

EAU  
 PROJECT ID: 8949-00-70  
 COUNTY: DUNN

SEPTEMBER 2022

# STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

## BALDWIN - MENOMONIE

WILSON CREEK TRIB CULVERT C-17-0048

USH 12  
 DUNN COUNTY

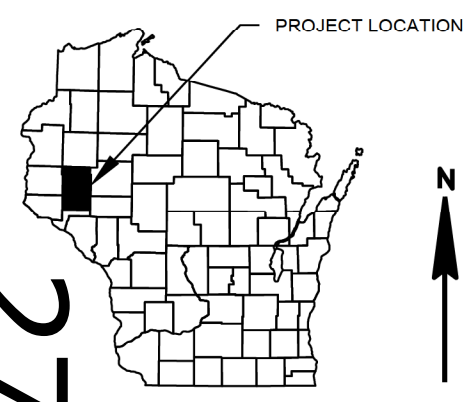
STATE PROJECT NUMBER
8949-00-70

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8949-00-70	WISC 2022527	1

ORDER OF SHEETS

Section No.	Title
1	Section No. 1 Title
2	Section No. 2 Typical Sections and Details
3	Section No. 3 Estimate of Quantities
3	Section No. 3 Miscellaneous Quantities
<del>4</del>	<del>Section No. 4 Right of Way Plan</del>
5	Section No. 5 Plan and Profile
6	Section No. 6 Standard Detail Drawings
<del>7</del>	<del>Section No. 7 Sign Plates</del>
8	Section No. 8 Structure Plans
9	Section No. 9 Computer Earthwork Data
9	Section No. 9 Cross Sections

TOTAL SHEETS = 48

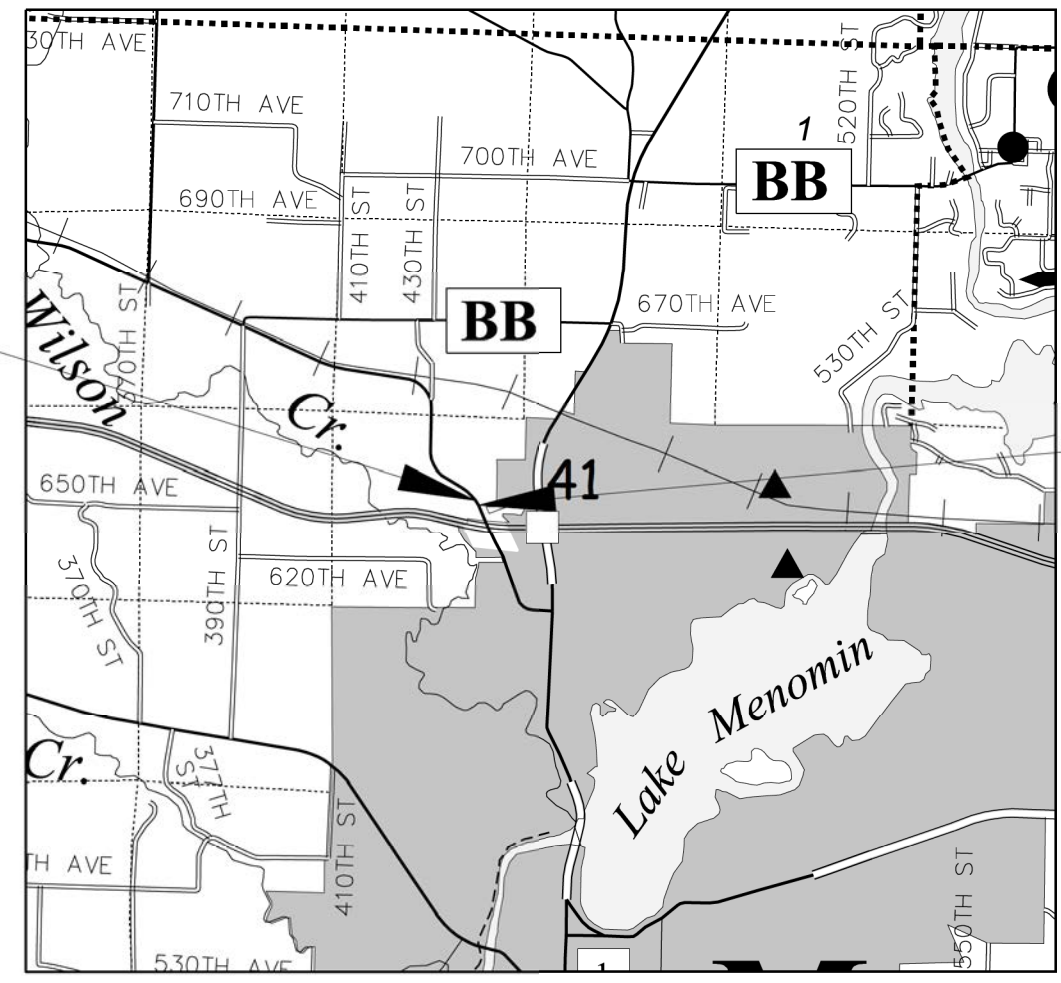


DESIGN DESIGNATION

A.A.D.T. 2022	=	4,000
A.A.D.T. 2042	=	4,900
D.H.V.	=	60 / 40
D.D.	=	505
T.	=	12.6 %
DESIGN SPEED	=	55 MPH
ESALS	=	810,000

BEGIN PROJECT  
 STA 9+00  
 Y = 182,673.954  
 X = 158,018.655

END PROJECT  
 STA 11+00



CONVENTIONAL SYMBOLS

<p><b>PLAN</b></p> <p>CORPORATE LIMITS </p> <p>PROPERTY LINE </p> <p>LOT LINE </p> <p>LIMITED HIGHWAY EASEMENT </p> <p>EXISTING RIGHT OF WAY </p> <p>PROPOSED OR NEW R/W LINE </p> <p>SLOPE INTERCEPT </p> <p>REFERENCE LINE </p> <p>EXISTING CULVERT </p> <p>PROPOSED CULVERT (Box or Pipe) </p> <p>COMBUSTIBLE FLUIDS </p> <p>MARSH AREA </p> <p>WOODED OR SHRUB AREA </p>	<p><b>PROFILE</b></p> <p>GRADE LINE </p> <p>ORIGINAL GROUND </p> <p>MARSH OR ROCK PROFILE (To be noted as such) </p> <p>SPECIAL DITCH </p> <p>GRADE ELEVATION </p> <p>CULVERT (Profile View) </p> <p><b>UTILITIES</b></p> <p>ELECTRIC </p> <p>FIBER OPTIC </p> <p>GAS </p> <p>SANITARY SEWER </p> <p>STORM SEWER </p> <p>TELEPHONE </p> <p>WATER </p> <p>UTILITY PEDESTAL </p> <p>POWER POLE </p> <p>TELEPHONE POLE </p>
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LAYOUT

SCALE 0 1 MI

TOTAL NET LENGTH OF CENTERLINE = 0.038 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), DUNN COUNTY, NAD83 ( 2011 ), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 ( 2012 ). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ORIGINAL PLANS PREPARED BY

DATE: 04/09/2021  
(Professional Engineer Signature)

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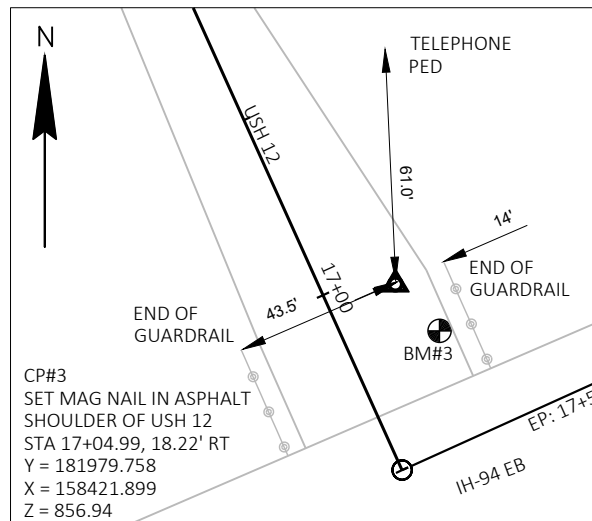
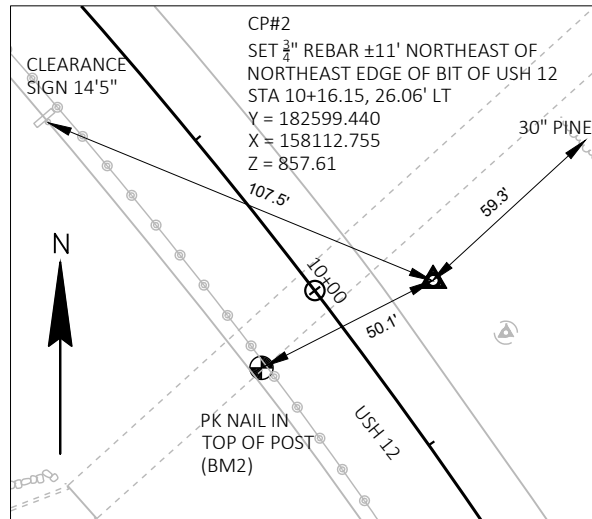
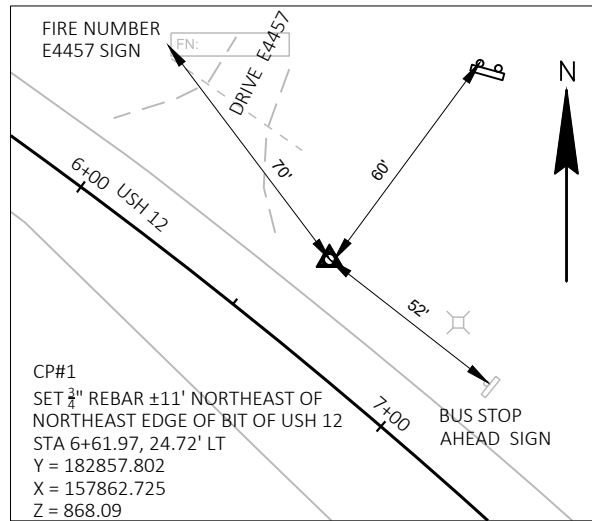
STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION

PREPARED BY \_\_\_\_\_ SEH  
 Designer \_\_\_\_\_ SEH  
 Project Manager \_\_\_\_\_ TYLER RONGSTAD  
 Regional Examiner \_\_\_\_\_ TOU YANG  
 Regional Supervisor \_\_\_\_\_ JIM KOENIG

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APPROVED FOR THE DEPARTMENT  
 DATE: \_\_\_\_\_  
(Signature)

ALIGNMENT TIES



GENERAL NOTES

WHEN THE QUANTITY OF BASE AGGREGATE OR ASPHALTIC SURFACE IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

THE LOCATION OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS IS APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

THE EXACT LOCATION OF THE EROSION CONTROL DEVICES SHALL BE DETERMINED IN THE FIELD.

SILT FENCE IS TO BE PLACED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER, AND IN PLACE PRIOR TO CULVERT REMOVAL.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, ARE TO BE 4-INCH SALVAGED TOPSOILED, SEEDED, TEMPORARY SEEDED, FERTILIZED, AND EMATTED AS SHOWN ON THE TYPICAL SECTIONS.

ALL PAVEMENT DIMENSIONS AND STATIONS ARE SHOWN TO THE EDGE OF PAVEMENT UNLESS NOTED OTHERWISE.

A VERTICAL SAWCUT SHALL BE MADE THROUGH EXISTING PAVEMENTS AT REMOVAL LIMITS.

EXCAVATION BELOW SUBGRADE (EBS) FOR REMOVAL OF UNSUITABLE MATERIAL IS NOT USED TO BALANCE YARDAGE AND IS NOT SHOWN ON THE CROSS SECTIONS BUT IS MEASURED AND PAID FOR AS EXCAVATION COMMON. EXACT EBS LOCATIONS WILL BE DETERMINED BY THE ENGINEER.

ASPHALTIC SURFACE SHALL BE CONSTRUCTED IN THREE LAYERS UTILIZING 4 MT 58-34 S MIXTURE REQUIREMENTS. USE A THICKNESS OF 2.5" FOR THE LOWER AND MIDDLE LAYERS AND 2" FOR THE UPPER LAYER.

WISDOT MONUMENTS WILL BE SUPPLIED BY THE STATE AND INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER.

UTILITY CONTACTS

AT&T WI - COMMUNICATIONS  
304 S DEWEY STREET  
EAU CLAIRE, WI 54701  
TELEPHONE: 715.839.5929 (OFFICE) 715.410.0656 (CELL)  
ATTENTION: RICK PODOLAK  
EMAIL: RP4514@ATT.COM

DUNN ENERGY COOPERATIVE - ELECTRICITY  
P.O. BOX 220  
MENOMONIE, WI 54751  
TELEPHONE: 715.232.6240 (OFFICE) 715.231.0214 (CELL)  
ATTENTION: MIKE ANDRASCHKO  
EMAIL: MANDRA@DUNNENERGY.COM

XCEL ENERGY - ELECTRICITY TRANSMISSION  
414 NICOLLET MALL  
MINNEAPOLIS, MN 55104  
TELEPHONE: 608.321.3109 (OFFICE) 608.386.2233 (CELL)  
ATTENTION: MITCHELL DIENGER  
EMAIL: MITCHELL.A.DIENGER@XCELENERGY.COM

**DIGGERS HOTLINE**  
Dial 811 or (800)242-8511  
www.DiggersHotline.com

RUNOFF COEFFICIENT TABLE

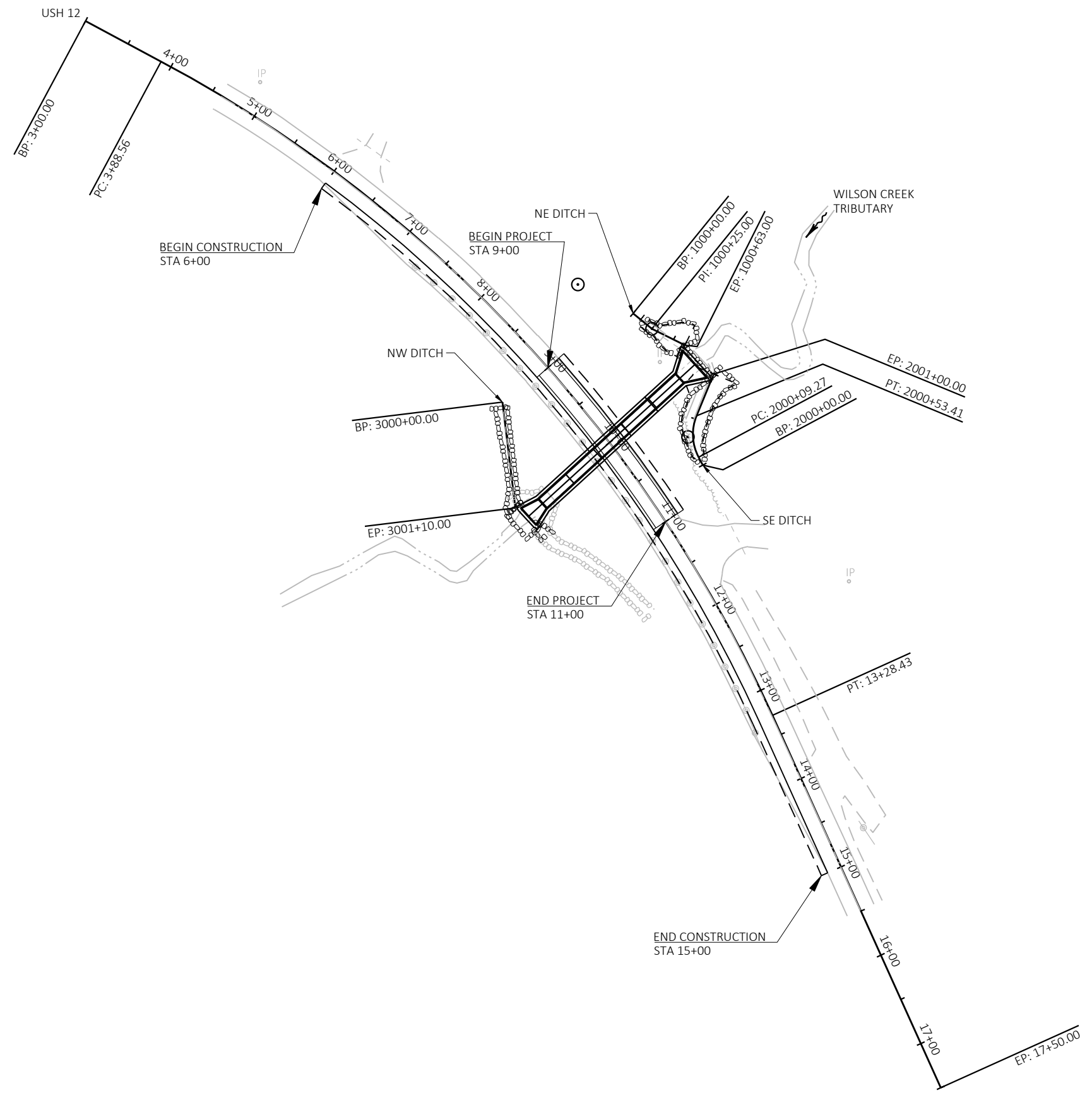
	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 5.0 ACRES  
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 1.3 ACRES

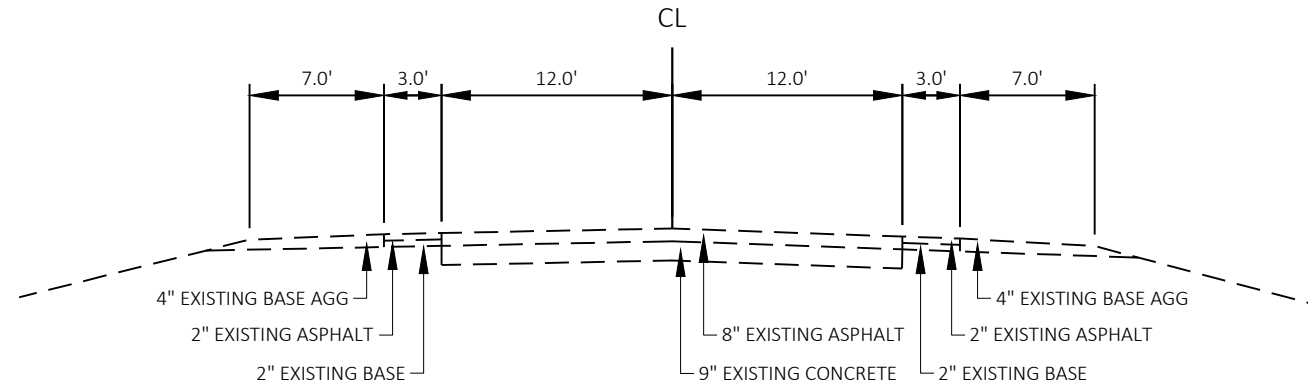
DESIGN CONTACT  
10 NORTH BRIDGE STREET  
CHIPPEWA FALLS, WI 54729  
TELEPHONE: 715.720.6291  
ATTENTION: TARA KRISTA  
EMAIL: TKRISTA@SEHINC.COM

WISDOT CONTACT  
718 W CLAIREMONT AVE  
EAU CLAIRE WI, 54701  
TELEPHONE: 715.461.0372  
ATTENTION: TYLER RONGSTAD  
EMAIL: TYLER.RONGSTAD@DOT.WI.GOV

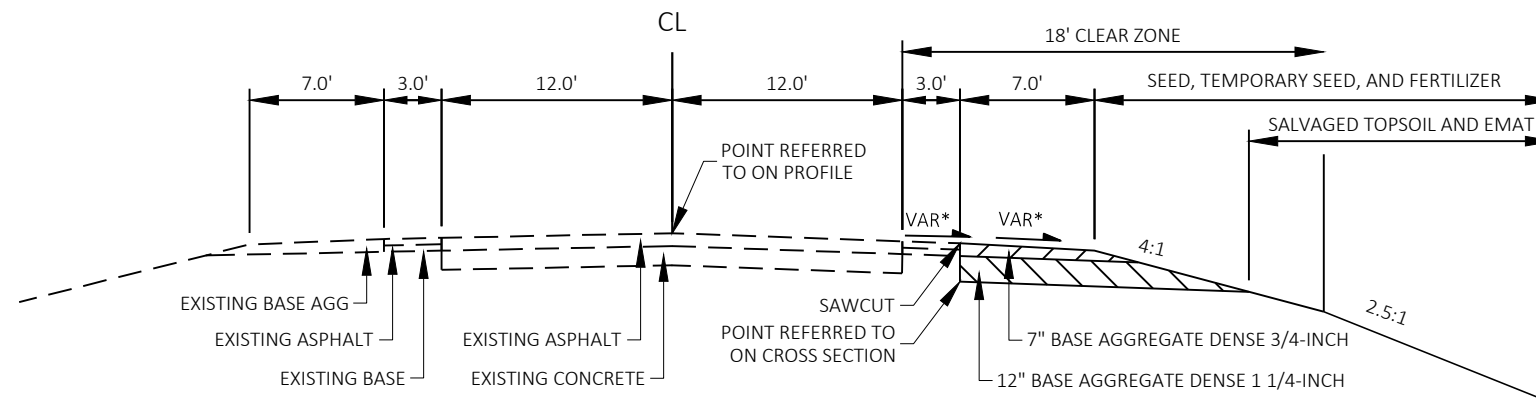
WDNR CONTACT  
DNR NORTHERN REGION HQ  
810 WEST MAPLE STREET  
SPOONER, WI 54701  
TELEPHONE: 715.836.6571  
ATTENTION: AMY LESIK  
EMAIL: AMYL.LESIK@WISCONSIN.GOV



PROJECT NO: 8949-00-70	HWY: USH 12	COUNTY: DUNN	PROJECT OVERVIEW	SHEET	E
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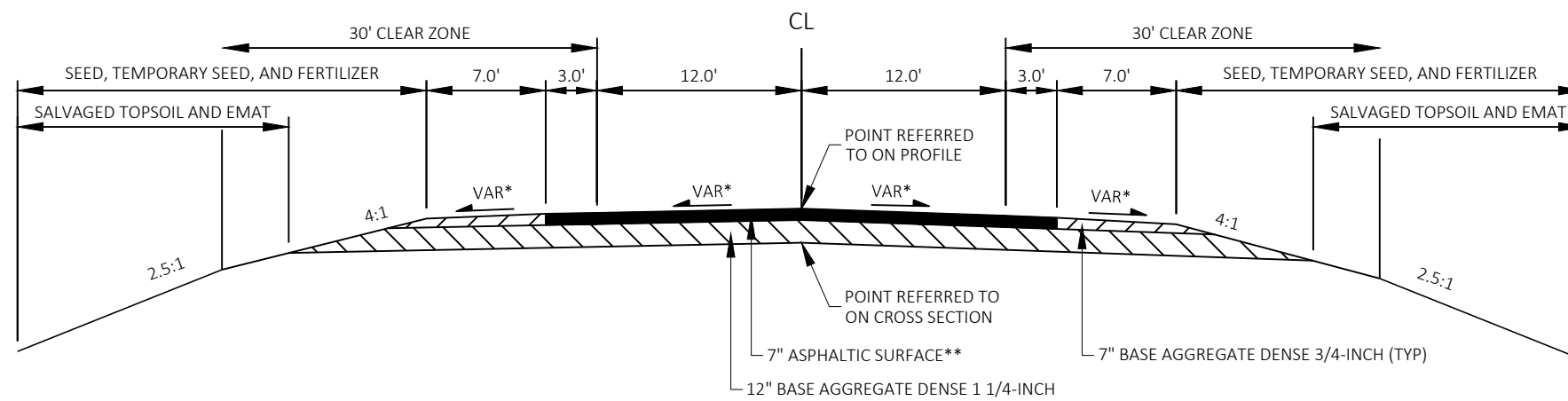


**TYPICAL EXISTING SECTION**  
STA 6+00 TO 15+00



**TYPICAL FINISHED SECTION**  
STA 6+00 TO 9+00  
STA 11+00 TO 15+00

\*SUPERELEVATE AS REQUIRED ACCORDING TO SUPERELEVATION DATA TABLE & CROSS SECTIONS  
\*\*SEE GENERAL NOTES SHEET FOR ADDITIONAL REQUIREMENTS



**TYPICAL FINISHED SECTION**  
STA 9+00 TO 11+00

\*SUPERELEVATE AS REQUIRED ACCORDING TO SUPERELEVATION DATA TABLE & CROSS SECTIONS  
\*\*SEE GENERAL NOTES SHEET FOR ADDITIONAL REQUIREMENTS

NOTES:

THE EXACT NUMBER, LOCATION AND SPACING OF ALL TRAFFIC CONTROL DEVICES ARE APPROXIMATE AND SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

DURING HOURS OF DARKNESS ALL BARRICADES USED TO SHIELD A HAZARD SHALL BE EQUIPPED WITH WARNING LIGHTS, TYPE A.

ALL SIGNS ARE TYPE 2 UNLESS OTHERWISE NOTED.

UNLESS OTHERWISE NOTED THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES DURING CONSTRUCTION.

ALL SIGNS EXISTING THAT CONFLICT WITH CONSTRUCTION TRAFFIC PATTERN SHALL BE REMOVED OR COVERED.

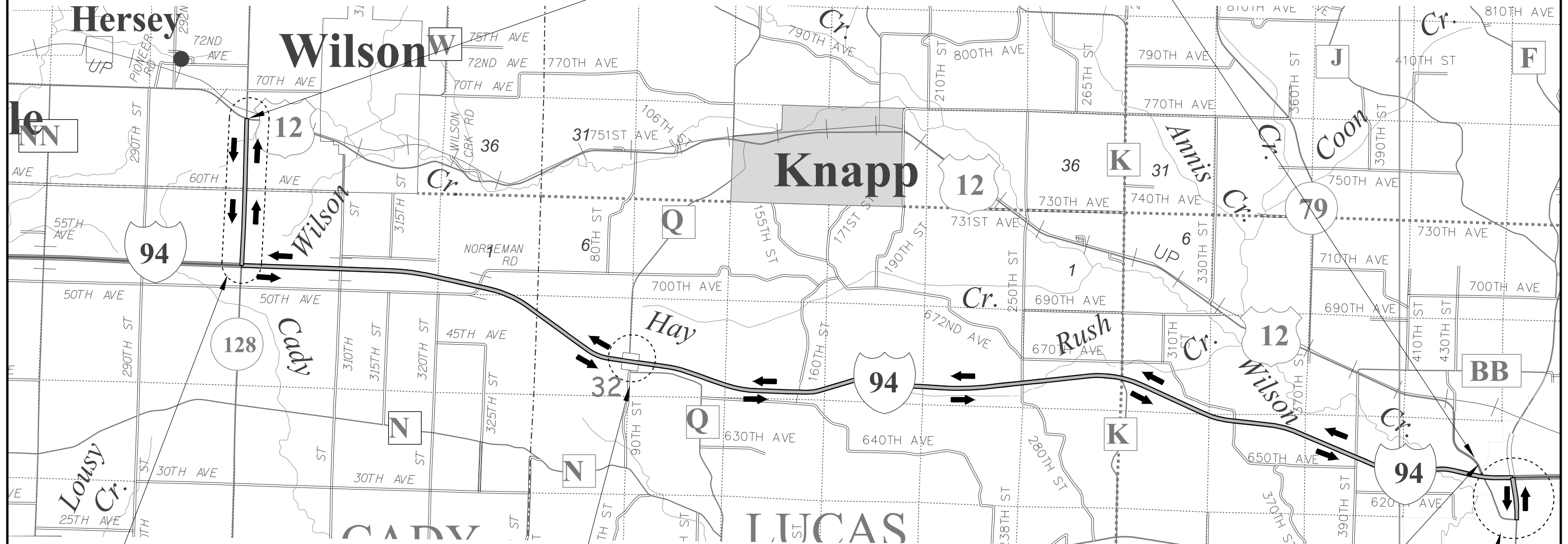
ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED BY THE CONTRACTOR.

ALL TRAFFIC CONTROL DEVICES SHALL BE IN CONFORMANCE WITH THE "WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (WMUTCD) AND STANDARD DETAIL DRAWING. "BARRICADES AND SIGNS FOR MAINLINE CLOSURES" AND "BARRICADES AND SIGNS FOR SIDE ROAD CLOSURES". ADDITIONAL TRAFFIC CONTROL DEVICES REQUIRED ARE NOT SHOWN.

PROVIDE TRAFFIC CONTROL PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) AS SPECIFIED IN THE SPECIAL PROVISIONS ONE WEEK BEFORE CLOSING USH 12 AND IMPLEMENTING THE DETOUR. PLACE THE EASTBOUND USH 12 TRAFFIC CONTROL PCMS AT THE WEST END OF THE WORK ZONE; PLACE THE WESTBOUND USH 12 TRAFFIC CONTROL PCMS AT THE EAST END OF THE WORK ZONE.

SIGN ASSEMBLIES ON IH 94 SHALL BE PLACED IN MEDIAN AND ON RIGHT SHOULDER, EXCEPT THOSE ASSEMBLIES WITH SIGN PLATE M6-2

SEE SDD "BARRICADES AND SIGNS FOR SIDE ROAD CLOSURES" DETAIL 3 FOR GUIDANCE ON SIGNS AND BARRICADES AT SIDEROADS WITHIN USH 12 CLOSURE. BARRICADES REQUIRED AT COUNTY TRUNK HIGHWAYS ONLY.

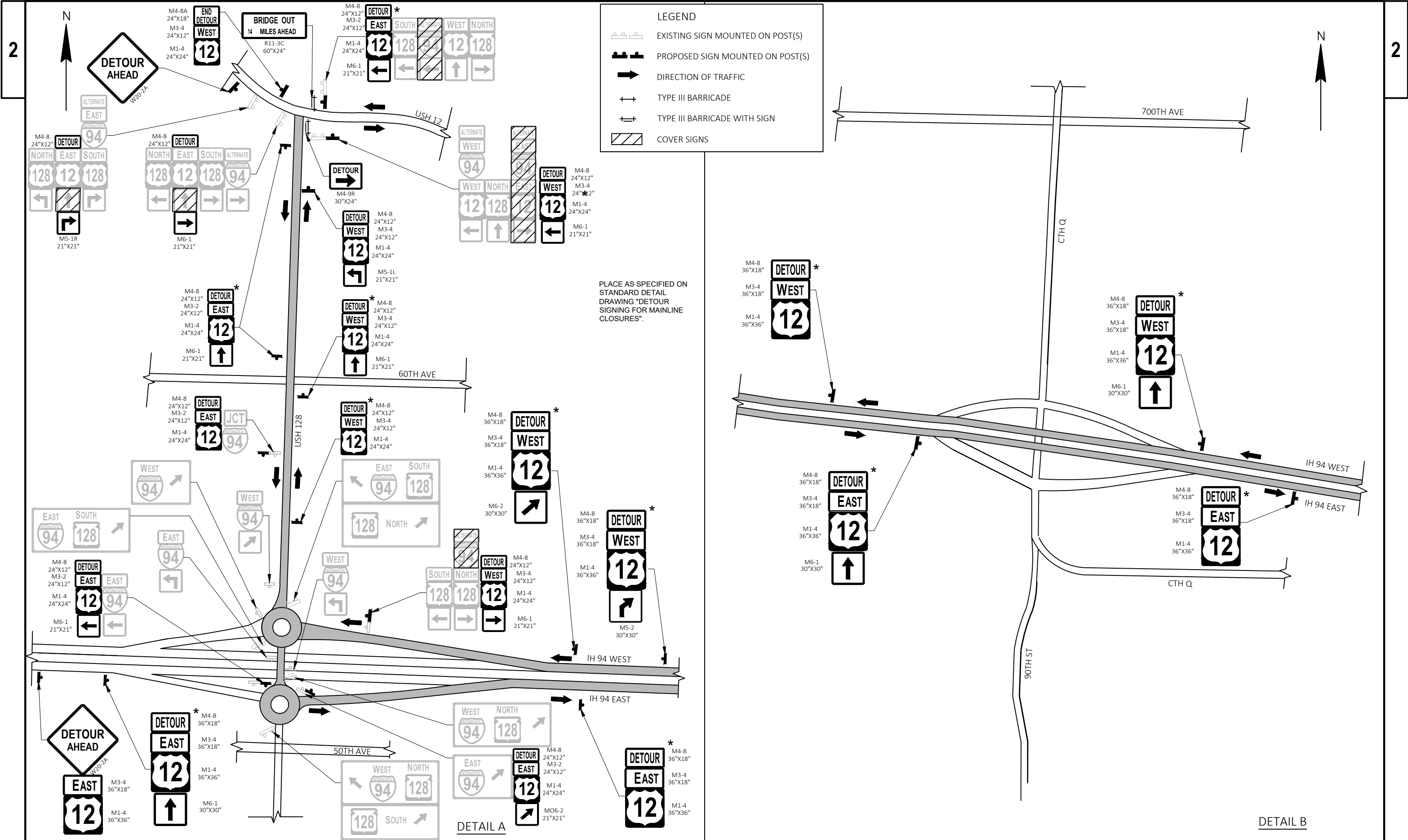


SEE DETAIL A

SEE DETAIL B

PROJECT LOCATION  
SEE SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURES" DETAIL C FOR SIGNS AND BARRICADES AT AND APPROACHING WORK ZONE

DETAIL C



**LEGEND**

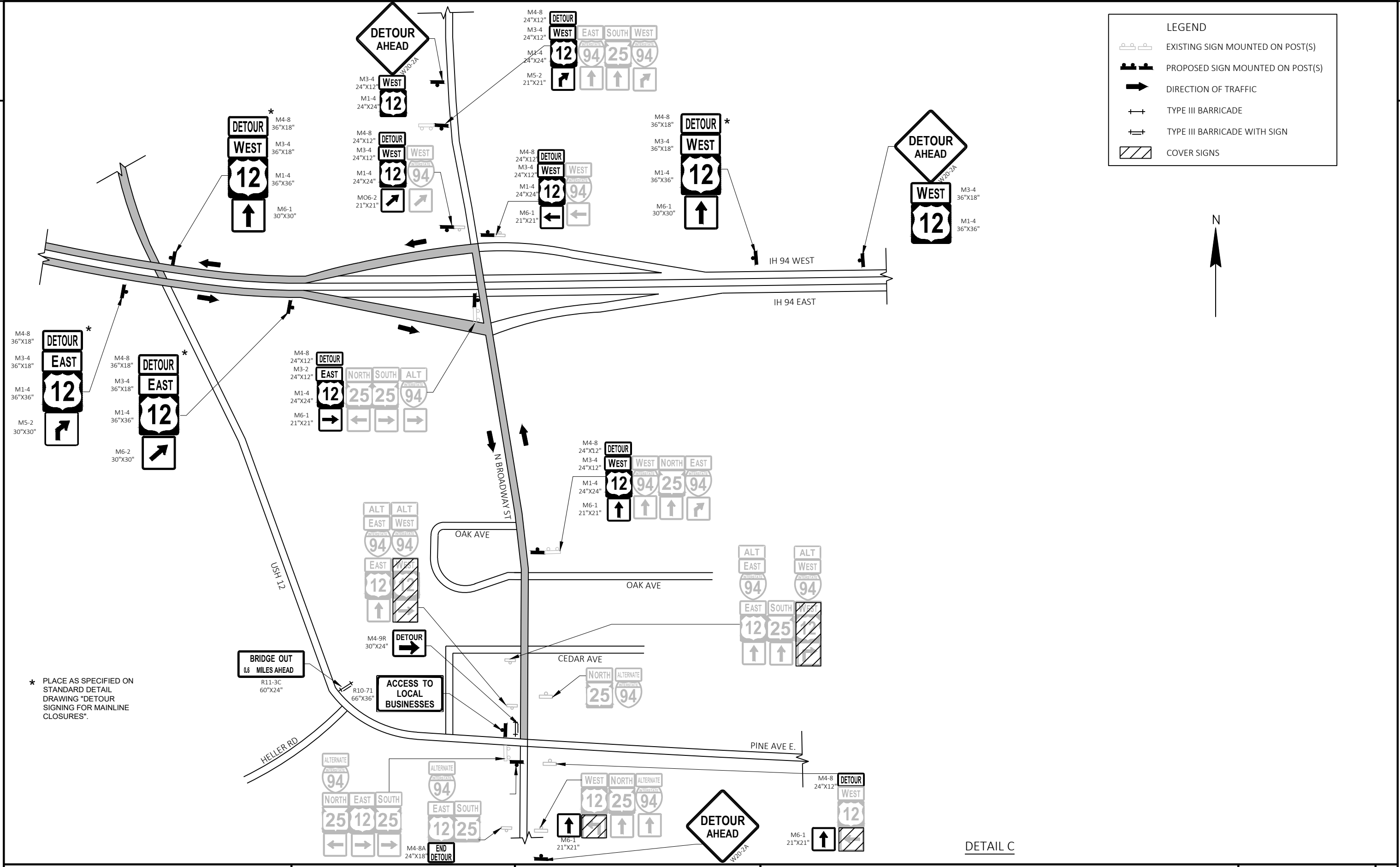
- EXISTING SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON POST(S)
- DIRECTION OF TRAFFIC
- TYPE III BARRICADE
- TYPE III BARRICADE WITH SIGN
- COVER SIGNS

DETAIL A

DETAIL B

**LEGEND**

- EXISTING SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON POST(S)
- DIRECTION OF TRAFFIC
- TYPE III BARRICADE
- TYPE III BARRICADE WITH SIGN
- COVER SIGNS



\* PLACE AS SPECIFIED ON STANDARD DETAIL DRAWING "DETOUR SIGNING FOR MAINLINE CLOSURES".

DETAIL C

**LEGEND**

- ALT IH 94 DETOUR ROUTE
- ▨ COVER SIGN
- ⊕ EXISTING SIGN MOUNTED ON POST(S)
- ⊕ PROPOSED SIGN MOUNTED ON POST(S)

**ALTERNATE**

M4-1 24"x12" INTERSTATE 94 (A)

M1-1 24"x24" (A)

MO6-1 21"x21" (B)

M4-1 24"x12" INTERSTATE 94 (C)

M1-1 24"x24" (C)

MO6-1 21"x21" (C)

M4-1 24"x12" INTERSTATE 94 (D)

M1-1 24"x24" (D)

MO5-1L 21"x21" (D)

M4-1 24"x12" INTERSTATE 94 (E)

M1-1 24"x24" (E)

MO5-1R 21"x21" (E)

**NOTES:**

RELOCATED IH-94 ALTERNATE ROUTE SIGNING SHALL BE IN PLACE 1-WEEK PRIOR TO STARTING WORK ON USH 12

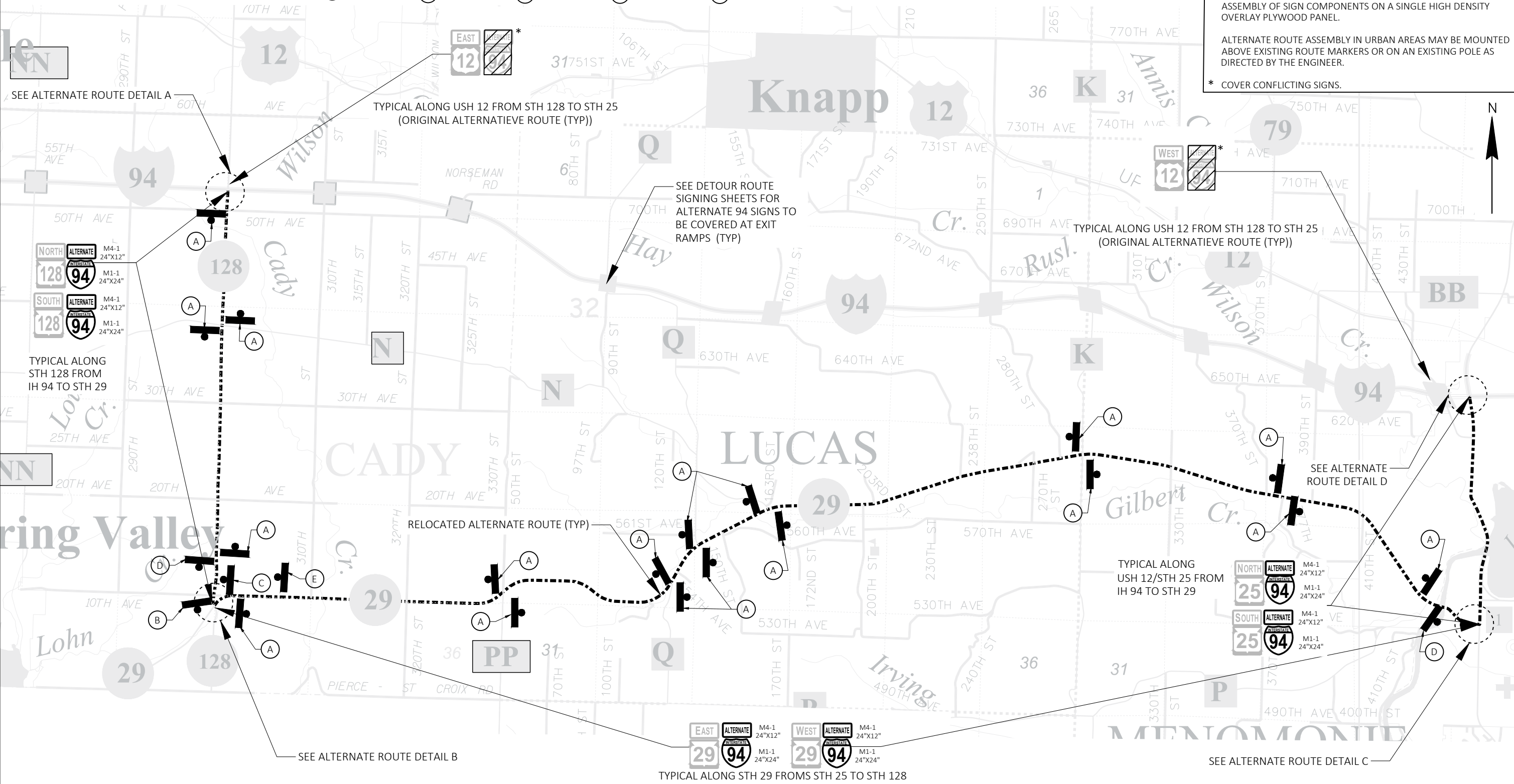
ALL ALTERNATIVE ROUTE SIGNS TO BE PAID FOR AS TRAFFIC CONTROL SIGNS. THE INTERSTATE 94 SHIELD HAS A BLUE BACKGROUND

SIGNS DESIGNATED AS "MO" SHALL HAVE A BLACK MESSAGE ON ORANGE BACKGROUND.

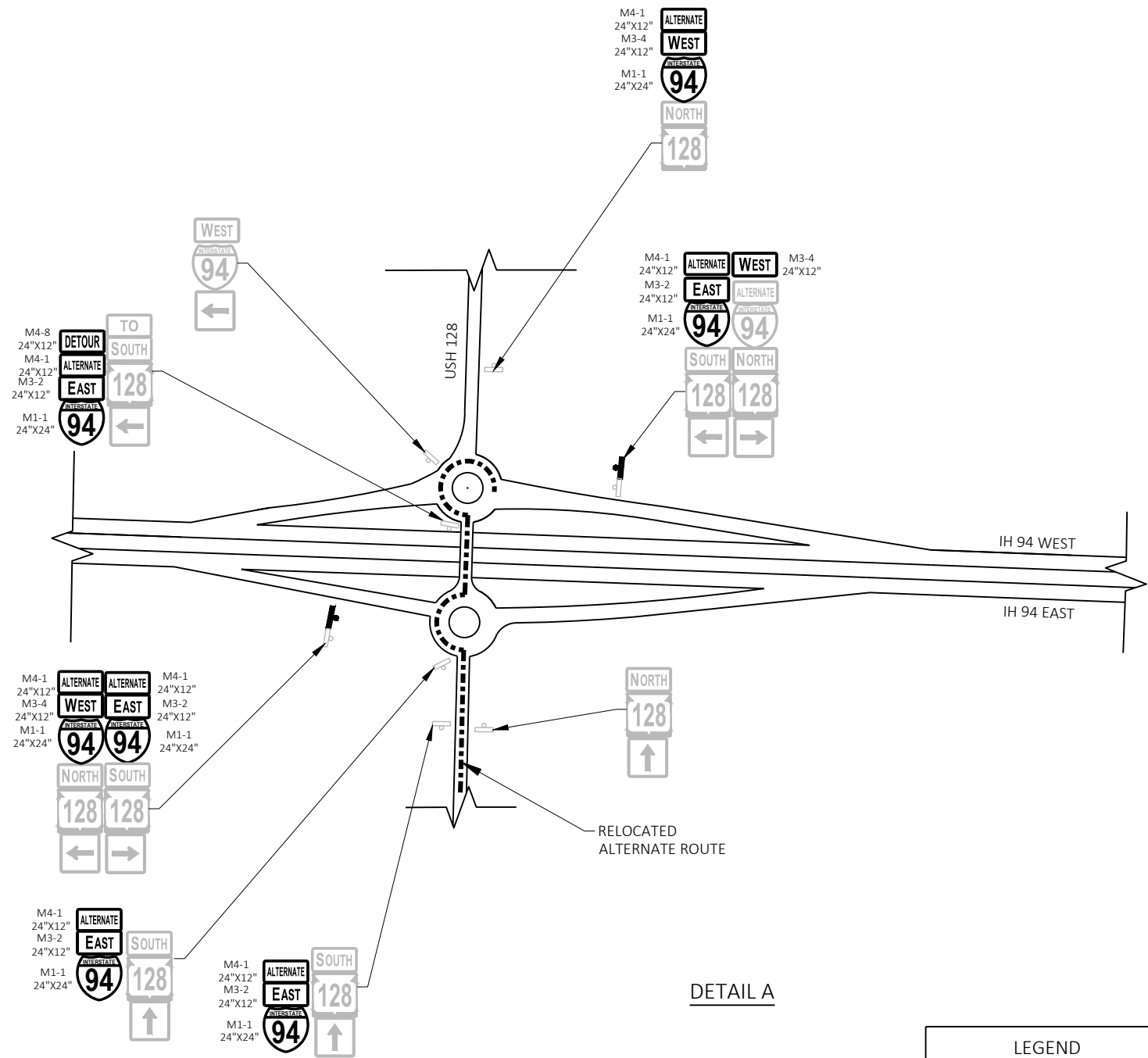
ALTERNATE ROUTE SIGNING MAY BE INDIVIDUAL SIGNS OR AN ASSEMBLY OF SIGN COMPONENTS ON A SINGLE HIGH DENSITY OVERLAY PLYWOOD PANEL.

ALTERNATE ROUTE ASSEMBLY IN URBAN AREAS MAY BE MOUNTED ABOVE EXISTING ROUTE MARKERS OR ON AN EXISTING POLE AS DIRECTED BY THE ENGINEER.

\* COVER CONFLICTING SIGNS.



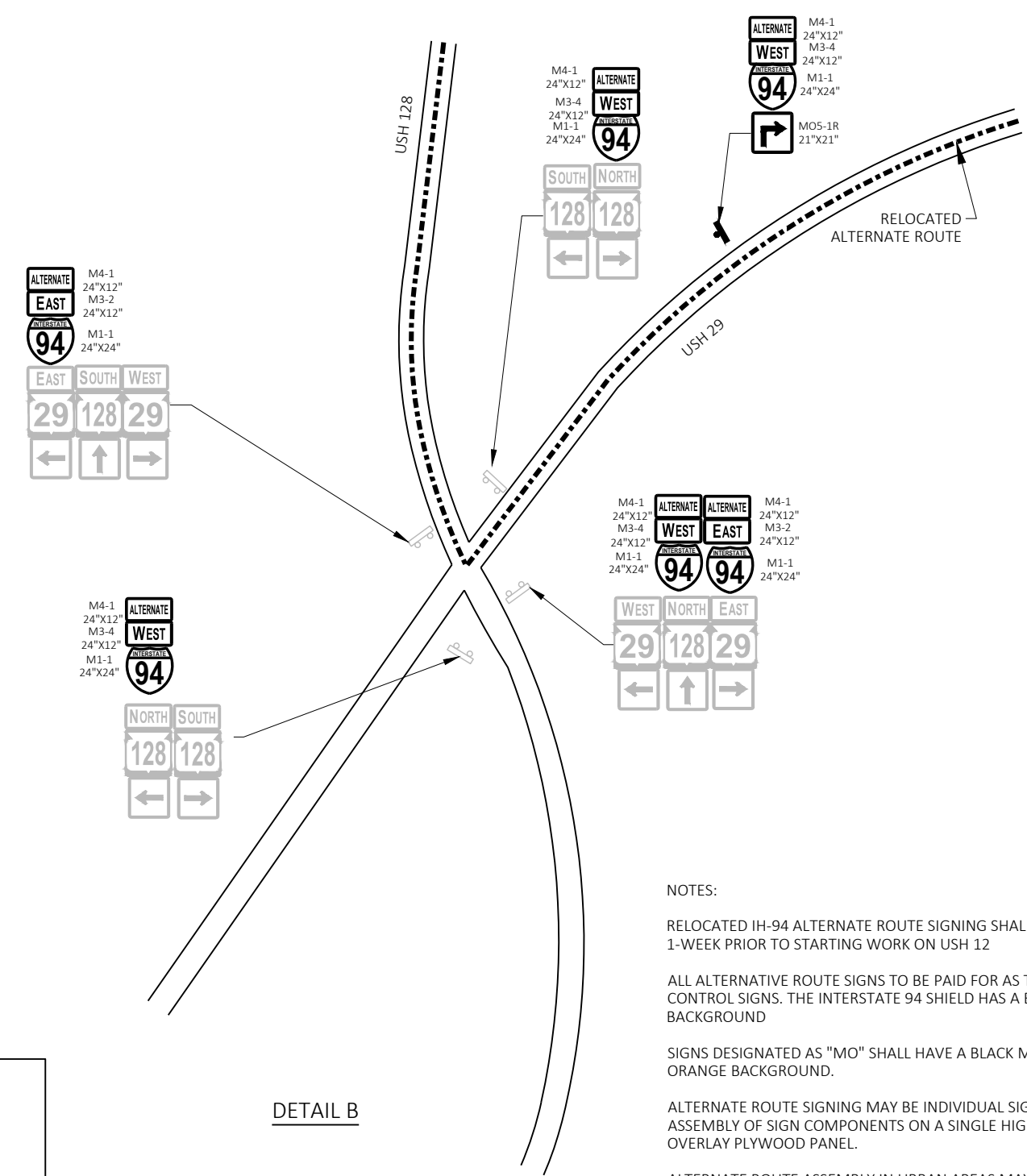




DETAIL A

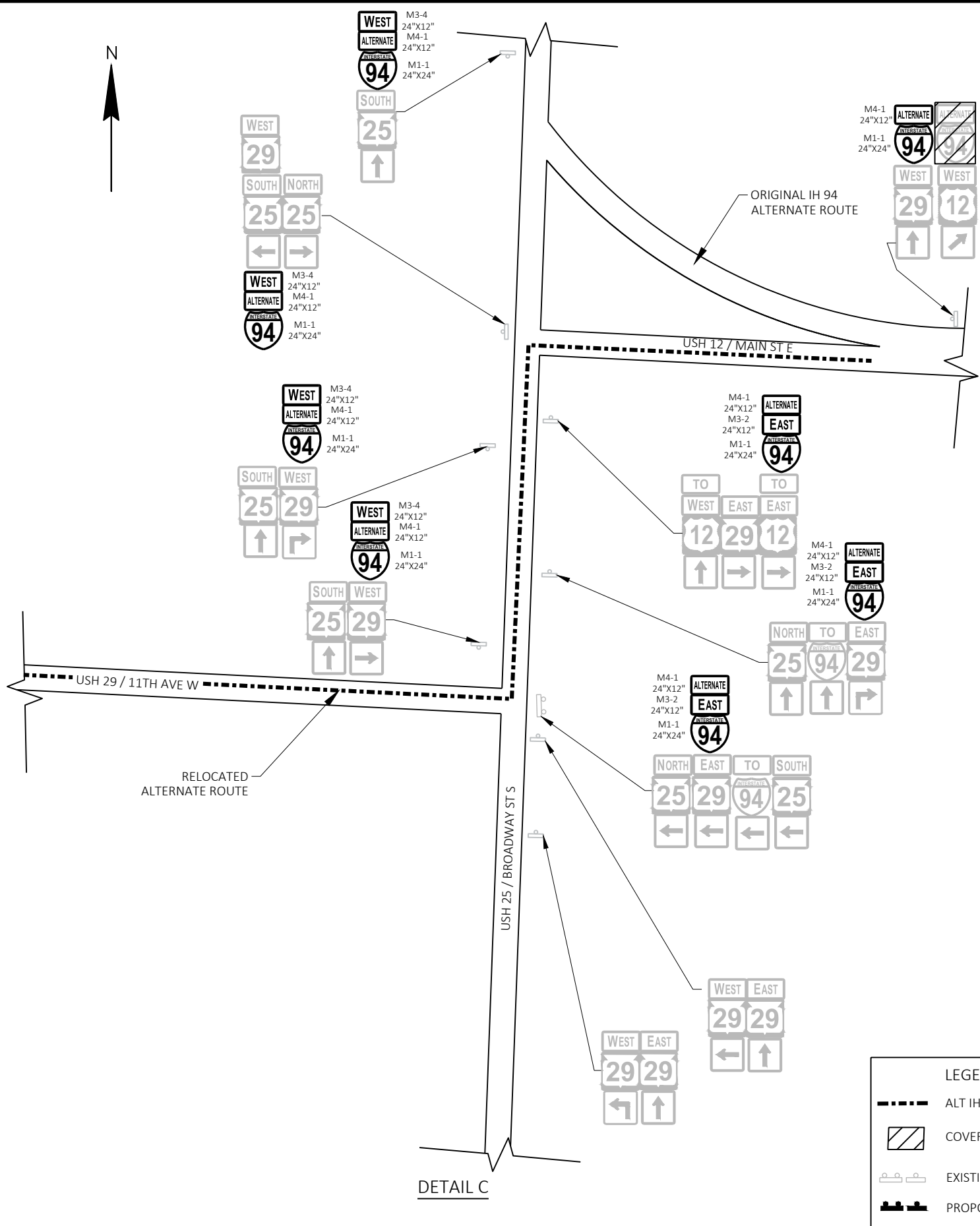
**LEGEND**

- ALT IH 94 DETOUR ROUTE
- COVER SIGN
- EXISTING SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON POST(S)

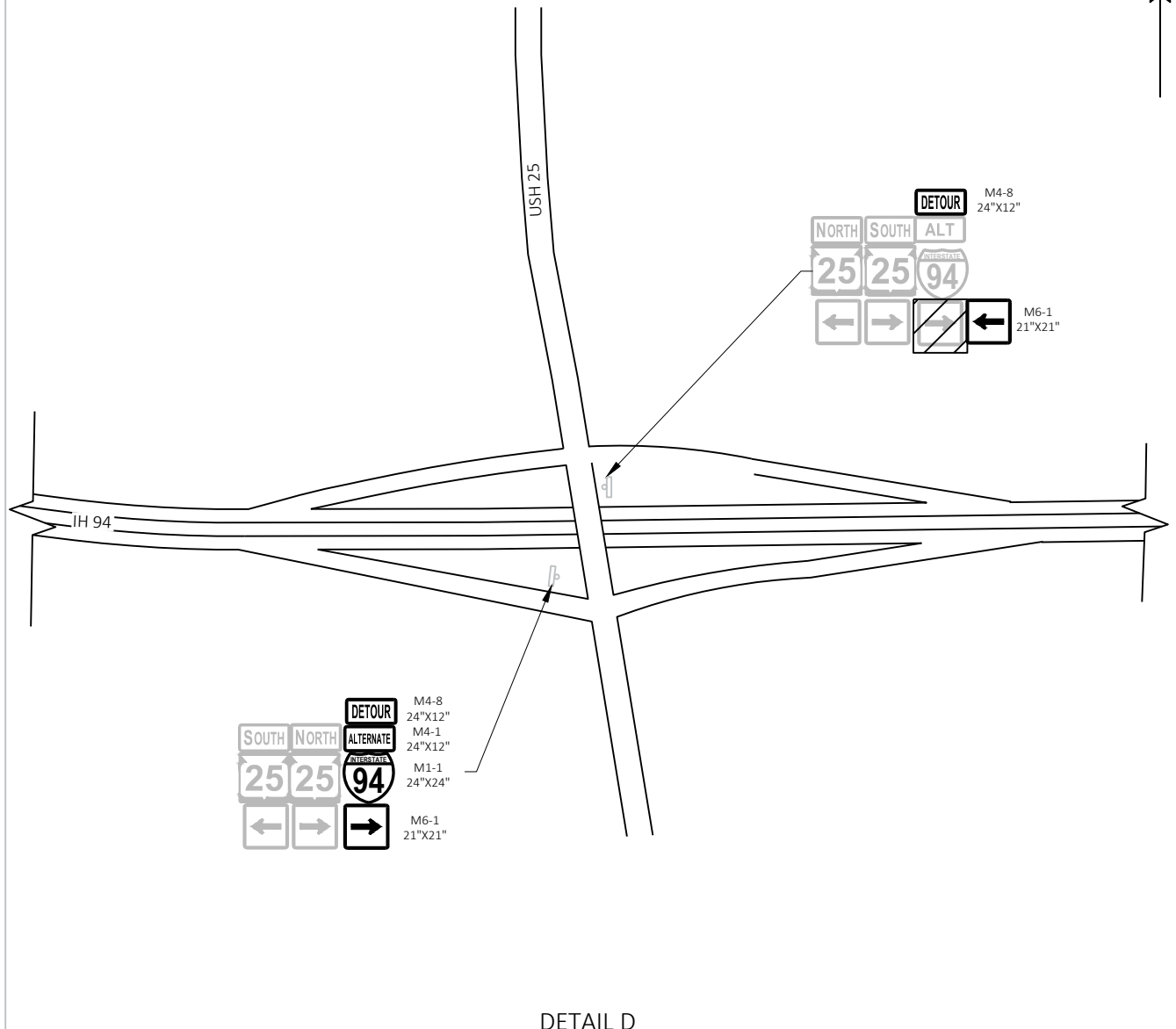


DETAIL B

- NOTES:**
- RELOCATED IH-94 ALTERNATE ROUTE SIGNING SHALL BE IN PLACE 1-WEEK PRIOR TO STARTING WORK ON USH 12
  - ALL ALTERNATIVE ROUTE SIGNS TO BE PAID FOR AS TRAFFIC CONTROL SIGNS. THE INTERSTATE 94 SHIELD HAS A BLUE BACKGROUND
  - SIGNS DESIGNATED AS "MO" SHALL HAVE A BLACK MESSAGE ON ORANGE BACKGROUND.
  - ALTERNATE ROUTE SIGNING MAY BE INDIVIDUAL SIGNS OR AN ASSEMBLY OF SIGN COMPONENTS ON A SINGLE HIGH DENSITY OVERLAY PLYWOOD PANEL.
  - ALTERNATE ROUTE ASSEMBLY IN URBAN AREAS MAY BE MOUNTED ABOVE EXISTING ROUTE MARKERS OR ON AN EXISTING POLE AS DIRECTED BY THE ENGINEER.
- \* COVER CONFLICTING SIGNS.



DETAIL C



DETAIL D

**NOTES:**

- RELOCATED IH-94 ALTERNATE ROUTE SIGNING SHALL BE IN PLACE 1-WEEK PRIOR TO STARTING WORK ON USH 12
- ALL ALTERNATIVE ROUTE SIGNS TO BE PAID FOR AS TRAFFIC CONTROL SIGNS. THE INTERSTATE 94 SHIELD HAS A BLUE BACKGROUND
- SIGNS DESIGNATED AS "MO" SHALL HAVE A BLACK MESSAGE ON ORANGE BACKGROUND.
- ALTERNATE ROUTE SIGNING MAY BE INDIVIDUAL SIGNS OR AN ASSEMBLY OF SIGN COMPONENTS ON A SINGLE HIGH DENSITY OVERLAY PLYWOOD PANEL.
- ALTERNATE ROUTE ASSEMBLY IN URBAN AREAS MAY BE MOUNTED ABOVE EXISTING ROUTE MARKERS OR ON AN EXISTING POLE AS DIRECTED BY THE ENGINEER.

\* COVER CONFLICTING SIGNS.

**LEGEND**

- ALT IH 94 DETOUR ROUTE
- ▨ COVER SIGN
- ⌊ EXISTING SIGN MOUNTED ON POST(S)
- ⌋ PROPOSED SIGN MOUNTED ON POST(S)

**ALIGNMENT DATA - USH 12**

TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	3+00.000	183031.567	157541.737
END:	3+88.563	182989.765	157619.815
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	88.563	COURSE:	S 61° 50' 13.2878" E
CURVE POINT DATA			
DESCRIPTION	STATION	NORTHING	EASTING
PC STA	3+88.563	182989.765	157619.815
PI STA		182759.68	158049.586
PT STA	3+28.428	182315.425	158250.276
CIRCULAR CURVE DATA			
PARAMETER	VALUE	PARAMETER	VALUE
DELTA:	37° 31' 34.7809"	TYPE:	RIGHT
RADIUS:	1435		
LENGTH:	939.865	TANGENT:	487.485
MID ORD:	76.261	EXTERNAL:	80.542
CHORD:	923.156	COURSE:	S 43° 04' 25.8973" E
TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	3+28.428	182315.425	158250.276
END:	3+50.000	181931.235	158423.83
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	471.572	COURSE:	S 74° 18' 38.5059" E

**SUPERELEVATION DATA - USH 12**

STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
3+00.00'	BEGIN ALIGNMENT	-4.00%	-2.00%	-2.00%	-4.00%
3+40.00'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
3+91.23'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
3+42.45'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
4+37.23'	BEGIN FULL SUPER	5.70%	5.70%	-5.70%	-5.70%
12+79.77'	END FULL SUPER	5.70%	5.70%	-5.70%	-5.70%
13+74.54'	REVERSE CROWN	2.00%	2.00%	-2.00%	-4.00%
14+25.77'	LEVEL CROWN	0.00%	0.00%	-2.00%	-4.00%
14+76.99'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
17+50.00'	END ALIGNMENT	-4.00%	-2.00%	-2.00%	-4.00%

**ALIGNMENT DATA - NW DITCH**

TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	3000+00.000	182638.52	157971.724
END:	3001+10.000	182529.34	157985.113
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	110	COURSE:	S 06° 59' 28.9566" E

**ALIGNMENT DATA - NE DITCH**

TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	1000+00.000	182729.924	158106.533
END:	1000+25.000	182714.156	158125.933
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	25	COURSE:	S 50° 53' 44.0791" E
TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	1000+25.000	182714.156	158125.933
END:	1000+63.000	182697.155	158159.918
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	38	COURSE:	S 63° 25' 26.2977" E

**ALIGNMENT DATA - SE DITCH**

TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	2000+00.000	182573.449	158178.455
END:	2000+09.273	182581.629	158174.085
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	9.273	COURSE:	N 28° 06' 45.2977" W
CURVE POINT DATA			
DESCRIPTION	STATION	NORTHING	EASTING
PC STA	2000+09.273	182581.629	158174.085
PI STA		182605.189	158218.186
PT STA	2000+53.409	182624.293	158171.98
CIRCULAR CURVE DATA			
PARAMETER	VALUE	PARAMETER	VALUE
DELTA:	50° 34' 32.4283"	TYPE:	RIGHT
RADIUS:	50		
LENGTH:	44.136	Tangent:	23.622
MID ORD:	4.791	External:	5.299
CHORD:	42.717	COURSE:	N 02° 49' 29.0836" W
TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	2000+53.409	182624.293	158171.98
END:	2001+00.000	182667.349	158189.782
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	46.591	COURSE:	N 27° 27' 47.1305" E

Estimate Of Quantities

8949-00-70

Line	Item	Item Description	Unit	Total	Qty
0002	201.0105	Clearing	STA	3.000	3.000
0004	201.0205	Grubbing	STA	3.000	3.000
0006	203.0100	Removing Small Pipe Culverts	EACH	2.000	2.000
0008	203.0220	Removing Structure (structure) 01. C-17-5038	EACH	1.000	1.000
0010	204.0100	Removing Concrete Pavement	SY	534.000	534.000
0012	204.0185	Removing Masonry	CY	10.000	10.000
0014	205.0100	Excavation Common	CY	2,044.000	2,044.000
0016	206.2000	Excavation for Structures Culverts (structure) 01. C-17-48	LS	1.000	1.000
0018	210.2500	Backfill Structure Type B	TON	4,630.000	4,630.000
0020	213.0100	Finishing Roadway (project) 01. 8949-00-70	EACH	1.000	1.000
0022	305.0110	Base Aggregate Dense 3/4-Inch	TON	383.000	383.000
0024	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	1,405.000	1,405.000
0026	311.0110	Breaker Run	TON	241.000	241.000
0028	311.0115	Breaker Run	CY	190.000	190.000
0030	455.0605	Tack Coat	GAL	40.000	40.000
0032	465.0105	Asphaltic Surface	TON	262.000	262.000
0034	504.0100	Concrete Masonry Culverts	CY	549.000	549.000
0036	505.0400	Bar Steel Reinforcement HS Structures	LB	65,560.000	65,560.000
0038	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	2,250.000	2,250.000
0040	516.0500	Rubberized Membrane Waterproofing	SY	47.000	47.000
0042	606.0300	Riprap Heavy	CY	66.000	66.000
0044	606.0600	Grouted Riprap Medium	CY	320.000	320.000
0046	614.0920	Salvaged Rail	LF	525.000	525.000
0048	614.0925	Salvaged Guardrail End Treatments	EACH	2.000	2.000
0050	618.0100	Maintenance And Repair of Haul Roads (project) 01. 8949-00-70	EACH	1.000	1.000
0052	619.1000	Mobilization	EACH	1.000	1.000
0054	624.0100	Water	MGAL	18.000	18.000
0056	625.0500	Salvaged Topsoil	SY	5,580.000	5,580.000
0058	628.1504	Silt Fence	LF	1,070.000	1,070.000
0060	628.1520	Silt Fence Maintenance	LF	1,070.000	1,070.000
0062	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0064	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0066	628.2008	Erosion Mat Urban Class I Type B	SY	5,580.000	5,580.000
0068	628.7504	Temporary Ditch Checks	LF	80.000	80.000
0070	629.0210	Fertilizer Type B	CWT	3.600	3.600
0072	630.0120	Seeding Mixture No. 20	LB	151.000	151.000
0074	630.0200	Seeding Temporary	LB	151.000	151.000
0076	630.0500	Seed Water	MGAL	126.000	126.000
0078	638.2102	Moving Signs Type II	EACH	2.000	2.000
0080	642.5001	Field Office Type B	EACH	1.000	1.000
0082	643.0300	Traffic Control Drums	DAY	70.000	70.000
0084	643.0420	Traffic Control Barricades Type III	DAY	1,542.000	1,542.000
0086	643.0705	Traffic Control Warning Lights Type A	DAY	1,500.000	1,500.000
0088	643.0900	Traffic Control Signs	DAY	16,400.000	16,400.000
0090	643.0920	Traffic Control Covering Signs Type II	EACH	40.000	40.000
0092	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0094	643.5000	Traffic Control	EACH	1.000	1.000
0096	645.0105	Geotextile Type C	SY	614.000	614.000
0098	645.0120	Geotextile Type HR	SY	928.000	928.000

Estimate Of Quantities

8949-00-70

Line	Item	Item Description	Unit	Total	Qty
0100	646.1020	Marking Line Epoxy 4-Inch	LF	1,500.000	1,500.000
0102	650.4500	Construction Staking Subgrade	LF	900.000	900.000
0104	650.5000	Construction Staking Base	LF	900.000	900.000
0106	650.6500	Construction Staking Structure Layout (structure) 01. C-17-48	LS	1.000	1.000
0108	650.9910	Construction Staking Supplemental Control (project) 01. 8949-00-70	LS	1.000	1.000
0110	650.9920	Construction Staking Slope Stakes	LF	900.000	900.000
0112	690.0150	Sawing Asphalt	LF	712.000	712.000
0114	690.0250	Sawing Concrete	LF	48.000	48.000
0116	715.0502	Incentive Strength Concrete Structures	DOL	3,294.000	3,294.000
0118	999.2000.S	Installing and Maintaining Bird Deterrent System (station) 01. 10+00	EACH	1.000	1.000
0120	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0122	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000
0124	SPV.0090	Special 01. Salvage and Reinstall Fence	LF	30.000	30.000

3

3

**CLEARING & GRUBBING**

STATION	LOCATION	201.0105 CLEARING STA	201.0205 GRUBBING STA
USH 12			
8+00 - 9+00	RT	1	1
9+00 - 11+00	LT	2	2
ITEM TOTALS		3	3

**BASE AGGREGATE**

STATION	LOCATION	305.0110 DENSE 3/4-INCH TON	305.0120 DENSE 1 1/4-INCH TON	311.0115* BREAKER RUN CY	624.0100 WATER MGAL	REMARKS
USH 12						
6+00 - 9+00	RT	104	255	-	4	
9+00 - 11+00	LT & RT	140	809	-	9	
10+00	LT & RT	-	-	241	-	FOR EBS BACKFILL
11+00 - 15+00	RT	139	341	-	5	
ITEM TOTALS		383	1405	241	18	

**MOBILIZATION**

STATION	619.1000 EACH
USH 12	1
ITEM TOTAL	1

\* ADDITIONAL QUANTITY LOCATED ELSEWHERE IN PLANS

**REMOVING**

STATION	LOCATION	203.0100 SMALL PIPE CULVERTS EACH	204.0100 CONCRETE PAVEMENT SY	204.0185 MASONRY CY
USH 12				
9+00 - 11+00	LT & RT	1	534	-
9+30 - 9+50	LT	1	-	10
ITEM TOTALS		2	534	10

**ASPHALTIC PAVEMENT ITEMS**

STATION	LT & RT	455.0605 TACK COAT GAL	465.0105 ASPHALTIC SURFACE TON
USH 12			
9+00 - 11+00	LT & RT	40	262
ITEM TOTALS		40	262

**SALVAGED TOPSOIL, MULCHING AND SEEDING**

STATION	LOCATION	625.0500 SALVAGED TOPSOIL SY	629.0210 FERTILIZER TYPE B CWT	630.0120 SEEDING MIXTURE NO. 20 LB	630.0200 SEEDING TEMPORARY LB	630.0500 SEED WATER MGAL
USH 12						
6+00 - 15+00	LT & RT	5580	3.6	151	151	126
ITEM TOTALS		5580	3.6	151	151	126

**EXCAVATION**

STATION	LOCATION	205.0100 COMMON CY	AVAILABLE MATERIAL CY	EXPANDED FILL CY	WASTE CY	REMARKS
USH 12						
6+00 - 15+00	LT & RT	1097	2503	974	1529	
10+00	LT & RT	250	-	-	250	EBS BELOW BOX CULVERT
1000+00 - 1000+63	LT & RT	204	164	-	164	NE DITCH
2000+00 - 2001+00	LT & RT	395	316	-	316	SE DITCH
3000+00 - 3001+10	LT & RT	98	49	-	49	NW DITCH
ITEM TOTALS		2044	3032	974	2308	

**GUARDRAIL ITEMS**

STATION	LOCATION	614.0920 SALVAGED RAIL LF	614.0925 SALVAGED GUARDRAIL END TREATMENTS LF
USH 12			
7+19 - 7+74	RT	-	1
7+74 - 13+05	RT	525	-
13+05 - 13+59	RT	-	1
ITEM TOTALS		525	2

**EROSION CONTROL ITEMS**

STATION	LOCATION	606.0300* RIPRAP HEAVY CY	606.0600 GROUTED RIPRAP MEDIUM CY	628.15 SILT FENCE LF	628.1520 SILT MAINTENANCE LF	628.2008 EROSION MAT CLASS I TYPE B SY	628.7504 TEMPORARY DITCH CHECKS LF	645.0120* GEOTEXTILE TYPE HR SY
USH 12								
6+00 - 15+00	LT & RT	6	320	1070	1070	5580	80	800
ITEM TOTALS		6	320	1070	1070	5580	80	800

NOTES:  
 1) UNUSABLE ASPHALT AND CONCRETE PAVEMENT MATERIAL IS INCLUDED IN COMMON EXCAVATION.  
 2) AVAILABLE MATERIAL INCLUDES USABLE MATERIAL LEFT OVER FROM EXCAVATION FOR STRUCTURES BID ITEM. DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUME.  
 3) EXPANSION FACTOR = 1.3  
 4) EBS ESTIMATED USING THE FOLLOWINGS AREAS AND DEPTHS UNDER THE BOX CULVERT 1-FOOT THICK BREAKER RUN FOUNDATION LAYER:  
 A) 1-FOOT DEPTH UNDER NORTH APRON ENDWALL AND AN ADDITIONAL 150' LENGTH OF BOX  
 B) 4-FOOT DEPTH UNDER SOUTH APRON ENDWALL AND AN ADDITIONAL 40' LENGTH OF BOX  
 5) ADDITIONAL CUT REQUIRED WITHIN EXCAVATION FOR STRUCTURES LIMITS (APPROX. STA 9+30 TO STA 10+70). SEE EARTHWORK DATA AND STRUCTURE PLANS FOR ADDITIONAL INFORMATION.

\*ADDITIONAL QUANTITY LOCATED ELSEWHERE IN PLANS

**FINISHING ROADWAY (PROJECT)**

STATION	213.0100 EACH
USH 12	1
ITEM TOTAL	1

**MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT)**

STATION	618.0100 EACH
USH 12	1
ITEM TOTAL	1

**MOBILIZATIONS EROSION CONTROL**

STATION	628.1910 EROSION CONTROL EACH	628.1905 EMERGENCY EROSION CONTROL EACH
USH 12	2	2
ITEM TOTALS	2	2

NOTE: ALL ITEMS AND QUANTITIES ON THIS SHEET ARE FOR ENGINEER ESTIMATE CATEGORY 0010 UNLESS OTHERWISE NOTED

PROJECT NO: 8949-00-70

HWY: USH 12

COUNTY: DUNN

MISCELLANEOUS QUANTITIES

SHEET

E

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**PERMANENT SIGNING**

STATION	LOCATION	638.2102 MOVING SIGNS TYPE II EACH	REMARKS
USH 12 9+20 12+30	RT	1	SALVAGE & REINSTALL
	RT	1	SALVAGE & REINSTALL
ITEM TOTAL		2	

**CONSTRUCTION STAKING**

STATION	LOCATION	650.4500 SUBGRADE LF	650.5000 BASE LF	*650.6500 STRUCTURE LAYOUT (STRUCTURE) LS	650.9910 SUPPLEMENTAL CONTROL (PROJECT) LS	650.9920 SLOPE STAKES LF
USH 12 6+00 - 15+00	LT & RT	900	900	1	1	900
ITEM TOTALS		900	900	1	1	900

\*CATEGORY 0020

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**FIELD OFFICE TYPE B**

STATION	642.5001 EACH
USH 12	1
ITEM TOTAL	1

**SAWING ASPHALT**

STATION	LOCATION	690.0150 ASPHALT LF	690.0250 CONCRETE LF
USH 12 6+00 - 9+00 9+00 - 14+00	LT & RT LT & RT	306 406	24 24
ITEM TOTAL		712	48

**TRAFFIC CONTROL**

STATION	643.0300 BARRICADES DRUMS DAYS	643.0420 BARRICADES TYPE III DAY	643.0705 WARNING LIGHTS TYPE A DAY	643.0900 SIGNS DAY	643.0920 COVERING SIGNS NUMBER OF CYCLES	643.1050 SIGNS PCMS DAY	643.5000 TRAFFIC CONTROL EACH
USH 12 6+00 - 15+00	70	1542	1500	16400	40	1	40
ITEM TOTAL	70	1542	1500	16400	40	14	1

**INSTALLING AND MAINTAINING  
BIRD DETERRENT SYSTEM**

STATION	999.2000.S EACH
USH 12	1
ITEM TOTAL	1

**PAVEMENT MARKING**

STATION	LOCATION	646.1020 MARKING LINE EPOXY 4-INCH LF	REMARKS
USH 12 6+00 - 15+00 9+00 - 11+00	LT & RT -	1100 400	WHITE EDGELINE DOUBLE YELLOW CENTERLIE
ITEM TOTALS		1500	

**SALVAGE AND REINSTALL FENCE**

STATION	LOCATION	SPV.0090.01 LF
USH 12 9+00 - 9+20	LT	30
ITEM TOTAL		30

NOTE: ALL ITEMS AND QUANTITIES ON THIS SHEET ARE FOR ENGINEER ESTIMATE CATEGORY 0010 UNLESS OTHERWISE NOTED

PROJECT NO: 8949-00-70

HWY: USH 12

COUNTY: DUNN

MISCELLANEOUS QUANTITIES

SHEET

E





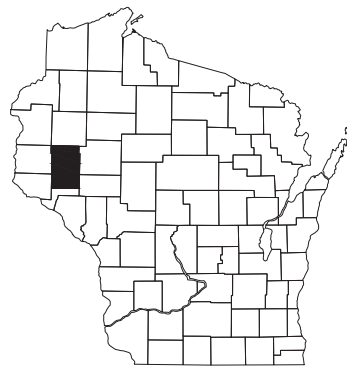
# STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION TRANSPORTATION PROJECT PLAT TITLE SHEET

## 8949-00-20

### BALDWIN - MENOMONIE

WILSON CREEK TRIB CULVERT C-17-0048

## USH 12 DUNN COUNTY



#### CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	----	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	-----	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
NEW REFERENCE LINE	—+—	SIXTEENTH CORNER MONUMENT		OFF-PREMISE SIGN	
NEW R/W LINE	—+—	SIGN		COMPENSABLE	
EXISTING R/W OR HE LINE	—+—	OFF-PREMISE SIGN		NON-COMPENSABLE	
PROPERTY LINE	—+—	COMPENSABLE		NON-COMPENSABLE	
LOT, TIE & OTHER MINOR LINES	-----	TELEPHONE POLE		NO ACCESS (BY STATUTORY AUTHORITY)	
SLOPE INTERCEPT	-----	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)		ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	
CORPORATE LIMITS	-----	ACCESS RESTRICTED BY ACQUISITION		NO ACCESS (NEW HIGHWAY)	
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	-----	NO ACCESS (BY STATUTORY AUTHORITY)		PARCEL NUMBER	
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	-----	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)		UTILITY NUMBER	
TEMPORARY LIMITED EASEMENT AREA	-----	NO ACCESS (NEW HIGHWAY)		PARALLEL OFFSETS	
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	-----	PARCEL NUMBER		BEGIN RELOCATION ORDER	
TRANSMISSION STRUCTURES	-----	UTILITY NUMBER		END RELOCATION ORDER	
BUILDING		TO BE REMOVED			
BRIDGE					

#### CONVENTIONAL ABBREVIATIONS

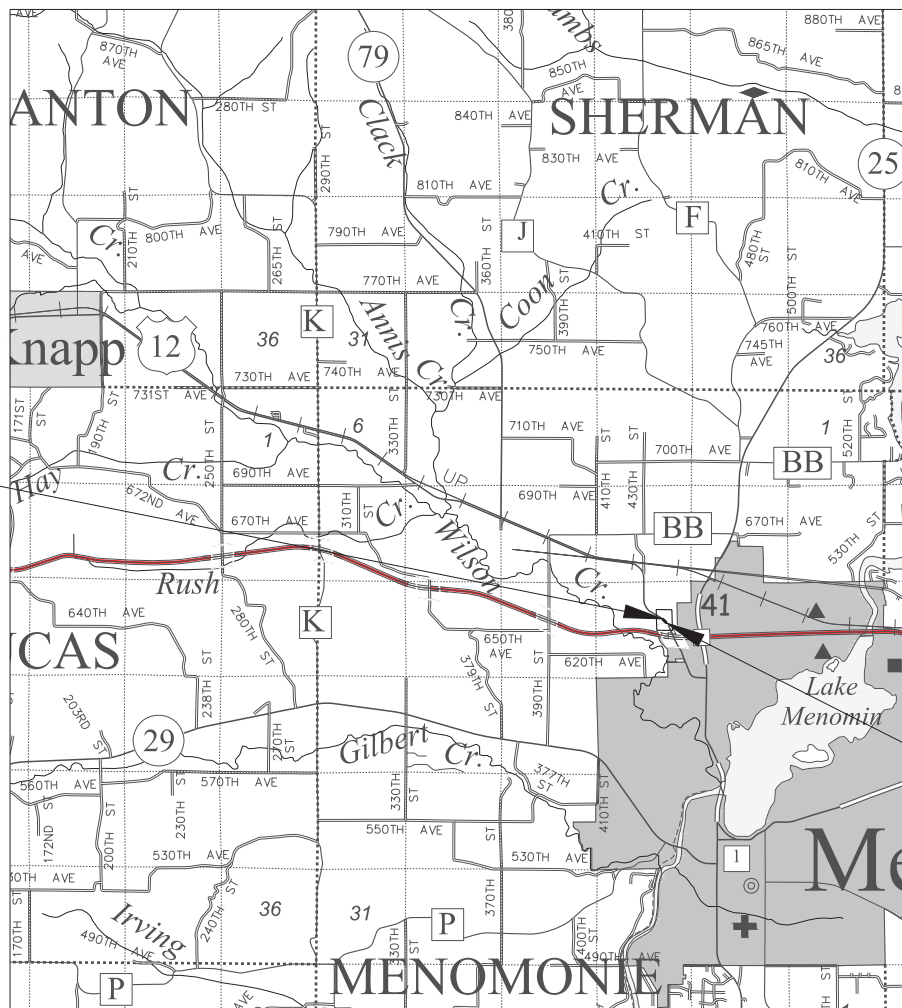
ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS (100')	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT	RDE
CENTERLINE	C/L	EASEMENT	
CERTIFIED SURVEY MAP	CSM	RIGHT	RT
CONCRETE	CONC	RIGHT OF WAY	R/W
COUNTY	CO	SECTION	SEC
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV
DISTANCE	DIST	SQUARE FEET	SF
CORNER	COR	STATE TRUNK HIGHWAY	STH
DOCUMENT NUMBER	DOC	STATION	STA
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED	TLE
GAS VALVE	GV	EASEMENT	
GRID NORTH	GN	TRANSPORTATION PROJECT	TPP
HIGHWAY EASEMENT	HE	PLAT	
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

#### CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

#### CONVENTIONAL UTILITY SYMBOLS

—W—	WATER
—G—	GAS
—T—	TELEPHONE
—OH—	OVERHEAD
—E—	ELECTRIC
—TV—	CABLE TELEVISION
—FO—	FIBER OPTIC
—SAN—	SANITARY SEWER
—SS—	STORM SEWER



THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 8949-00-20

#### NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), DUNN COUNTY, NAD83 2011 IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

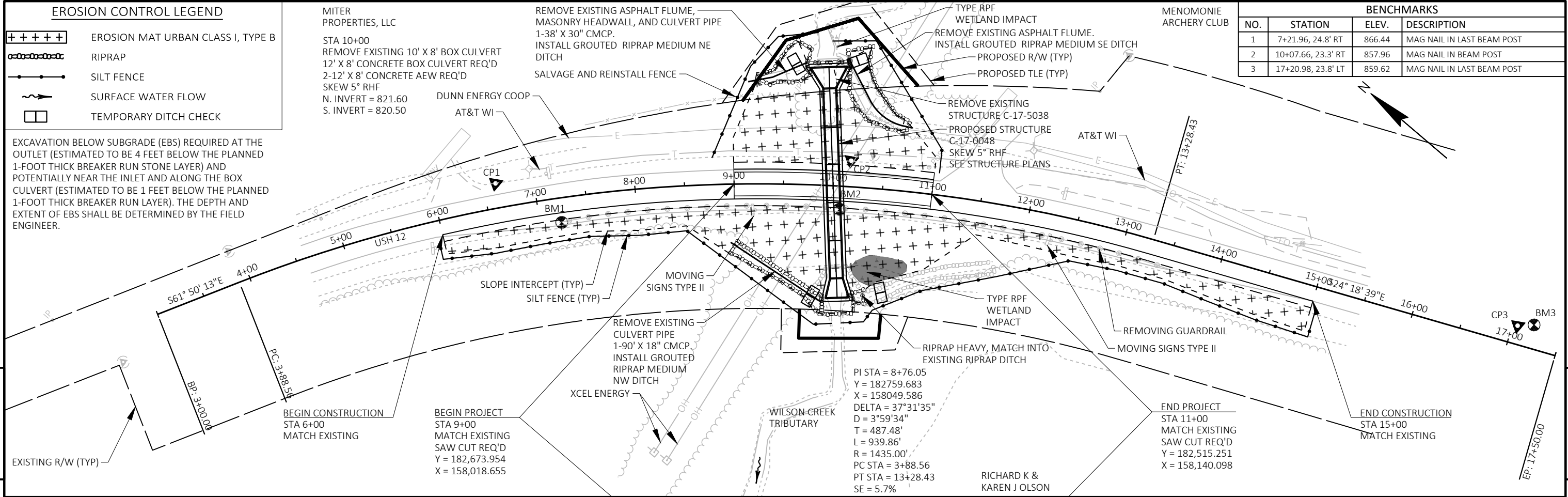
A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN EAU CLAIRE.

PARCEL IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE SCHEDULE OF LANDS & INTERESTS REQUIRED.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.



BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1	7+21.96, 24.8' RT	866.44	MAG NAIL IN LAST BEAM POST
2	10+07.66, 23.3' RT	857.96	MAG NAIL IN BEAM POST
3	17+20.98, 23.8' LT	859.62	MAG NAIL IN LAST BEAM POST

**EROSION CONTROL LEGEND**

- ++++ EROSION MAT URBAN CLASS I, TYPE B
- RIPRAP
- SILT FENCE
- SURFACE WATER FLOW
- TEMPORARY DITCH CHECK

EXCAVATION BELOW SUBGRADE (EBS) REQUIRED AT THE OUTLET (ESTIMATED TO BE 4 FEET BELOW THE PLANNED 1-FOOT THICK BREAKER RUN STONE LAYER) AND POTENTIALLY NEAR THE INLET AND ALONG THE BOX CULVERT (ESTIMATED TO BE 1 FEET BELOW THE PLANNED 1-FOOT THICK BREAKER RUN LAYER). THE DEPTH AND EXTENT OF EBS SHALL BE DETERMINED BY THE FIELD ENGINEER.

MITER PROPERTIES, LLC  
 STA 10+00  
 REMOVE EXISTING 10' X 8' BOX CULVERT  
 12' X 8' CONCRETE BOX CULVERT REQ'D  
 2-12' X 8' CONCRETE AEW REQ'D  
 SKEW 5° RHF  
 N. INVERT = 821.60  
 S. INVERT = 820.50

REMOVE EXISTING ASPHALT FLUME, MASONRY HEADWALL, AND CULVERT PIPE 1-38' X 30" CMCP.  
 INSTALL GROUTED RIPRAP MEDIUM NE DITCH  
 SALVAGE AND REINSTALL FENCE

TYPE RPF WETLAND IMPACT  
 REMOVE EXISTING ASPHALT FLUME. INSTALL GROUTED RIPRAP MEDIUM SE DITCH  
 PROPOSED R/W (TYP)  
 PROPOSED TLE (TYP)

MENOMONIE ARCHERY CLUB

REMOVE EXISTING STRUCTURE C-17-5038  
 PROPOSED STRUCTURE C-17-0048  
 SKEW 5° RHF  
 SEE STRUCTURE PLANS

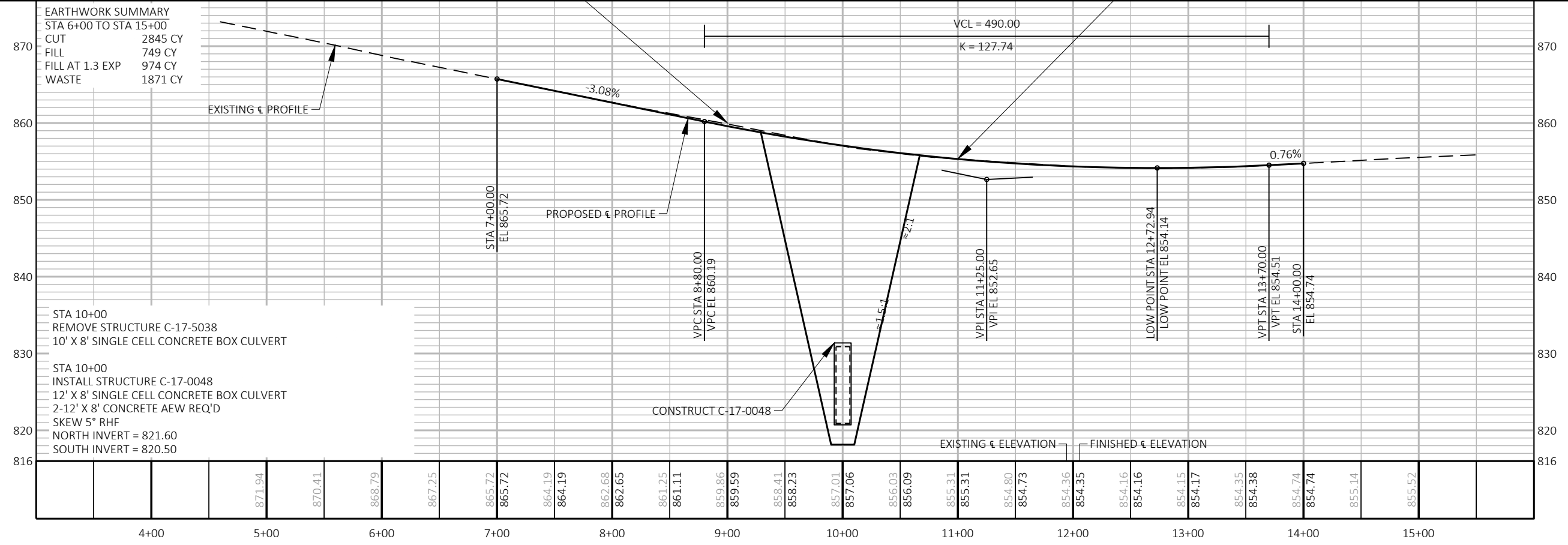
REMOVE EXISTING CULVERT PIPE 1-90' X 18" CMCP.  
 INSTALL GROUTED RIPRAP MEDIUM NW DITCH

PI STA = 8+76.05  
 Y = 182759.683  
 X = 158049.586  
 DELTA = 37°31'35"  
 D = 3°59'34"  
 T = 487.48'  
 L = 939.86'  
 R = 1435.00'  
 PC STA = 3+88.56  
 PT STA = 13+28.43  
 SE = 5.7%

RICHARD K & KAREN J OLSON

**EARTHWORK SUMMARY**  
 STA 6+00 TO STA 15+00

CUT	2845 CY
FILL	749 CY
FILL AT 1.3 EXP	974 CY
WASTE	1871 CY

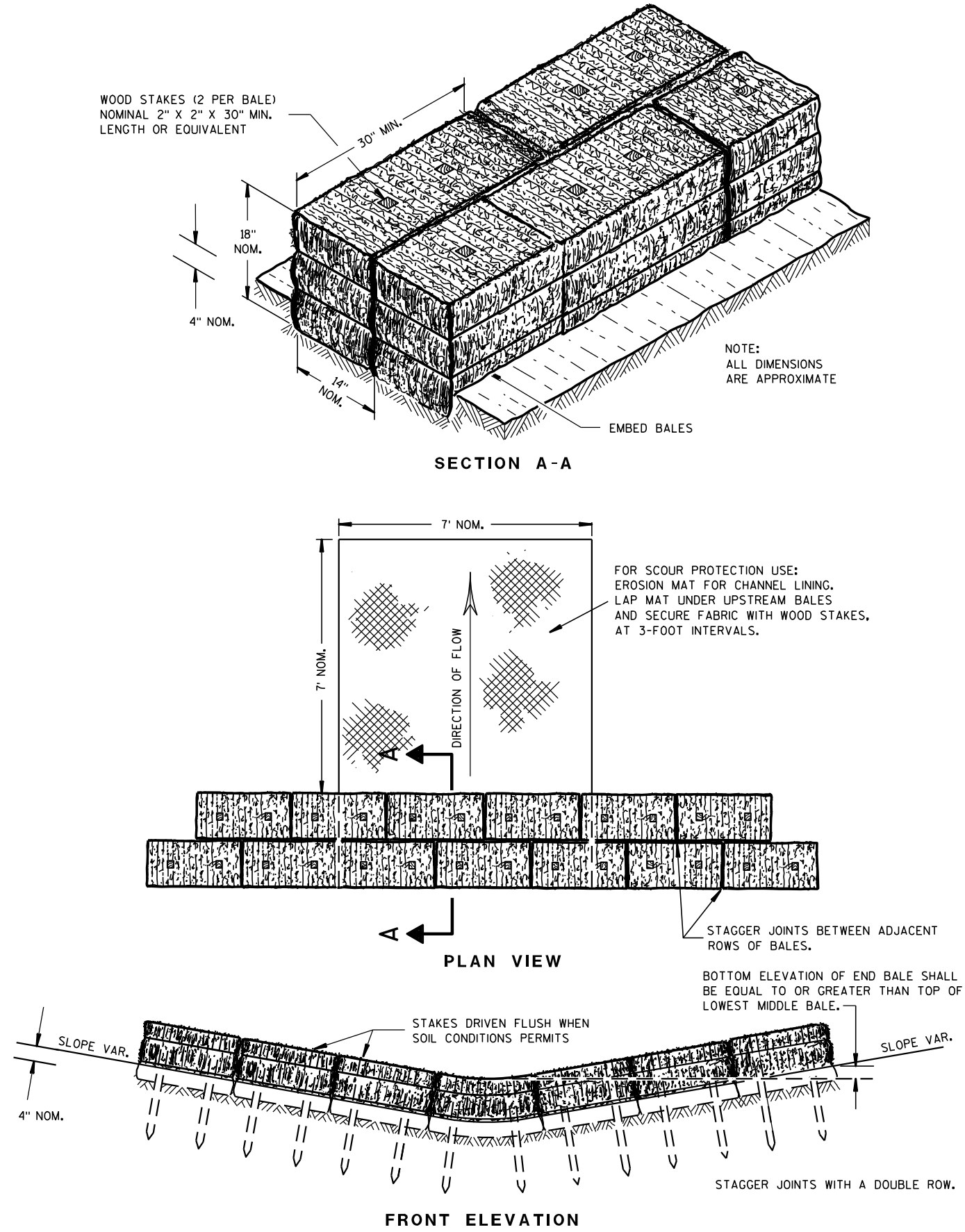


STA 10+00  
 REMOVE STRUCTURE C-17-5038  
 10' X 8' SINGLE CELL CONCRETE BOX CULVERT

STA 10+00  
 INSTALL STRUCTURE C-17-0048  
 12' X 8' SINGLE CELL CONCRETE BOX CULVERT  
 2-12' X 8' CONCRETE AEW REQ'D  
 SKEW 5° RHF  
 NORTH INVERT = 821.60  
 SOUTH INVERT = 820.50

## Standard Detail Drawing List

08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
12A03-10	NAME PLATE (STRUCTURES)
13C19-03	HMA LONGITUDINAL JOINTS
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-08C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-20A	LONGITUDINAL MARKING (MAINLINE)
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

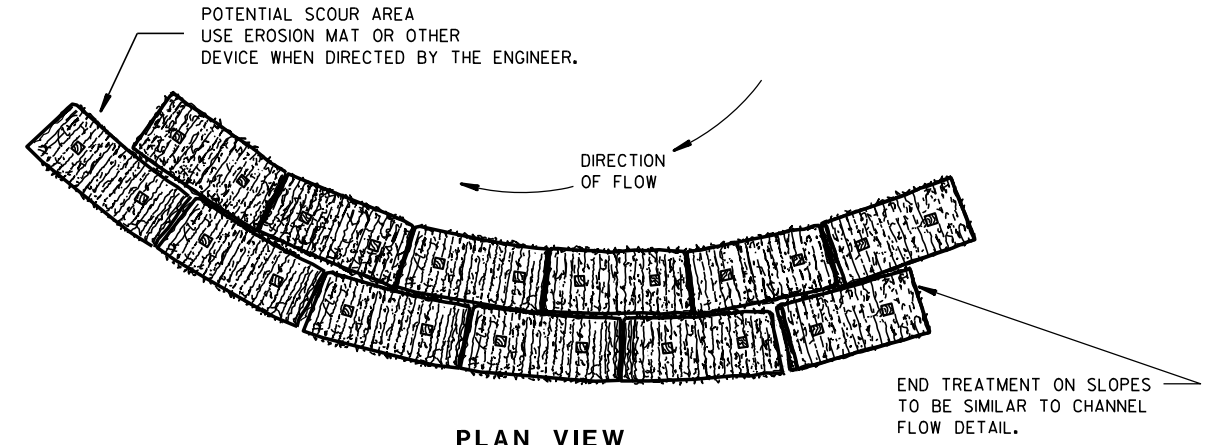


TEMPORARY DITCH CHECK USING EROSION BALES ①

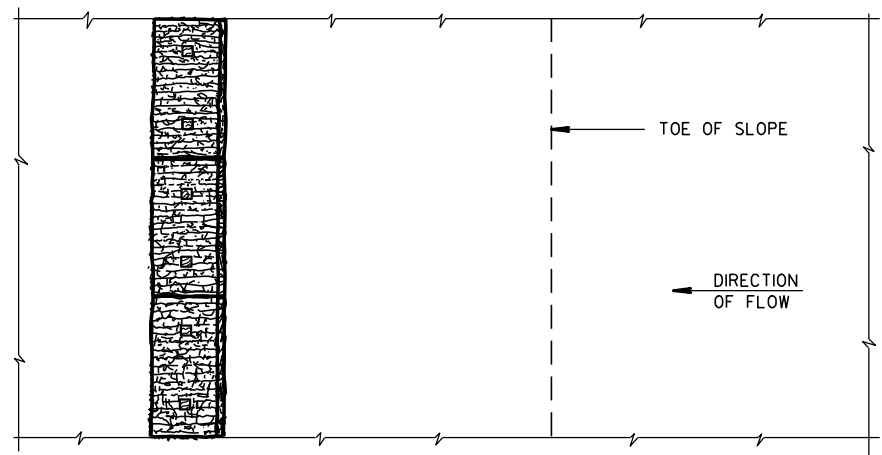
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

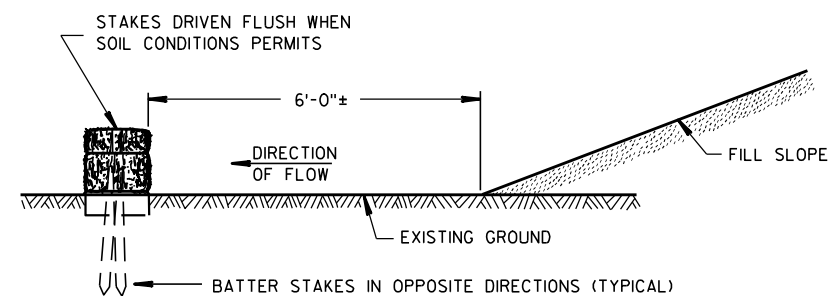
- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.



PLAN VIEW WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW

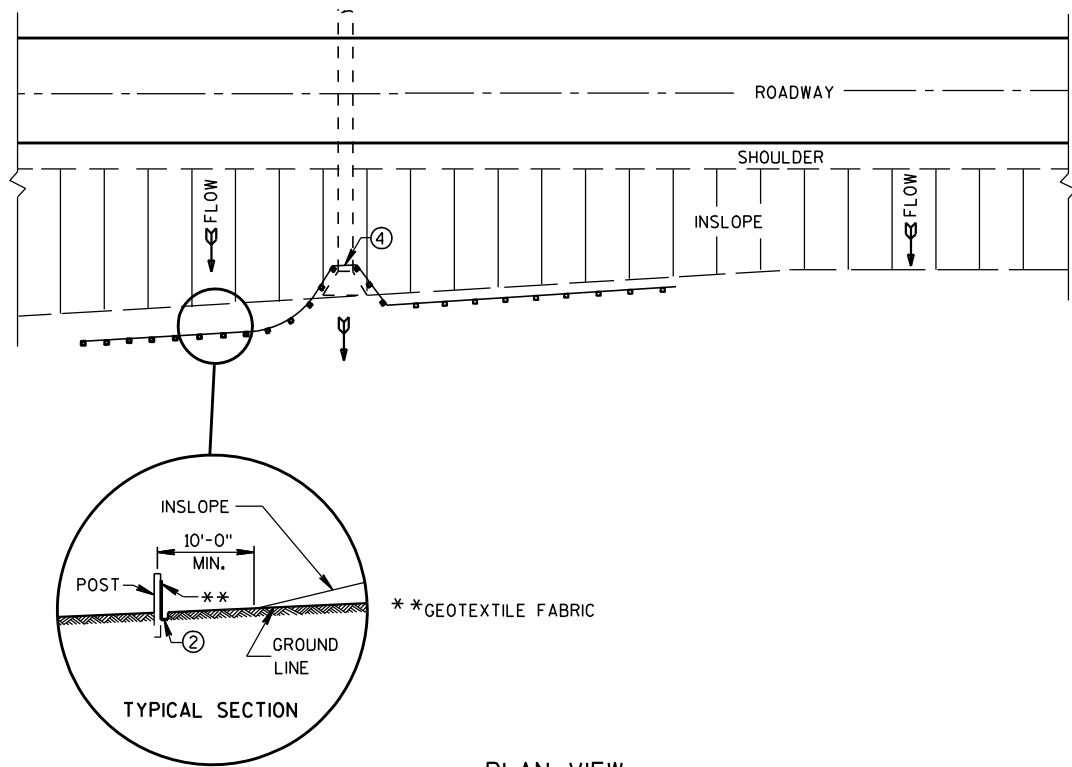


FRONT ELEVATION WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE EROSION BALES FOR SHEET FLOW

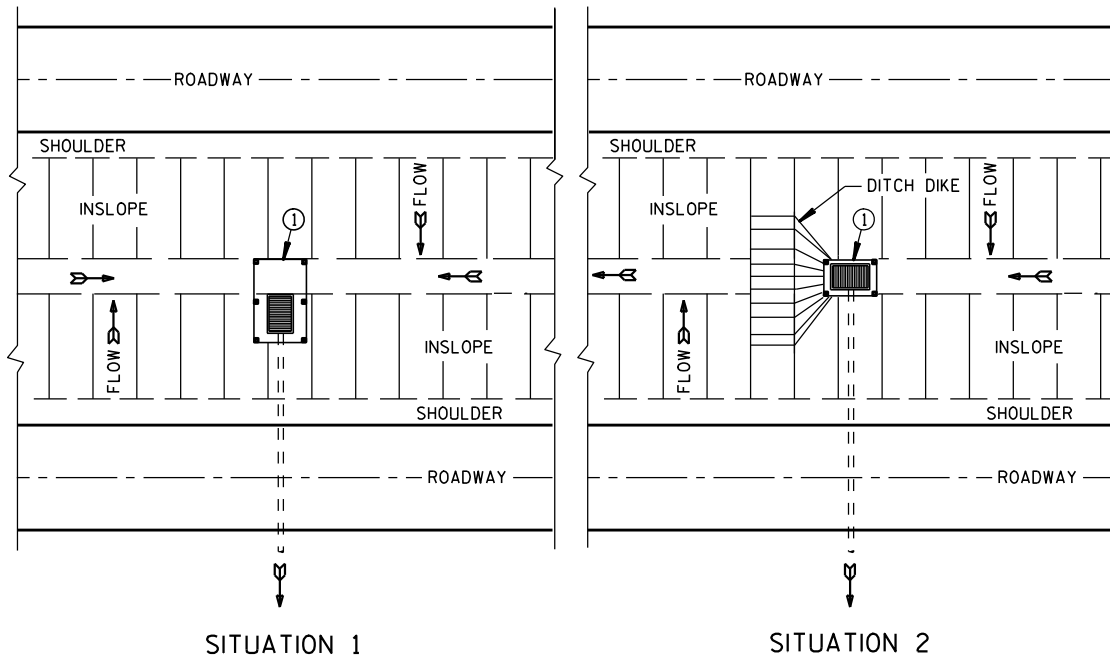
TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED  
 6/04/02 /S/ Beth Canestra  
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER  
 FHWA



PLAN VIEW  
TYPICAL APPLICATION OF SILT FENCE

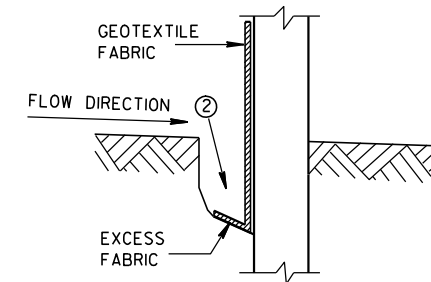


SITUATION 1 SITUATION 2  
PLAN VIEW  
SILT FENCE AT MEDIAN SURFACE DRAINS

**GENERAL NOTES**

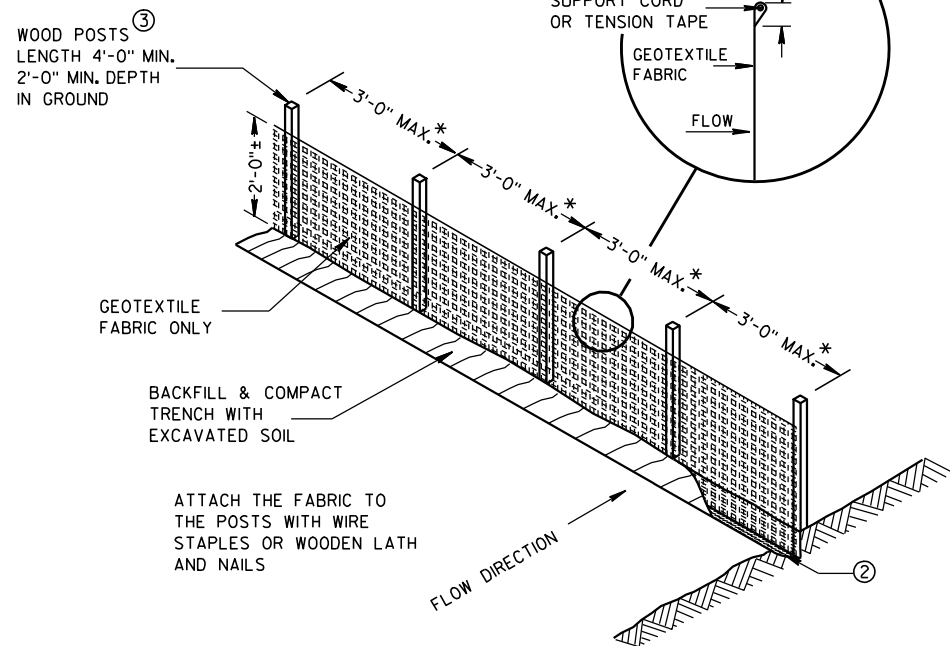
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



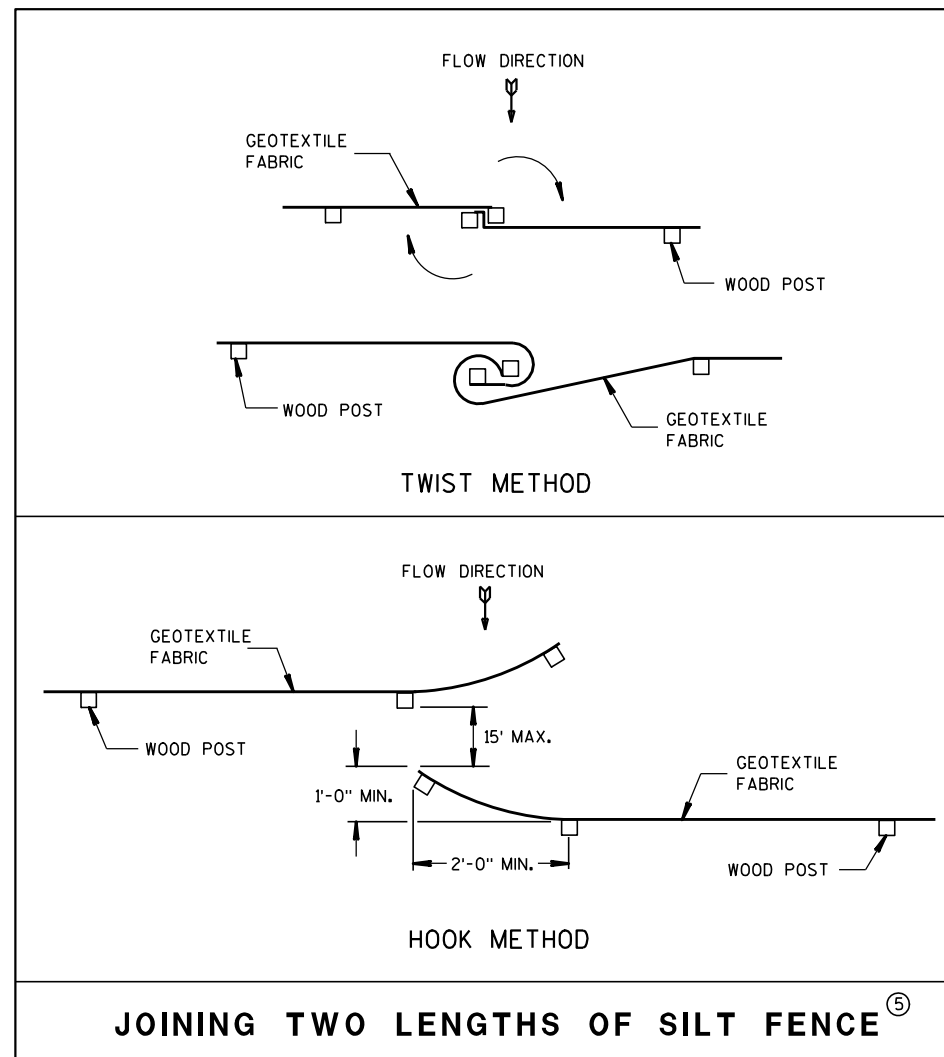
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

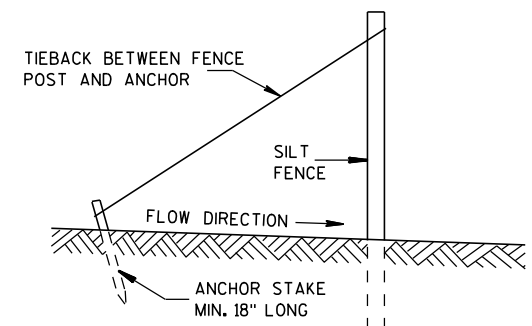


SILT FENCE

\* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤

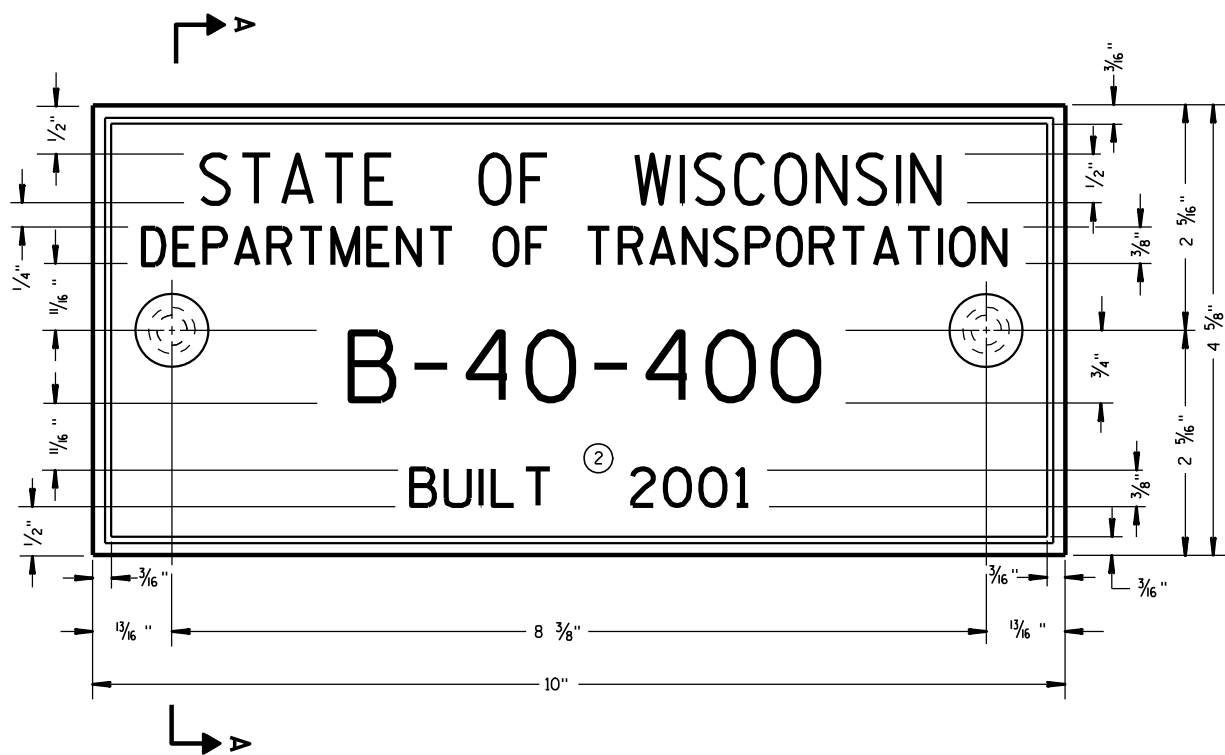


SILT FENCE TIE BACK  
(WHEN REQUIRED BY THE ENGINEER)

**SILT FENCE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
4-29-05 /S/ Beth Cannestra  
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER  
FHWA



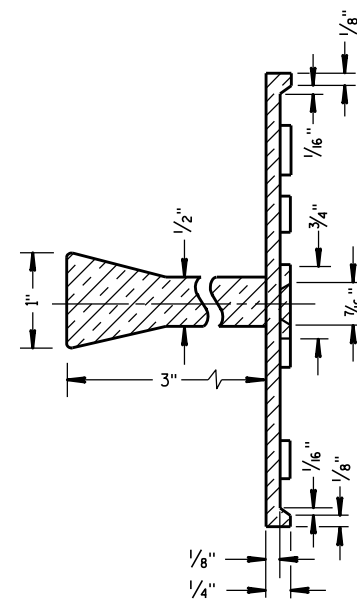
**TYPICAL NAME PLATE**  
(BRIDGES, CULVERTS, AND RETAINING WALLS)

**GENERAL NOTES**

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

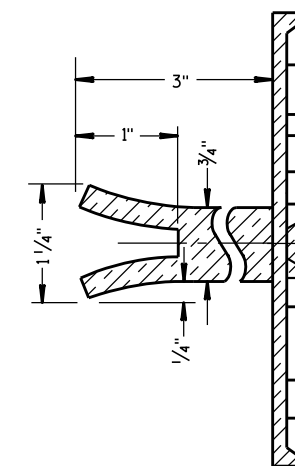
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.

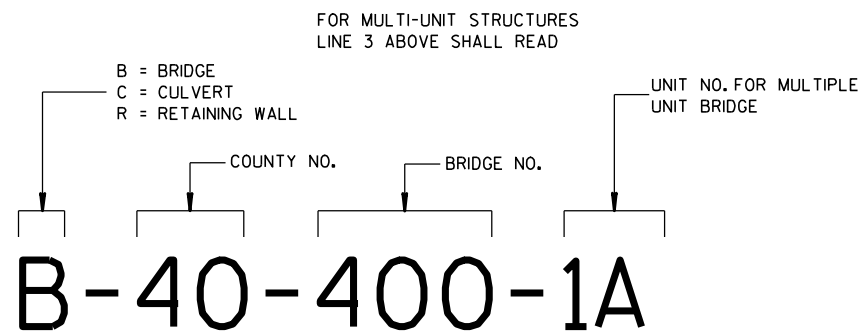


**SECTION A-A**

SPREAD OPEN SO THE TOP OF LUG IS 1 1/4" WIDE

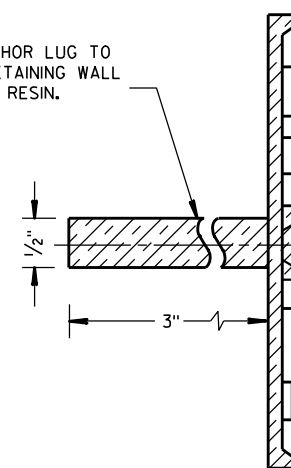


**ALTERNATE LUG**



**NUMBERING DESIGNATION  
MULTI-UNIT STRUCTURES**

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.

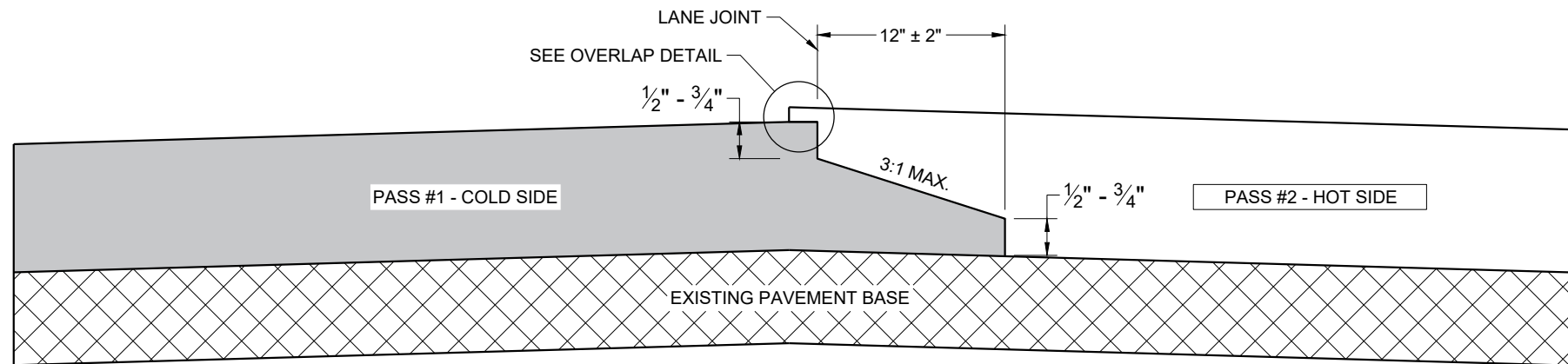


**ALTERNATE LUG**  
(FOR ATTACHMENT TO PRECAST STRUCTURES)

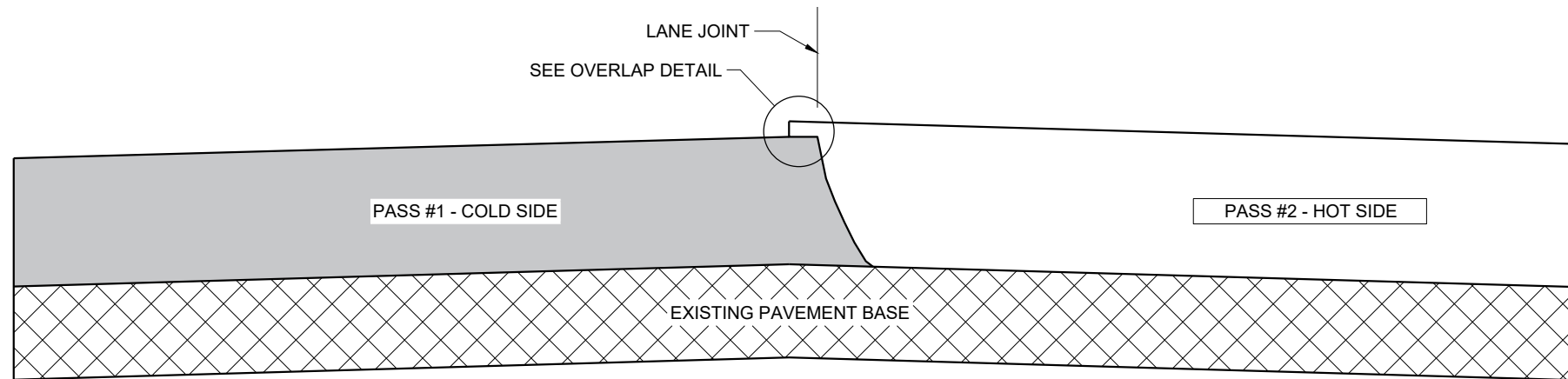
**NAME PLATE  
(STRUCTURES)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

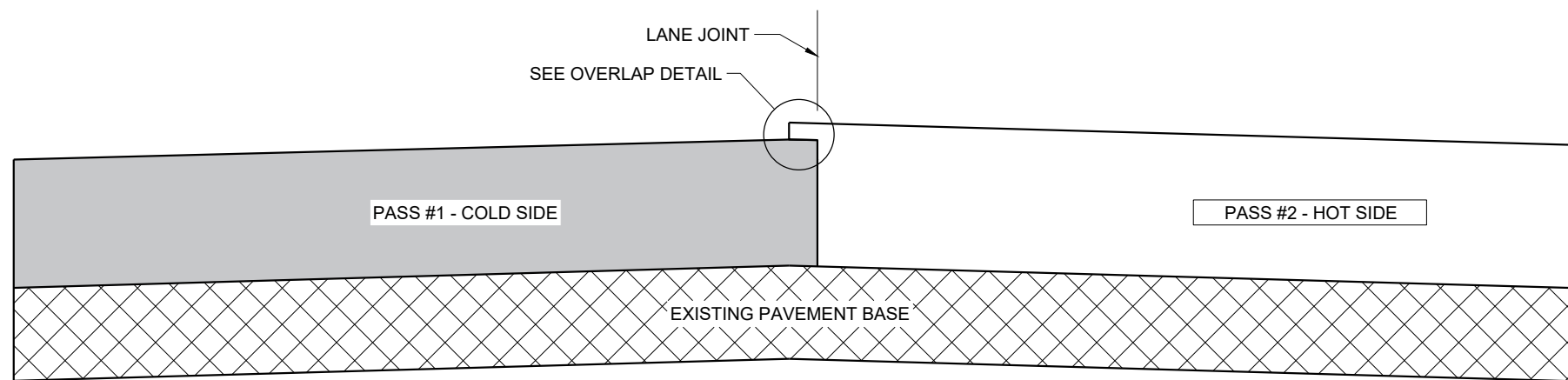
APPROVED  
DATE 3/26/10 /S/ Scot Becker  
CHIEF STRUCTURAL DEVELOPMENT ENGINEER  
FHWA



**TYPICAL PAVEMENT CROSS SECTION  
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT (MILLED)**

**GENERAL NOTES**

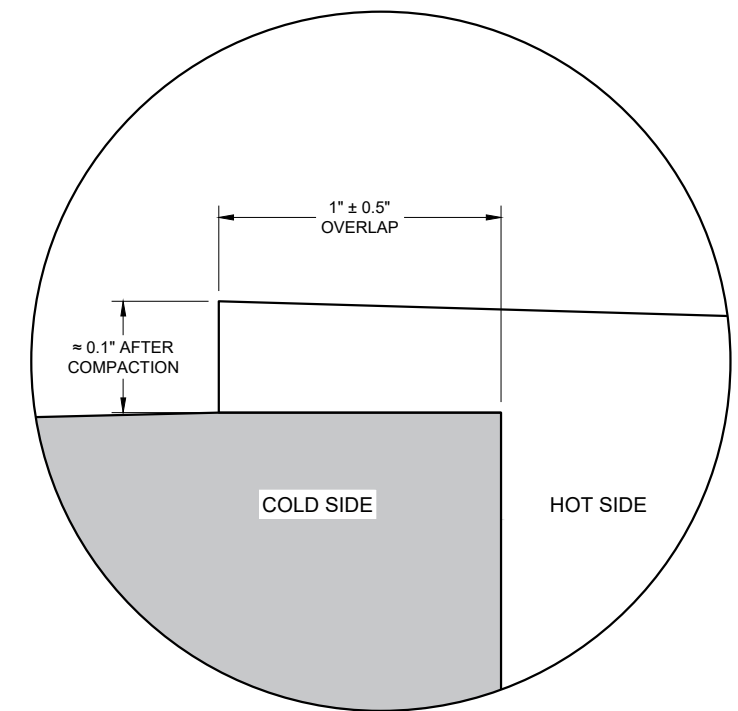
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY  $1" \pm 0.5"$  AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY  $0.1"$  AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO  $2"$  FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



**OVERLAP DETAIL (TYPICAL)**

6

6

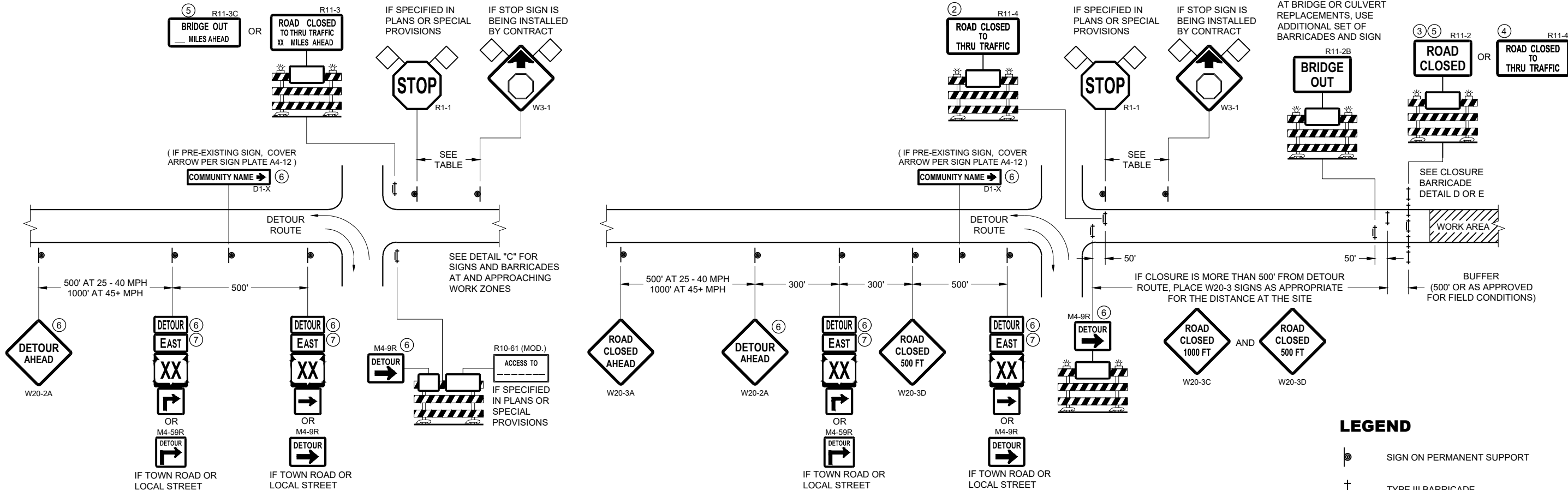
SDD 13C19 - 03

SDD 13C19 - 03

**HMA LONGITUDINAL JOINTS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2020 /S/ Steven Hefel  
DATE HMA PAVEMENT ENGINEER  
FHWA



**DETAIL A  
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

**DETAIL B  
MAINLINE CLOSURE WITH POSTED DETOUR**

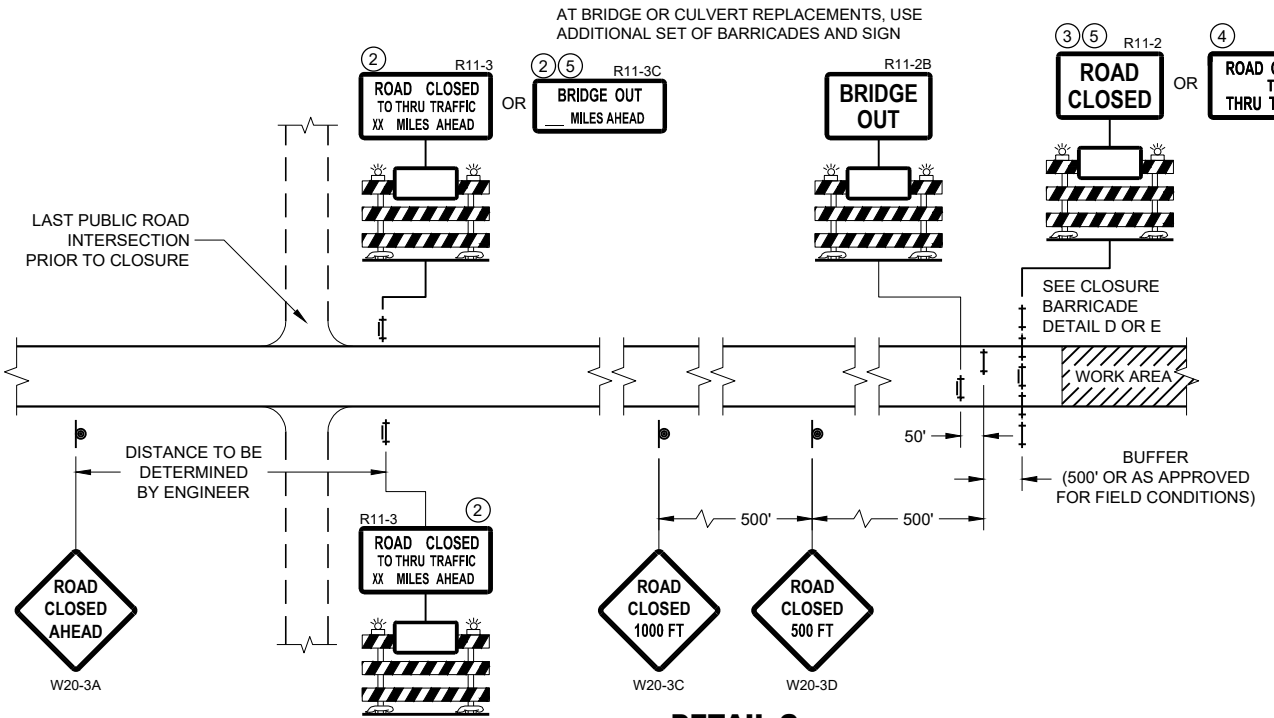
WORK ZONE LESS THAN 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

**LEGEND**

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- M4 - 8
- M3 - X
- M1 - 4 OR M1 - 6 OR M1 - 5A
- M05 - 1 OR M06 - 1

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"  
FOR GENERAL NOTES  
AND FOOTNOTES ① THROUGH ⑦



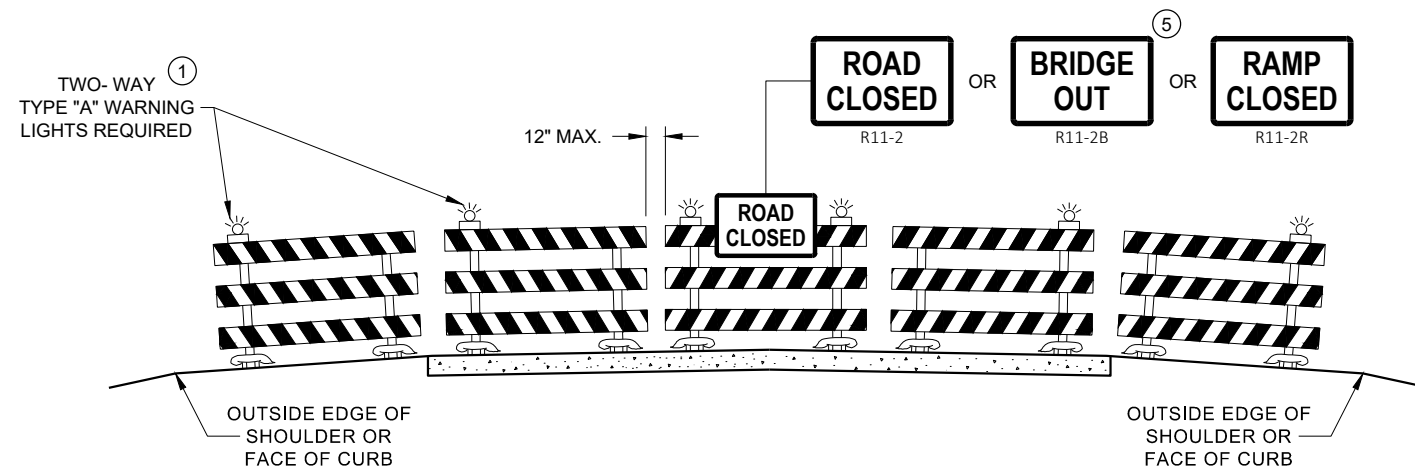
**DETAIL C  
MAINLINE CLOSURE, NO POSTED DETOUR**

**BARRICADES AND SIGNS  
FOR MAINLINE CLOSURES**

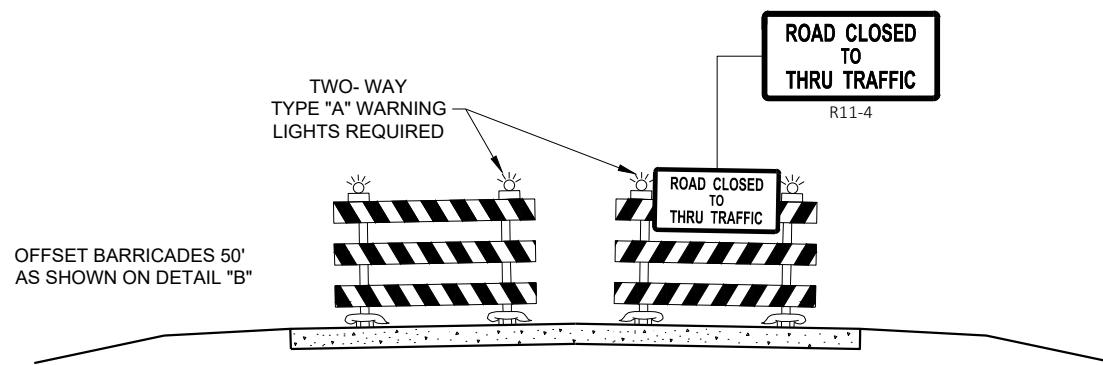
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER





**DETAIL D  
ROAD CLOSURE BARRICADE DETAIL  
APPROACH VIEW**



**DETAIL E  
LANE CLOSURE BARRICADE DETAIL  
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS  
FOR  
VARIOUS CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA

### LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DETOUR M4 - 8
- EAST M3 - X
- XX M1 - 4 OR XX M1 - 6 OR COUNTY X M1 - 5A
- M05 - 1 OR M06 - 1 OR M06 - 1

### GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

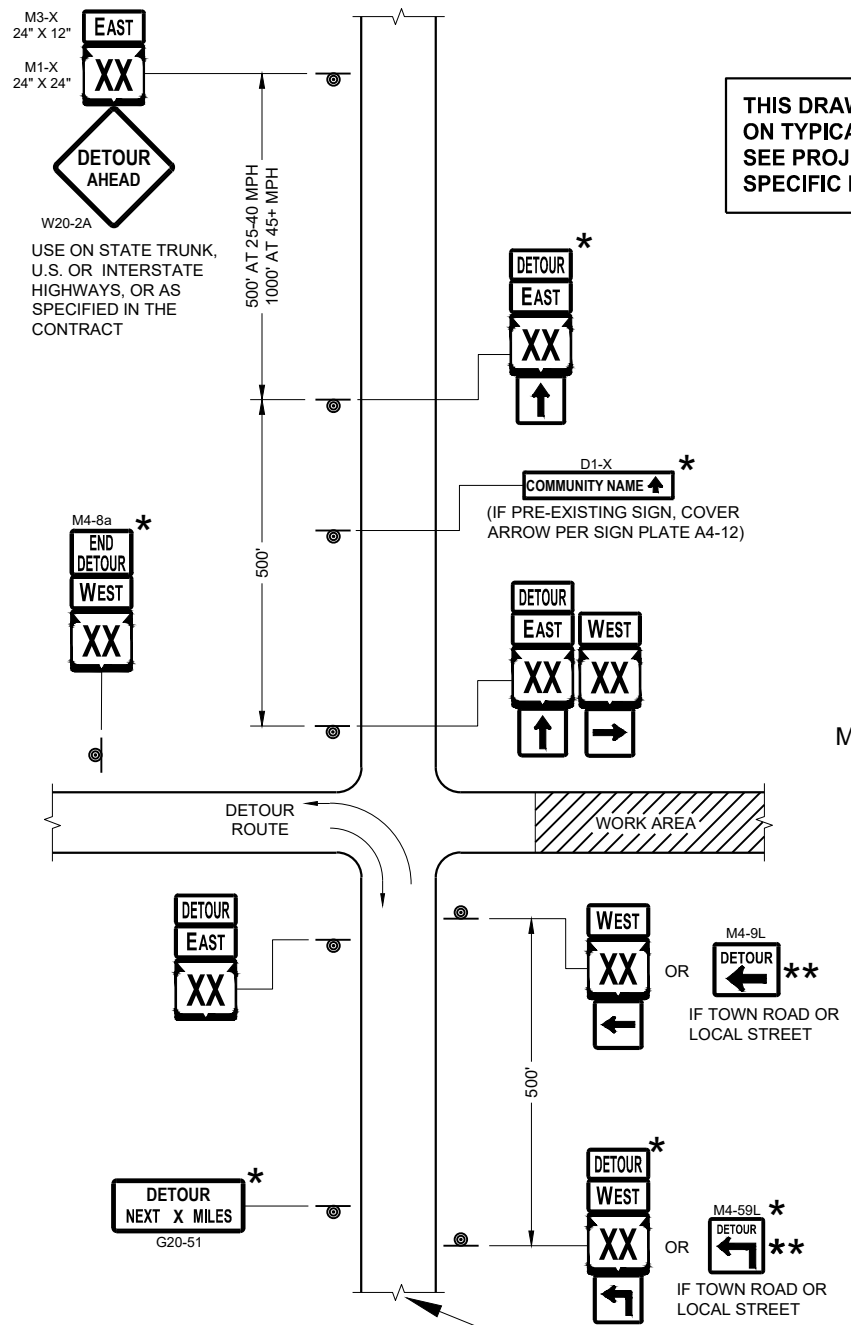
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

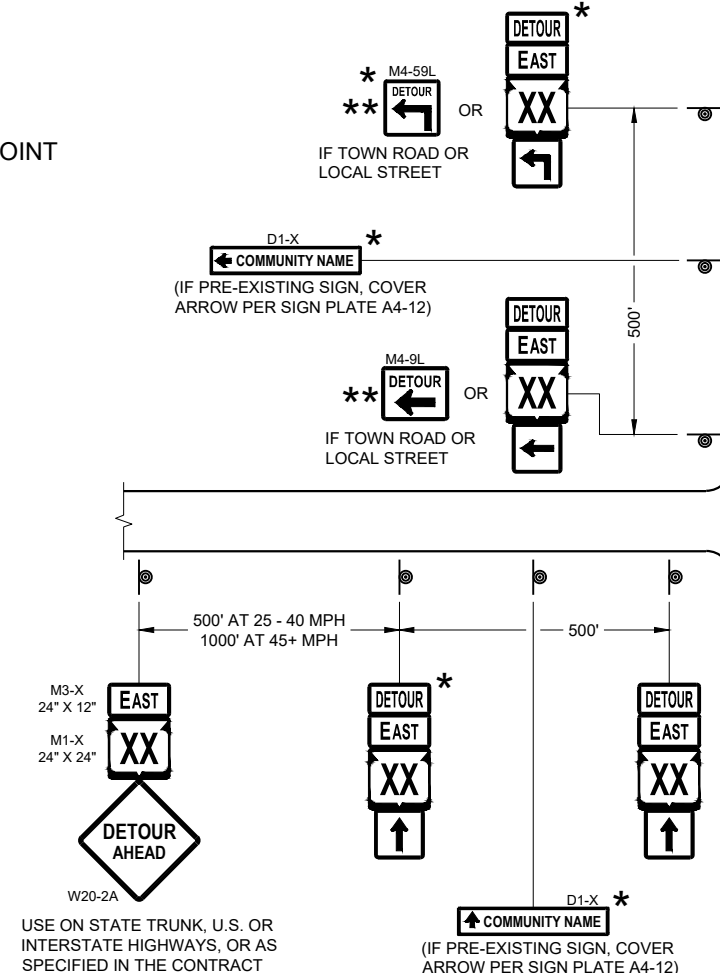
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- \* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- \*\* FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

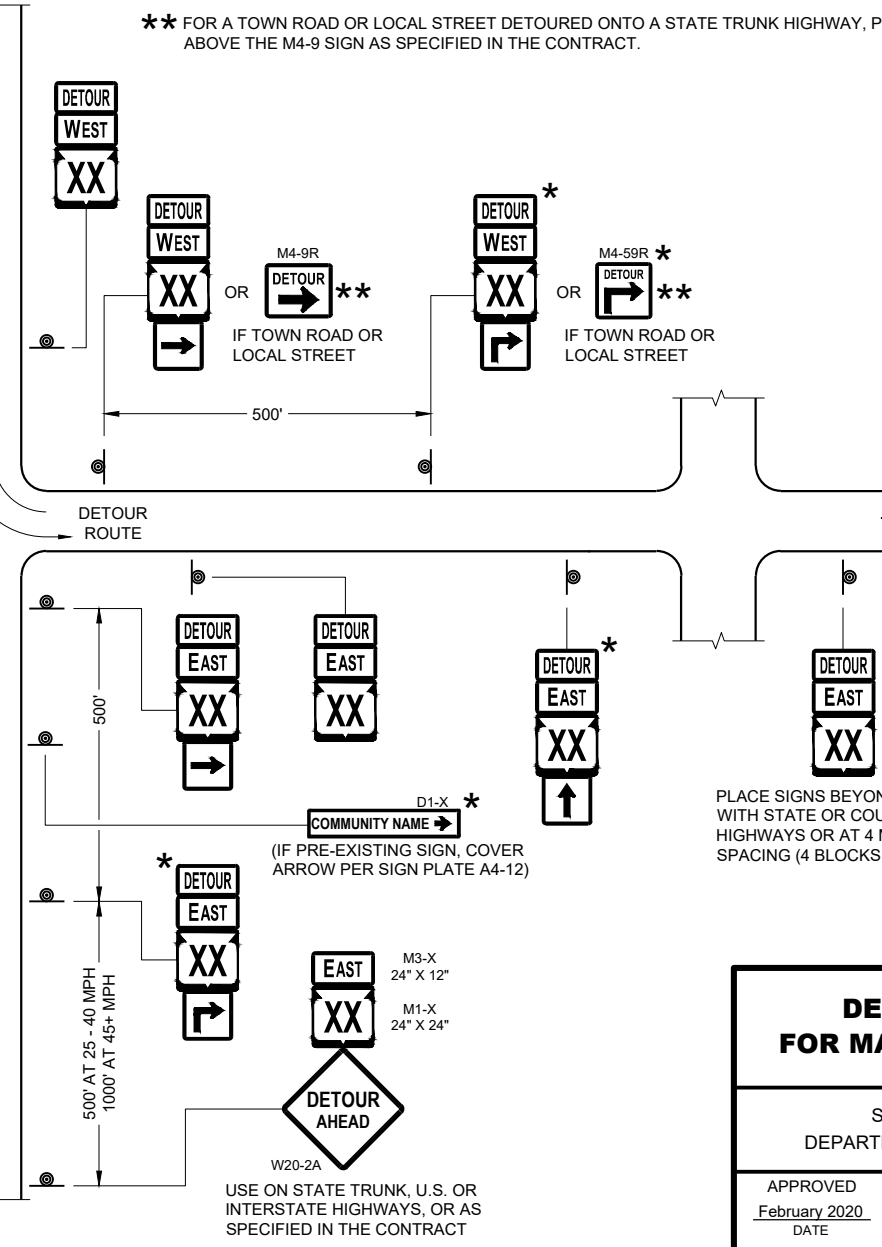
THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.



MATCH POINT

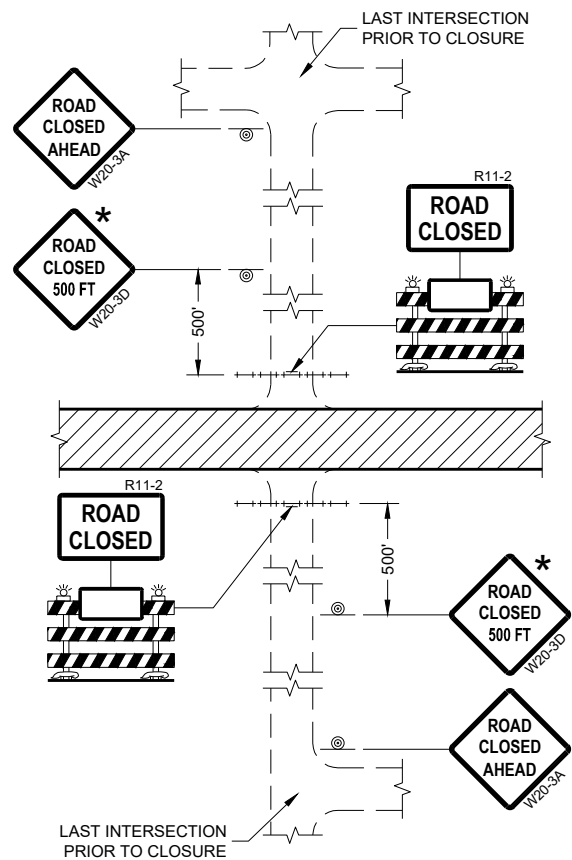


### DETAIL F DETOUR SIGNING

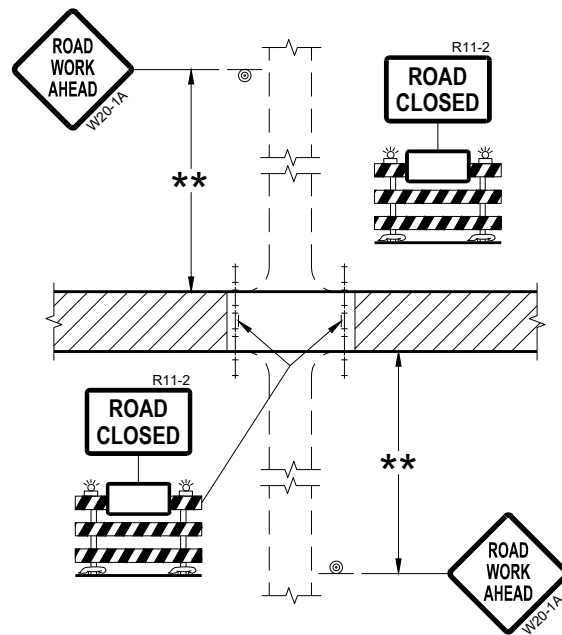


SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

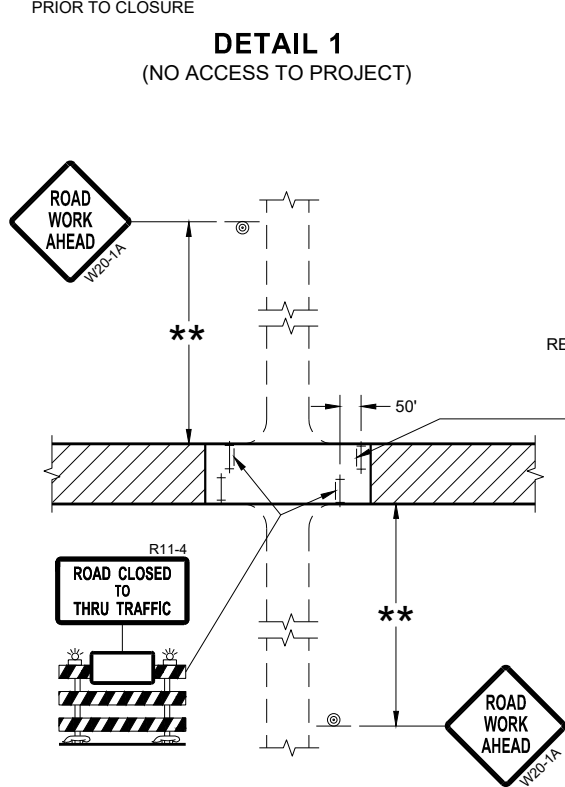
DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2020 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



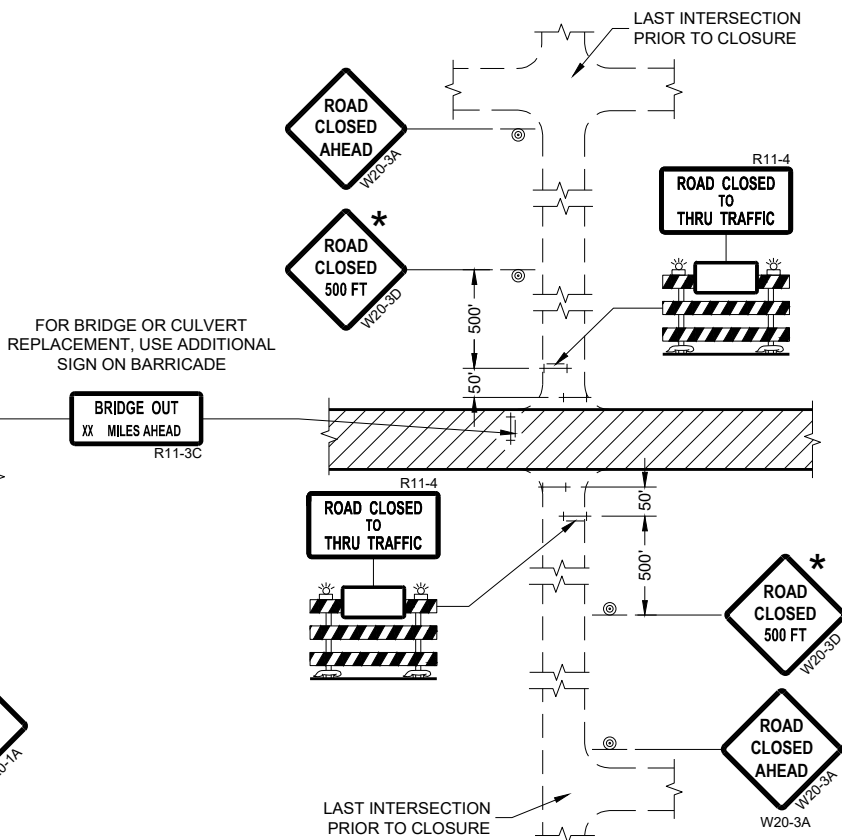
**DETAIL 1**  
(NO ACCESS TO PROJECT)



**DETAIL 2**  
(PUBLIC CROSS-TRAFFIC MAINTAINED.  
NO ACCESS TO PROJECT)



**DETAIL 3**  
(PUBLIC CROSS-TRAFFIC MAINTAINED.  
CONTRACTOR, LOCAL BUSINESS AND  
RESIDENT ACCESS TO PROJECT)



**DETAIL 4**  
(CONTRACTOR, LOCAL BUSINESS AND  
RESIDENT ACCESS TO PROJECT)

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:  
R11-2 SHALL BE 48" X 30".  
R11-4 AND R11-3 SHALL BE 60" X 30".

- \* OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- \*\* 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

**LEGEND**

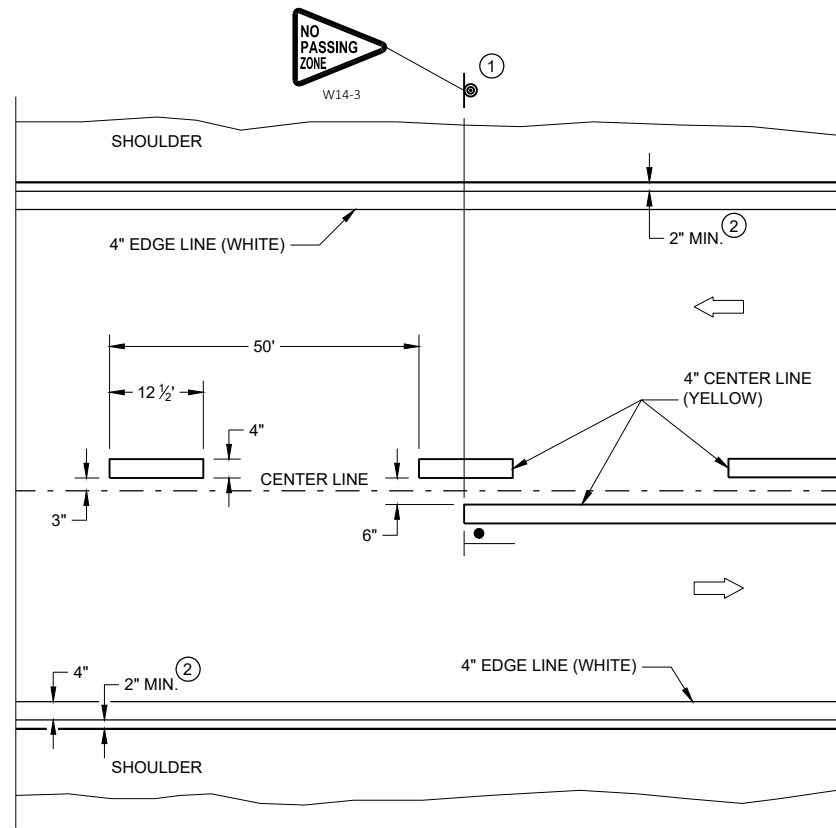
- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS  
FOR  
SIDEROAD CLOSURES**

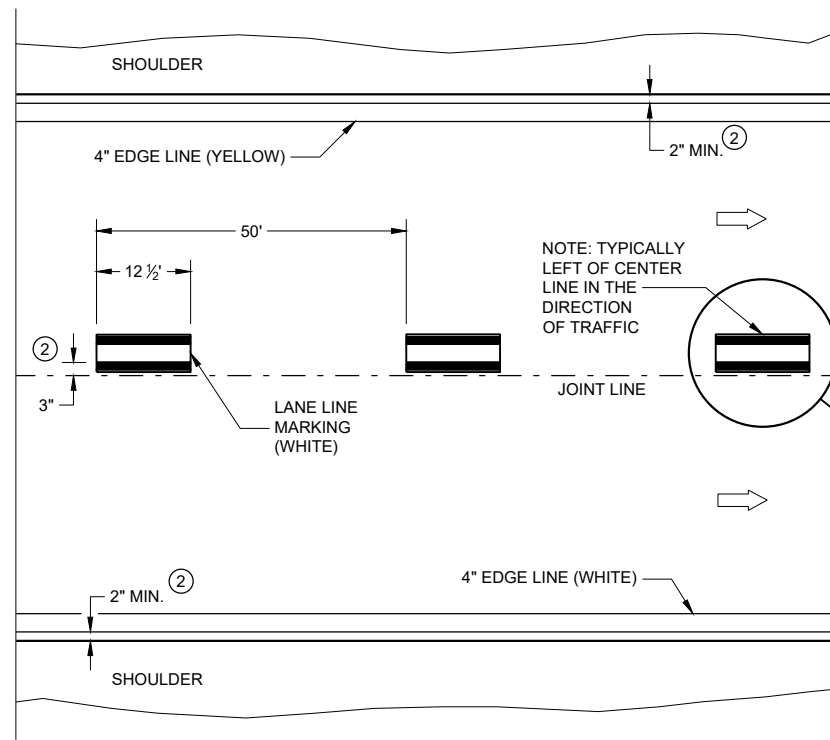
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
July 2018 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

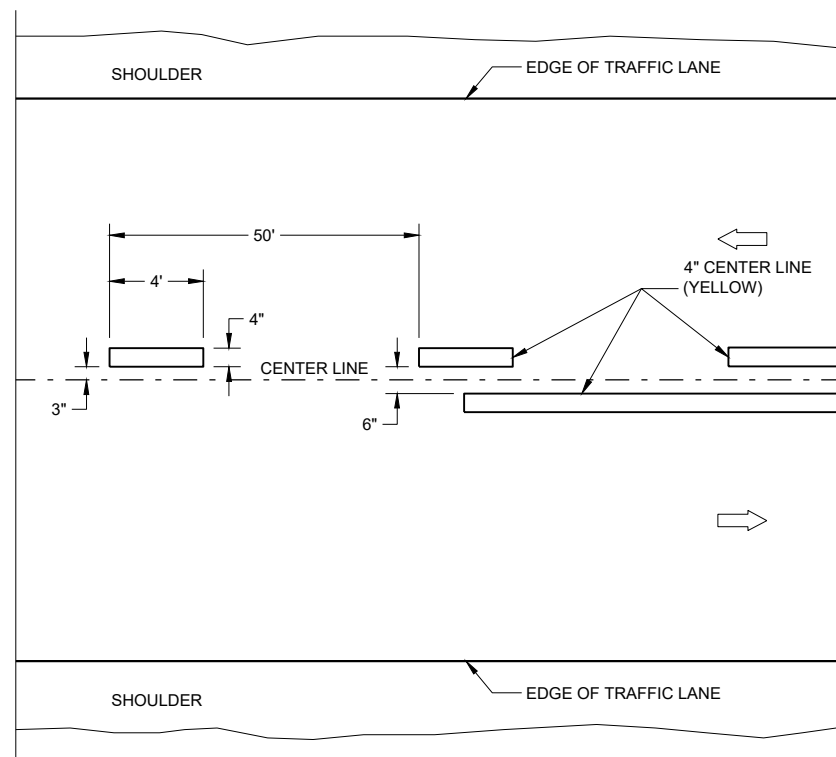


**TWO WAY TRAFFIC**

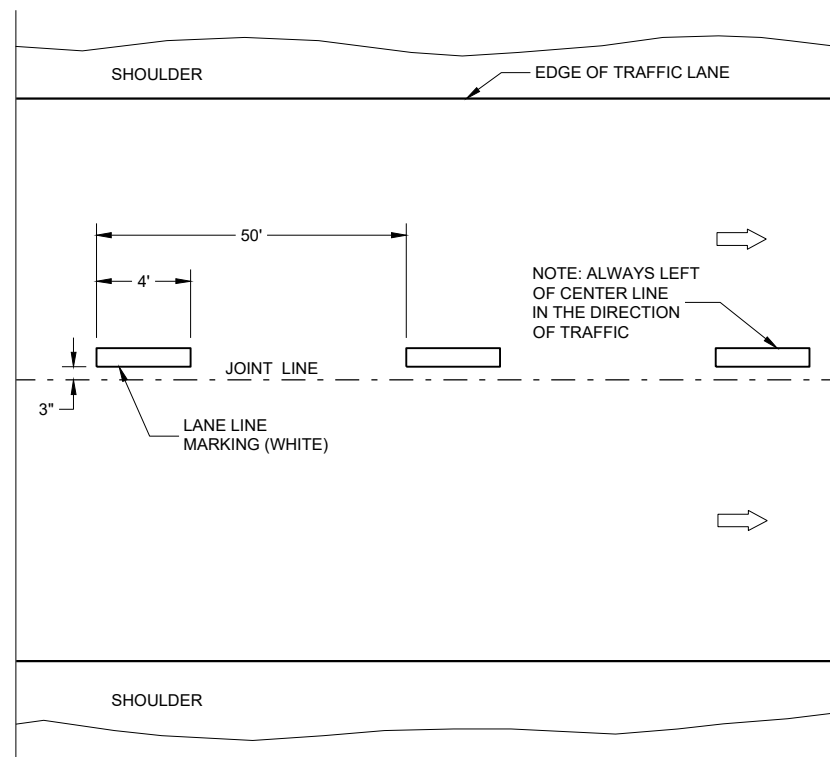


**ONE WAY TRAFFIC**

**PERMANENT PAVEMENT MARKING**



**TWO WAY TRAFFIC**



**ONE WAY TRAFFIC**

**TEMPORARY PAVEMENT MARKING**

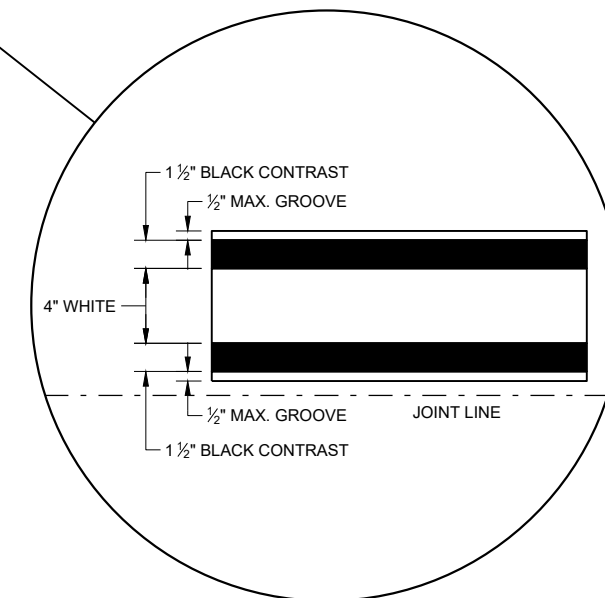
**GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

**LEGEND**

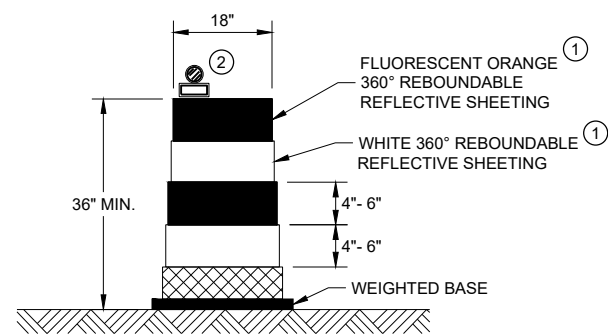
- |— "T" MARKING
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC



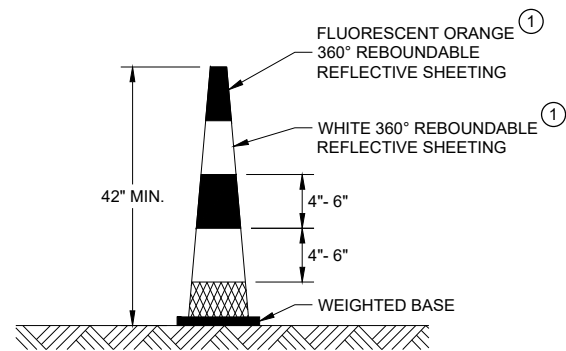
**LONGITUDINAL MARKING  
(MAINLINE)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Matthew Rauch  
DATE STATEWIDE SIGNING AND MARKING  
ENGINEER



**DRUM**

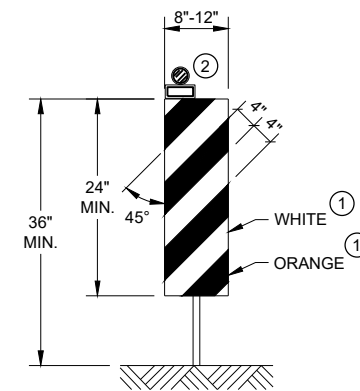


**42" CONE**

DO NOT USE IN TAPERS  
1/2 SPACING OF DRUMS

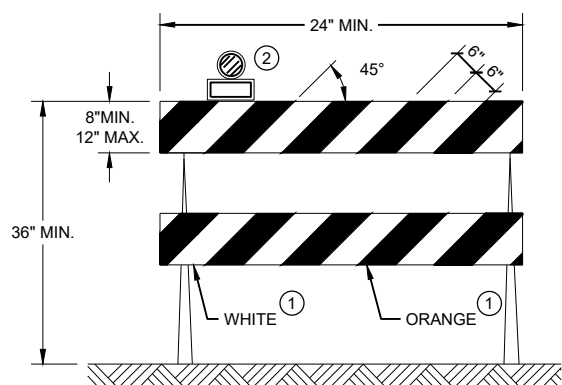
**GENERAL NOTES**

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



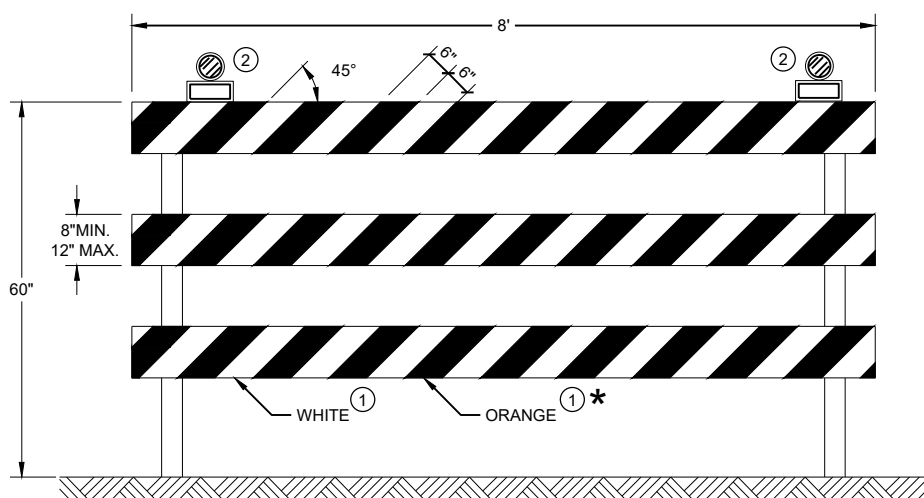
**VERTICAL PANEL**

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



**TYPE II BARRICADE**

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



**TYPE III BARRICADE**

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

\* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES  
DRUMS, CONES, BARRICADES  
AND VERTICAL PANELS**

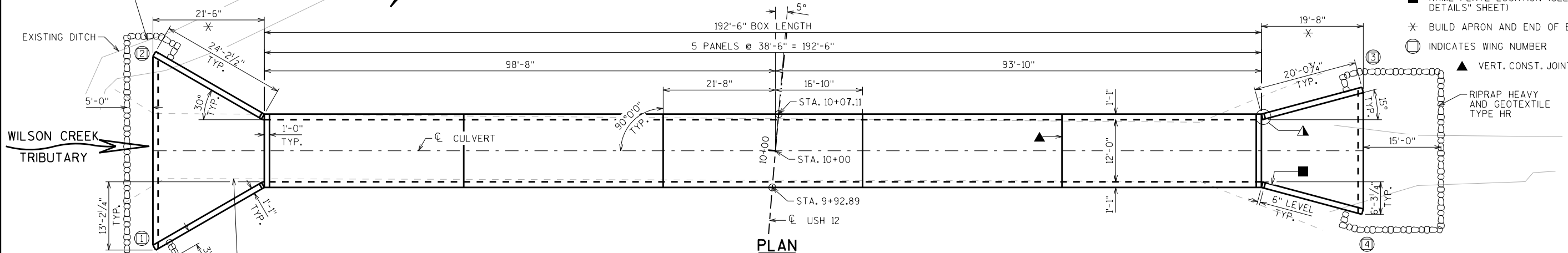
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2021 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

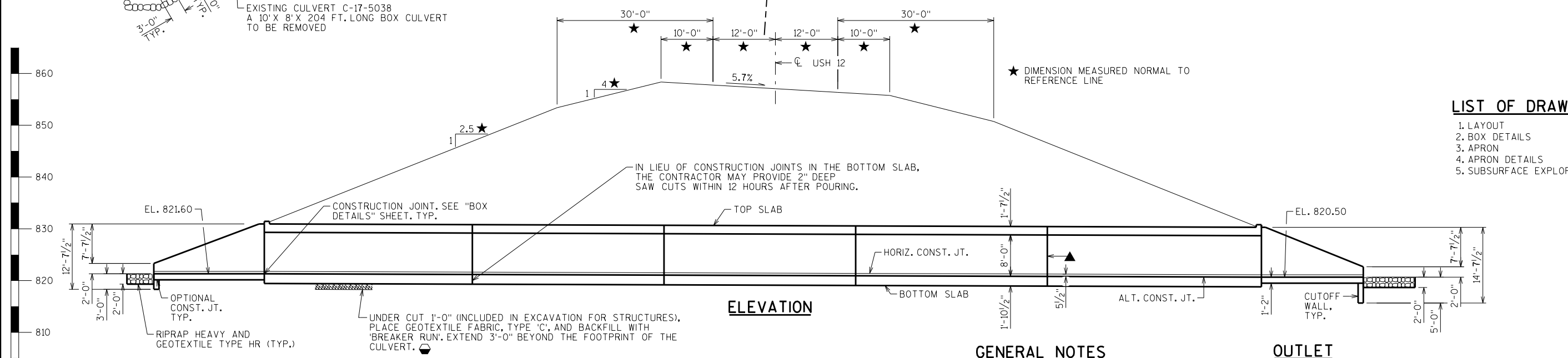
- ▲ SEE CORNER DETAILS ON "APRON DETAILS" SHEET (TYP.)
- NAME PLATE LOCATION (SEE "APRON DETAILS" SHEET)
- ✱ BUILD APRON AND END OF BOX LEVEL
- INDICATES WING NUMBER
- ▲ VERT. CONST. JOINT (TYP.)

NOTE: STRUCTURE BACKFILL REQUIRED BEHIND ALL WING WALLS.

RIPRAP HEAVY AND GEOTEXTILE TYPE HR.



**PLAN**



**ELEVATION**

**LIST OF DRAWINGS**

1. LAYOUT
2. BOX DETAILS
3. APRON
4. APRON DETAILS
5. SUBSURFACE EXPLORATION

**GENERAL NOTES**

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE 3/4" UNLESS OTHERWISE NOTED.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES CULVERTS C-17-48" SHALL BE THE EXISTING GROUNDLINE.

ALL VOLUME WHICH CANNOT BE PLACED BEFORE CULVERT CONSTRUCTION AND NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL WITHIN THE LENGTH OF THE CULVERT INCLUDING THE APRON WING WALLS.

THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.

THE CONCRETE IN THE CUTOFF WALLS MAY BE PLACED UNDERWATER IF THE EXCAVATION CANNOT BE DEWATERED.

THE CONTRACTOR MAY FURNISH A PRECAST CONCRETE BOX CULVERT IN LIEU OF THE CAST-IN-PLACE BOX CULVERT WITH THE ACCEPTANCE OF THE SHOP DRAWINGS BY THE STRUCTURES DESIGN SECTION. THE PRECAST CONCRETE BOX CULVERT SHALL CONFORM TO PRECAST DETAILS ON CHAPTER 36 STANDARDS OF THE CURRENT WISC. DOT BRIDGE MANUAL. PAYMENT FOR THE PRECAST CULVERT SHALL BE BASED ON THE QUANTITIES AND PRICES BID FOR THE ITEMS LISTED IN THE "TOTAL ESTIMATED QUANTITIES".

THE CONTRACTOR MAY ELECT TO SUBSTITUTE #10R #2 CONCRETE COARSE AGGREGATE, SELECT CRUSHED MATERIAL OR OTHER GRANULAR MATERIAL AS APPROVED BY THE FIELD ENGINEER, IN LIEU OF THE BREAKER RUN, TO BE UTILIZED AS A CONSTRUCTION PLATFORM FOR THE BOX. THE CONTRACTOR IS RESPONSIBLE FOR BASE STABILITY WITH ANY SUBSTITUTED MATERIAL.

**OUTLET**

**STRUCTURE DESIGN CONTACTS:**  
STACIE WEIS (608) 261-6109  
LAURA SHADEWALD (608) 267-9592

**DESIGN DATA**

**LIVE LOAD:**  
DESIGN LOADING: HL-93  
INVENTORY RATING FACTOR: RF = 1.05  
OPERATING RATING FACTOR: RF = 1.35  
WISCONSIN STANDARD PERMIT VEHICLE (WIS.-SPV): 255(KIPS)

**EARTHLOAD:**  
DESIGNED FOR 28 FT. OF FILL.

**MATERIAL PROPERTIES:**  
CONCRETE MASONRY: f'c = 3,500 P.S.I.  
BAR STEEL REINFORCEMENT: fy = 60,000 P.S.I.

**HYDRAULIC DATA**

**100 YEAR FREQUENCY**  
Q<sub>100</sub> = 600 C.F.S.  
VEL<sub>100</sub> = 11.7 F.P.S.  
HW<sub>100</sub> = EL. 828.70  
WATERWAY AREA = 96 SQ. FT.  
DRAINAGE AREA = 4.9 SQ. MI.  
ROADWAY OVERTOPPING = N/A  
SCOUR CRITICAL CODE = 8

**TRAFFIC VOLUME**

**USH 12**  
ADT = 4,900 (2042)  
R.D.S. = 55 M.P.H.

**TOTAL ESTIMATED QUANTITIES**

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
203.0220	REMOVING STRUCTURE C-17-5038	EACH	1
206.2000	EXCAVATION FOR STRUCTURES CULVERTS C-17-48	LS	1
210.2500	BACKFILL STRUCTURE TYPE B	TON	4,630
311.0115	BREAKER RUN	CY	190
504.0100	CONCRETE MASONRY CULVERTS	CY	549
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	65,560
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	2,250
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	47
606.0300	RIPRAP HEAVY	CY	60
645.0105	GEOTEXTILE TYPE C	SY	614
645.0120	GEOTEXTILE TYPE HR	SY	128
NON-BID ITEMS			
	FILLER	SIZE	3/4"

EXCAVATION BELOW SUBGRADE (EBS) REQUIRED AT THE OUTLET AND INLET. USE BREAKER RUN TO FILL IN AREAS OF EBS. SEE ROADWAY PLANS FOR QUANTITIES.

IN LIEU OF CONSTRUCTION JOINTS IN THE BOTTOM SLAB, THE CONTRACTOR MAY PROVIDE 2" DEEP SAW CUTS WITHIN 12 HOURS AFTER POURING.

★ DIMENSION MEASURED NORMAL TO REFERENCE LINE

8

**BUREAU OF STRUCTURES**

ACCEPTED *[Signature]* 5/27/22  
CHIEF STRUCTURES DESIGN ENGINEER DATE

**STRUCTURE C-17-48**

USH 12 OVER WILSON CREEK TRIBUTARY

COUNTY DUNN CITY MENOMONIE

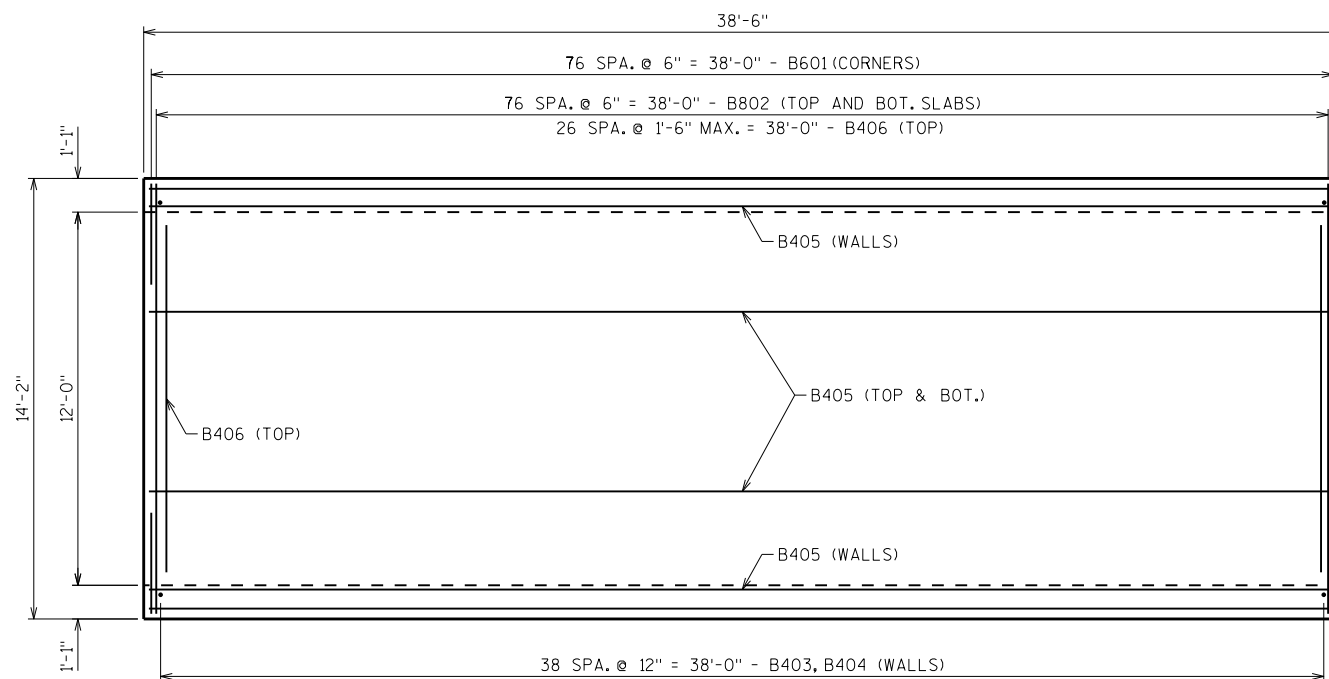
DESIGN SPEC. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS

DESIGNED BY SEW DESIGNED CK'D. MWB DRAWN BY SEW PLANS CK'D. MWB

**LAYOUT**

SHEET 1 OF 5

SCALE = 10.00



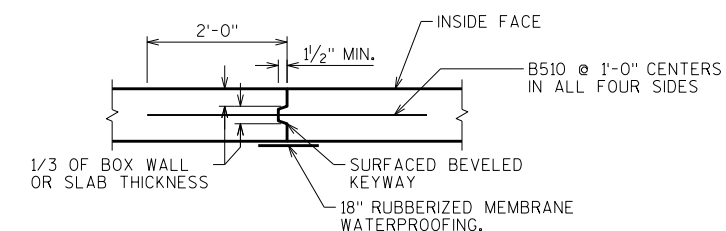
**PLAN**

EXTERIOR PANEL SHOWN, OTHER PANELS SIMILAR  
APRON AND HEADER NOT SHOWN FOR CLARITY

**BILL OF BARS**

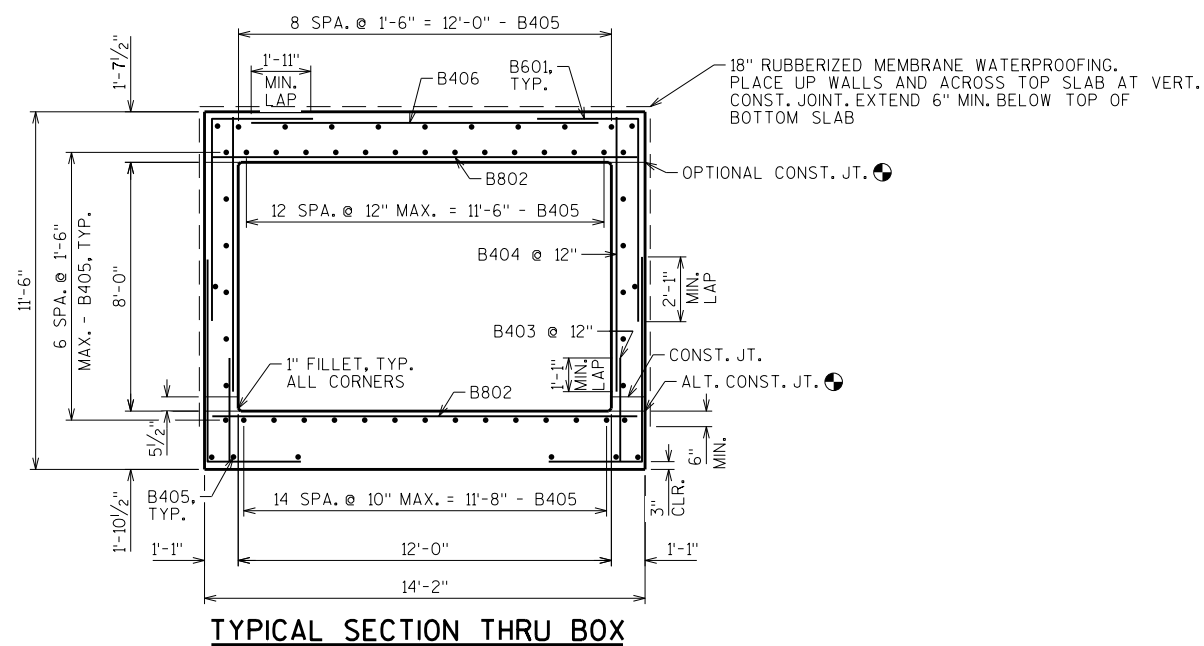
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE  
BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
B601		1540	9'-8"	X		CORNERS
B802		770	13'-10"			TOP & BOTTOM - TRANS.
B403		390	3'-4"			WALLS - DOWELS - VERT.
B404		390	8'-10"			WALLS - VERT.
B405		305	38'-2"			TOP & BOT. SLAB & WALLS - LONGIT.
B406		135	11'-2"			TOP SLAB - HORIZ. - TRANS.
B407		4	13'-10"			HEADER - HORIZ.
B308		20	4'-3"	X		INLET HEADER- VERT.
B309		20	4'-7"	X		OUTLET HEADER - VERT.
B510		208	4'-0"			VERT. CONST. JOINT
B511		30	4'-0"			APRON CONNECTION



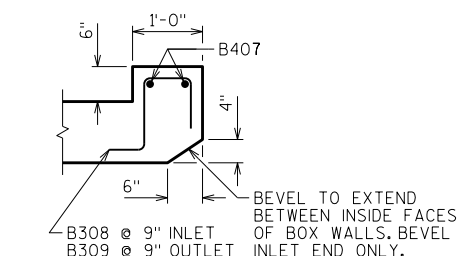
**VERTICAL CONSTRUCTION JOINT**

2" DEEP SAW CUT WITHIN 12 HOURS AFTER POURING MAY  
BE USED IN LIEU OF CONST. JT. IN BOTTOM SLAB.

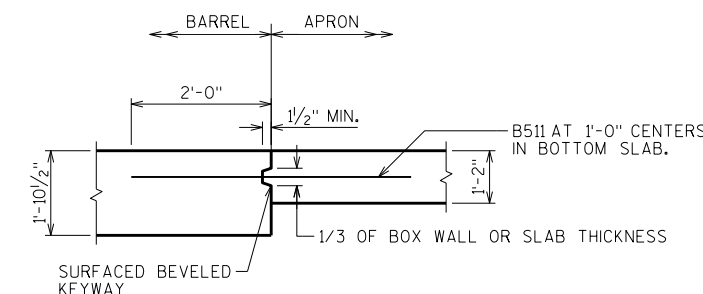


**TYPICAL SECTION THRU BOX**

OMIT 1" FILLETS IF JOINT IS USED

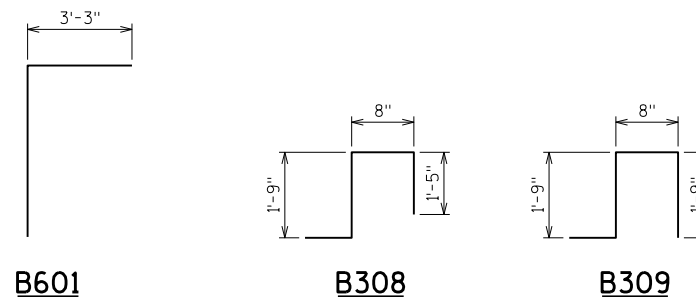


**SECTION THRU HEADER**



**APRON CONNECTION DETAIL**

2" DEEP SAW CUT WITHIN 12 HOURS AFTER POURING MAY  
BE USED IN LIEU OF CONST. JT. IN BOTTOM SLAB.

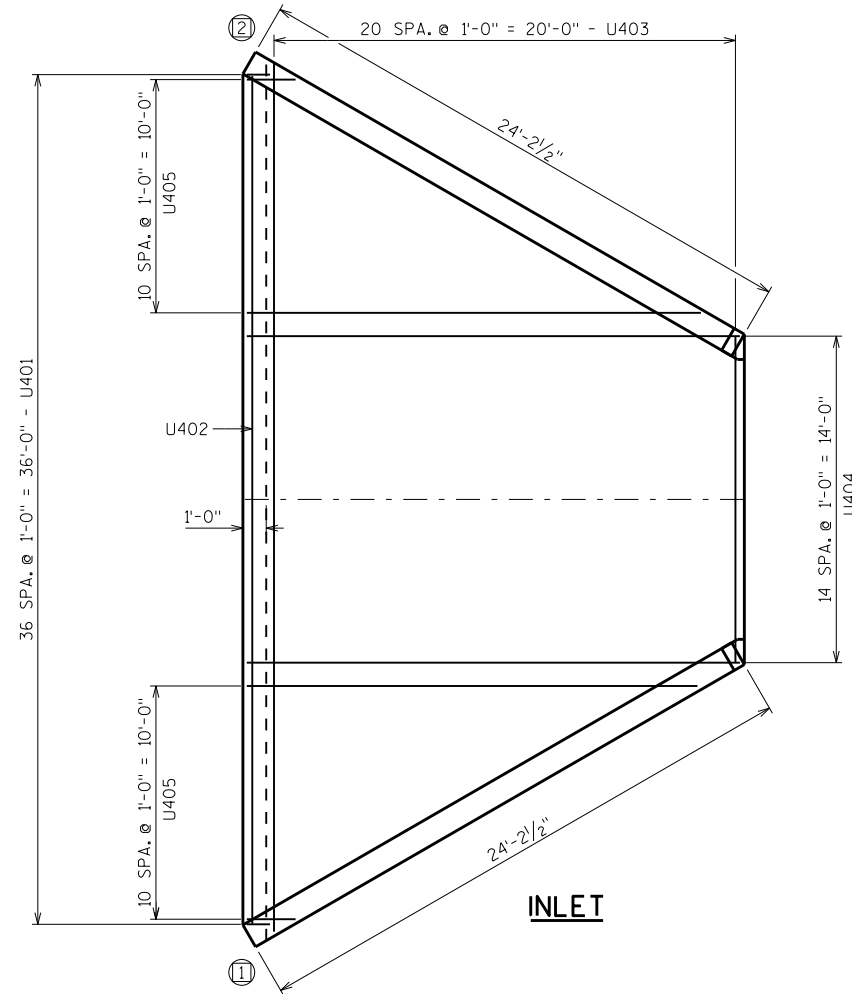


**B601**

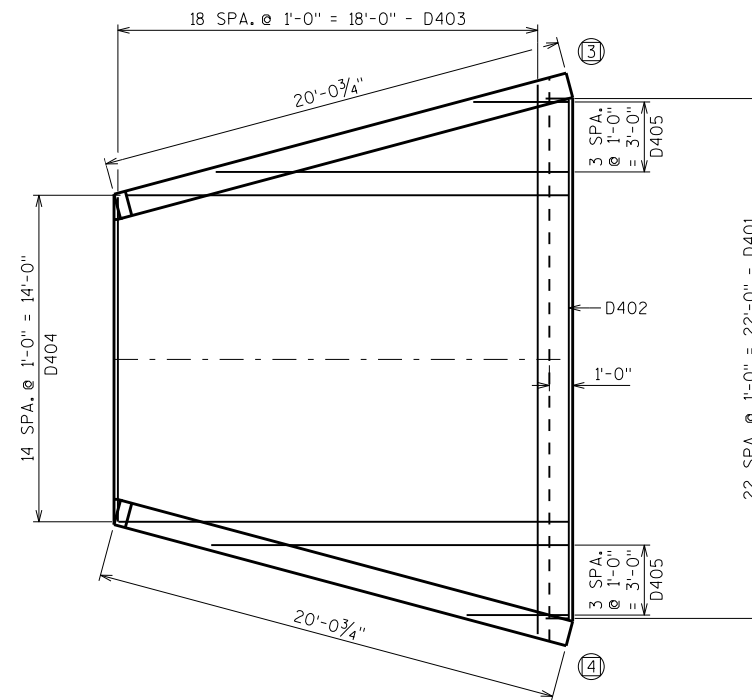
**B308**

**B309**

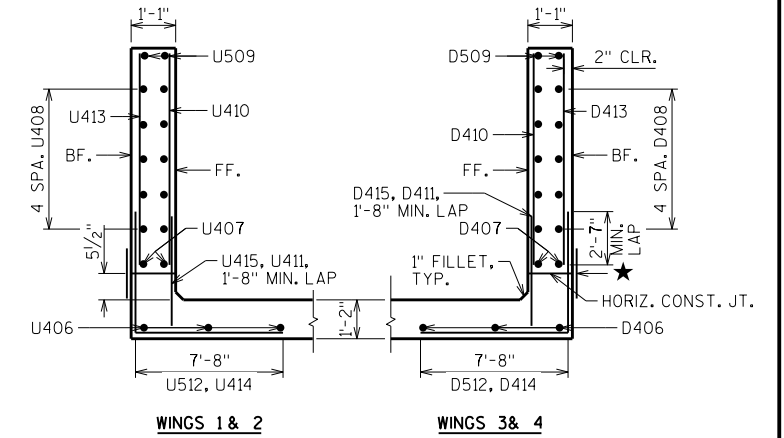
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
<b>STRUCTURE C-17-48</b>			
DRAWN BY		SEW	PLANS CK'D. <b>MWB</b>
<b>BOX DETAILS</b>		SHEET 2	



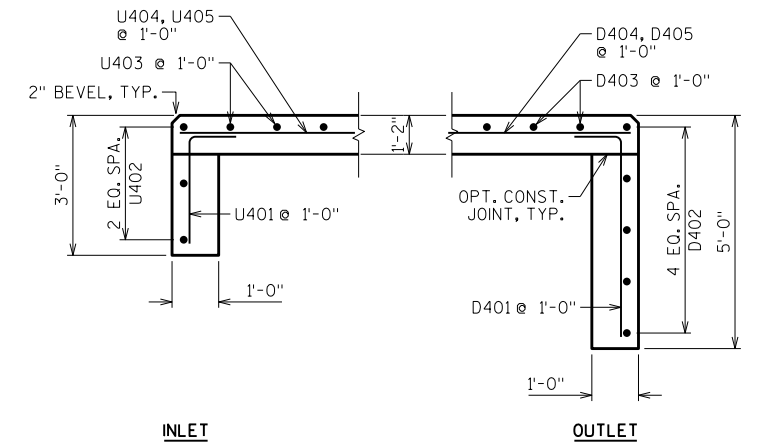
**INLET**



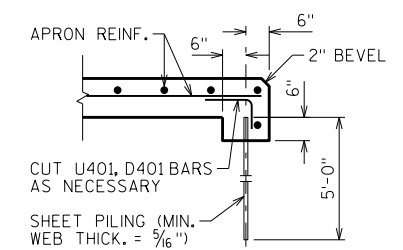
**OUTLET**



**SECTION THRU WINGWALLS**

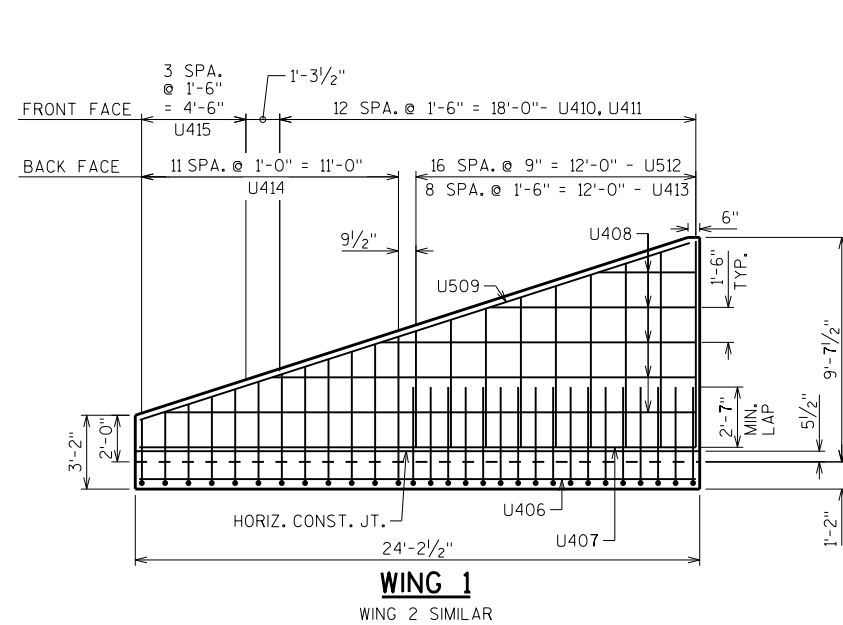


**CUT-OFF WALLS**



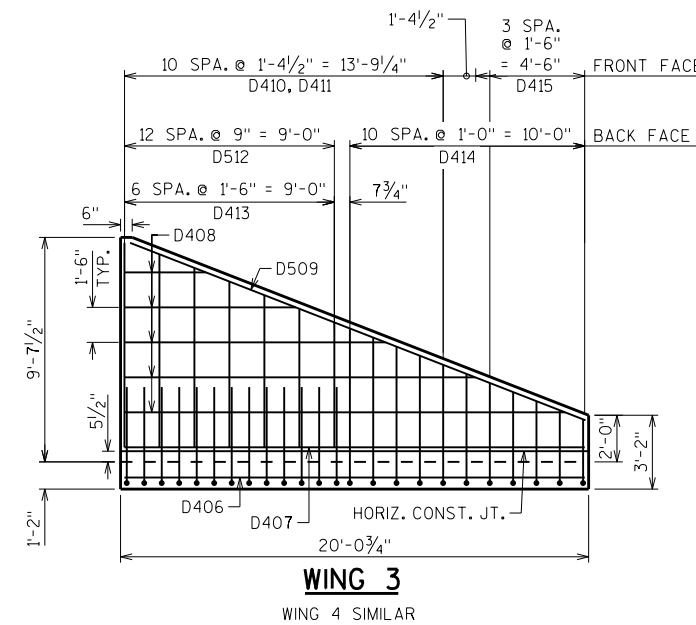
**ALTERNATE CUT-OFF WALLS**

THE ABOVE ALTERNATIVE MAY BE USED IN LIEU OF CAST-IN-PLACE CONCRETE CUT-OFF WALLS. PAYMENT WILL BE BASED ON THE CONCRETE CUT-OFF WALLS.



**WING 1**

WING 2 SIMILAR



**WING 3**

WING 4 SIMILAR

⊙ INDICATES WING NUMBER

★ 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING ALONG HORIZ. CONST. JOINT IN WING.

8

8

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
<b>STRUCTURE C-17-48</b>			
DRAWN BY		SEW	PLANS CK'D. <b>MWB</b>
<b>APRON</b>		SHEET 3	



**BILL OF BARS**

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

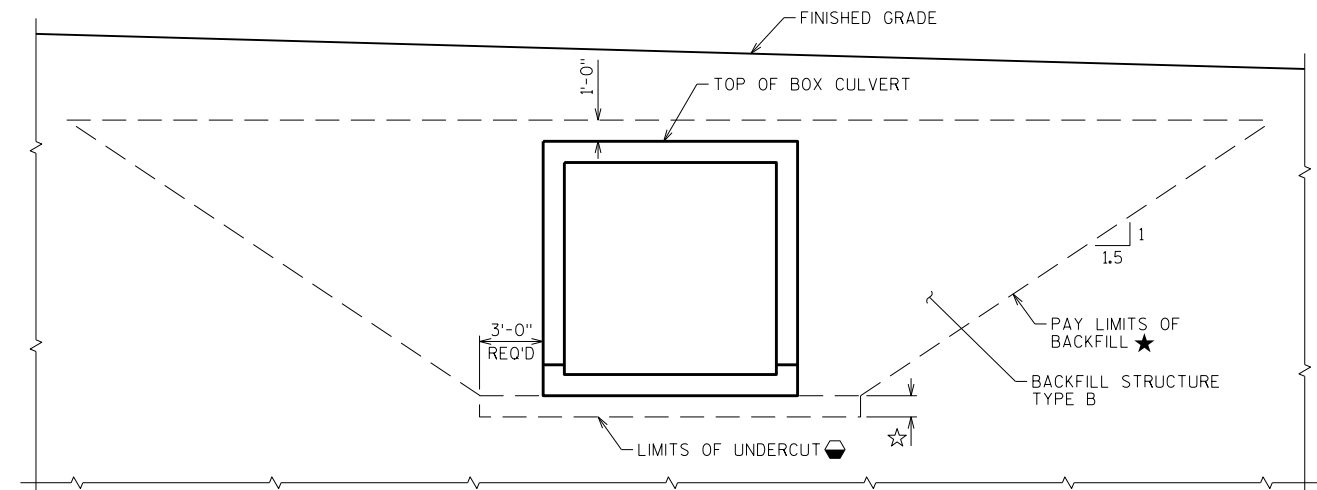
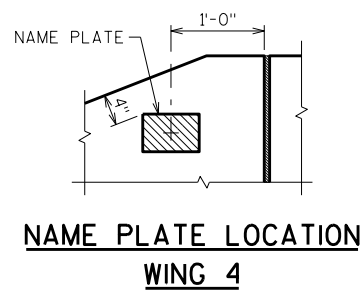
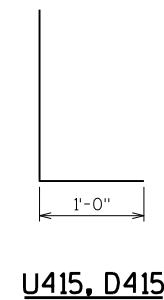
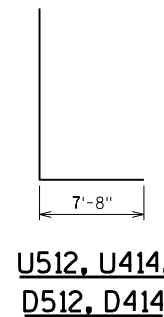
BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
U401		37	3'-6"	X		INLET APRON CUTOFF - VERT.
U402		3	36'-5"			INLET APRON CUTOFF - HORIZ.
U403		21	25'-6"	▲		INLET APRON - TRANS.
U404		15	21'-2"			INLET APRON - LONGIT.
U405		22	10'-9"	▲		INLET APRON - LONGIT.
U406		6	23'-10"			WINGS 1 & 2 - HORIZ. - BOT.
U407	X	4	23'-10"			WINGS 1 & 2 - HORIZ.
U408	X	20	13'-10"	▲		WINGS 1 & 2 - HORIZ.
U509	X	4	24'-9"			WINGS 1 & 2 - HORIZ. - TOP
U410	X	26	6'-1"	▲		WINGS 1 & 2 - VERT. - FF
U411	X	26	3'-2"			WINGS 1 & 2 - VERT. - DOWELS - FF
U512	X	34	11'-8"	X		WINGS 1 & 2 - VERT. - BOT. - BF
U413	X	18	7'-0"	▲		WINGS 1 & 2 - VERT. - BF
U414	X	24	12'-2"	X	▲	WINGS 1 & 2 - VERT. - BOT. - BF
U415	X	8	4'-4"	X	▲	WINGS 1 & 2 - VERT. - FF
D401		23	5'-6"	X		OUTLET APRON CUTOFF - VERT.
D402		5	22'-4"			OUTLET APRON CUTOFF - HORIZ.
D403		19	18'-11"	▲		OUTLET APRON - TRANS.
D404		15	19'-4"			OUTLET APRON - LONGIT.
D405		8	9'-11"	▲		OUTLET APRON - LONGIT.
D406		6	19'-8"			WINGS 3 & 4 - HORIZ. - BOT.
D407	X	4	19'-8"			WINGS 3 & 4 - HORIZ.
D408	X	20	11'-5"	▲		WINGS 3 & 4 - HORIZ.
D509	X	4	20'-10"			WINGS 3 & 4 - HORIZ. - TOP
D410	X	22	6'-3"	▲		WINGS 3 & 4 - VERT. - FF
D411	X	22	3'-2"			WINGS 3 & 4 - VERT. - DOWELS - FF
D512	X	26	11'-8"	X		WINGS 3 & 4 - VERT. - BOT. - BF
D413	X	14	7'-2"	▲		WINGS 3 & 4 - VERT. - BF
D414	X	22	12'-3"	X	▲	WINGS 3 & 4 - VERT. - BOT. - BF
D415	X	8	4'-4"	X	▲	WINGS 3 & 4 - VERT. - FF

▲ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

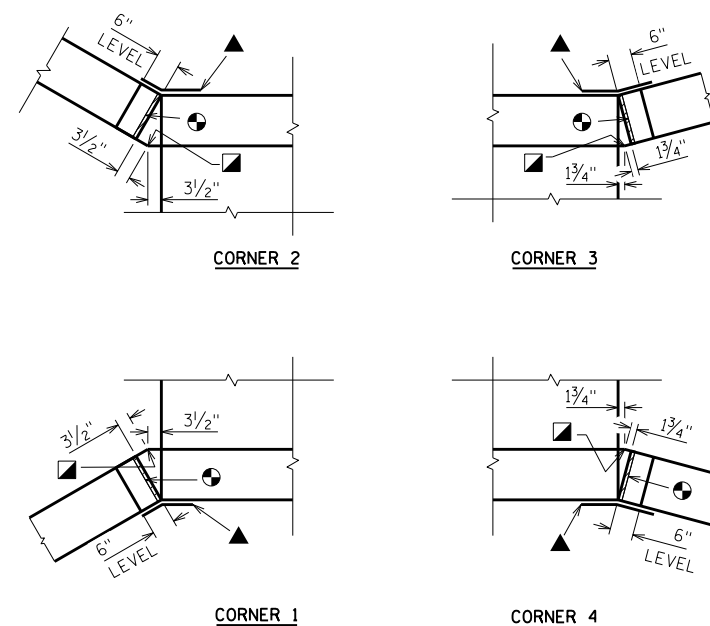
**BAR SERIES TABLE**

BAR MARK	NO. REQ'D.	LENGTH
U403	1 SERIES OF 21	13'-11" TO 37'-1"
U405	2 SERIES OF 12	2'-1" TO 19'-5"
U408	2 SERIES OF 5	4'-6" TO 23'-2"
U410	2 SERIES OF 13	3'-3" TO 8'-10"
U413	2 SERIES OF 9	5'-2" TO 8'-10"
U414	2 SERIES OF 12	10'-4" TO 13'-11"
U415	2 SERIES OF 4	3'-6" TO 5'-2"
D403	1 SERIES OF 19	14'-1" TO 23'-9"
D405	2 SERIES OF 4	4'-4" TO 15'-6"
D408	2 SERIES OF 5	3'-9" TO 19'-1"
D410	2 SERIES OF 11	3'-8" TO 8'-10"
D413	2 SERIES OF 7	5'-6" TO 8'-10"
D414	2 SERIES OF 11	10'-4" TO 14'-3"
D415	2 SERIES OF 4	3'-6" TO 5'-2"

BUNDLE AND TAG EACH SERIES SEPARATELY.



- ★ BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.
- ☆ UNDER CUT 1'-0". EXCAVATION FOR UNDER CUT TO BE INCLUDED IN EXCAVATION FOR STRUCTURES. PLACE "GEOTEXTILE TYPE C" AND BACKFILL WITH "BREAKER RUN".
- ☉ EXCAVATION BELOW SUBGRADE (EBS) REQUIRED AT THE OUTLET AND INLET. USE BREAKER RUN TO FILL IN AREAS OF EBS. SEE ROADWAY PLANS FOR QUANTITIES.



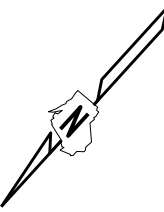
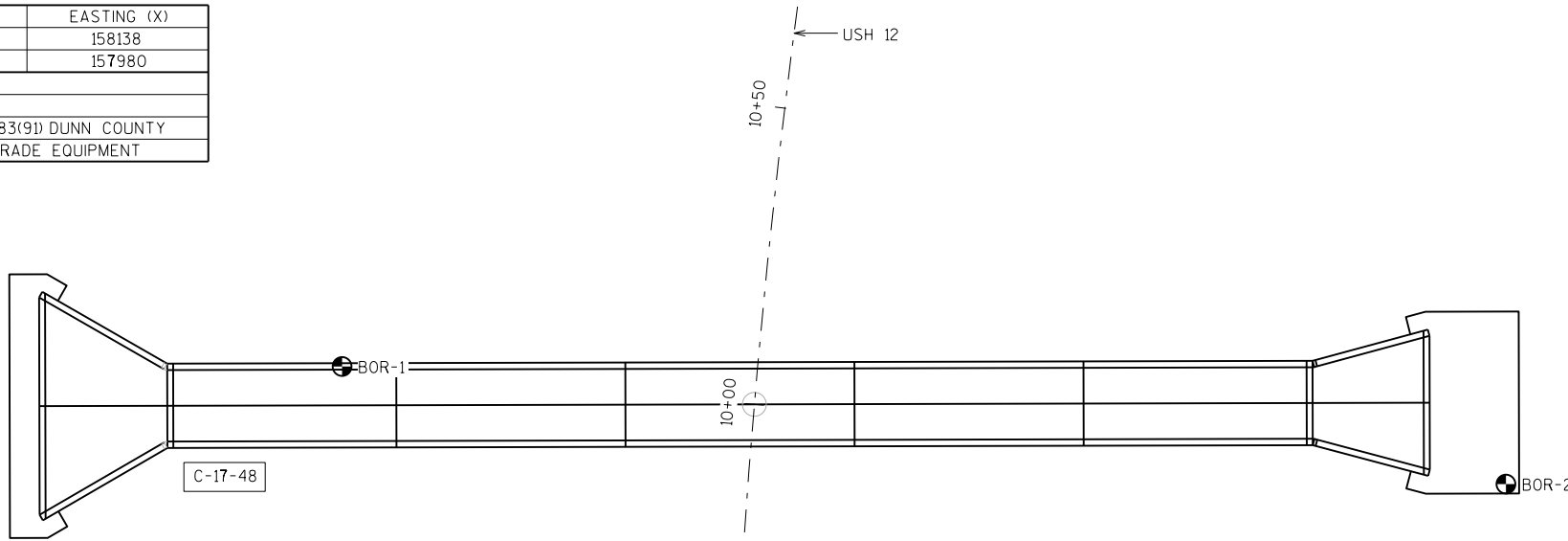
- 1" BEVEL
- ▲ 18" RUBBERIZED MEMBRANE WATERPROOFING, EXTEND FROM HORIZ. CONST. JT. TO TOP OF WALL
- ☉ 3/4" FILLER, TYP. EXTEND FILLER FROM HORIZ. CONST. JT. TO TOP OF WING.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
<b>STRUCTURE C-17-48</b>			
		DRAWN BY	PLANS CK'D. <b>MWB</b>
		SHEET 4	
<b>APRON DETAILS</b>			

BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	4/10/2019	182639	158138
2	4/09/2019	182522	157980

BORINGS COMPLETED BY: WISDOT  
 REPORT COMPLETED BY: WISDOT  
 ALL COORDINATES REFERENCED TO WCCS NAD 83(91) DUNN COUNTY  
 COORDINATES COLLECTED USING NON-SURVEY GRADE EQUIPMENT

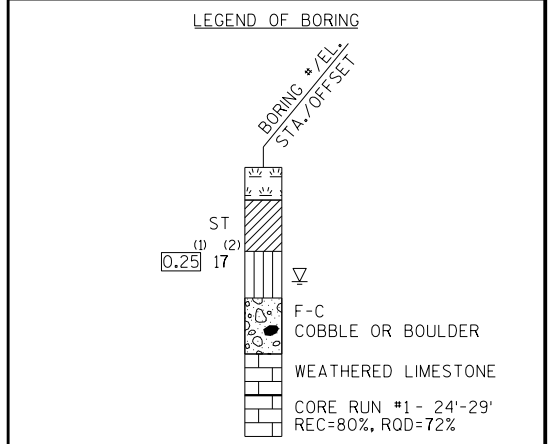
WILSON CREEK  
 TRIBUTARY



STATE PROJECT NUMBER  
**8949-00-00**

MATERIAL SYMBOLS

	ASPHALT		TOPSOIL		PEAT
	CONCRETE		FILL		GRAVEL
	SAND		CLAY		SILT
	BOULDERS OR COBBLES		LIMESTONE		BEDROCK (UNKNOWN)
	SHALE		SANDSTONE		IGNEOUS/META



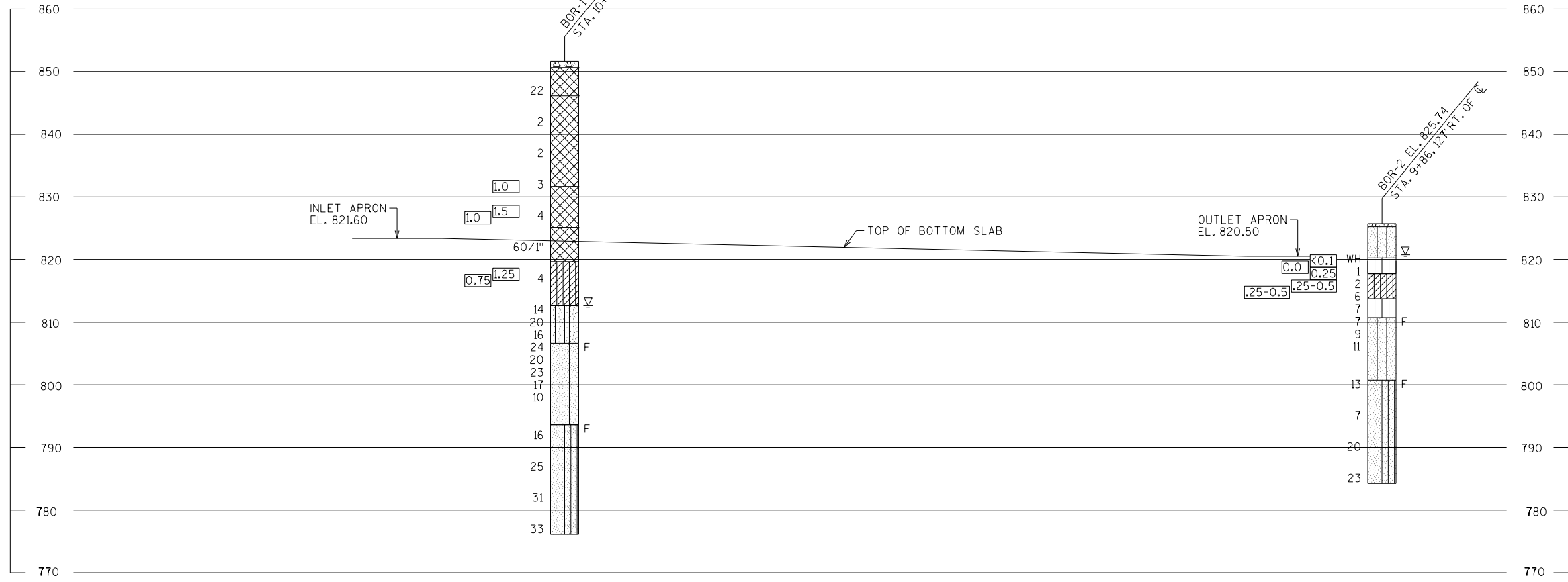
(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)  
 (2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION  
 ▽ AT TIME OF DRILLING  
 ▽ END OF DRILLING  
 ▽ AFTER DRILLING

ABBREVIATIONS  
 F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.



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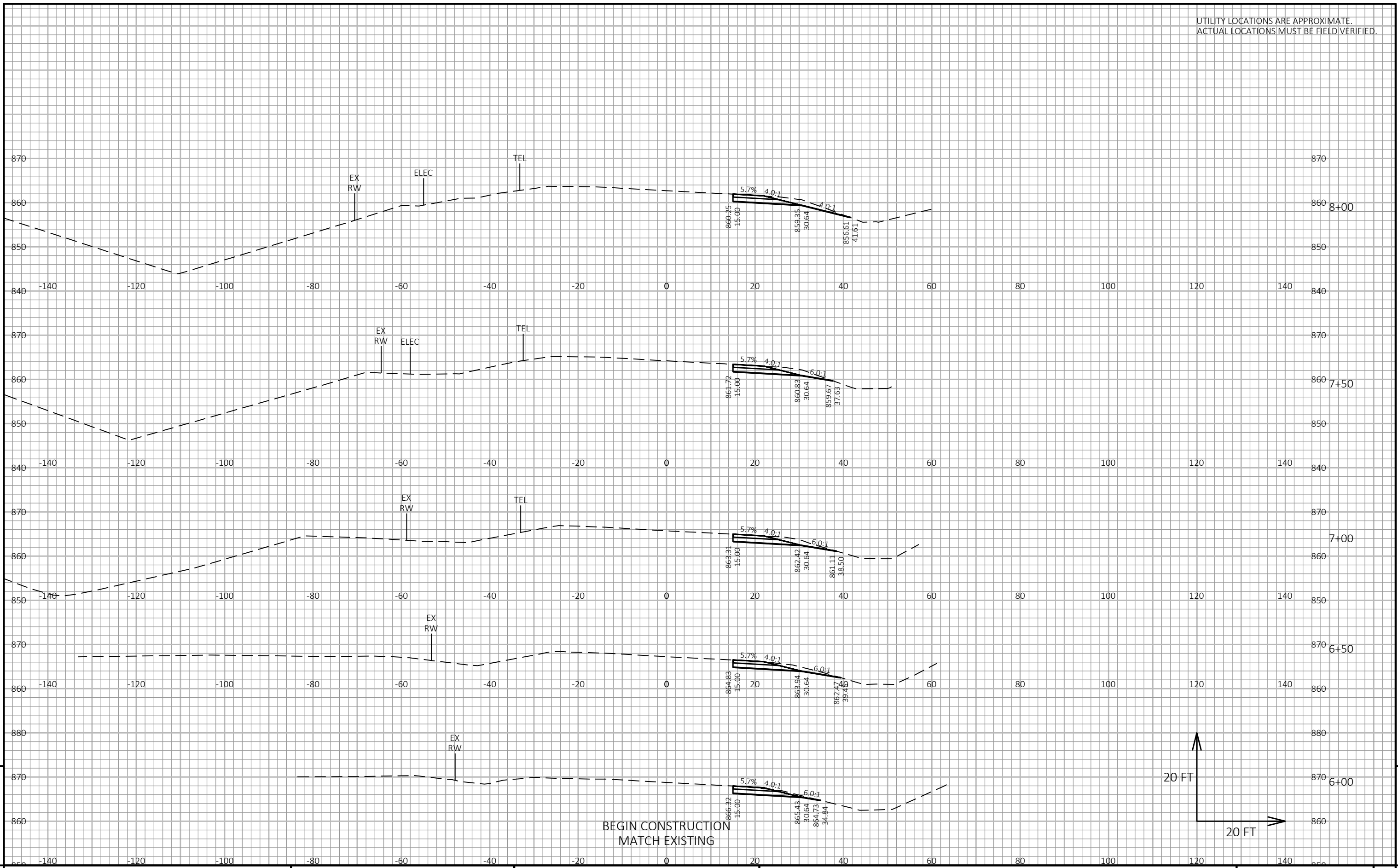
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
<b>STRUCTURE C-17-48</b>			
DRAWN BY: TLP/SEW		PLANS CKD. MWB	
<b>SUBSURFACE EXPLORATION</b>		SHEET 5	

SCALE = 1/5

USH 12												
Station	Distance	AREA (SF)			Incremental Vol (CY) (Unadjusted)			Cumulative Vol (CY)				Mass Ordinate
		Cut	Unusable Pavement Material	Fill	Cut	Unusable Pavement Material	Fill	Cut 1.00	Unusable Pavement Material	Available Material 1.00	Expanded Fill 1.30	
Note 7		Note 1, 6			Note 1, 6			Note 2, 6		Note 3	Note 4	Note 5
6+00	0.0	20.9	0.5	0.0	0.0	0.0	0	0	0	0	0	0
6+50	50.0	28.3	0.5	0.0	45.6	0.9	0.0	46	1	45	0	46
7+00	50.0	28.7	0.5	0.0	52.8	0.9	0.0	98	2	97	0	98
7+50	50.0	29.6	0.5	0.0	54.0	0.9	0.0	152	3	150	0	152
8+00	50.0	32.0	0.5	0.0	57.0	0.9	0.0	209	4	206	0	209
8+50	50.0	29.2	0.5	0.0	56.6	0.9	0.0	266	5	261	0	266
8+99	49.0	29.2	0.5	0.0	53.0	0.9	0.0	319	6	314	0	319
9+00	1.0	92.0	35.5	0.0	2.2	0.7	0.0	321	6	315	0	321
9+30	30.0	92.0	35.5	0.0	102.2	39.4	0.0	424	46	378	0	424
9+50	50.0	188.6	35.5	63.7	259.9	65.7	59.0	683	111	572	77	607
10+00	50.0	679.8	35.5	161.6	804.1	65.7	208.6	1487	177	1310	348	1140
10+50	50.0	92.9	35.5	94.8	715.4	65.7	237.4	2203	243	1960	656	1546
10+70	20.0	92.9	35.5	94.8	68.8	26.3	70.3	2272	269	2002	748	1524
10+99	49.0	92.9	35.5	94.8	168.5	64.4	172.1	2440	334	2107	972	1469
11+00	1.0	24.4	0.5	0.0	2.2	0.7	1.8	2442	334	2108	974	1468
11+50	50.0	24.4	0.5	0.0	45.1	0.9	0.0	2487	335	2152	974	1514
12+00	50.0	27.4	0.5	0.0	47.9	0.9	0.0	2535	336	2199	974	1561
12+50	50.0	27.4	0.5	0.0	50.7	0.9	0.0	2586	337	2249	974	1612
13+00	50.0	26.2	0.5	0.0	49.6	0.9	0.0	2636	338	2298	974	1662
13+50	50.0	25.4	0.5	0.0	47.7	0.9	0.0	2683	339	2344	974	1709
14+00	50.0	27.5	0.5	0.0	49.0	0.9	0.0	2732	340	2392	974	1758
14+50	50.0	31.8	0.5	0.0	54.9	0.9	0.0	2787	341	2446	974	1813
15+00	50.0	30.6	0.5	0.0	57.7	0.9	0.0	2845	342	2503	974	1871

1) Unusable asphalt and concrete pavement material is included in Cut.  
2) Excavation Common is the sum of the Cut column minus excavation for structures material. Item number 205.0100  
Notes: 3) Available material includes usable material left over from excavation for structures bid item. Does not include Unusable Pavement Excavation volume.  
4) Will be backfilled with Excavation Common or Borrow.  
5) Plus quantity indicates an excess of material. Minus indicates a shortage of material.  
6) Cut required within excavation for structures limits (approx. Sta 9+30 to Sta 10+70) is paid for as Excavation for Structures with the exception of removing concrete pavement and is shown for information only. See structure plans for additional information.  
7) Select stationing shown for information only where cross sections do not exist.

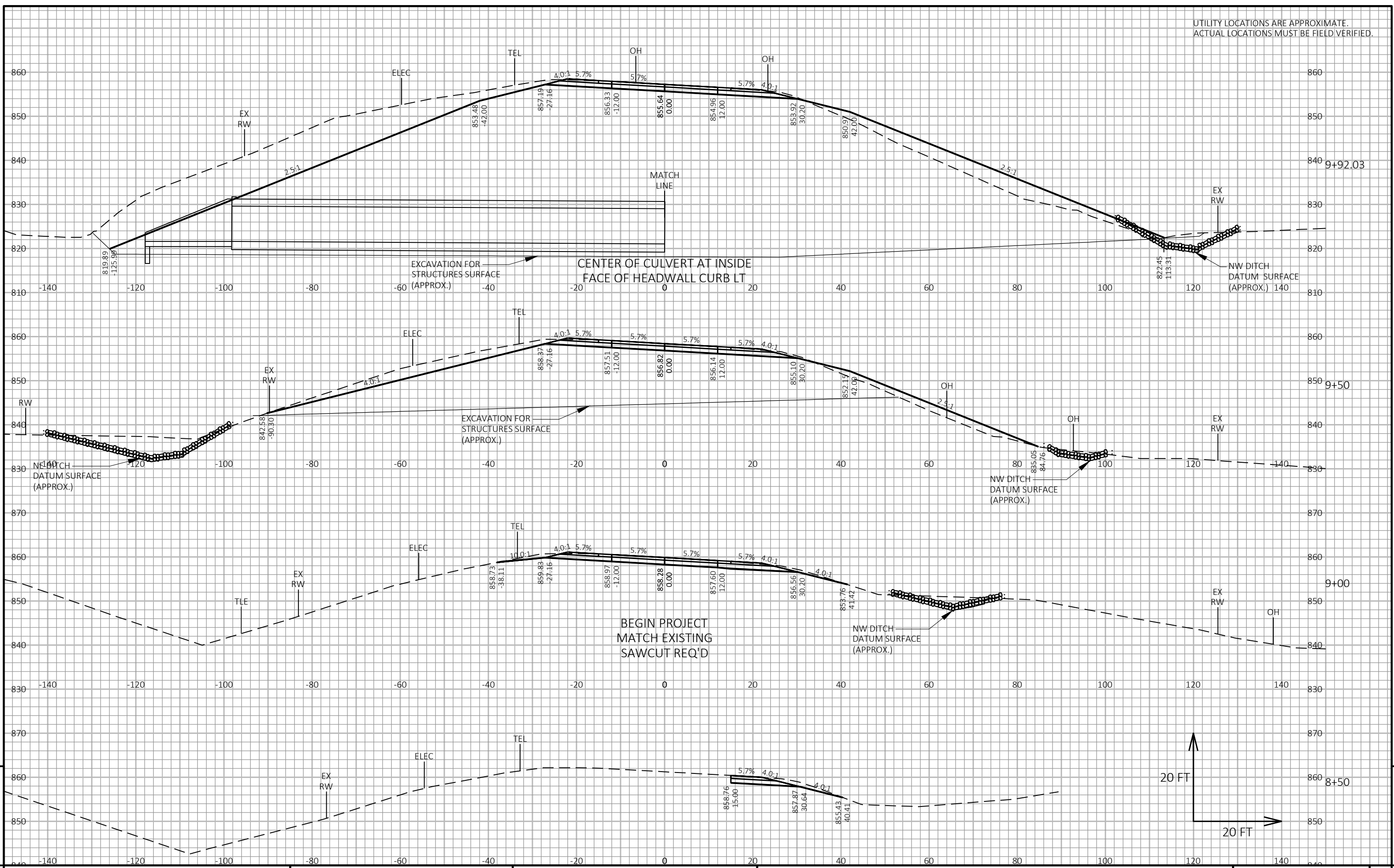
UTILITY LOCATIONS ARE APPROXIMATE.  
ACTUAL LOCATIONS MUST BE FIELD VERIFIED.



PROJECT NO: 8949-00-70      HWY: USH 12      COUNTY: DUNN      CROSS SECTIONS: USH 12      SHEET      E

FILE NAME: \\SEHCF1\PROJECTS\UZ\W\WITNW\145597\CIVIL 3D\SHEETSPLAN\090201-XS-2018.DWG      PLOT DATE: 4/26/2021 12:45 PM      PLOT BY: JUSTIN SHAVLIK      PLOT NAME:      PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT.      WISDOT/CADD SHEET 49

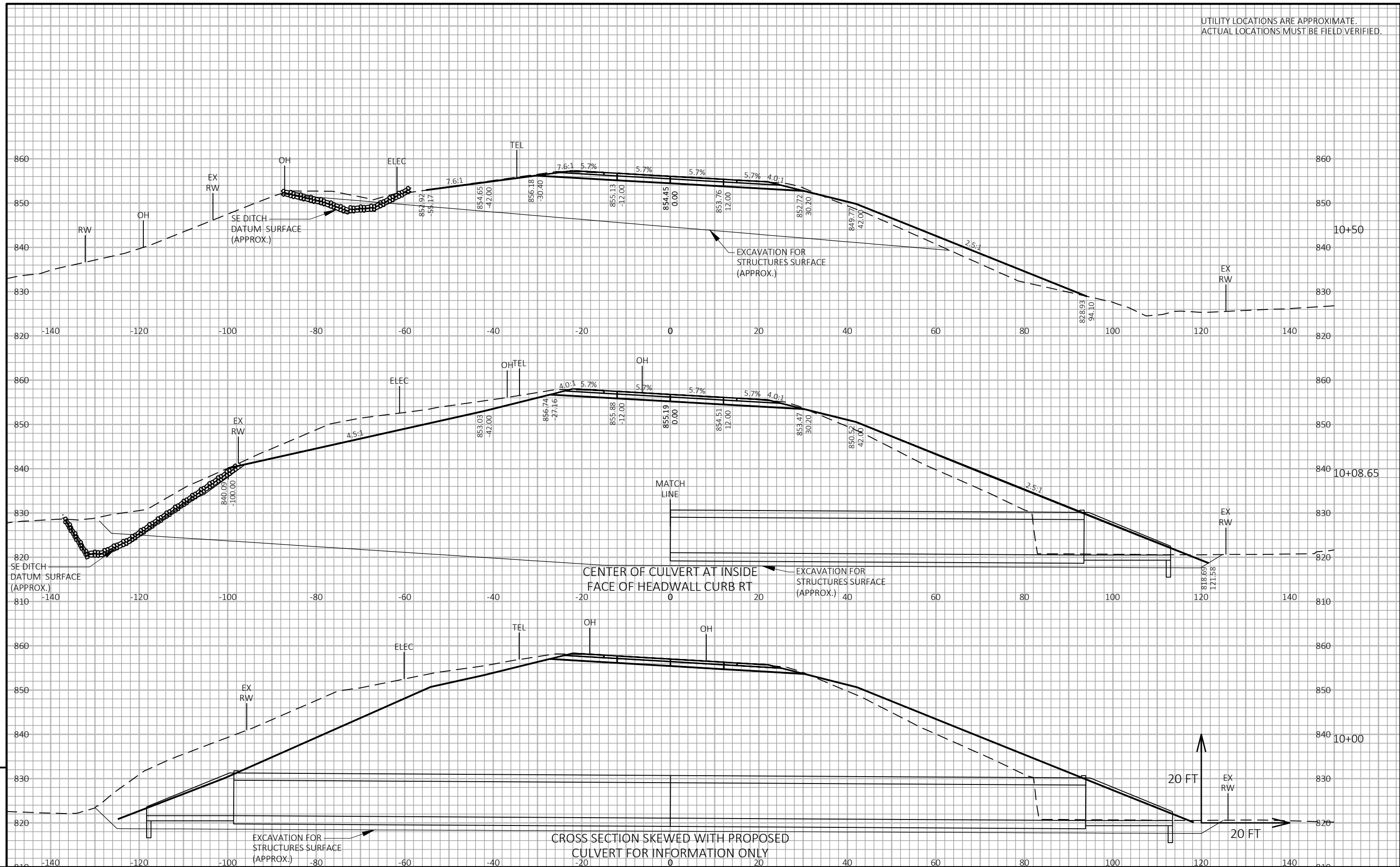
UTILITY LOCATIONS ARE APPROXIMATE.  
ACTUAL LOCATIONS MUST BE FIELD VERIFIED.



PROJECT NO: 8949-00-70	HWY: USH 12	COUNTY: DUNN	CROSS SECTIONS: USH 12	SHEET 9
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 PLOT DATE: 4/26/2021 12:45 PM  
 PLOT BY: JUSTIN SHAVLIK  
 PLOT NAME:  
 PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT.  
 WISDOT/CADD SHEET 49

UTILITY LOCATIONS ARE APPROXIMATE.  
ACTUAL LOCATIONS MUST BE FIELD VERIFIED.



PROJECT NO: 8949-00-70

HWY: USH 12

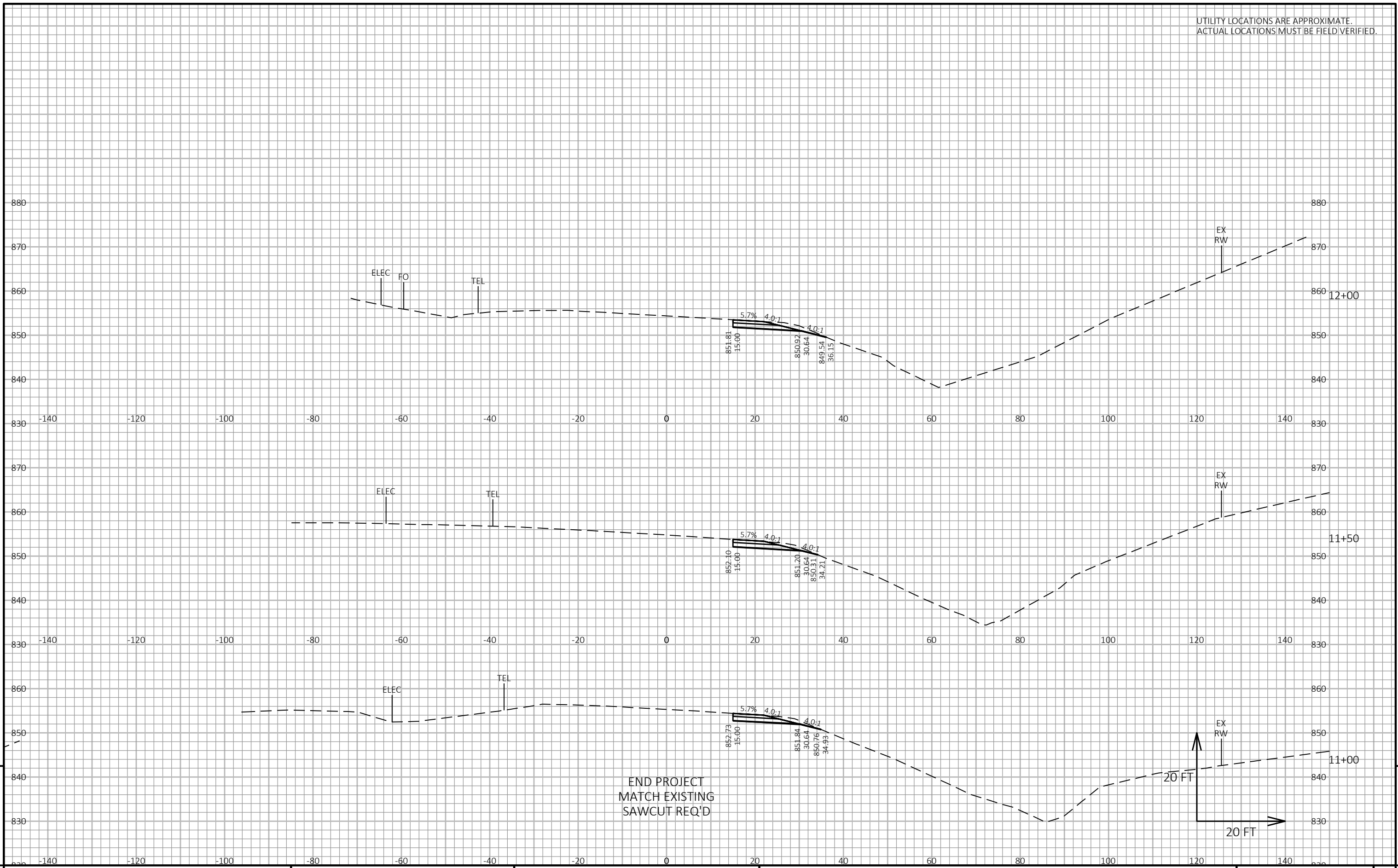
COUNTY: DUNN

CROSS SECTIONS: USH 12

SHEET

E

UTILITY LOCATIONS ARE APPROXIMATE.  
ACTUAL LOCATIONS MUST BE FIELD VERIFIED.

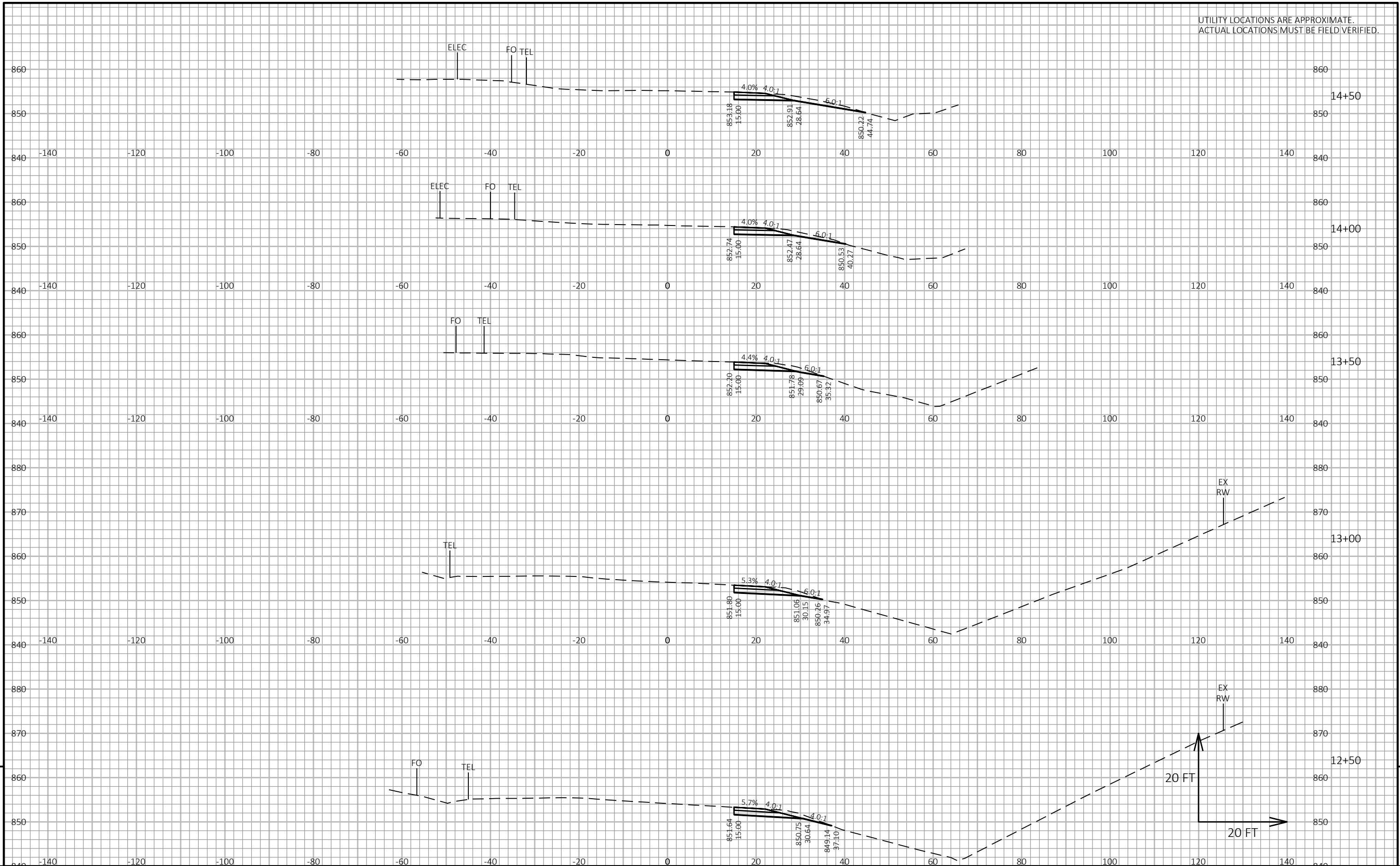


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PROJECT NO: 8949-00-70	HWY: USH 12	COUNTY: DUNN	CROSS SECTIONS: USH 12	SHEET	E
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UTILITY LOCATIONS ARE APPROXIMATE.  
ACTUAL LOCATIONS MUST BE FIELD VERIFIED.



PROJECT NO: 8949-00-70

HWY: USH 12

COUNTY: DUNN

CROSS SECTIONS: USH 12

SHEET

E



UTILITY LOCATIONS ARE APPROXIMATE.  
ACTUAL LOCATIONS MUST BE FIELD VERIFIED.

END CONSTRUCTION  
MATCH EXISTING

ELEC

FO TEL

20 FT

20 FT

860

15+00

850

9

9

PROJECT NO: 8949-00-70

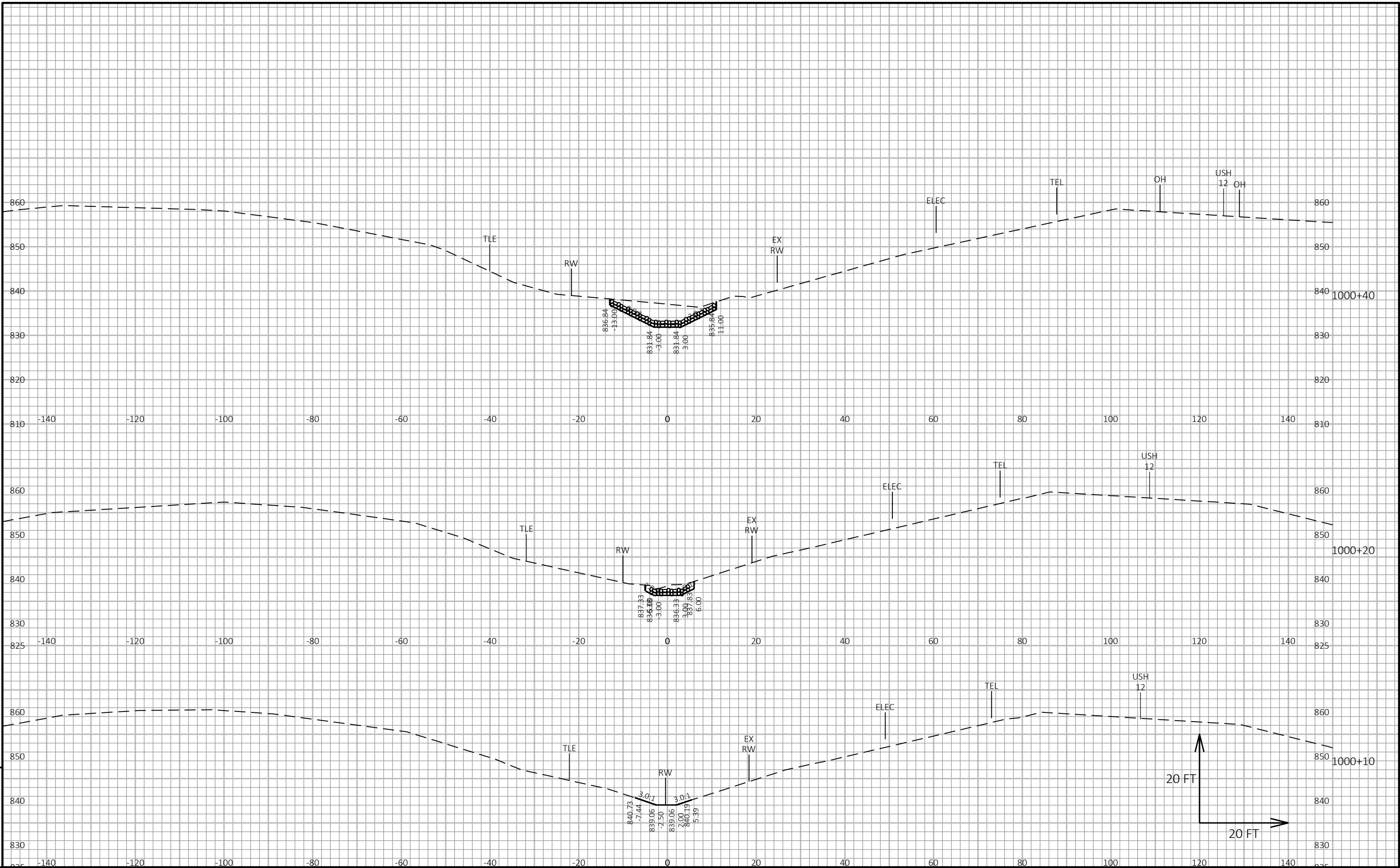
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COUNTY: DUNN

CROSS SECTIONS: USH 12

SHEET

E



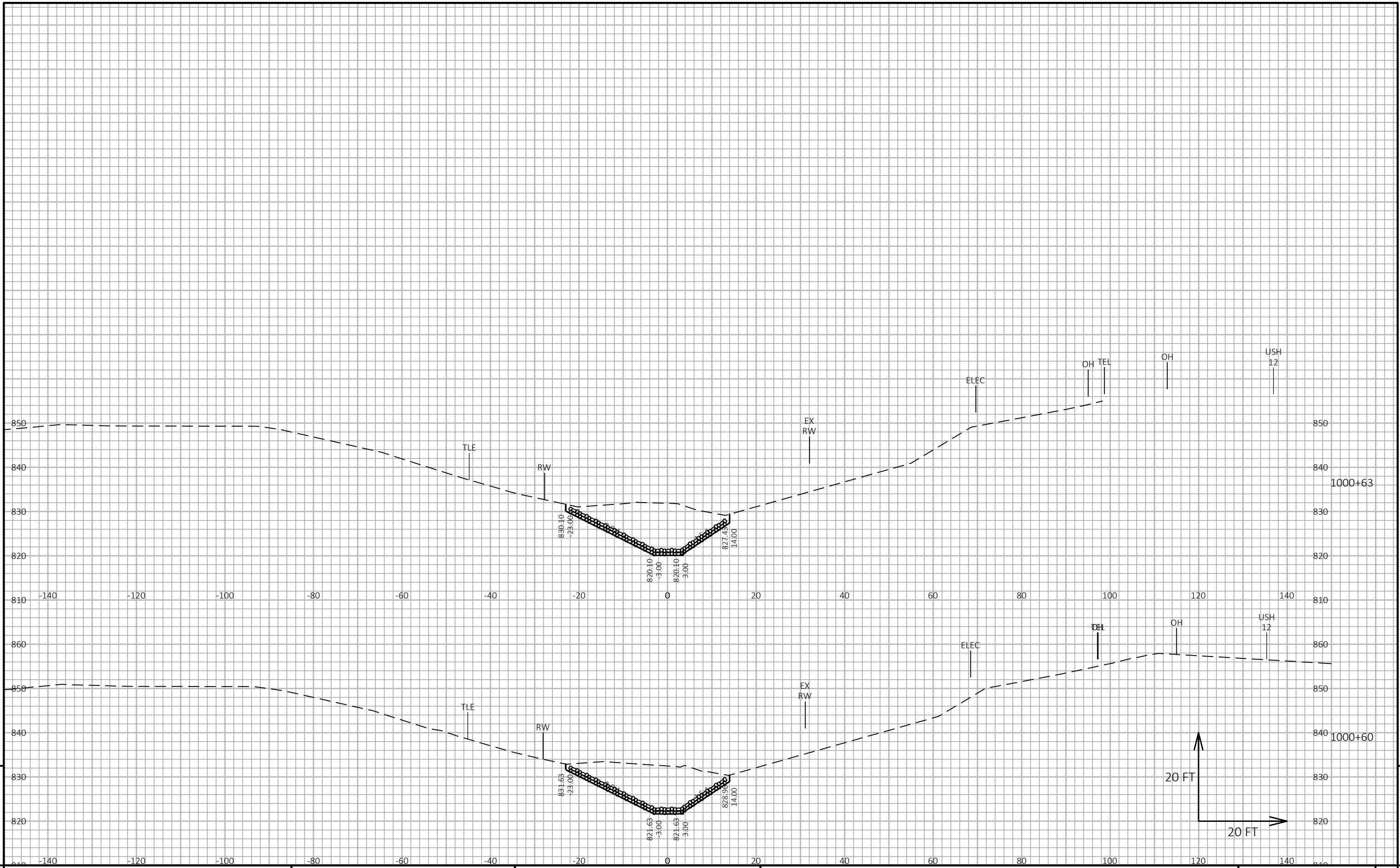
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PROJECT NO: 8949-00-70      HWY: USH 12      COUNTY: DUNN      CROSS SECTIONS: NE DITCH      SHEET      E

FILE NAME : \\SEHCF1\PROJECTS\UZ\W\WITNW\145597\CIVIL 3D\SHEETSPLAN\090201-XS-2018.DWG      PLOT DATE : 4/26/2021 12:45 PM      PLOT BY : JUSTIN SHAVLIK      PLOT NAME :      PLOT SCALE : 1 IN:20 FT HORZ. / 1 IN:20 FT VERT.      WISDOT/CADD SHEET 49

LAYOUT NAME - 1001



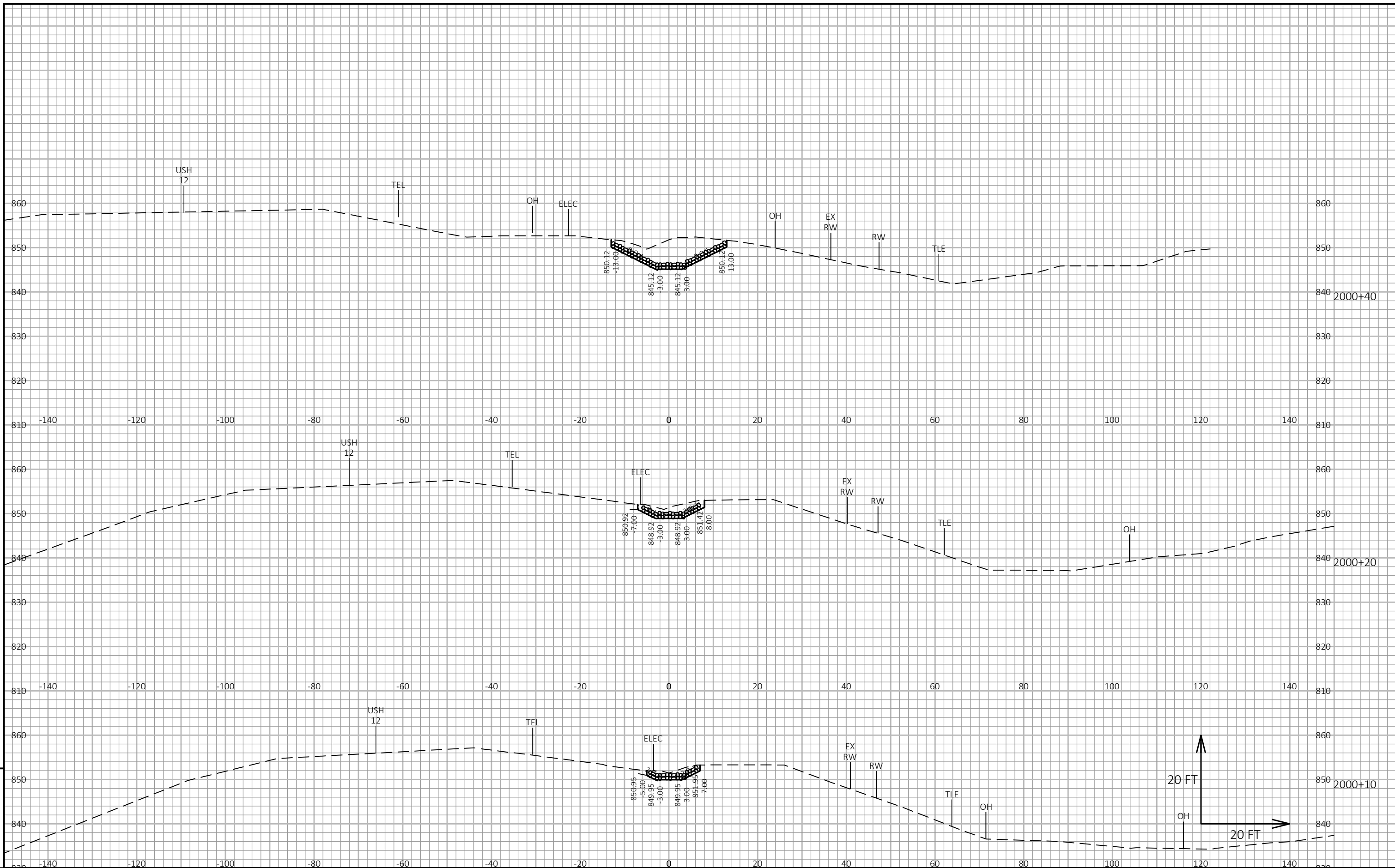
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PROJECT NO: 8949-00-70      HWY: USH 12      COUNTY: DUNN      CROSS SECTIONS: NE DITCH      SHEET      E

FILE NAME: \\SEHCF1\PROJECTS\UZ\W\WITNW\145597\CIVIL 3D\SHEETSPLAN\090201-XS-2018.DWG      PLOT DATE: 4/26/2021 12:45 PM      PLOT BY: JUSTIN SHAVLIK      PLOT NAME:      PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT.      WISDOT/CADD SHEET 49

LAYOUT NAME - 1002



PROJECT NO: 8949-00-70

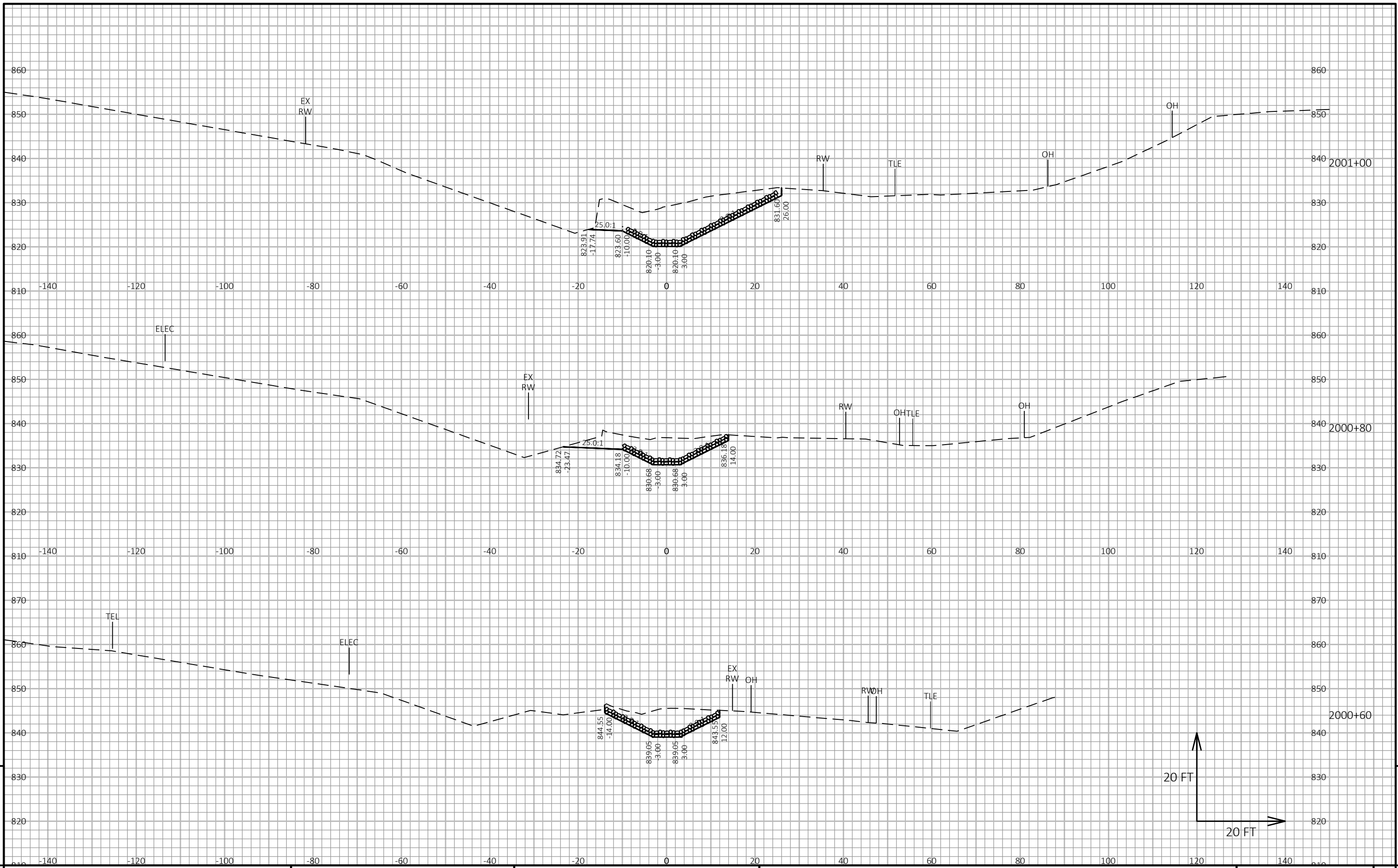
HWY: USH 12

COUNTY: DUNN

CROSS SECTIONS: SE DITCH

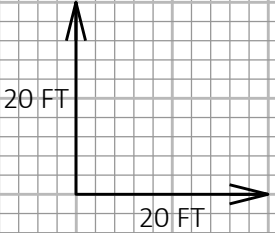
SHEET

E



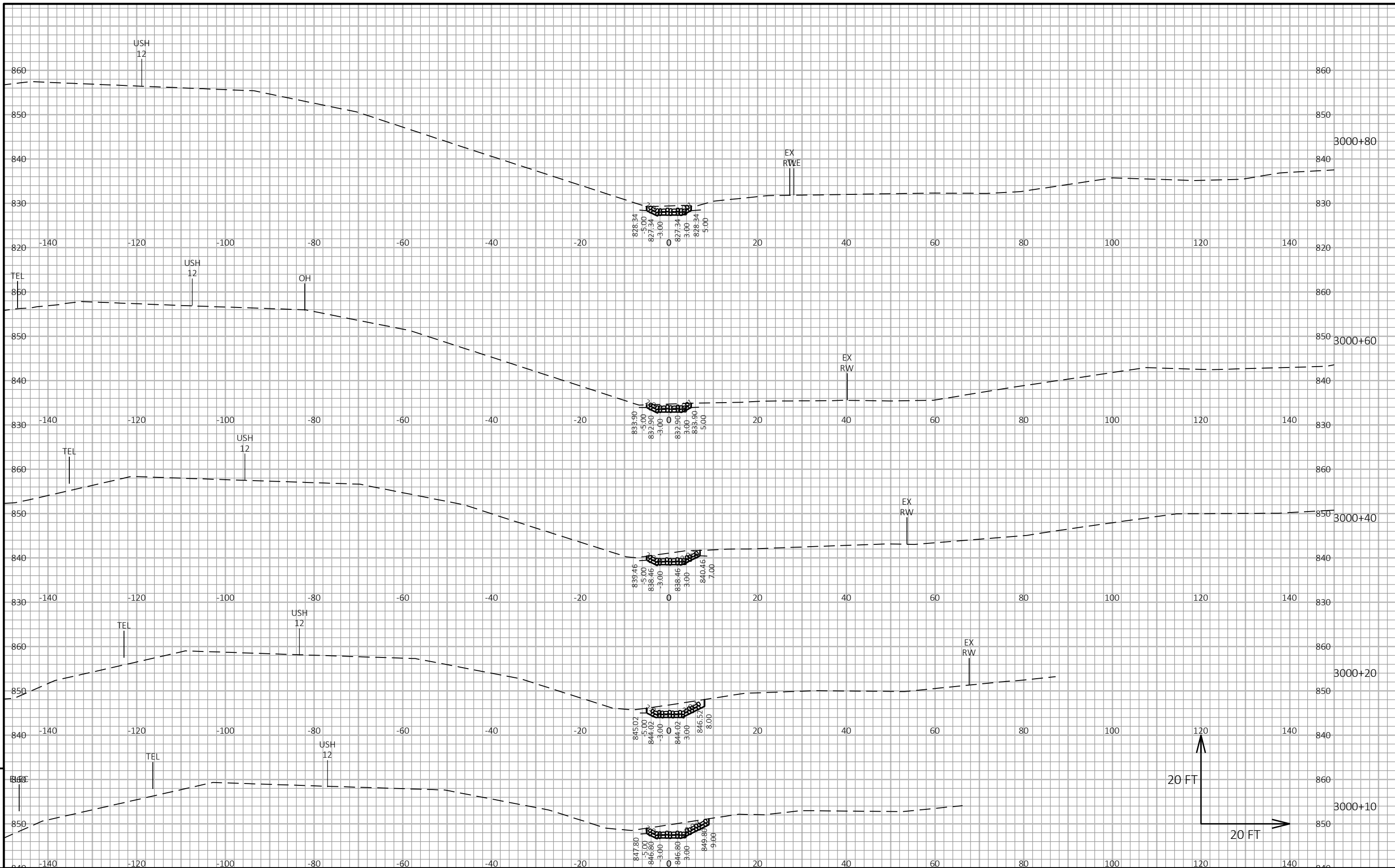
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PROJECT NO: 8949-00-70      HWY: USH 12      COUNTY: DUNN      CROSS SECTIONS: SE DITCH      SHEET      E

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PROJECT NO: 8949-00-70

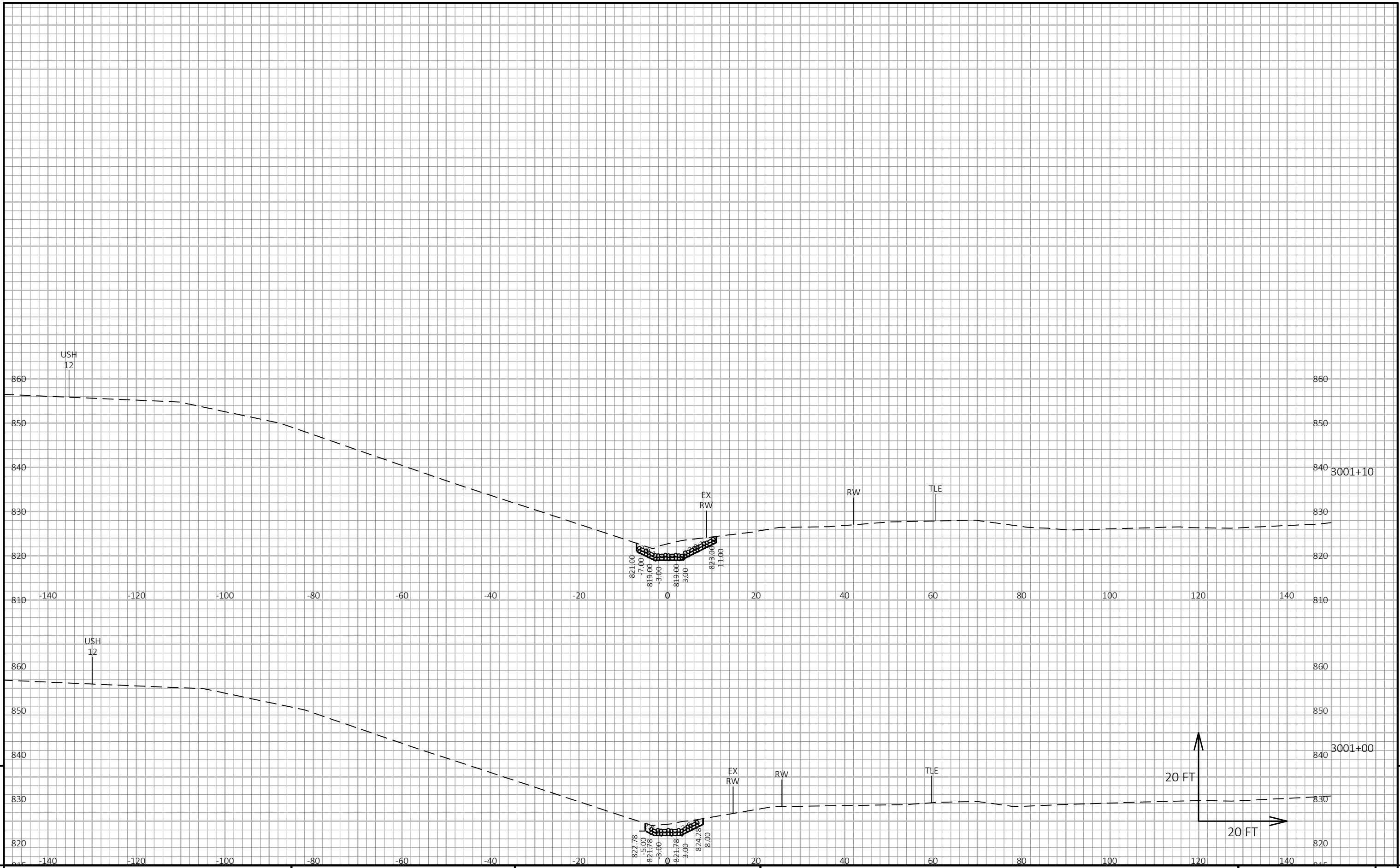
HWY: USH 12

COUNTY: DUNN

CROSS SECTIONS: NW DITCH

SHEET

E



9

9

PROJECT NO: 8949-00-70	HWY: USH 12	COUNTY: DUNN	CROSS SECTIONS: NW DITCH	SHEET	E
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## *Wisconsin Department of Transportation*

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>