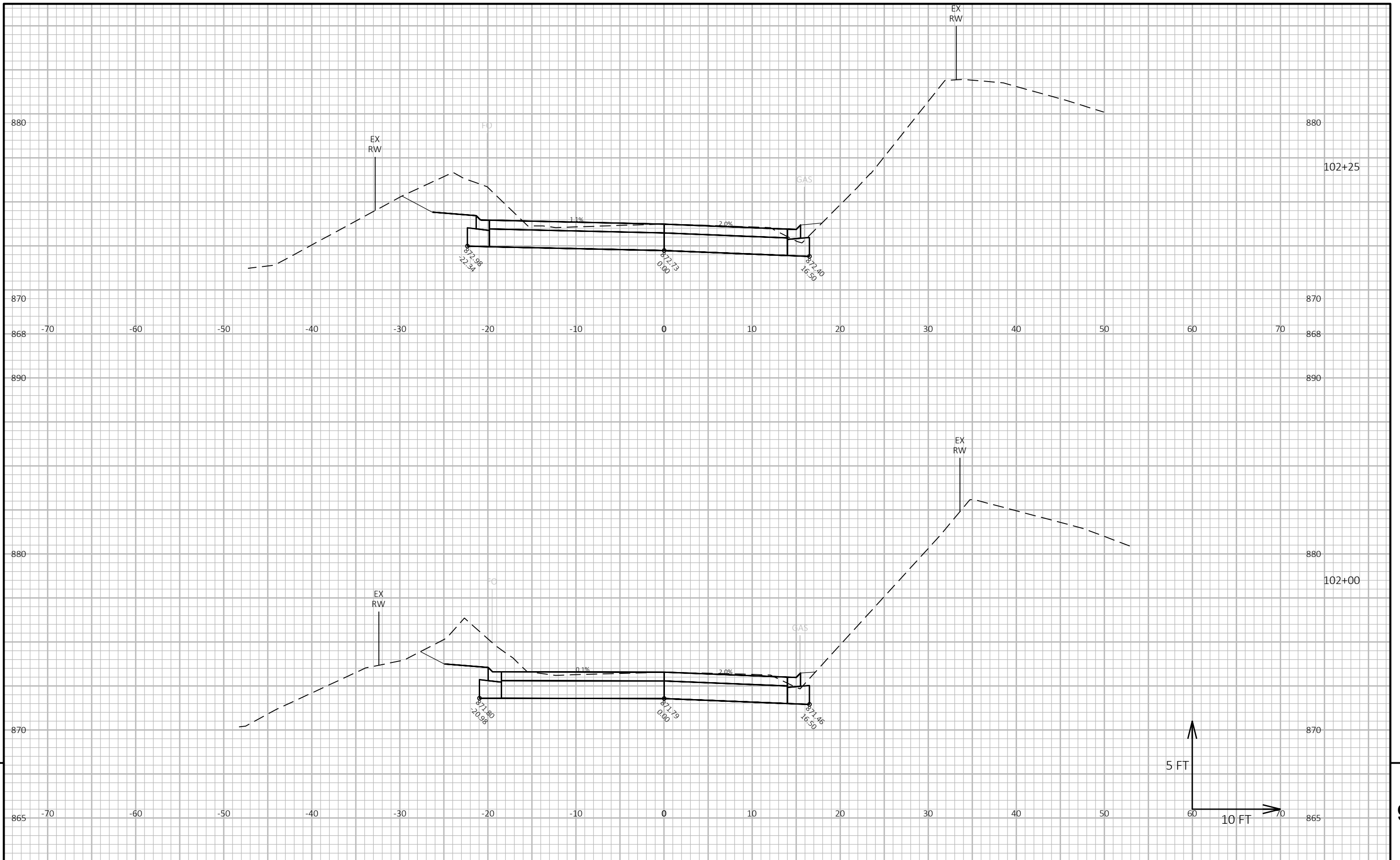
9

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|------------------------|------------|-------------------------|-----------------------|----------|---|
| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 28 | E |
|------------------------|------------|-------------------------|-----------------------|----------|---|



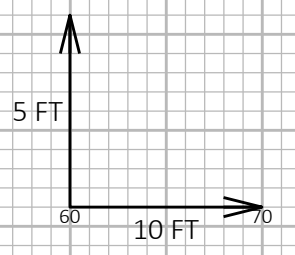
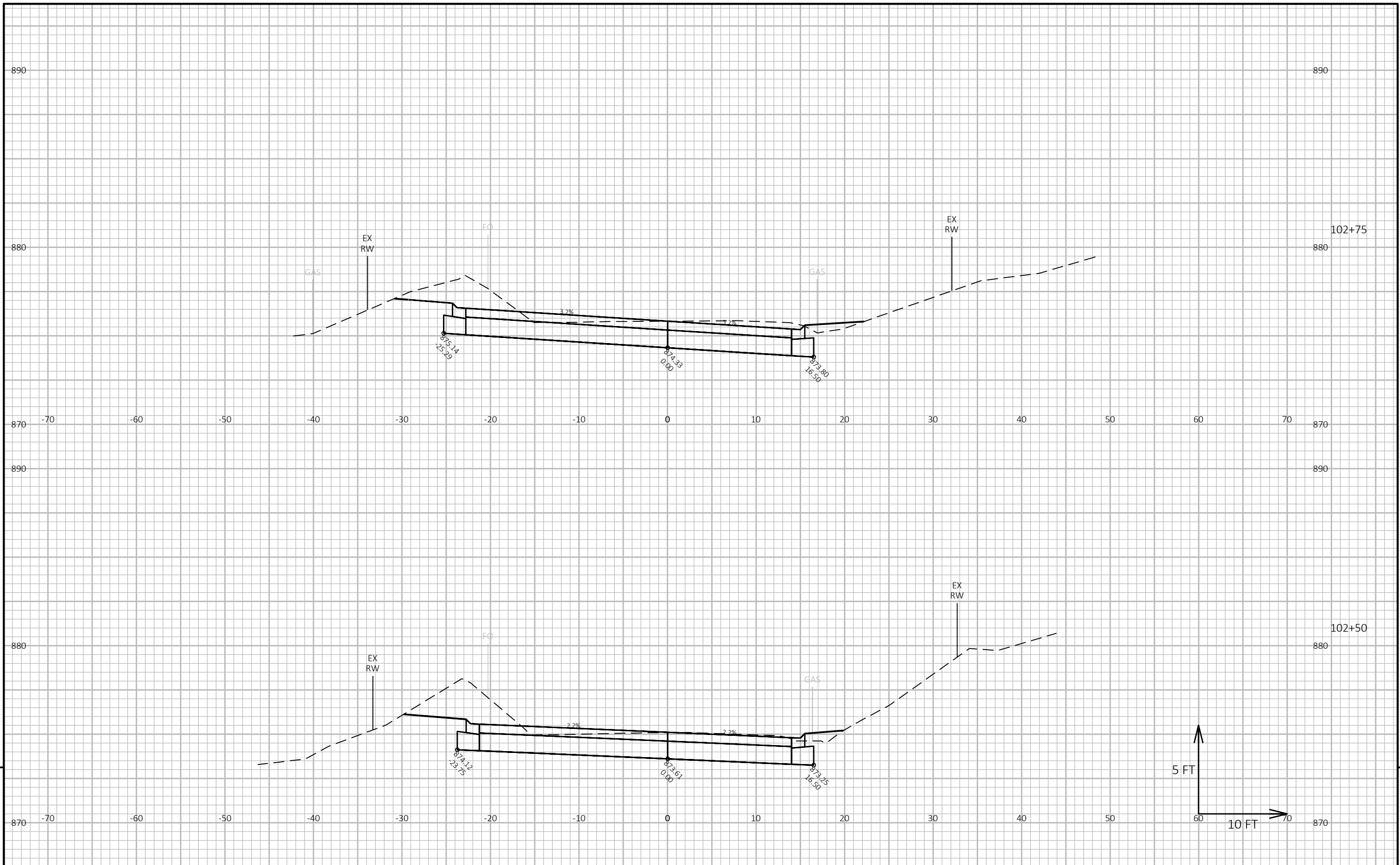
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|------------------------|------------|-------------------------|-----------------------|----------|---|
| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 29 | E |
|------------------------|------------|-------------------------|-----------------------|----------|---|

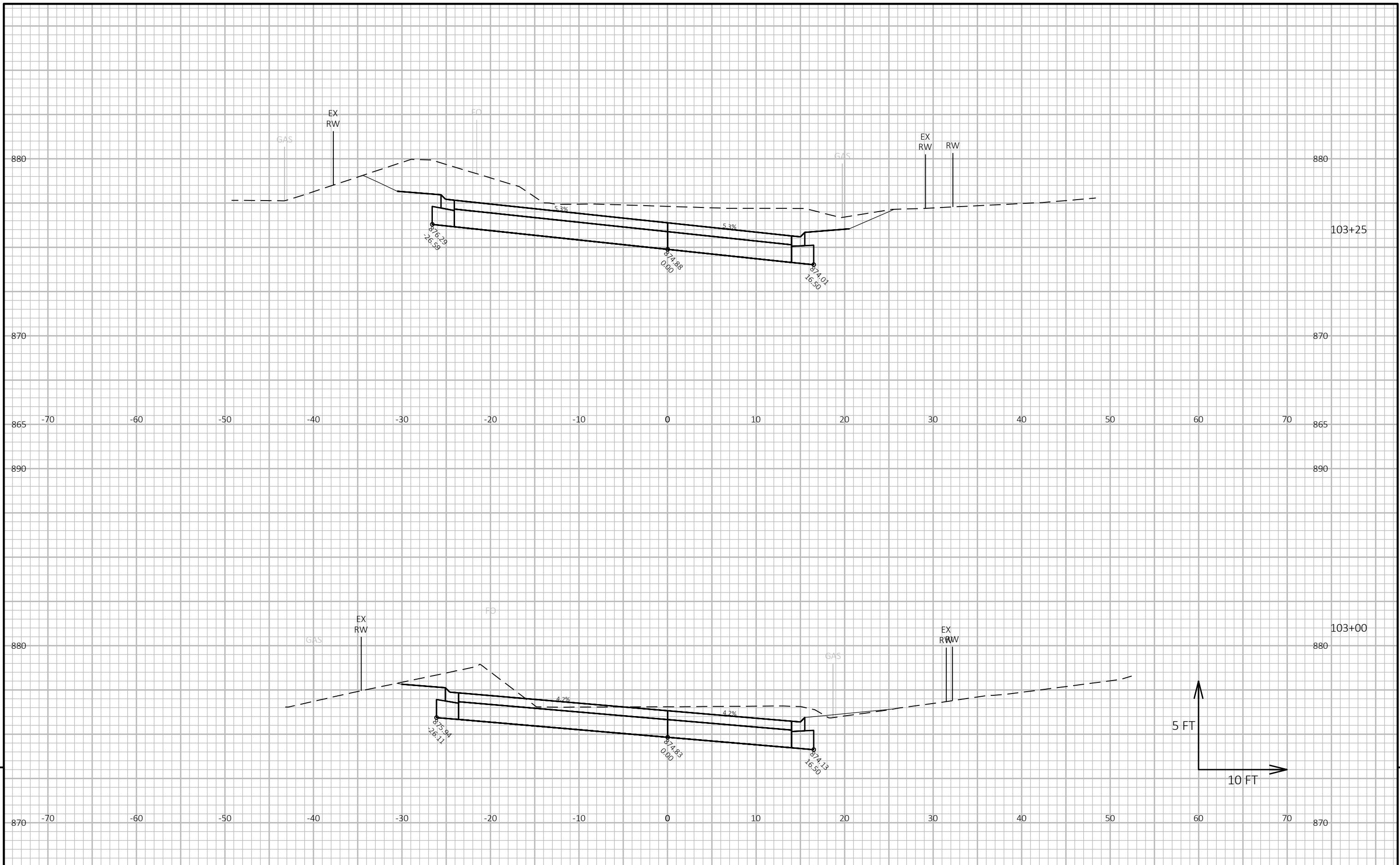


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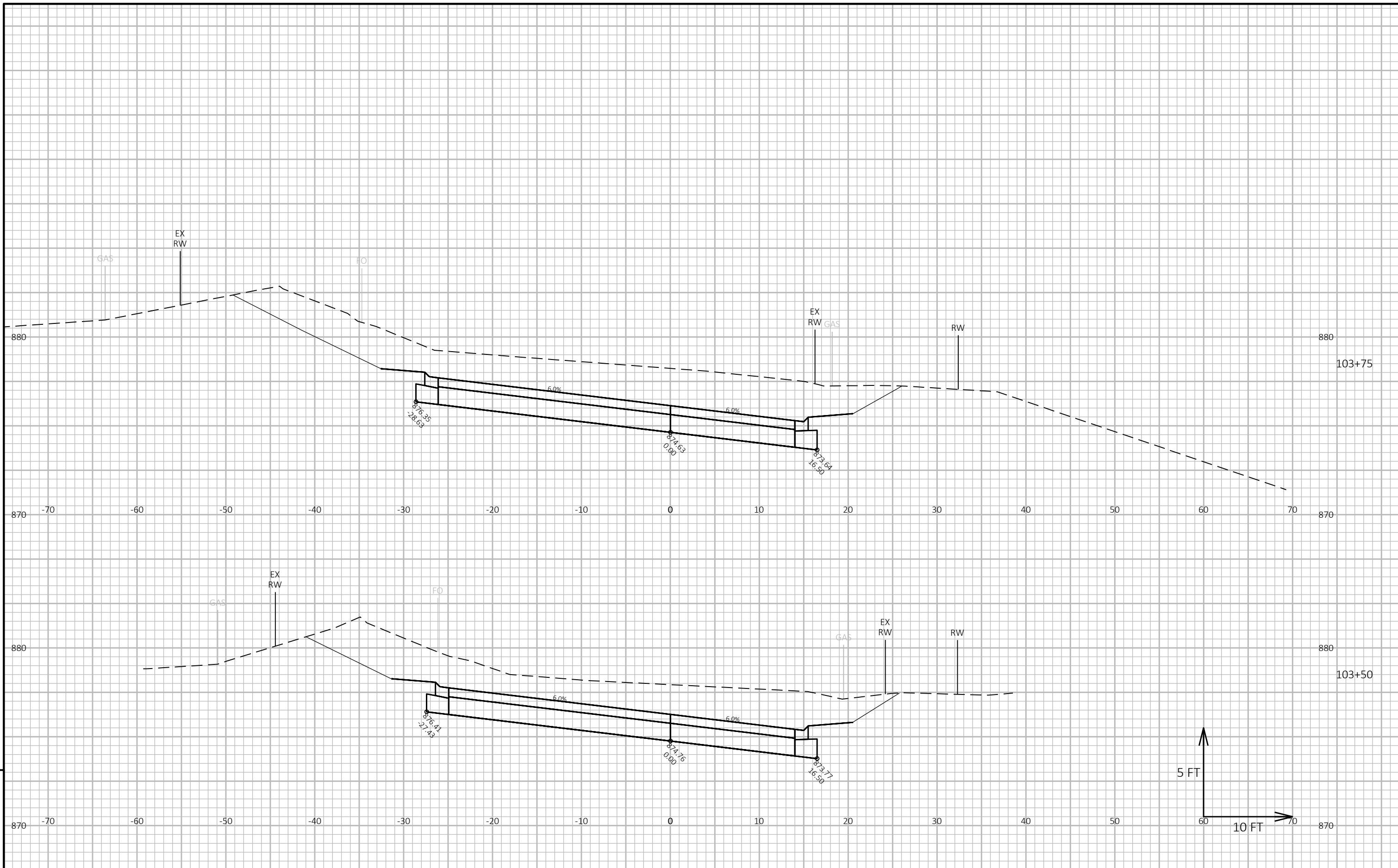
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| | | | | | |
|------------------------|------------|-------------------------|-----------------------|----------|---|
| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 30 | E |
|------------------------|------------|-------------------------|-----------------------|----------|---|





PROJECT NO: 3779-03-70 HWY: CTH B COUNTY: WAUKESHA COUNTY CROSS SECTIONS: CTH B SHEET 32 E



PROJECT NO: 3779-03-70

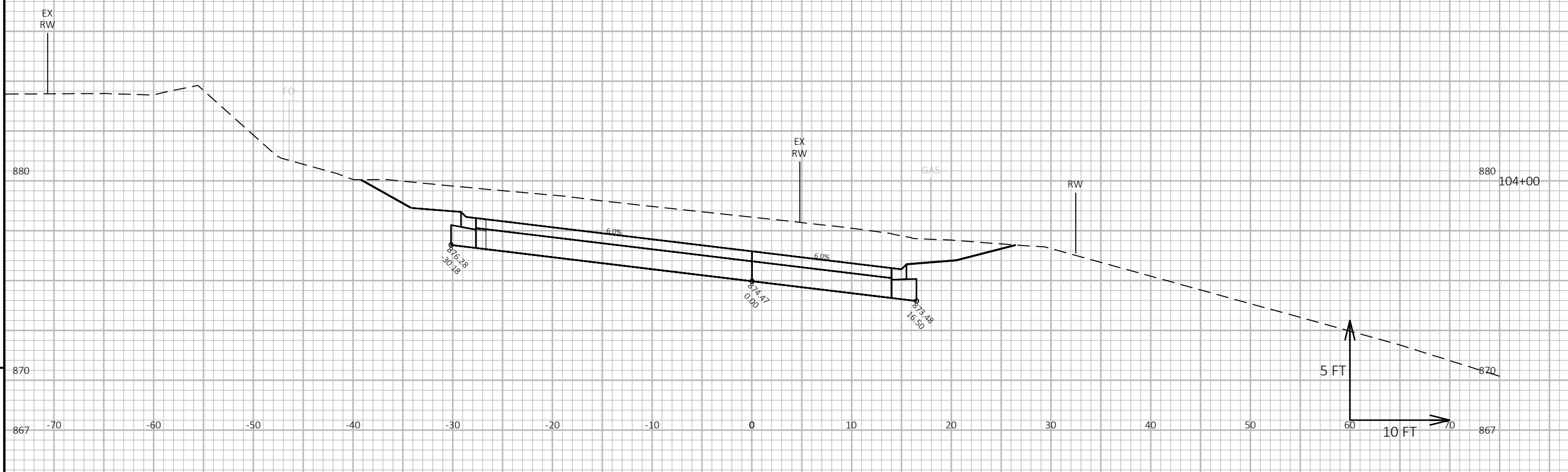
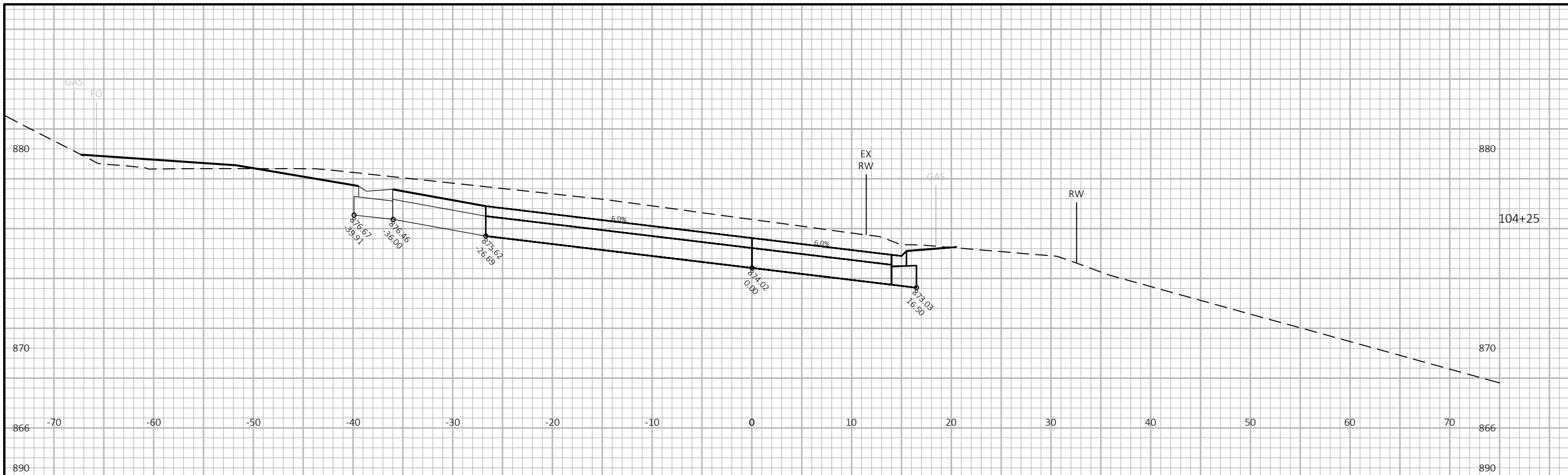
HWY: CTH B

COUNTY: WAUKESHA COUNTY

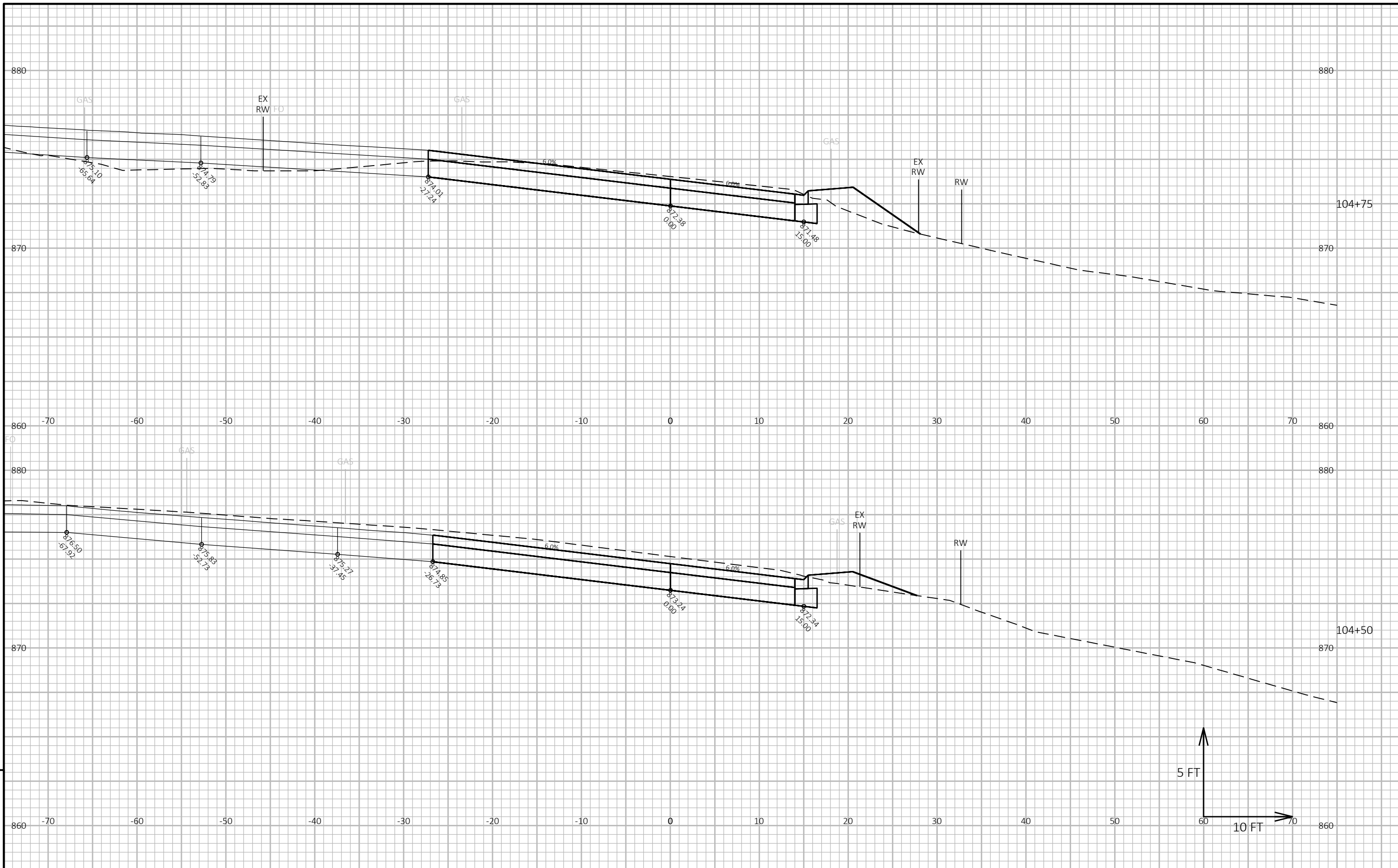
CROSS SECTIONS: CTH B

SHEET 33

E



| | | | | | |
|------------------------|------------|-------------------------|-----------------------|----------|---|
| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 34 | E |
|------------------------|------------|-------------------------|-----------------------|----------|---|

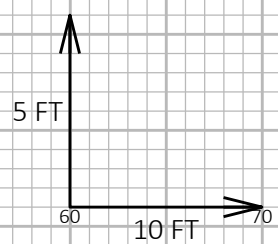


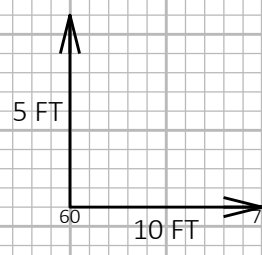
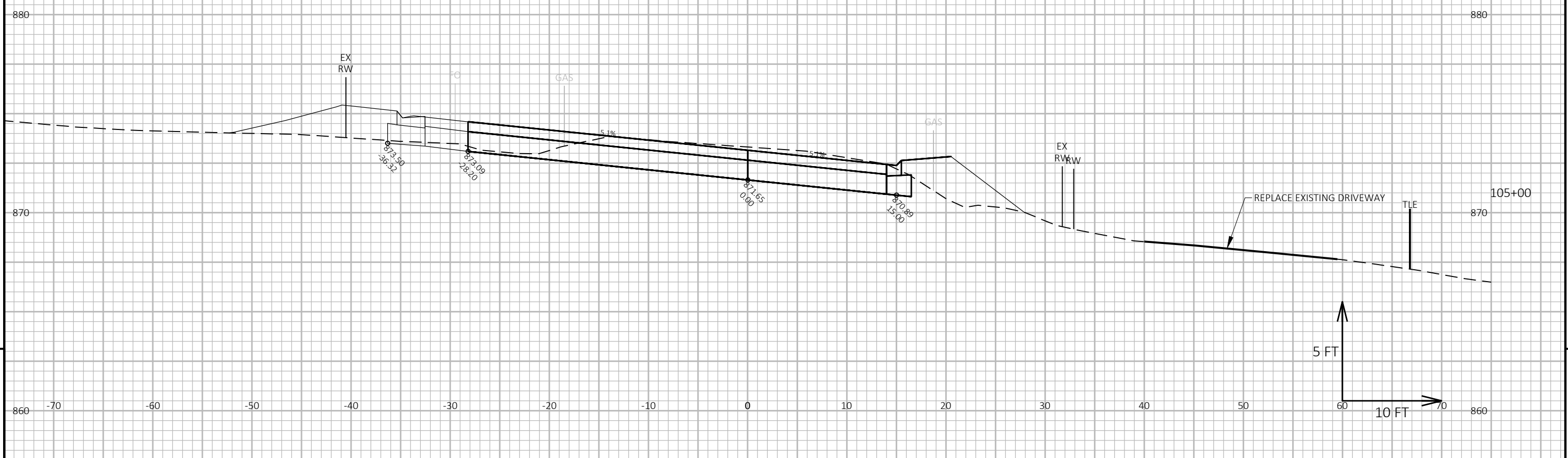
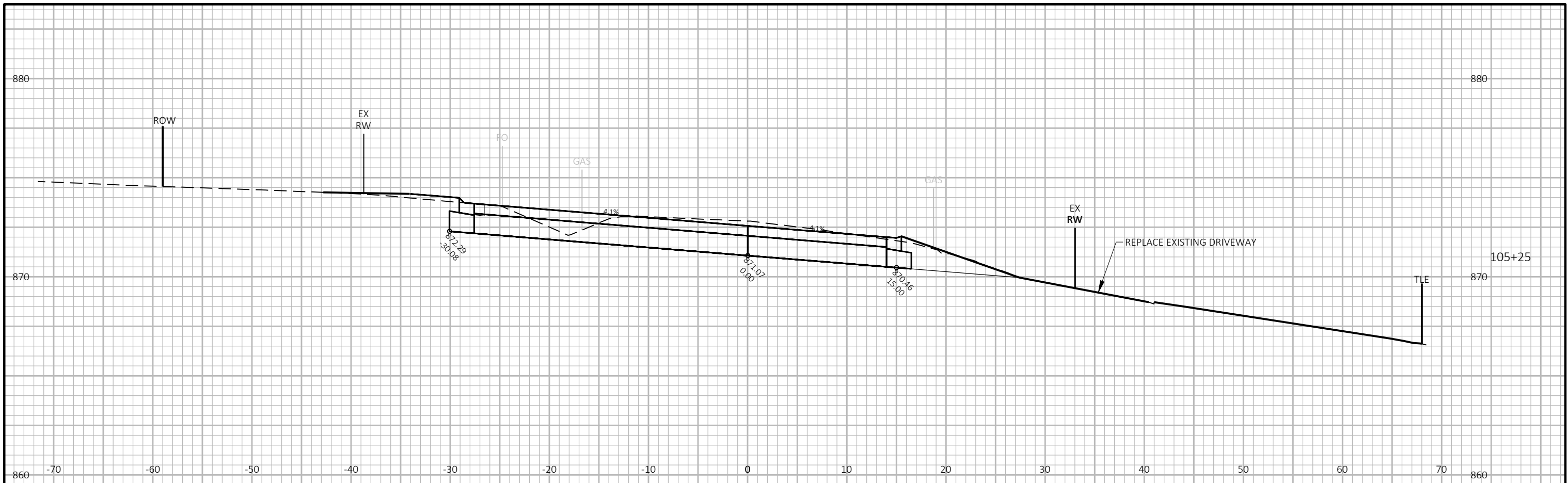
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|------------------------|------------|-------------------------|-----------------------|----------|---|
| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 35 | E |
|------------------------|------------|-------------------------|-----------------------|----------|---|

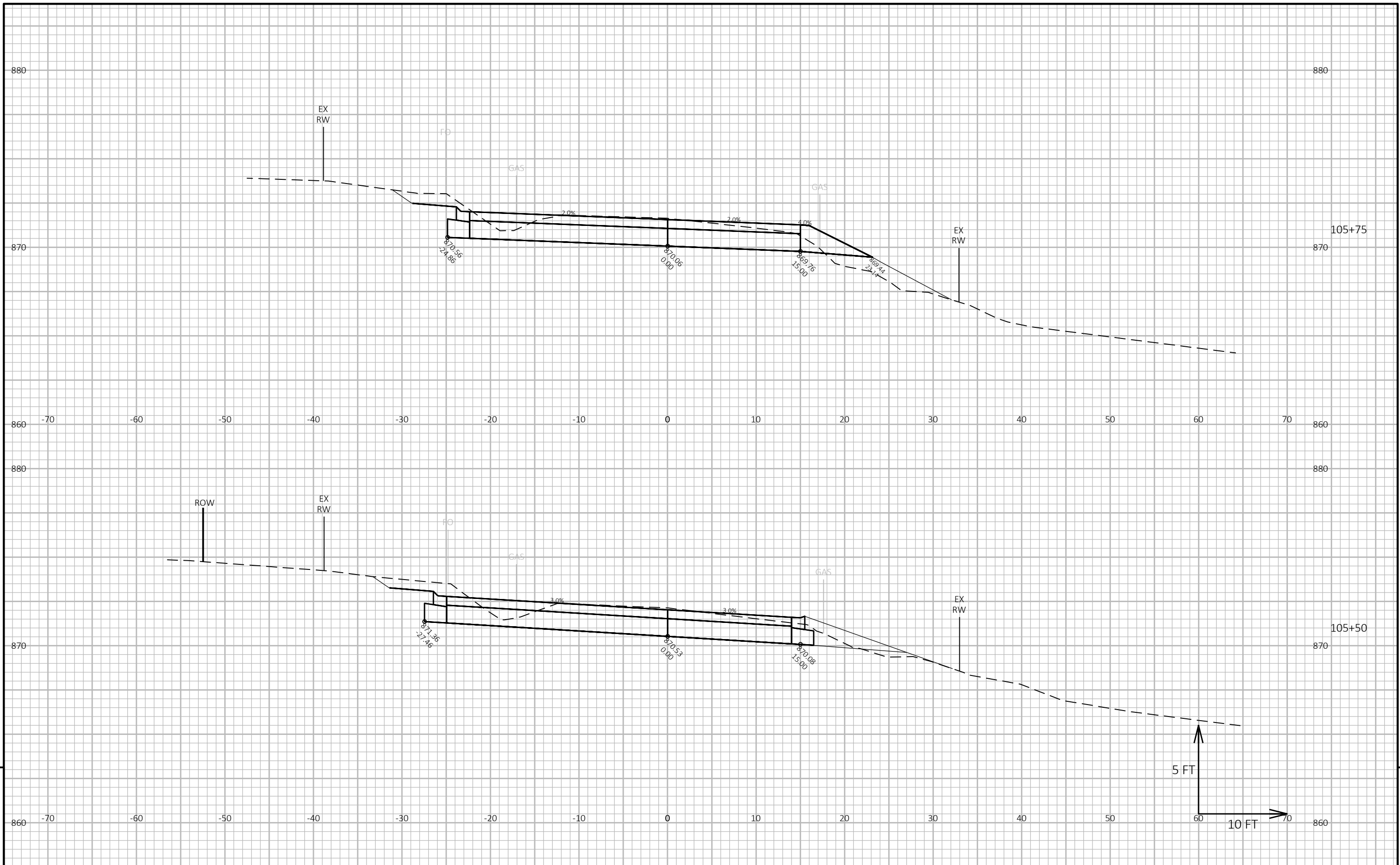
FILE NAME : N:\DPW\ENGINEER\PROJECTS\B AT MORGAN RD HSIP 3779-03-00 & -70\ACAD\DESIGN\MODEL\CRDR_B & MORGAN.DWG PLOT DATE : 5/23/2022 12:48 PM PLOT BY : NATHAN BETH PLOT NAME : PLOT SCALE : 1 IN:20 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49





9 860 -70 -60 -50 -40 -30 -20 -10 0 10 20 30 40 50 60 70 860 9

| | | | | | |
|------------------------|------------|-------------------------|-----------------------|----------|---|
| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 36 | E |
|------------------------|------------|-------------------------|-----------------------|----------|---|



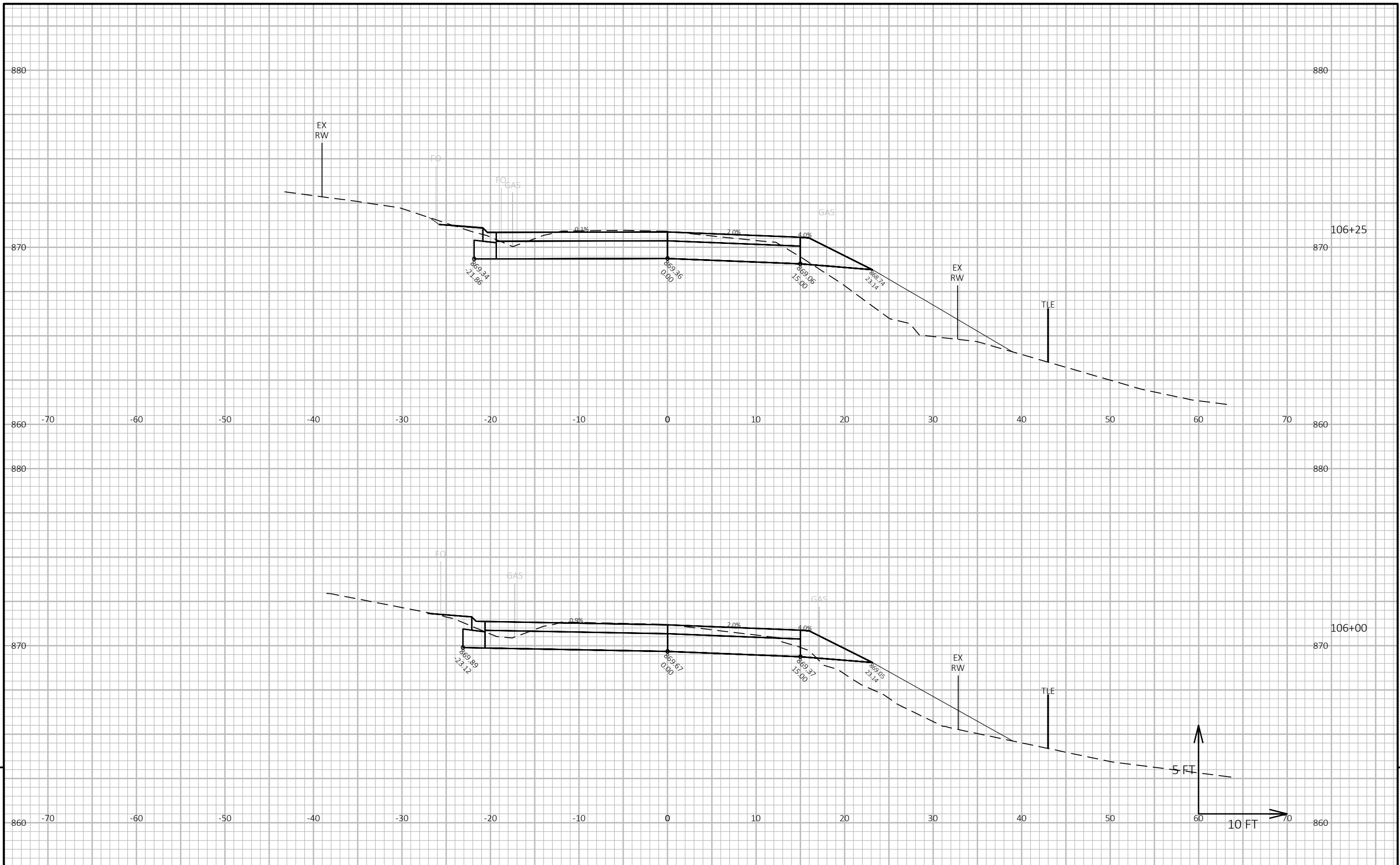
PROJECT NO: 3779-03-70 HWY: CTH B COUNTY: WAUKESHA COUNTY CROSS SECTIONS: CTH B SHEET 37

FILE NAME: N:\DPW\ENGINEER\PROJECTS\B AT MORGAN RD HSIP 3779-03-00 & -70\ACAD\DESIGN\MODEL\CRDR_B & MORGAN.DWG PLOT DATE: 5/23/2022 12:48 PM PLOT BY: NATHAN BETH PLOT NAME: PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

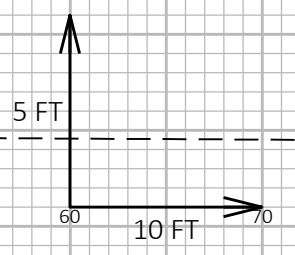
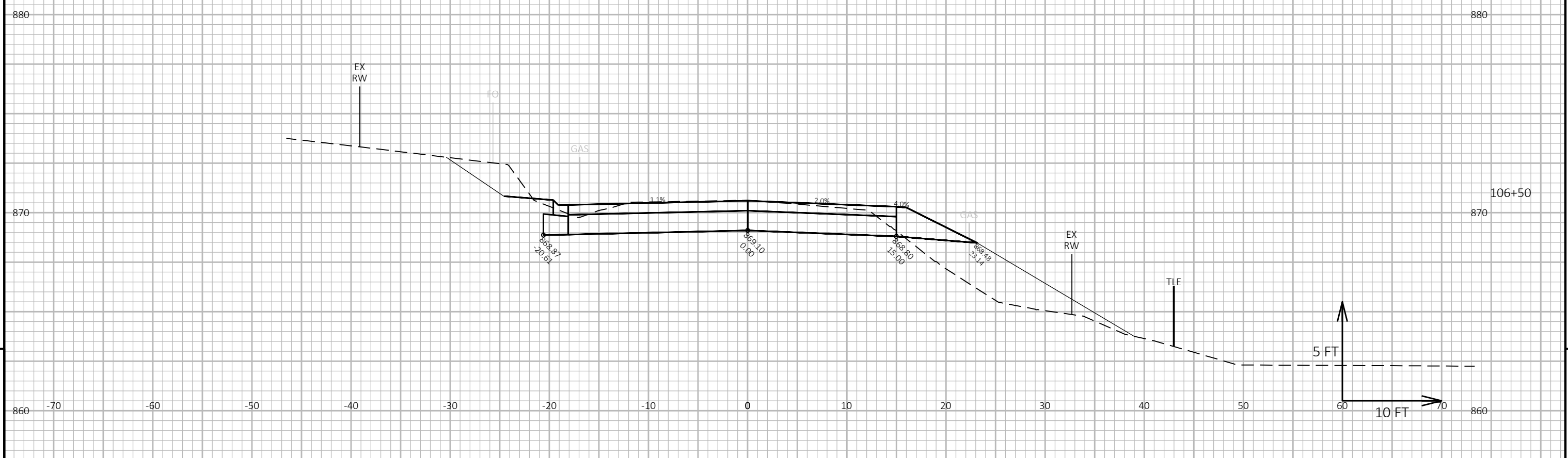
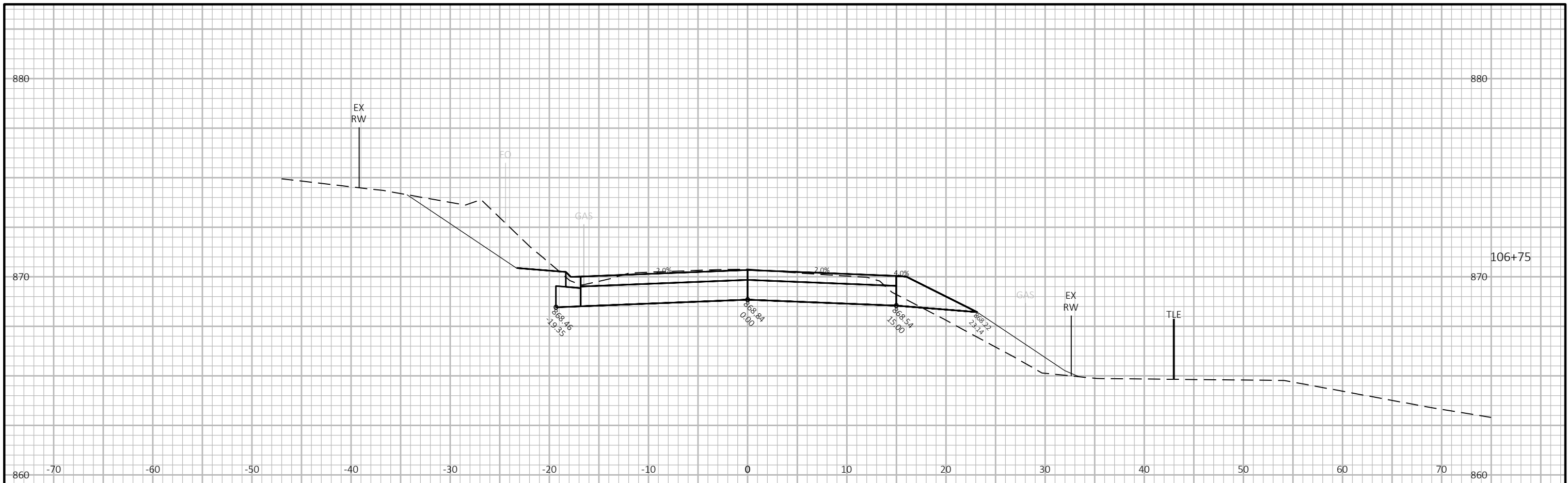
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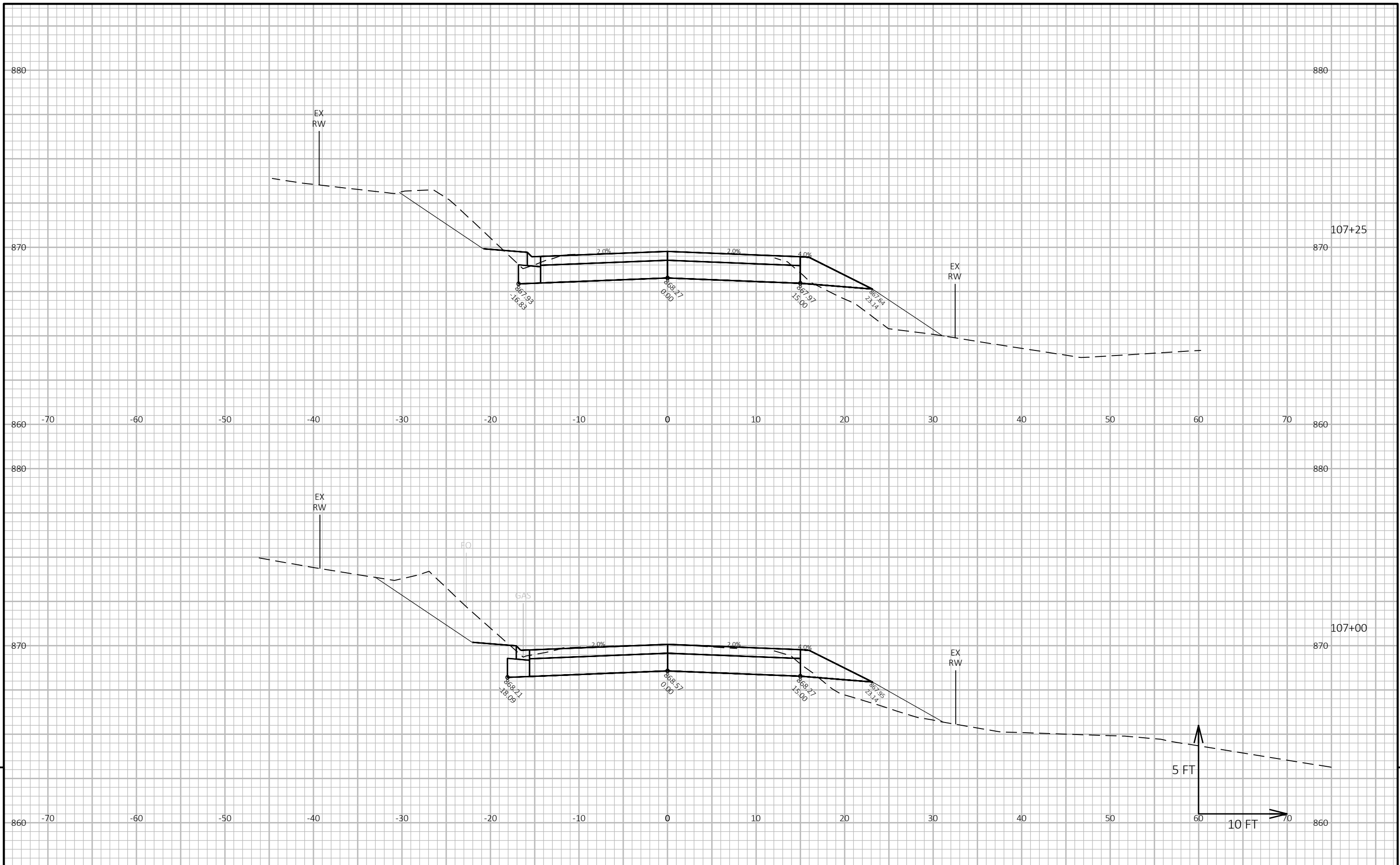
E



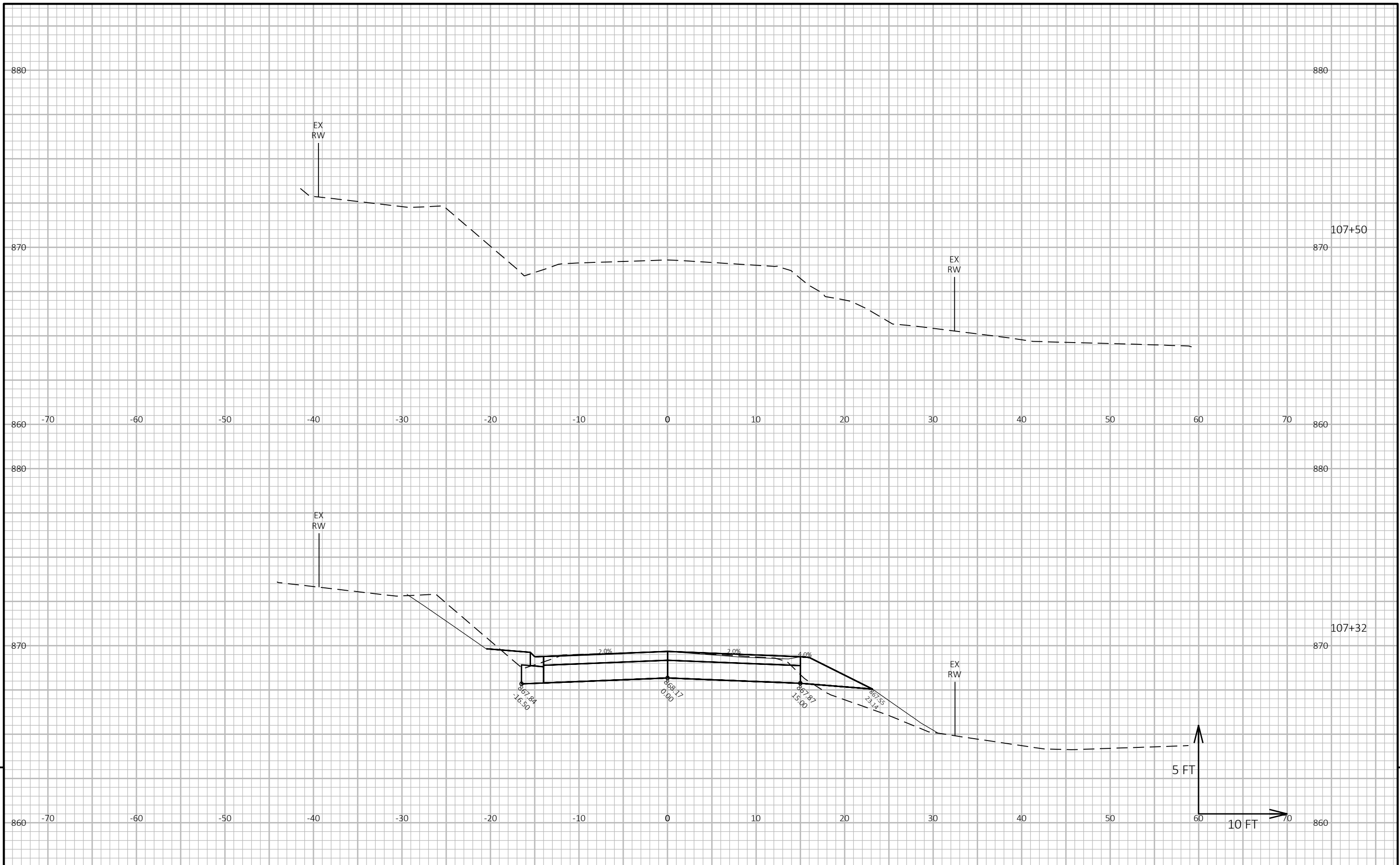
PROJECT NO: 3779-03-70 HWY: CTH B COUNTY: WAUKESHA COUNTY CROSS SECTIONS: CTH B SHEET 38



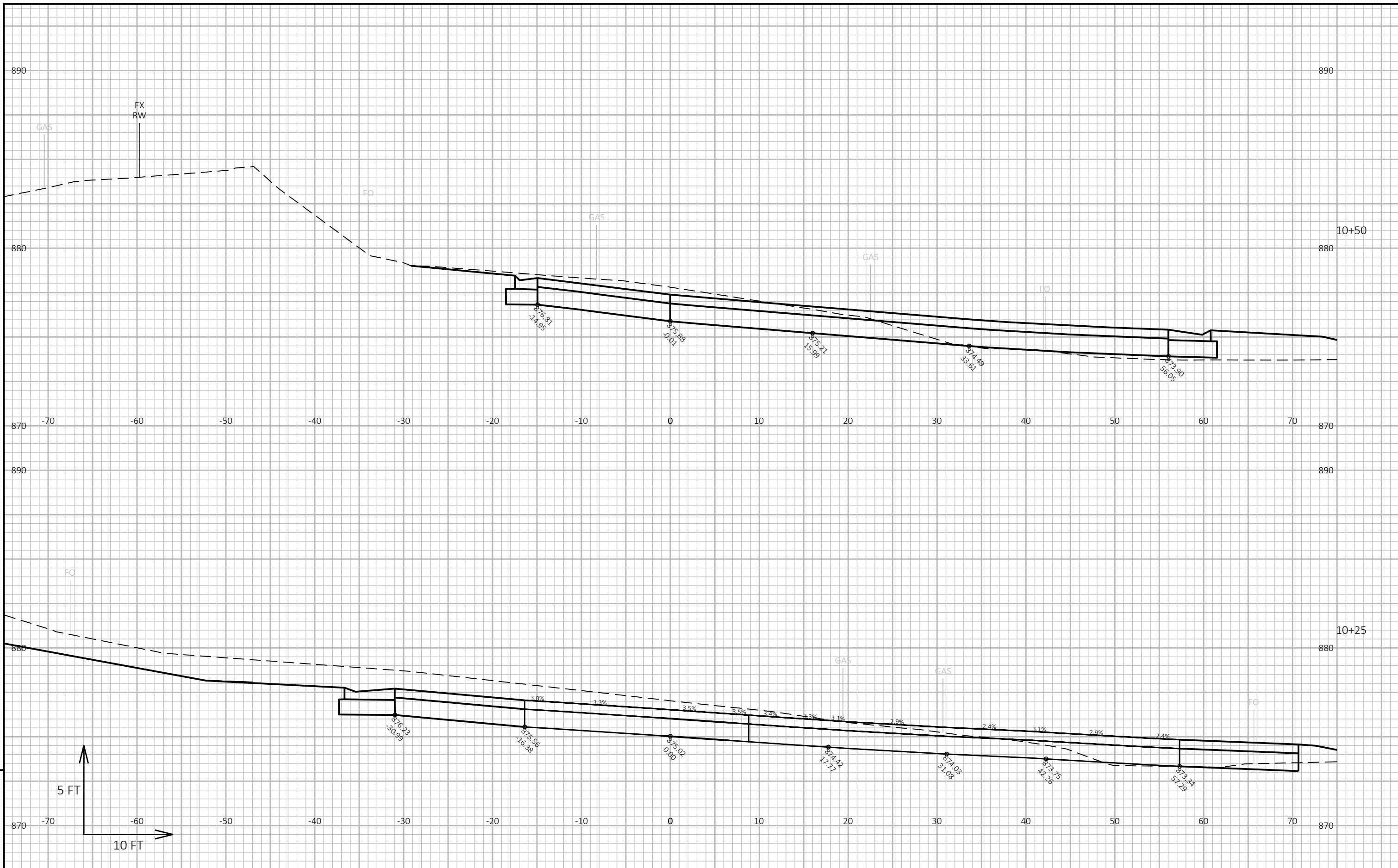
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| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 39 | E |
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| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 40 | E |
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| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: CTH B | SHEET 41 | E |
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PROJECT NO: 3779-03-70

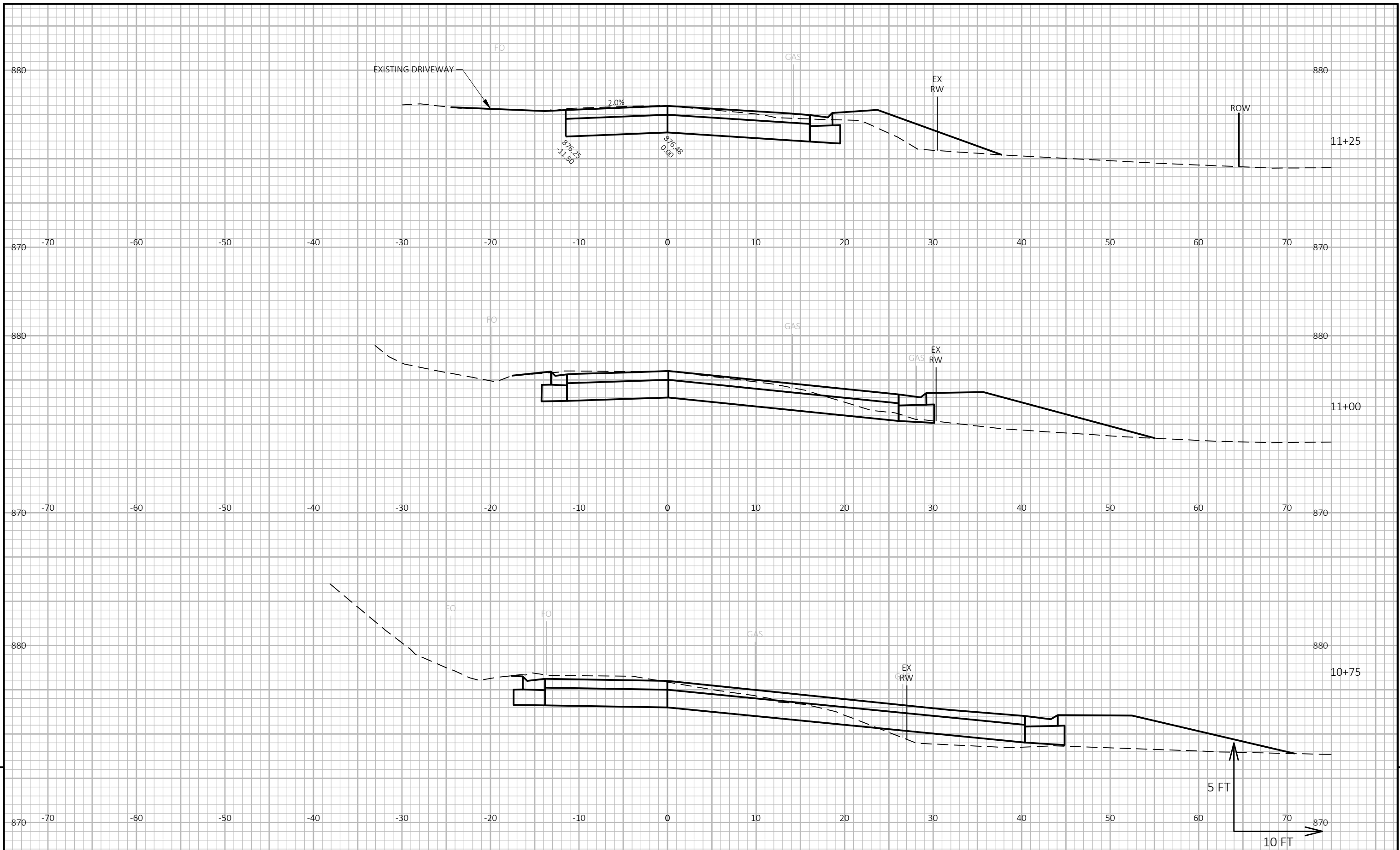
HWY: CTH B

COUNTY: WAUKESHA COUNTY

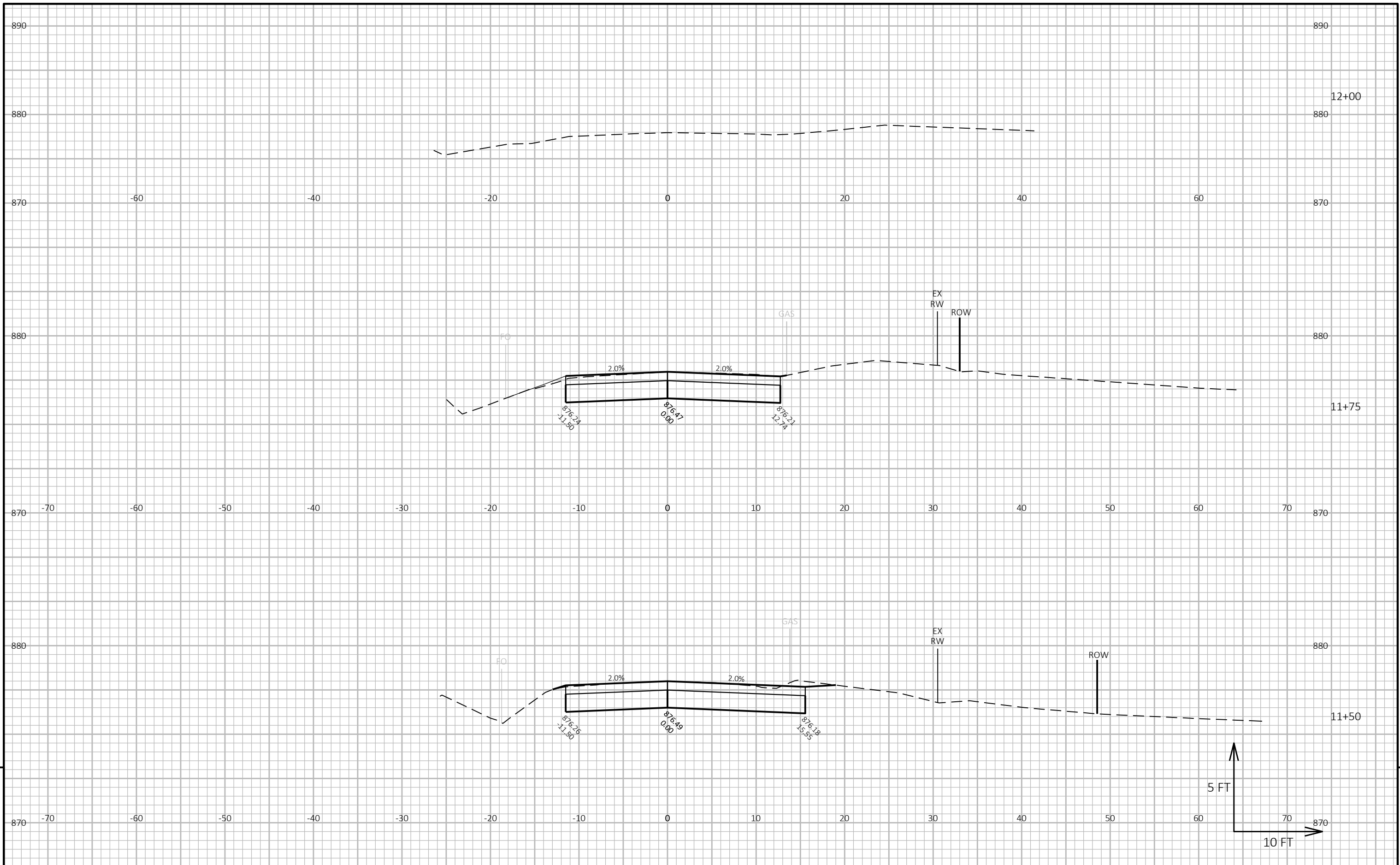
CROSS SECTIONS: MORGAN ROAD

SHEET 42

E



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| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: MORGAN ROAD | SHEET 43 | E |
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| PROJECT NO: 3779-03-70 | HWY: CTH B | COUNTY: WAUKESHA COUNTY | CROSS SECTIONS: MORGAN ROAD | SHEET 44 | E |
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