

EAU

OCTOBER 2022

PROJECT ID: 7505-00-76

COUNTY: JACKSON

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plans
Section No.	8	Structure Plans
Section No.	9	Cross Sections

TOTAL SHEETS = 112



DESIGN DESIGNATION

A.A.D.T.	2010	=	530
A.A.D.T.	2044	=	610
D.H.V.		=	21
D.D.		=	50/50
T.		=	7.7%
DESIGN SPEED		=	55 MPH
ESALS		=	80,300

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

NORTHFIELD - ALMA CENTER

CTH FF SOUTH TO STH 95

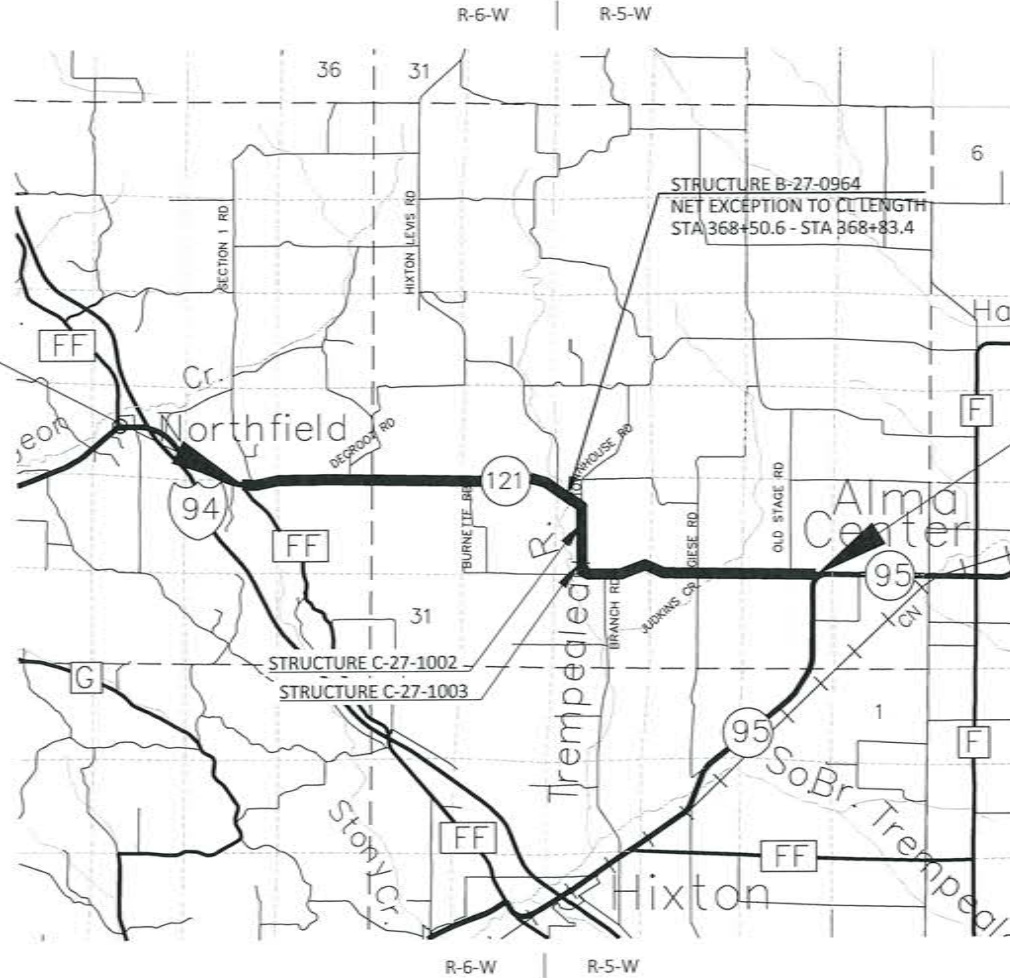
STH 121

JACKSON COUNTY

STATE PROJECT NUMBER
7505-00-76

BEGIN PROJECT
STA 183+38
Y=153,937.35
X=29,220.49

END PROJECT
STA 550+35



LAYOUT
SCALE 0 2.0 MI
TOTAL NET LENGTH OF CENTERLINE = 6.944

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), JACKSON COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7505-00-76	WISC 2023009	1

ORIGINAL PLANS PREPARED BY
KL Engineering
[A] Better Experience



STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	WisDOT, KL ENGINEERING
Surveyor	KL ENGINEERING
Designer	NATE ULNESS
Project Manager	TOU YANG
Regional Examiner	JIM KOENIG
Regional Supervisor	

APPROVED FOR THE DEPARTMENT
DATE: 7-12-22 *Patricia Williams*
(Signature)

E

ABBREVIATIONS

AEW	APRON ENDWALL
ALUM	ALUMINUM
AGG	AGGREGATE
ASPH	ASPHALT
AVG	AVERAGE
BAD	BASE AGGREGATE DENSE
BM	BENCHMARK
C&G	CURB AND GUTTER
CONC	CONCRETE
CP	CULVERT PIPE
CPCA	CULVERT PIPE CORRUGATED ALUMINUM
CPCS	CULVERT PIPE CORRUGATED STEEL
CPRC	CULVERT PIPE REINFORCED CONCRETE
D	DEGREE OF CURVE
DWY	DRIVEWAY
EAT	ENERGY ABSORBING TERMINAL
EB	EASTBOUND
EWAL	ENDWALL
EXIST	EXISTING
FE	FARM ENTRANCE
HMA	HOT MIX ASPHALT
INV	INVERT
L	LENGTH OF CURVE
LHF	LEFT HAND FORWARD
LT	LEFT
M/L	MATCHLINE
MAX	MAXIMUM
MGS	MIDWEST GUARDRAIL SYSTEM
MIN	MINIMUM
NC	NORMAL CROWN
NOR	NORMAL
PAVT	PAVEMENT
PC	POINT OF CURVATURE
PE	PRIVATE ENTRANCE
PI	POINT OF INTERSECTION
PT	POINT OF TANGENT
R	RADIUS
REIN	REINFORCED
RL	REFERENCE LINE
RAW	RIGHT OF WAY
RC	REVERSE CROWN
REQD	REQUIRED
RHF	RIGHT HAND FORWARD
RO	RUN OFF LENGTH
RT	RIGHT
SALV	SALVAGED
SDD	STANDARD DETAIL DRAWINGS
SE	SUPER ELEVATION
SHLD	SHOULDER
STA	STATION
T	TANGENT LENGTH
TYP	TYPICAL
WB	WESTBOUND
DELTA	DELTA

UTILITY CONTACTS

AT&T Legacy - Communication Line

Kenneth Colwell (Primary Contact)
 222 W Jackson
 Woodstock, IL 60098
 (312) 734-2223
 kc1298@att.com

CenturyLink - Communication Line

Brian Stelplugh
 333 North Front Street
 La Crosse, WI 54601
 (608) 796-5142
 Brian.Stelplugh@lumen.com

Jackson Electric Cooperative - Electricity

Eric Steien (Primary Contact)
 N6868 Co Hwy F
 P.O. Box 546
 Black River Falls, WI 54615
 (715) 284-5385
 esteien@jackelec.com

Tri-County Communications Coop. - Communication Line

Buck Webb (Primary Contact)
 417 5th Ave N
 P.O. Box 578
 Strum, WI 54770
 (715) 695-2691
 bwebb@tccpro.net

We Energies - Gas/Petroleum

We Energies Utility Coordinator (Primary Contact)
 500 S 116th Street
 West Allis, WI 53214
 (414) 221-2738
 We-Utility-relocations@we-energies.com

Xcel Energy - Electricity

Jordan Jandt
 3215 Commerce Street
 La Crosse, WI 54603
 (608) 789-3629
 Jordan.R.Jandt@xcelenergy.com



Dial **811** or (800)242-8511

www.DiggersHotline.com

** DENOTES A UTILITY IS NOT A DIGGERS MEMBER

GENERAL NOTES

THE LOCATION OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN IN THE PLAN, ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. CONTACT DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO START OF WORK.

UTILITY REFERENCE LINES ON THE CROSS SECTIONS ARE FOR APPROXIMATE HORIZONTAL REFERENCE ONLY.

REMOVAL ITEMS REQUIRING RESTORATION OF CONCRETE OR ASPHALT SHALL BE REMOVED TO AN EXISTING JOINT OR SAWED AS DETERMINED BY THE ENGINEER.

EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT SUGGESTED LOCATIONS. THE ENGINEER MAY MODIFY LOCATIONS AS NEEDED. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

NO TREES AND/OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

PIPE ELEVATIONS, LENGTHS, AND LOCATIONS AS SHOWN ON THE PLANS, MAY BE ADJUSTED TO FIT EXISTING FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

STATIONING AND OFFSETS TO APRON ENDWALLS FOR CULVERT PIPES ARE SHOWN AT THE TOP, OUTSIDE EDGE OF ENDWALL WHERE IT MEETS THE SIDESLOPE.

CONTRACTOR IS RESPONSIBLE FOR RESHAPING AND FINISHING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY THEIR OPERATION OUTSIDE THE NORMAL CONSTRUCTION LIMITS.

THE CONTRACTOR'S HMA PAVING OPERATION SHALL BE CONSISTENT WITH THE TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING OR PARKING LANE.

THE HMA PAVEMENT CALCULATIONS ARE BASED ON 112 LBS/SY/IN.

ORDER OF DETAIL SHEETS

- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- EROSION CONTROL
- PAVEMENT MARKING
- TRAFFIC CONTROL

HMA PAVEMENT, WHEN INDICATED ON THE PLANS, SHALL CONSIST OF COURSE THICKNESSES AS FOLLOWS:

TOTAL DEPTH	LAYERS	HMA MIXTURES	LOCATIONS
2.5-INCH	1.5" UPPER 1" LEVELING	5 MT 58-34 V	STH 121 & SIDE ROADS
6-INCH	6" LOWER	ASPHALTIC SURFACE	CULVERT REPLACEMENTS

RUNOFF COEFFICIENT TABLE

LAND USE:	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	0.08	0.16	0.22	0.12	0.20	0.27	0.15	0.24	0.33	0.19	0.28	0.38
	0.22	0.30	0.38	0.26	0.34	0.44	0.30	0.37	0.50	0.34	0.41	0.56
MEDIAN STRIP-TURF	0.19	0.20	0.24	0.19	0.22	0.26	0.20	0.23	0.30	0.20	0.25	0.30
	0.24	0.26	0.30	0.25	0.28	0.33	0.26	0.30	0.37	0.27	0.32	0.40
SIDE SLOPE-TURF			0.25			0.27			0.28			0.30
			0.32			0.34			0.36			0.38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 55.6 ACRES
 TOTAL AREA EXPECTED TO BE DISTRIBUTED BY CONSTRUCTION ACTIVITIES = 1.5 ACRES

DESIGN CONTACT

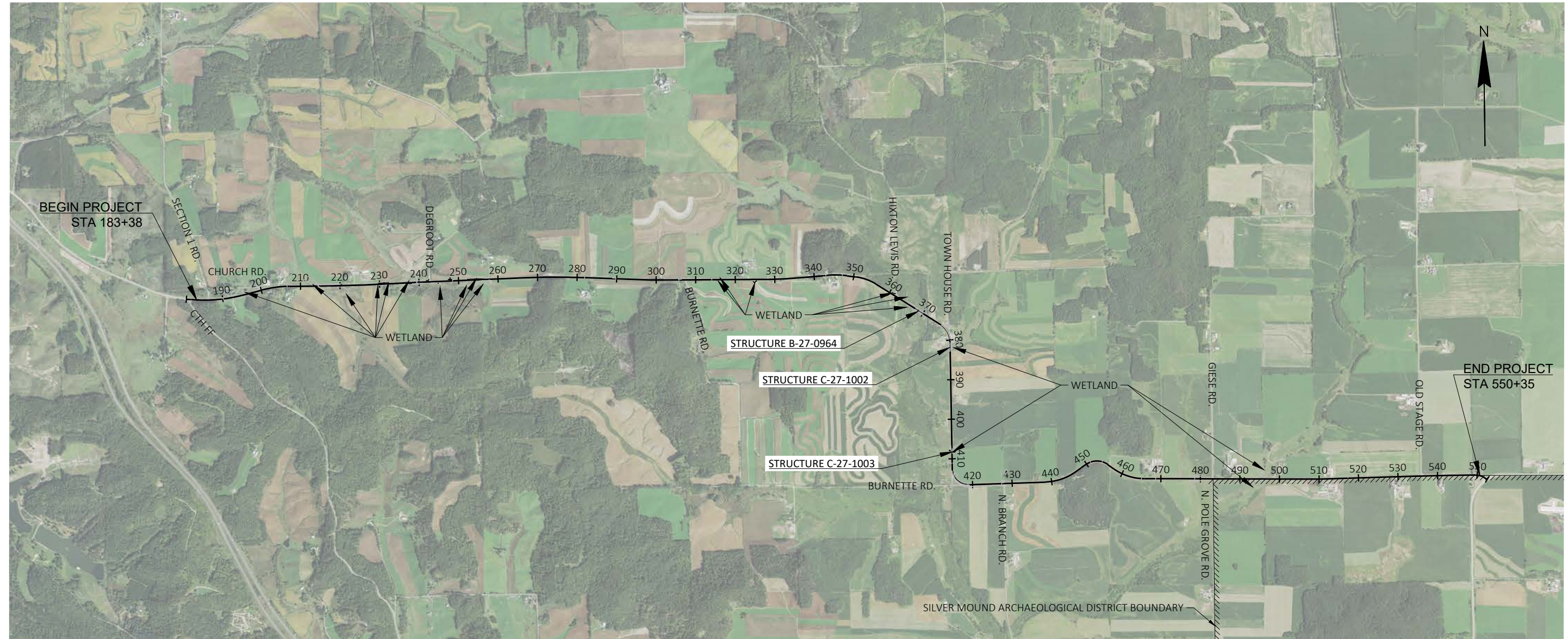
KL ENGINEERING
 JON BLOMQUIST
 393 RED CEDAR STREET, SUITE 2
 MENOMONIE, WI 54751
 (715) 231-1600
 jblomquist@klengineering.com

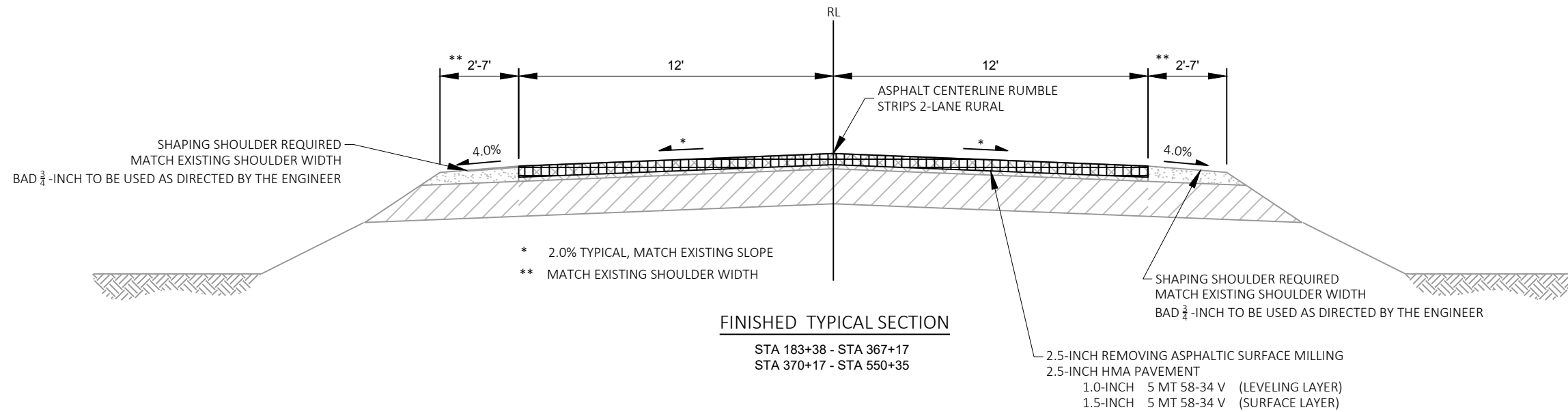
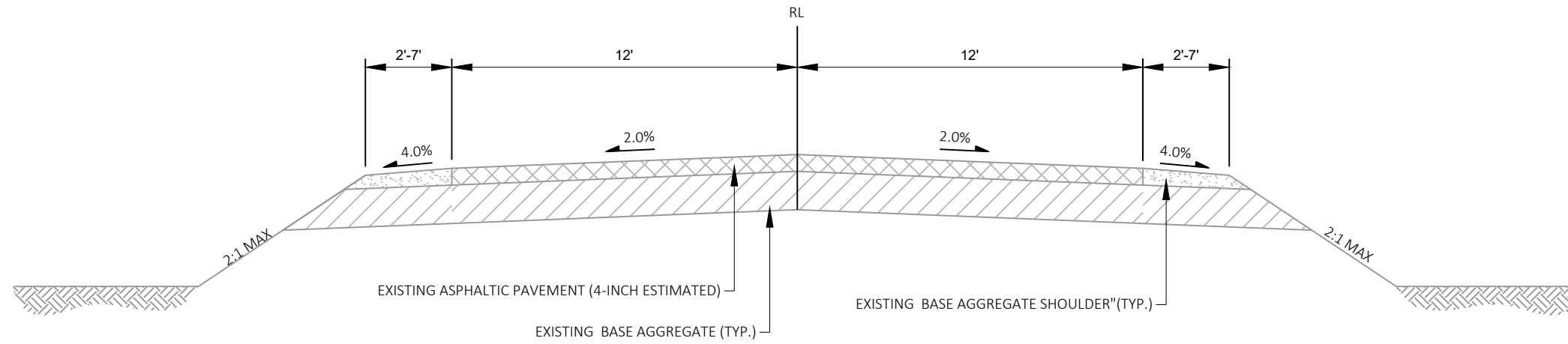
DNR LIAISON

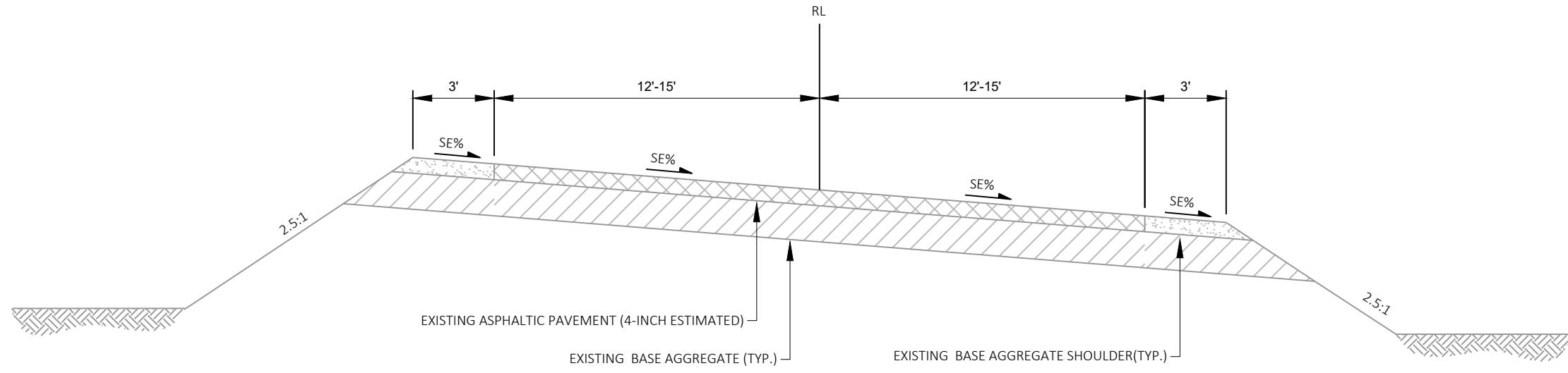
DEPARTMENT OF NATURAL RESOURCES
 DNR WEST CENTRAL REGION HEADQUARTERS
 LEAH NICOL
 1300 WEST CLAIREMONT AVE
 EAU CLAIRE, WI 54701
 (715) 934-9014
 Leah.Nicol@Wisconsin.gov

PROJECT MANAGER

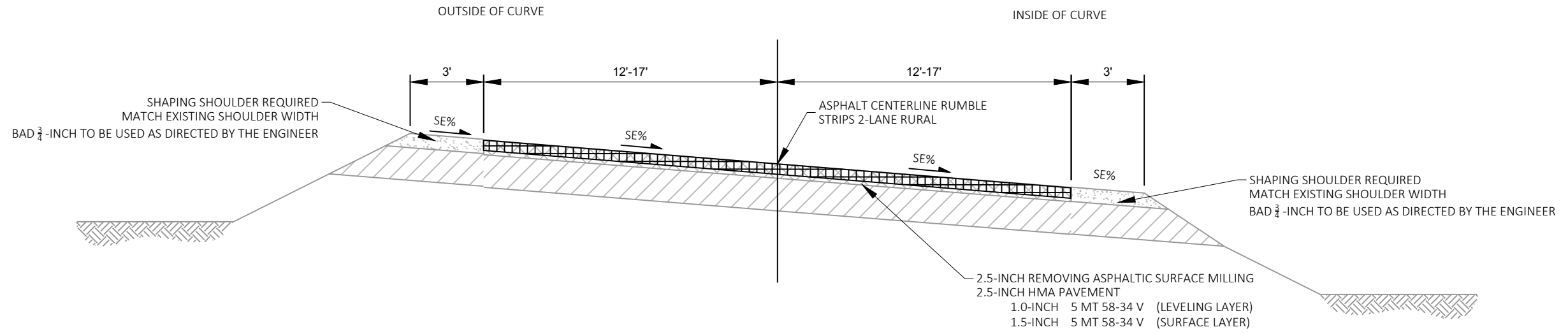
WISDOT NW REGION
 NATHAN ULNESS
 718 W CLAIREMONT AVE
 EAU CLAIRE, WI 54701
 (715) 836-3914
 Nathan.Ulness@dot.wi.gov



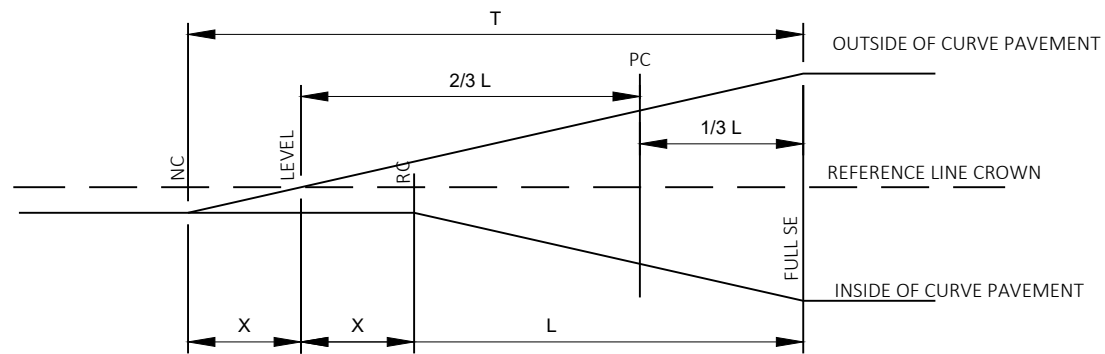




EXISTING TYPICAL SE SECTION
FOR STATION RANGES - SEE TABLE BELOW

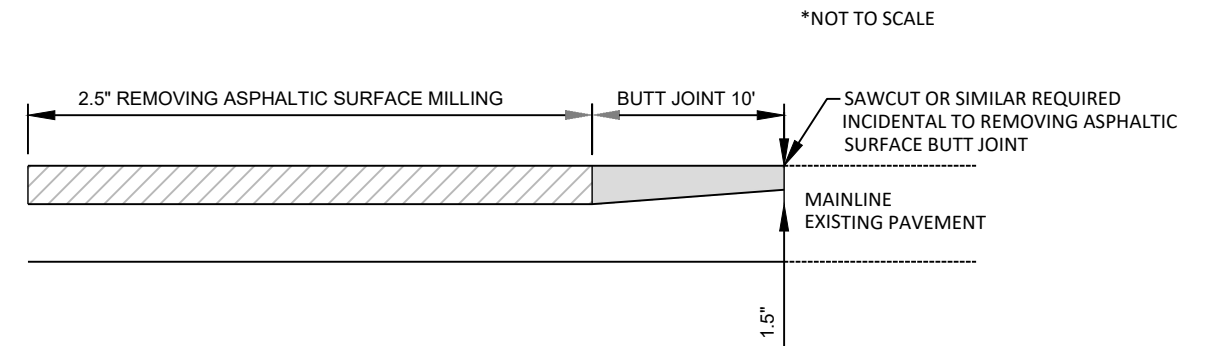
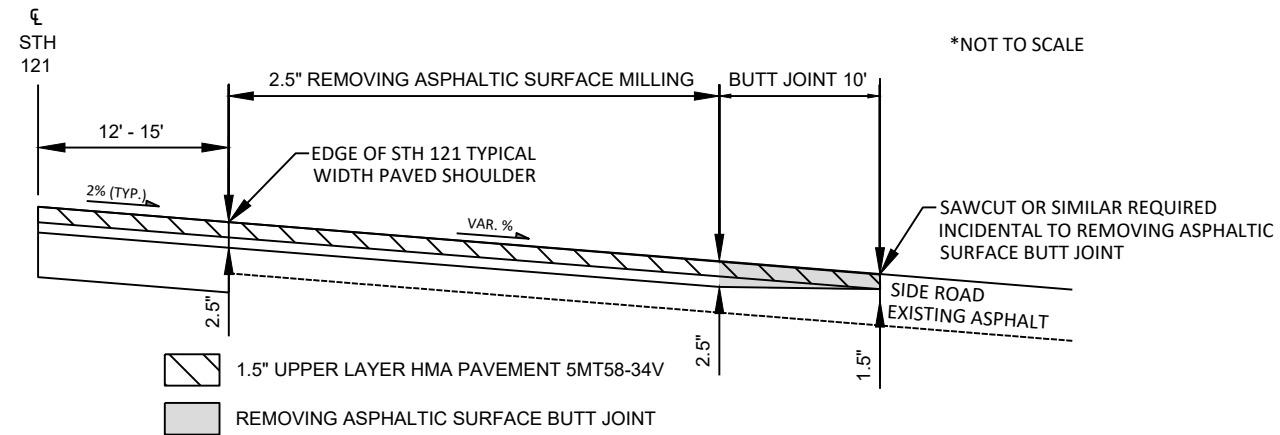


FINISHED TYPICAL SE SECTION
FOR STATION RANGES - SEE TABLE BELOW



PC Station	PT Station	Radius:	Design Speed:	*SE	X	L	T
181+92	195+10	3,745.00	60	3.8%	53	101	154
200+09	207+20	2,864.00	60	5.2%	48	139	187
328+44	332+96	5,729.58	50	RC	48	48	96
343+35	348+35	2,291.83	50	4.1%	48	98	146
350+62	355+94	1,273.24	50	5.5%	48	132	180
375+13	380+51	545.67	35	5.4%	39	105	144
412+67	418+96	396.08	35	9.9%	39	192	231
438+48	446+90	1,380.62	50	5.3%	48	127	175
448+40	449+96	2,083.48	35	2.7%	39	52	91
450+08	455+68	471.57	35	5.7%	39	110	149
457+51	465+92	1,348.14	35	3.6%	39	70	109
549+67	551+77	365.00	30	5.5%	36	100	136

*CROSS SLOPES ARE APPROXIMATED, MATCH EXISTING CROSS SLOPES

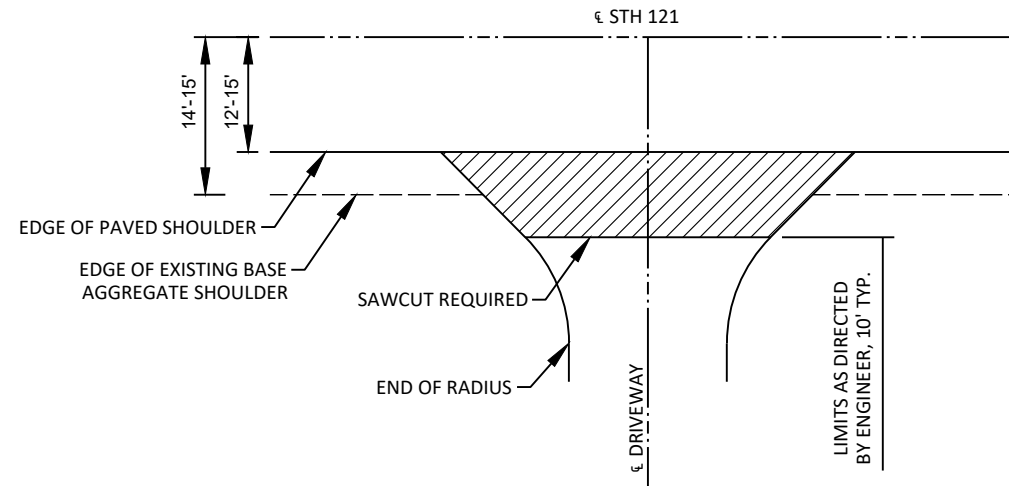


DETAIL OF SIDE ROAD BUTT JOINT - RURAL

- | | |
|-----------------|--------------------------|
| SECTION 1 ROAD | TOWNHOUSE RD x 2 |
| CHURCH RD | BRANCH RD |
| DEGROOT RD | GIESE x 2 |
| BURNETTE RD x 3 | OLD STAGE RD |
| HIXTON LEVIS RD | PARK ENTRANCE STA 548+10 |

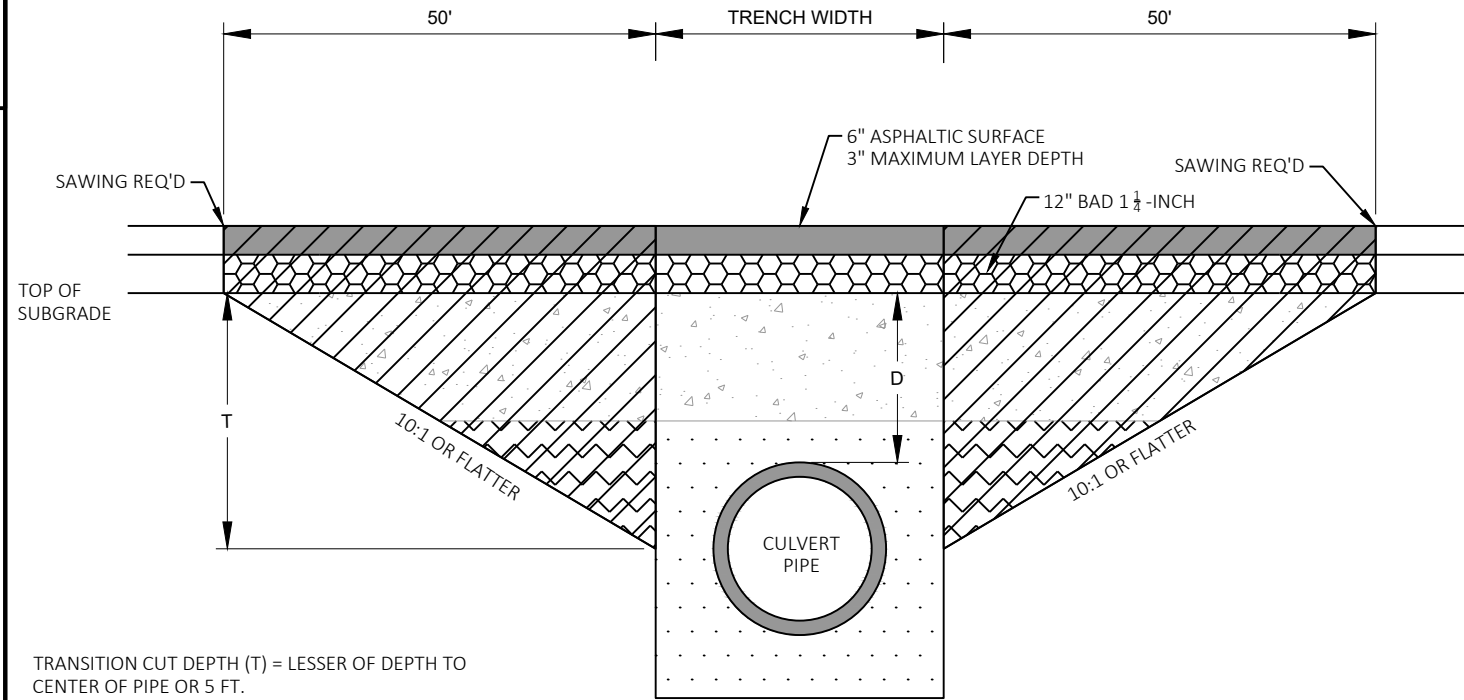
DETAIL OF MAINLINE BUTT JOINT - RURAL

- STH 121
- 183+38
- 550+35



- REMOVING ASPHALTIC SURFACE (FULL DEPTH), REPLACE WITH ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES.
- MATCH EXISTING PAVED SURFACE WIDTH, RADII AND TAPERS.
- MATCH EXISTING PAVEMENT THICKNESS.
- ANY ADDITIONAL BASE AGG. DENSE REQUIRED SHALL BE PAID UNDER ITEM - "BASE AGGREGATE DENSE 3/4-INCH"
- SAWCUT PAID FOR UNDER ITEM "SAWING ASPHALT"

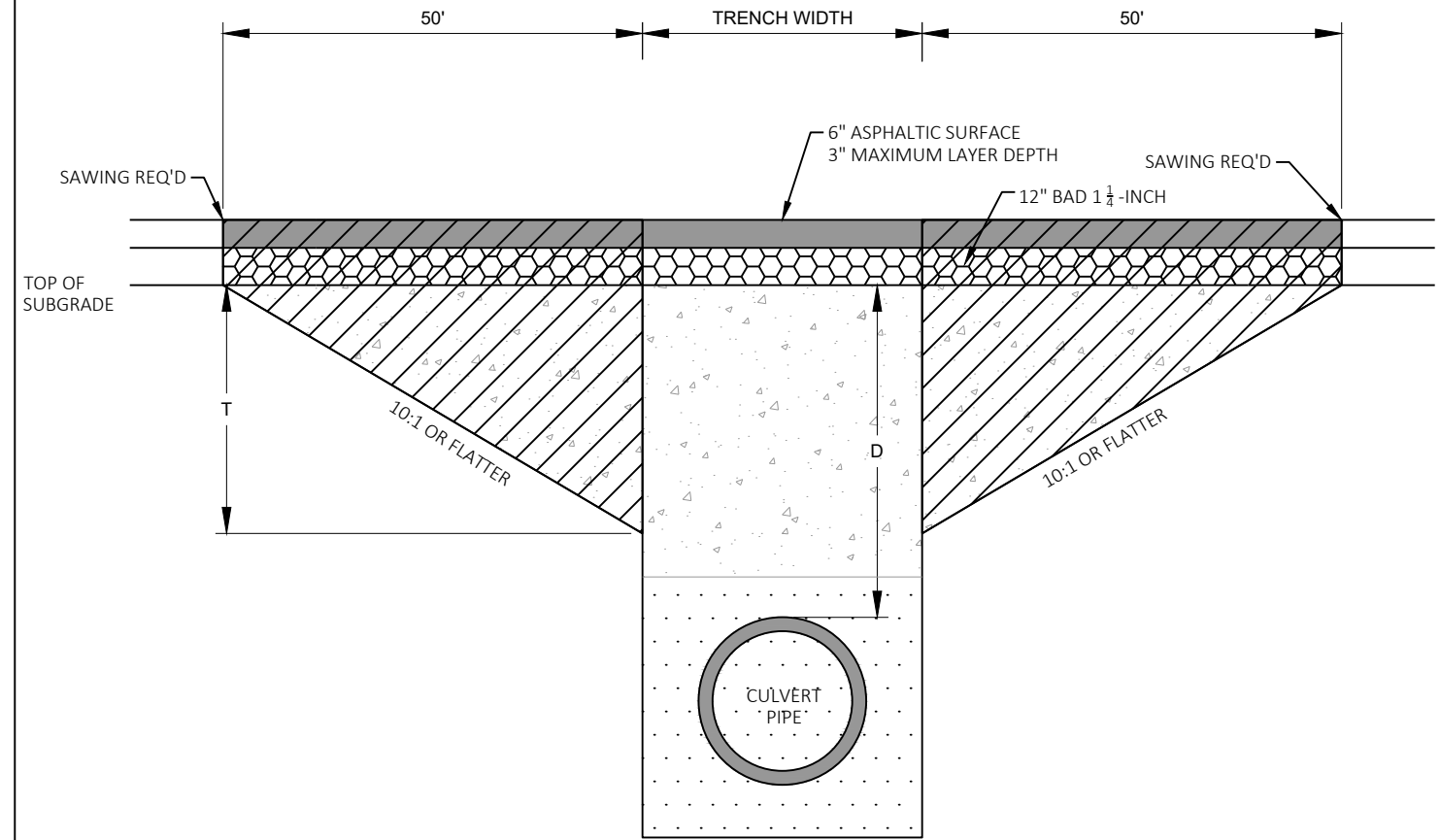
PAVED DRIVEWAY DETAIL - RURAL



TRANSITION CUT DEPTH (T) = LESSER OF DEPTH TO CENTER OF PIPE OR 5 FT.







DO NOT EXTEND TRANSITION CUT BELOW HORIZONTAL CENTER OF PIPE.

DEPTH D < 6 FT



DEPTH D ≥ 6 FT

LEGEND

-  PROPOSED SURFACE
-  PROPOSED BASE
-  TRENCH BACKFILL
-  TRENCH OR FOUNDATION BACKFILL
-  FOUNDATION BACKFILL
-  TRANSITION CUT

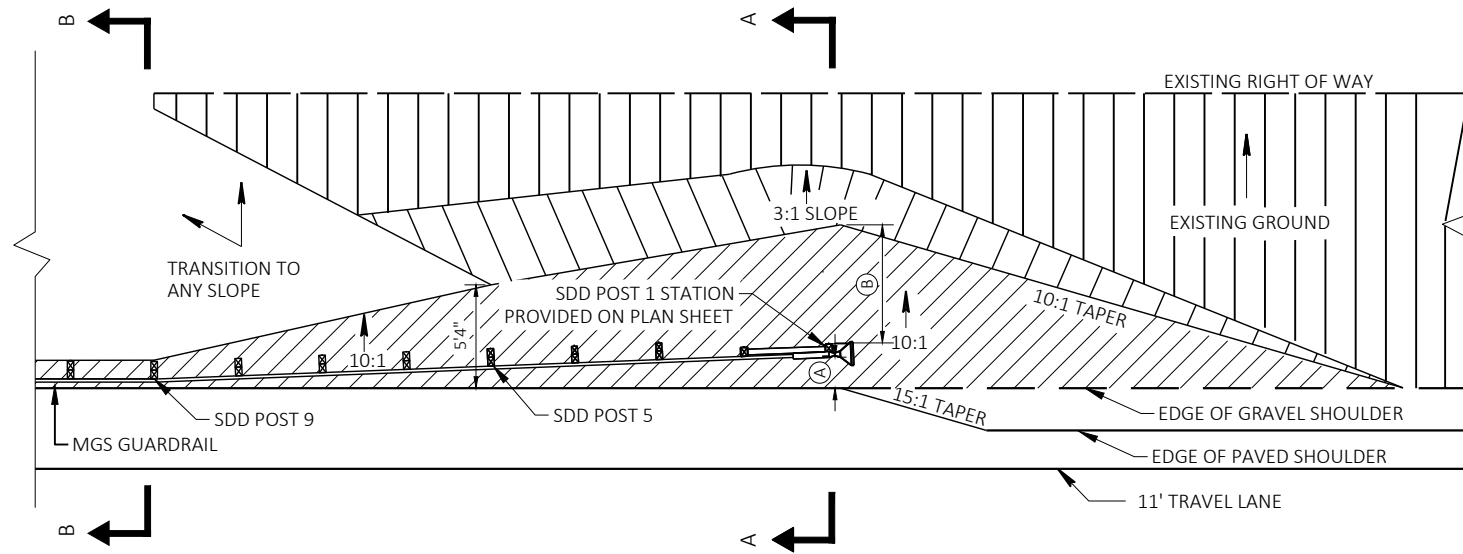
NOTES

- TRANSITION CUT IS PAID AS EXCAVATION COMMON.
- TRANSITION CUT WIDTH IS FROM SUBGRADE SHOULDER POINT TO SUBGRADE SHOULDER POINT.
- BACKFILL TRANSITION CUT AREAS WITH FOUNDATION AND TRENCH BACKFILL AS SPECIFIED IN STANDARD SPEC 520.
- PERFORM CULVERT PIPE INSTALLATION BEFORE MILLING AND PAVING.
- COMPLETE CULVERT PIPE TRANSITION PRIOR TO ASPHALTIC SURFACE MILLING OPERATIONS.

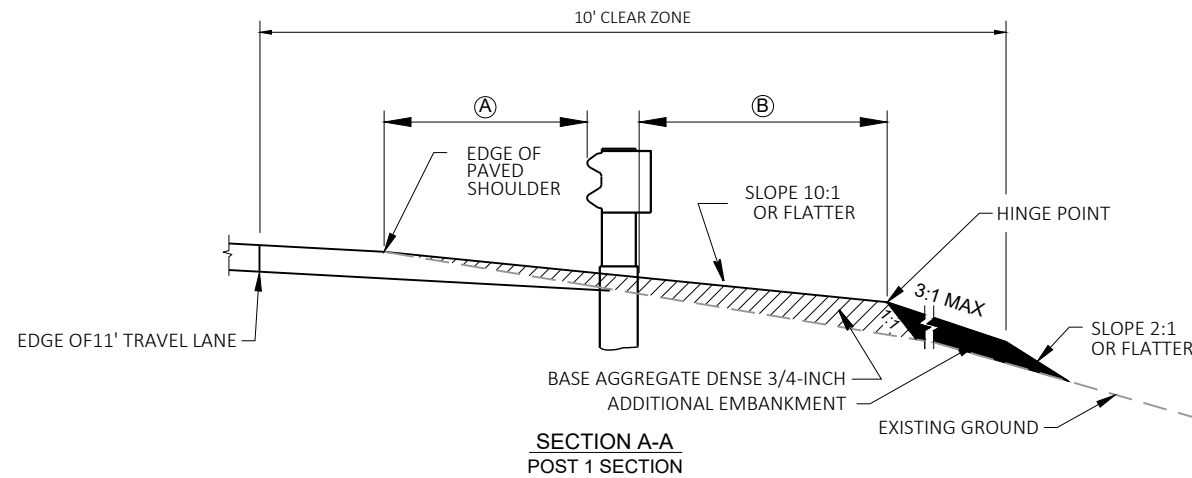
CULVERT PIPE TRANSITION

ROUTE	STA (CL)	DEPTH D (FT)	PIPE DIA (IN)	REMARKS
STH 121	STA. 190+01	10.5	30	NEW CPRC CLASS III
STH 121	STA. 239+37	5	60	NEW CPRC CLASS III
STH 121	STA. 239+47	5	60	NEW CPRC CLASS III
STH 121	STA. 248+49	1.5	24	NEW CPRC CLASS IV
STH 121	STA. 294+78	3.5	24	NEW CPRC CLASS III
STH 121	STA. 314+08	1.5	24	NEW CPRC CLASS IV
STH 121	STA. 454+15	3.5	24	NEW CPRC CLASS III
STH 121	STA. 465+12	2	72	NEW PIPE CATTLE PASS REINF. CONC.
STH 121	STA. 468+83	2.5	24	NEW CPRC CLASS III
STH 121	STA. 547+32	1	24	NEW CPRC CLASS IV

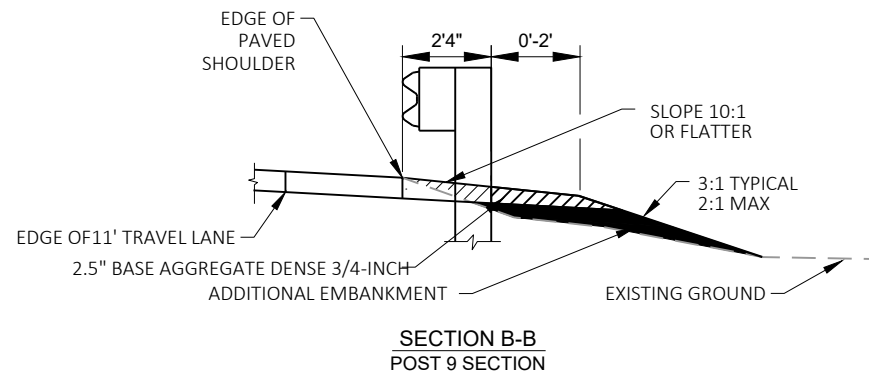
"SDD" ON THIS SHEET REFERS TO STANDARD DETAIL DRAWING "MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)"
 INFORMATION ON THIS SHEET SUPERCEDES INFORMATION GIVEN ON S.D.D.



PLAN VIEW



SECTION A-A
POST 1 SECTION

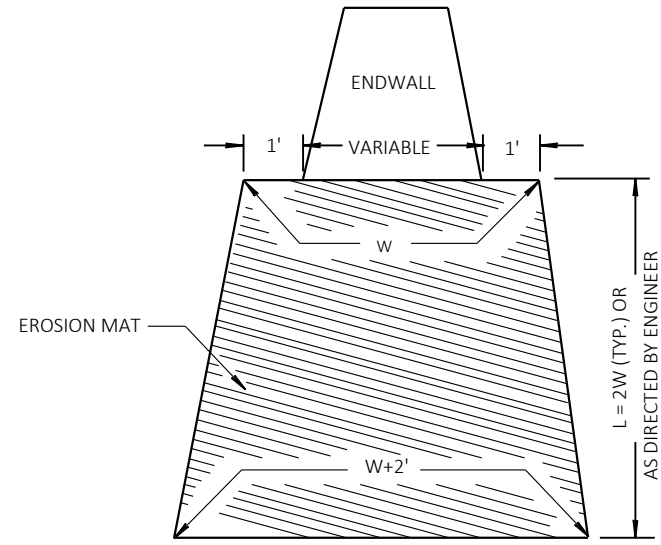


SECTION B-B
POST 9 SECTION

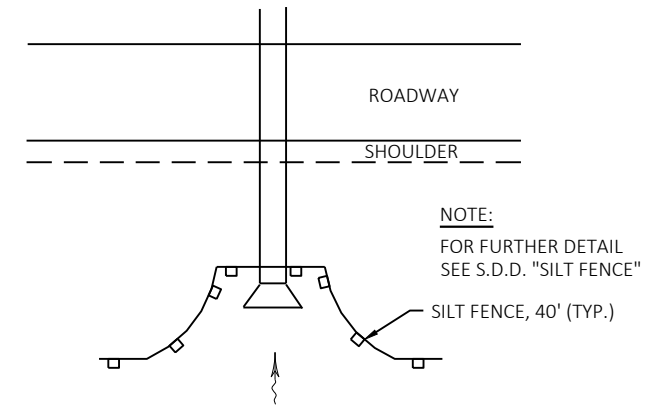
- (A) DISTANCE TO FACE OF RAIL FROM NORMAL SHOULDER
- (B) DISTANCE TO HINGE POINT FROM BACK OF POST 1

	APPROACH			DEPARTURE		
	POST1 STA	(A)	(B)	POST1 STA	(A)	(B)
EB	380+64.34	1.5	3.5	382+88.84	0	5
WB	383+58.00	2	3.5	380+64.25	2	5
EB	406+48.50	2	3	408+80.00	0	5
WB	409+49.18	2	3	407+17.69	0	5

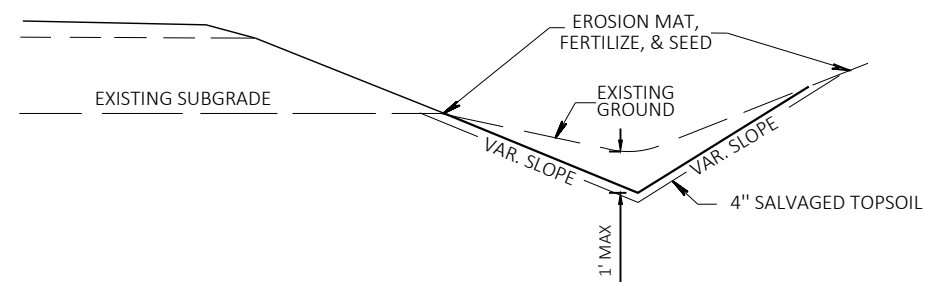
MGS TERMINAL GRADING DETAIL



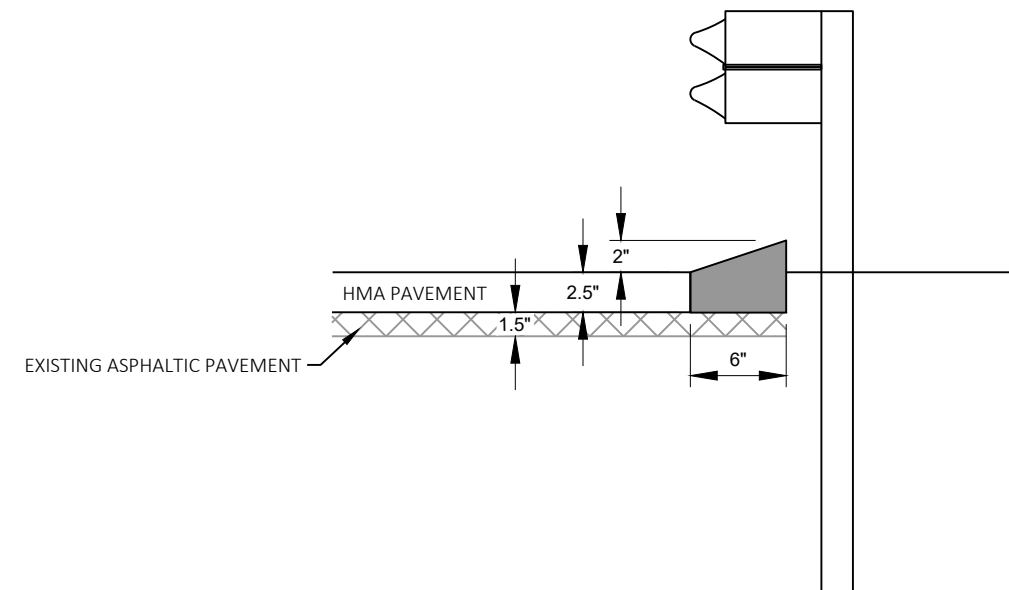
EROSION MAT TREATMENT AT CULVERTS



SILT FENCE AT PIPE INLET
SEE EROSION CONTROL PLAN FOR LOCATIONS

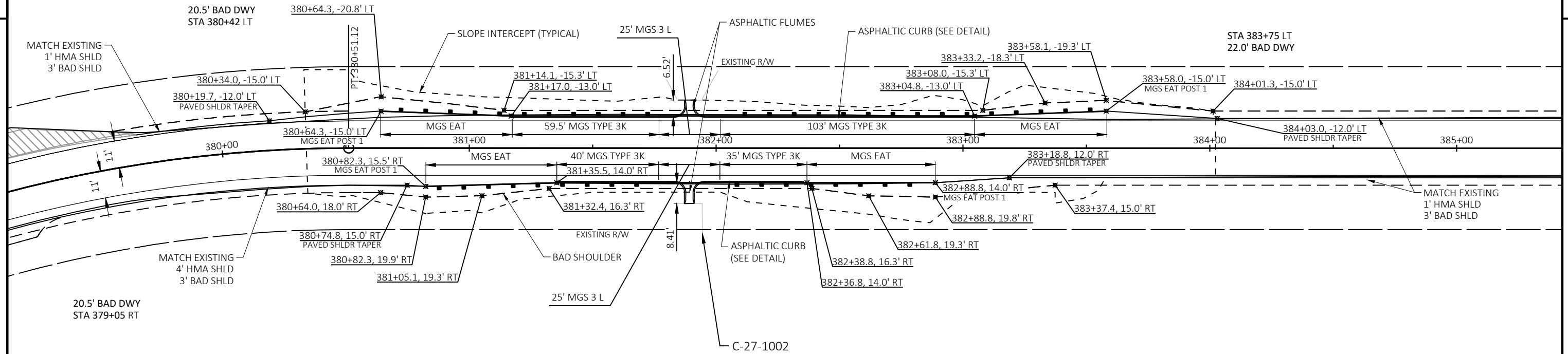


CLEANING DITCH DETAIL



ASPHALTIC CURB DETAIL

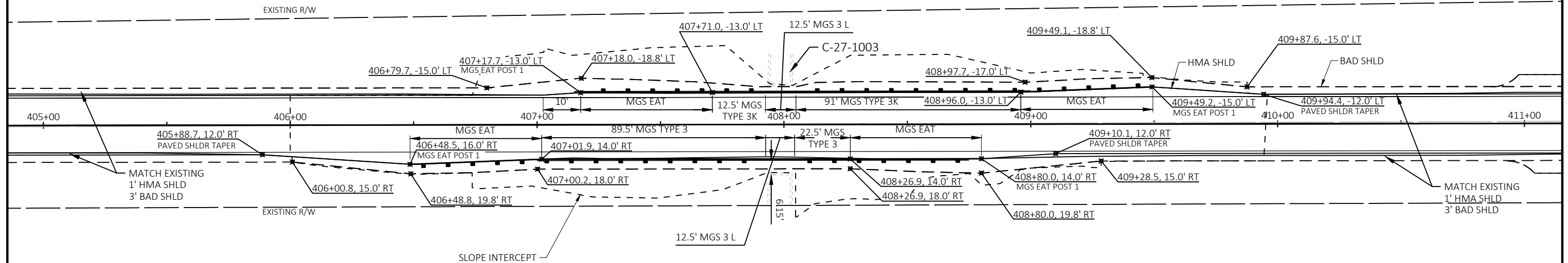
C-27-1002 MGS LAYOUT DETAIL

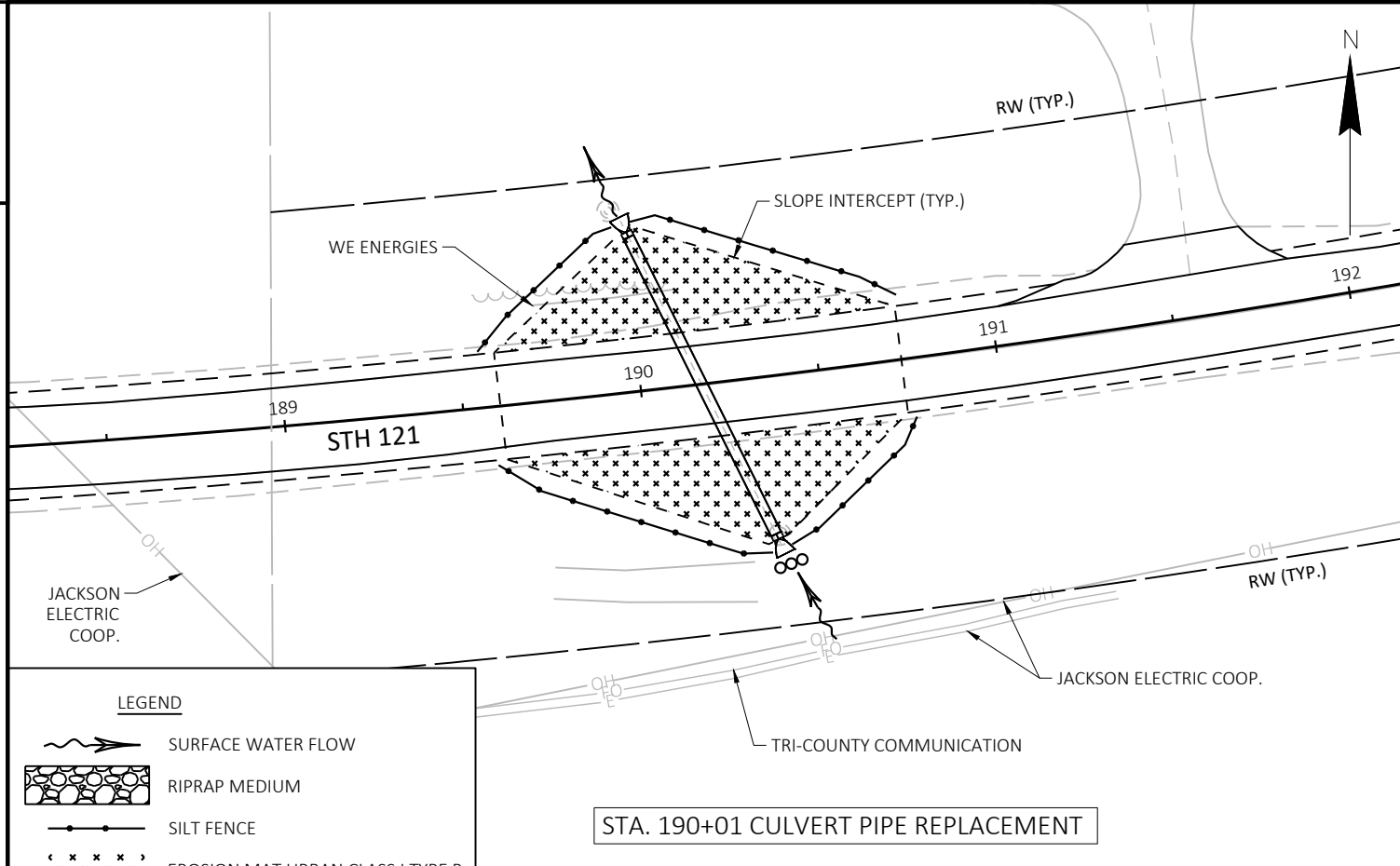


C-27-1003 MGS LAYOUT DETAIL

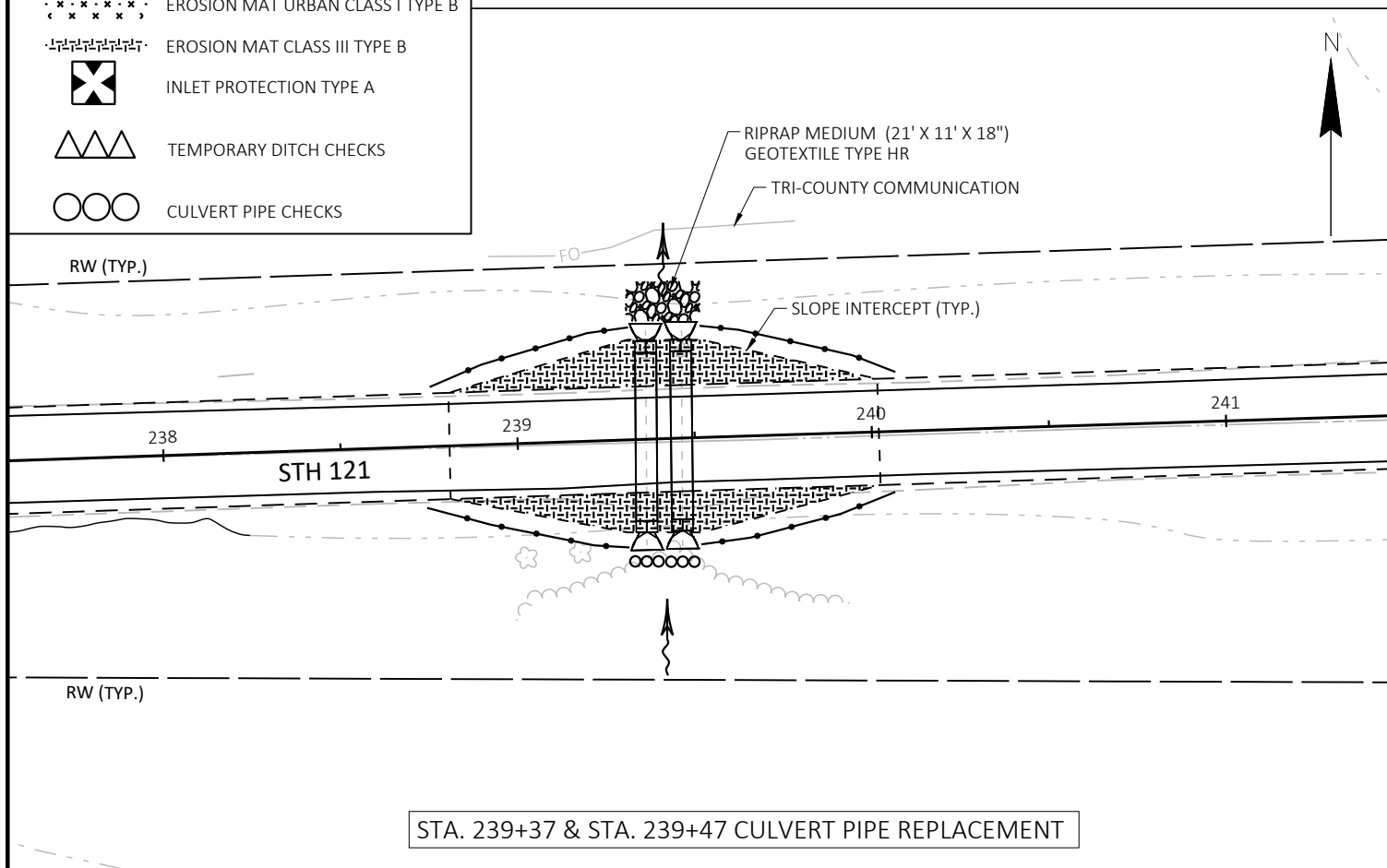


FOR ADDITIONAL DETAILS SEE
 CONSTRUCTION DETAIL "MGS TERMINAL GRADING"
 SDD "MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL"
 SDD "MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL"
 SDD "MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L) TWO WAY TRAFFIC"

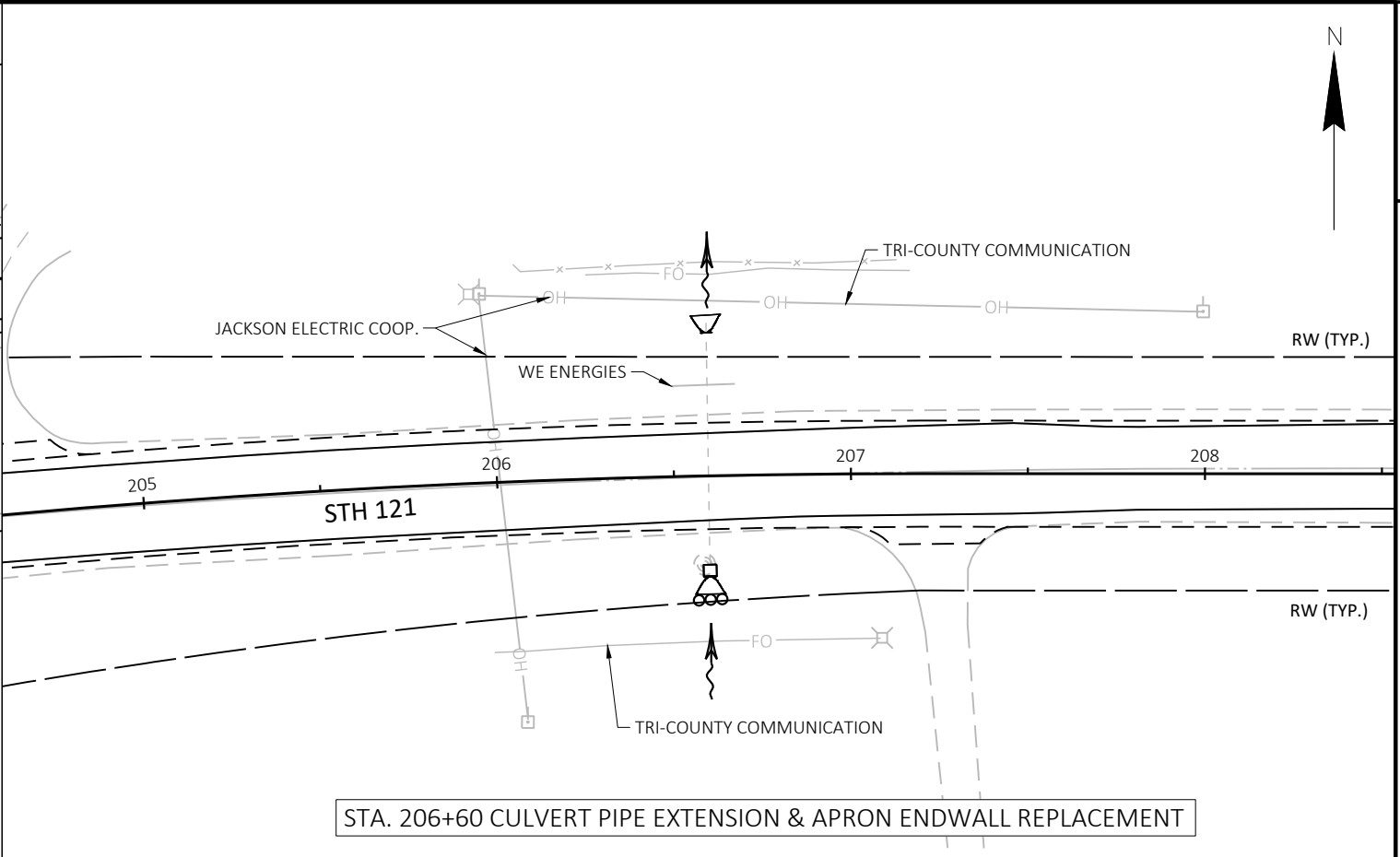




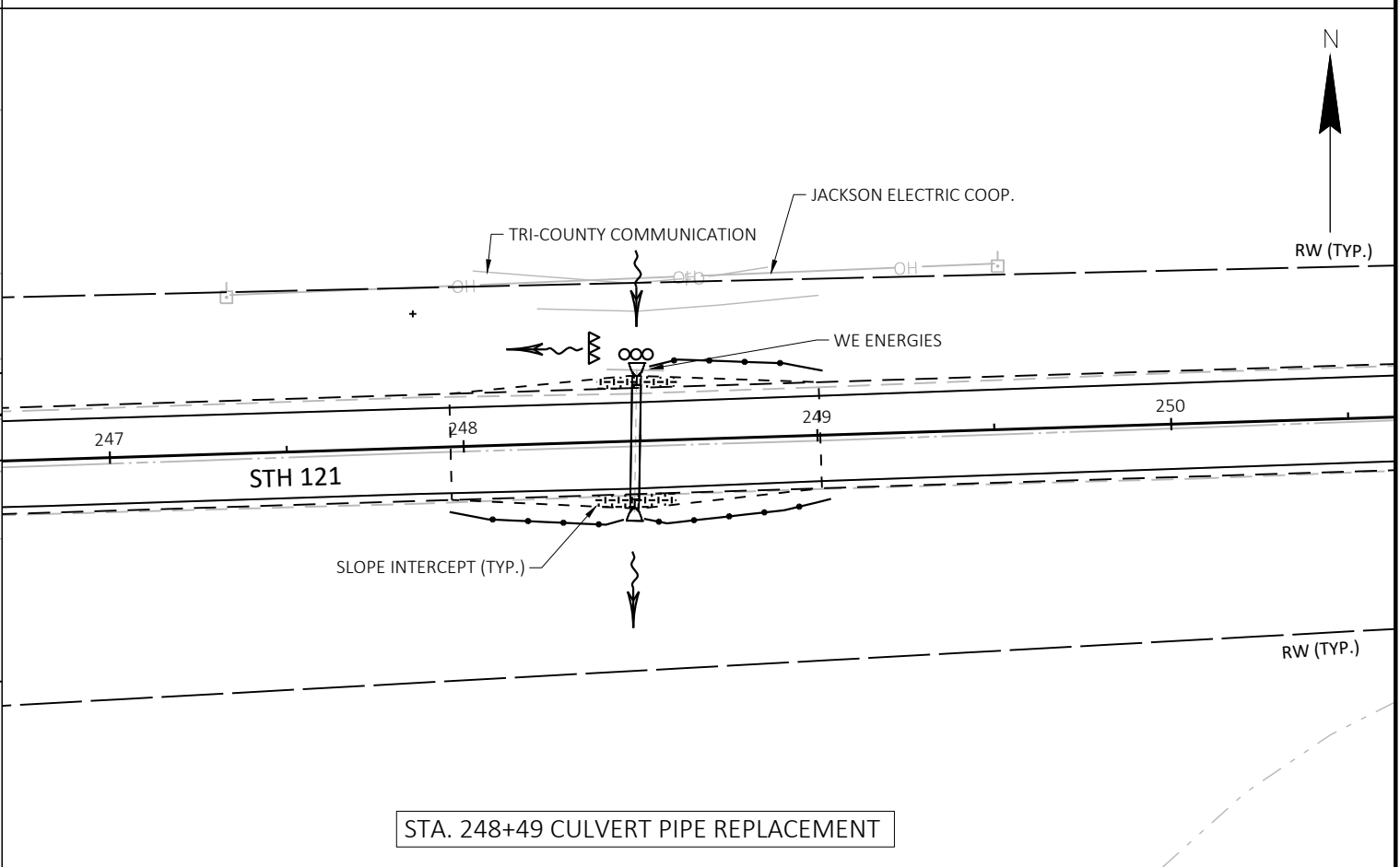
STA. 190+01 CULVERT PIPE REPLACEMENT



STA. 239+37 & STA. 239+47 CULVERT PIPE REPLACEMENT



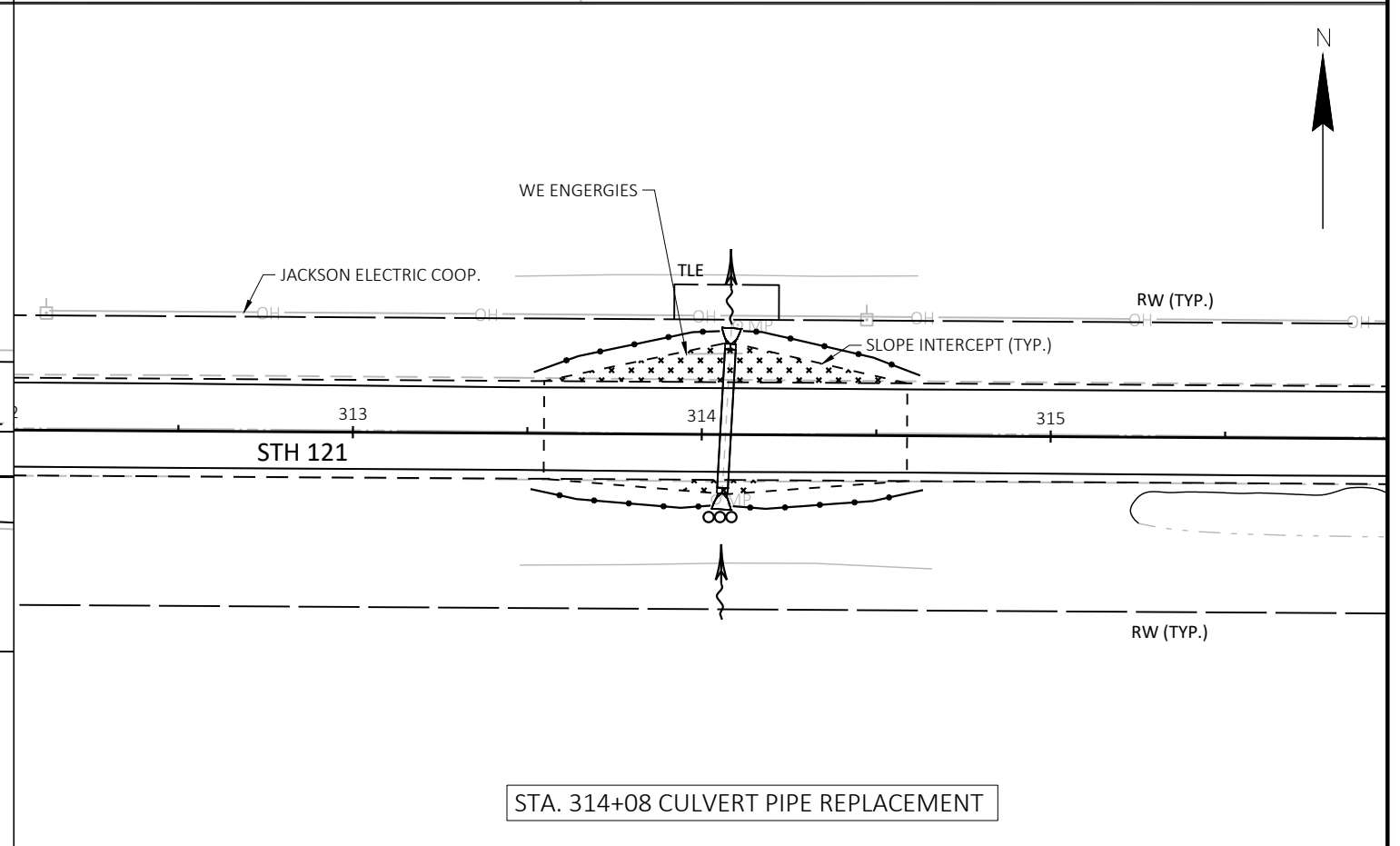
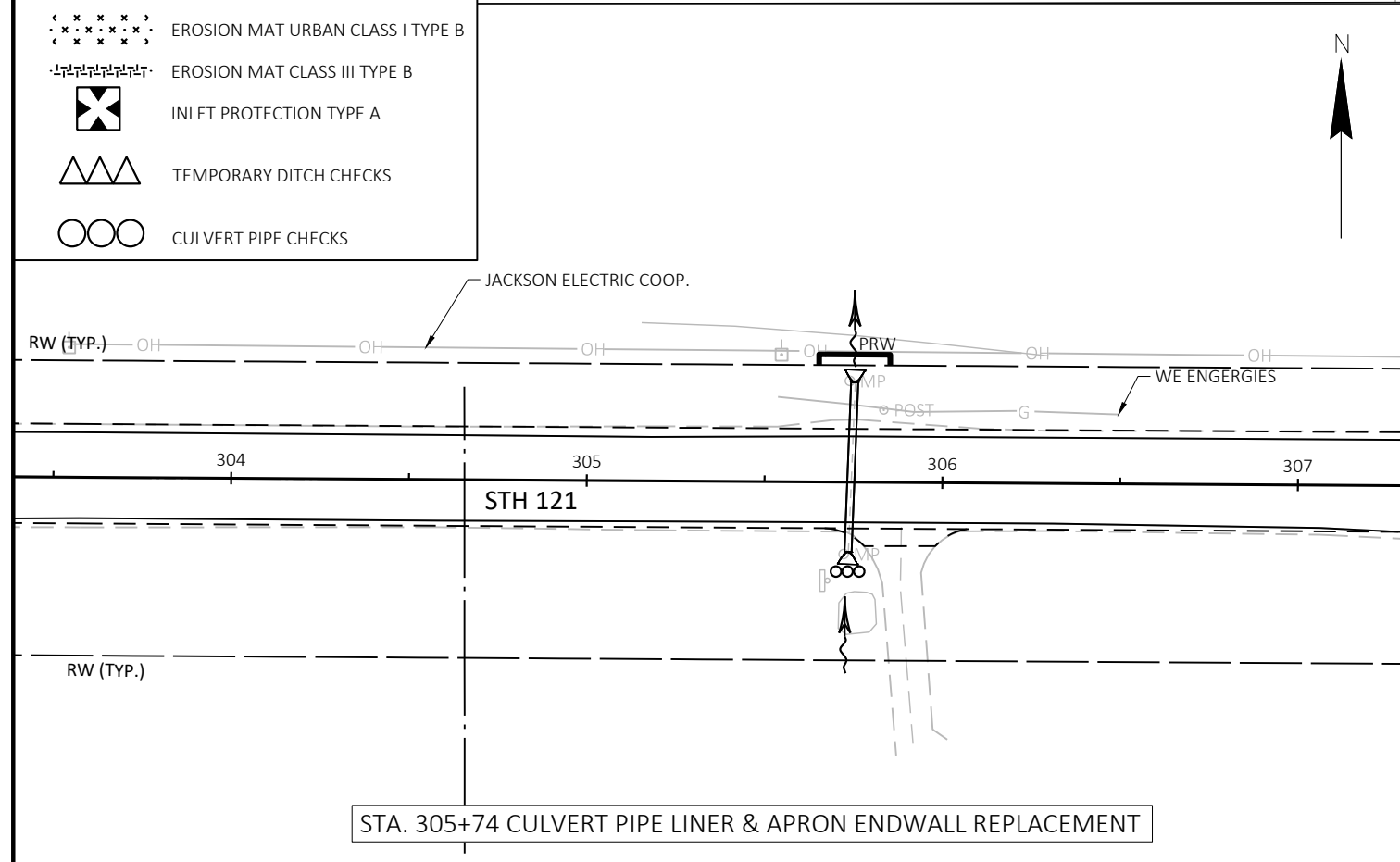
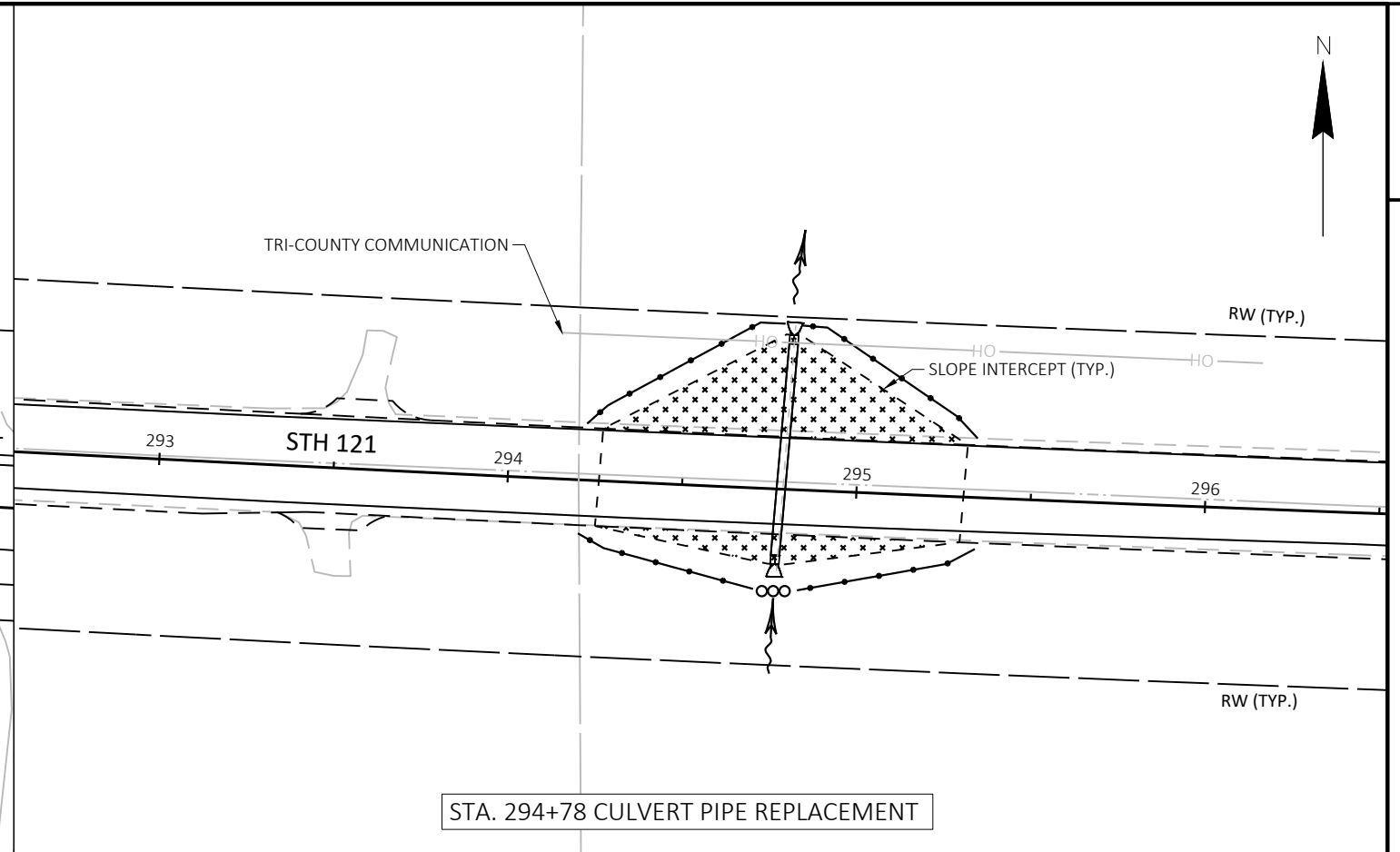
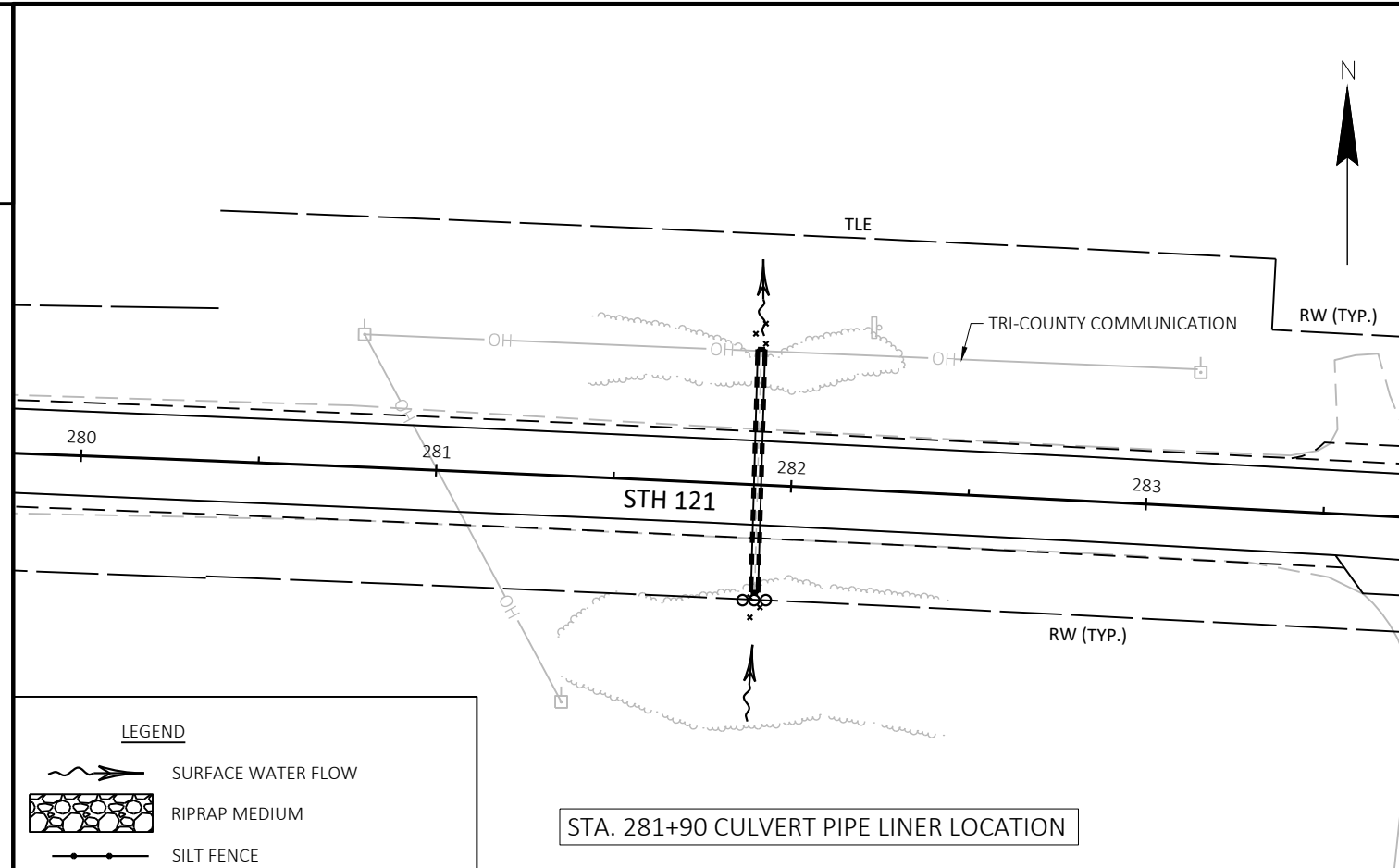
STA. 206+60 CULVERT PIPE EXTENSION & APRON ENDWALL REPLACEMENT

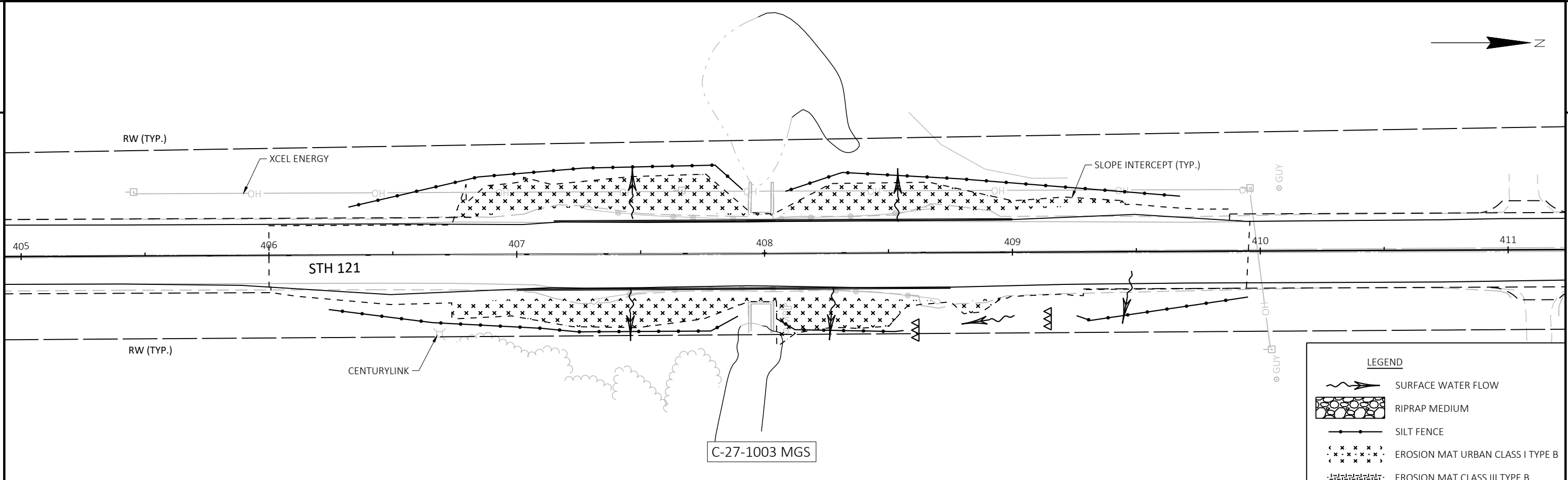


STA. 248+49 CULVERT PIPE REPLACEMENT

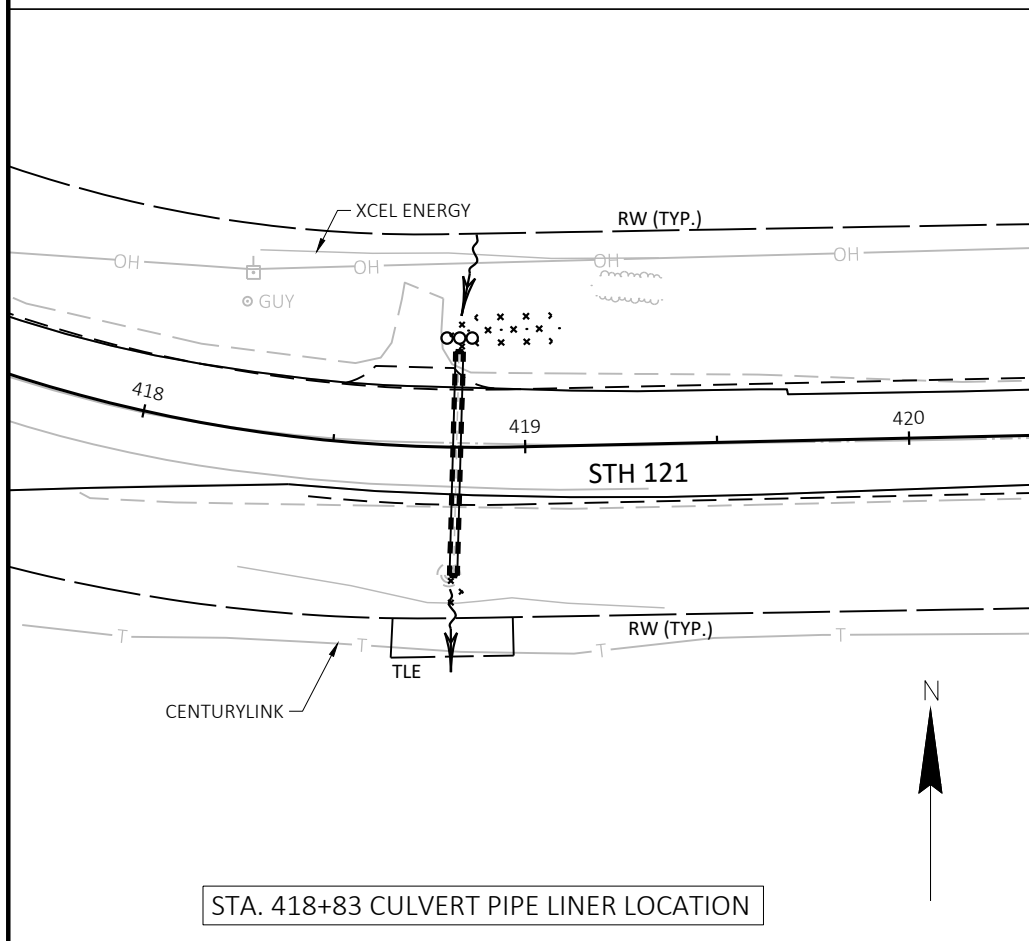
LEGEND

- SURFACE WATER FLOW
- RIPRAP MEDIUM
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE B
- EROSION MAT CLASS III TYPE B
- INLET PROTECTION TYPE A
- TEMPORARY DITCH CHECKS
- CULVERT PIPE CHECKS

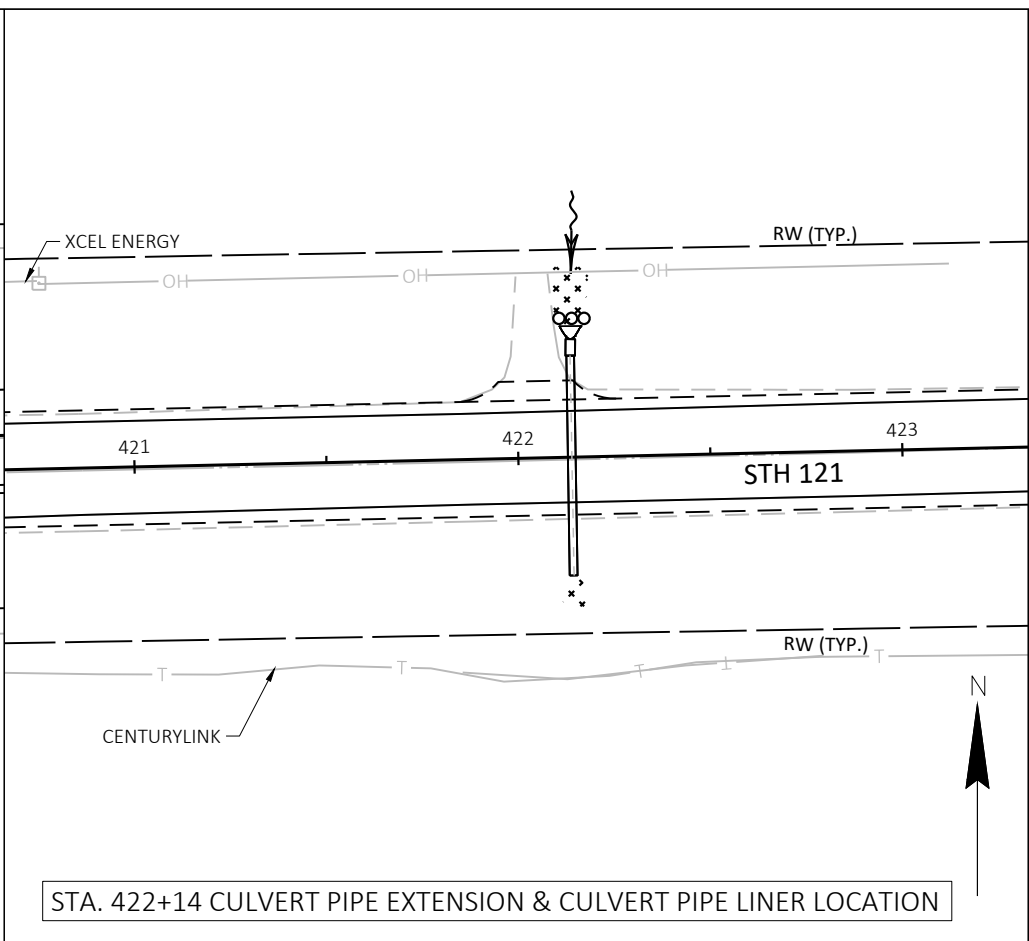




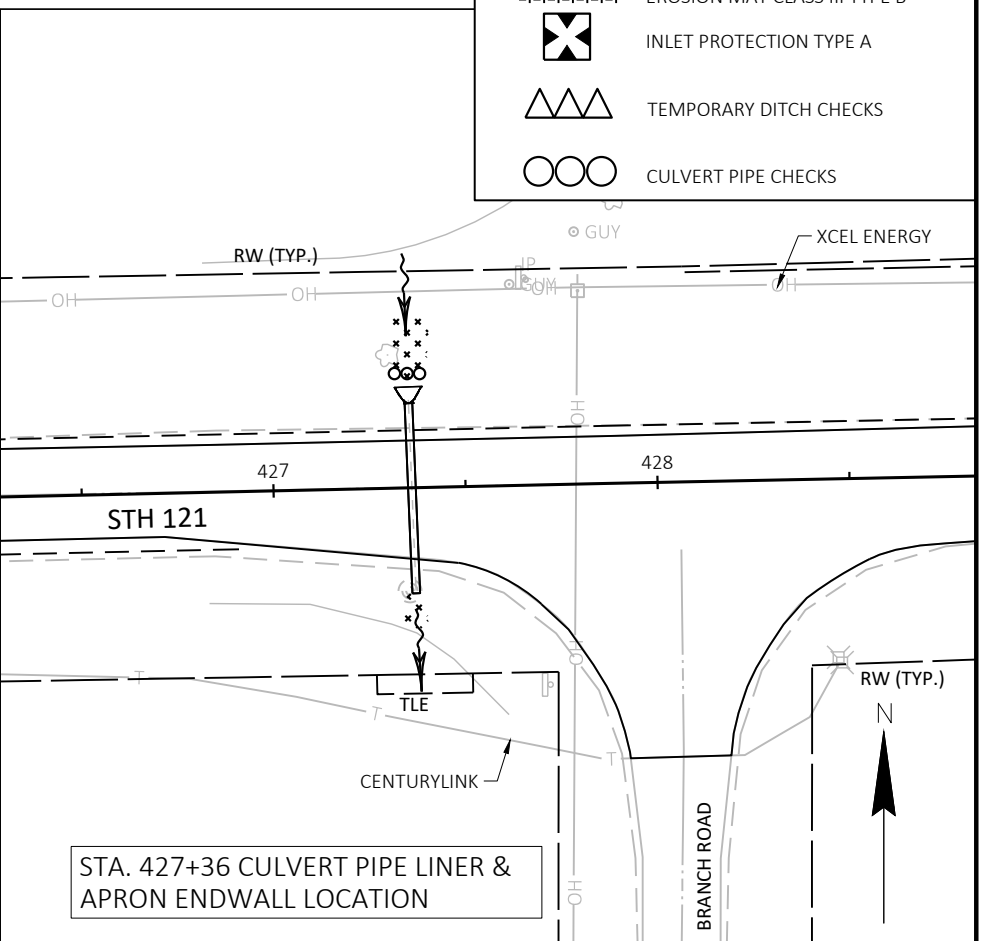
LEGEND	
	SURFACE WATER FLOW
	RIPRAP MEDIUM
	SILT FENCE
	EROSION MAT URBAN CLASS I TYPE B
	EROSION MAT CLASS III TYPE B
	INLET PROTECTION TYPE A
	TEMPORARY DITCH CHECKS
	CULVERT PIPE CHECKS



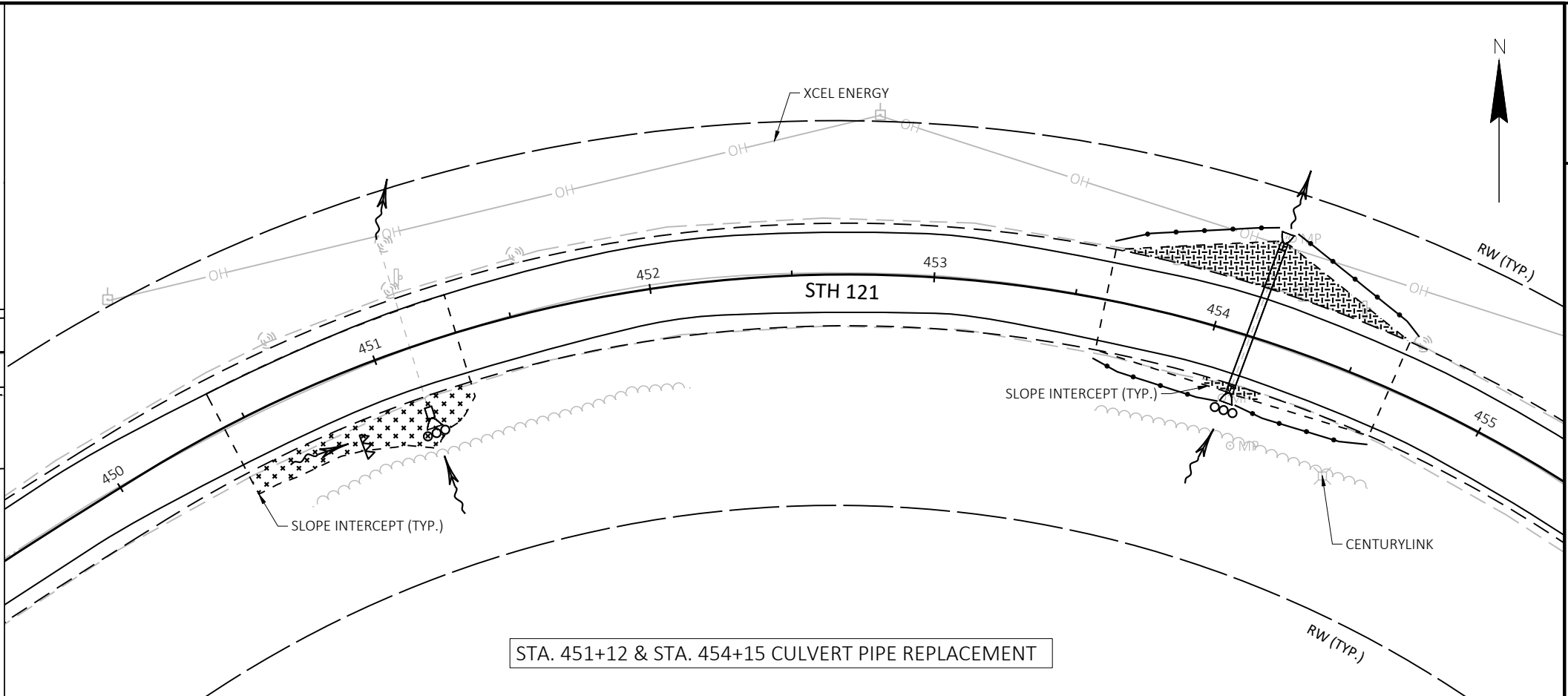
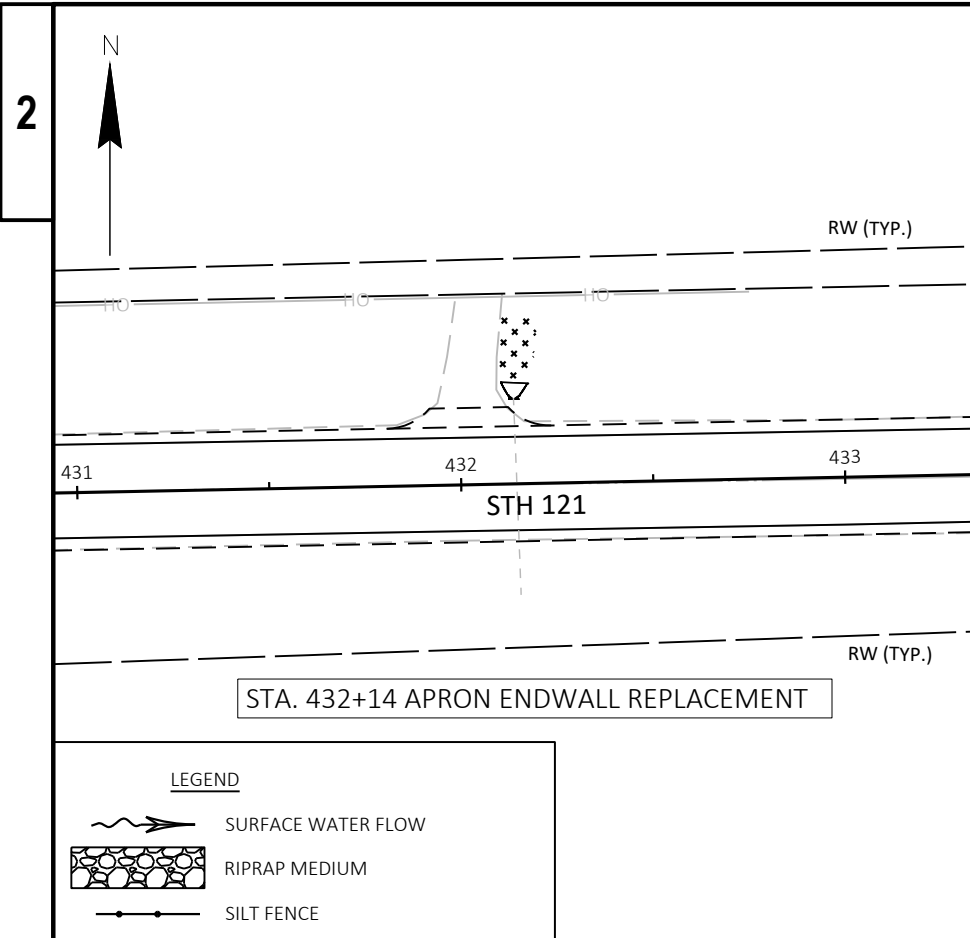
STA. 418+83 CULVERT PIPE LINER LOCATION



STA. 422+14 CULVERT PIPE EXTENSION & CULVERT PIPE LINER LOCATION

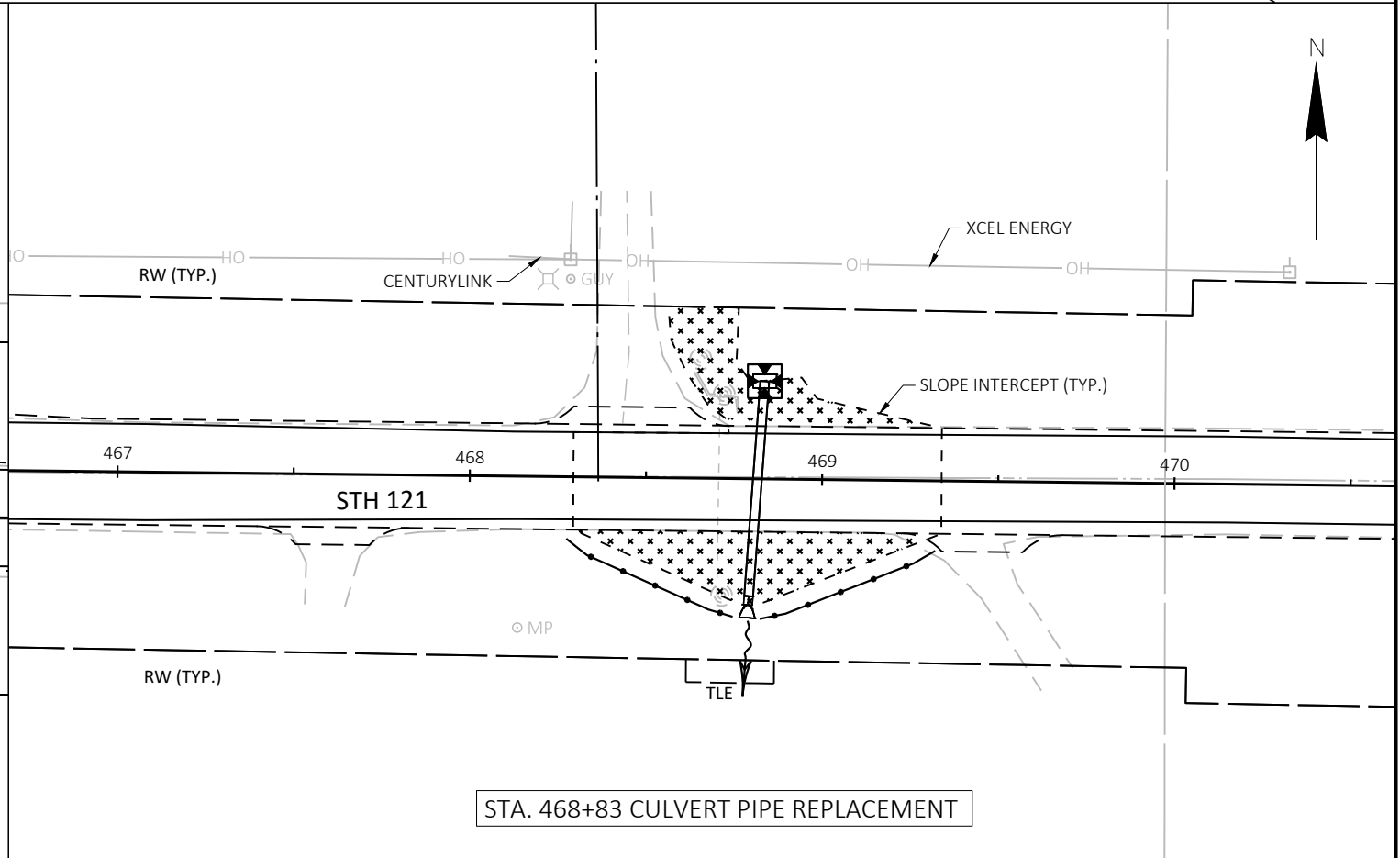
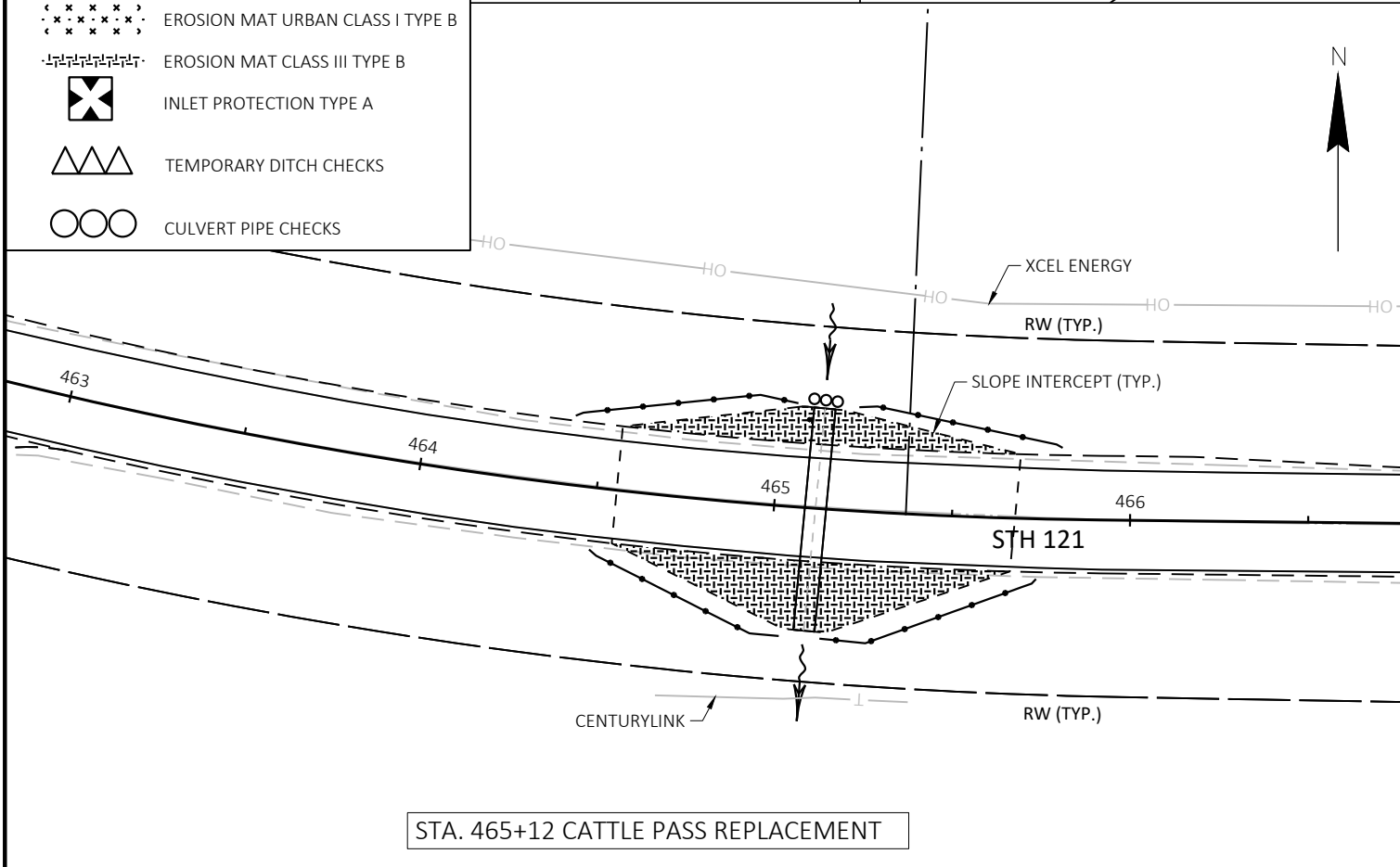


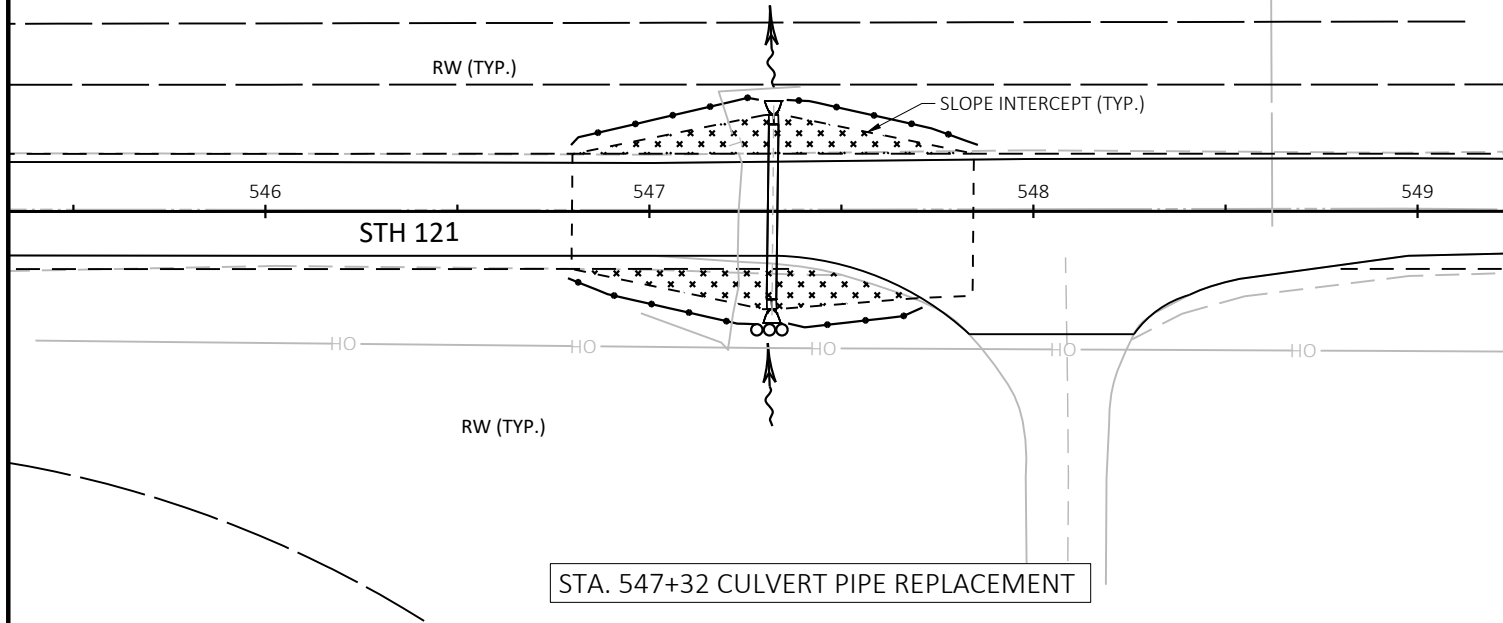
STA. 427+36 CULVERT PIPE LINER & APRON ENDWALL LOCATION







LEGEND

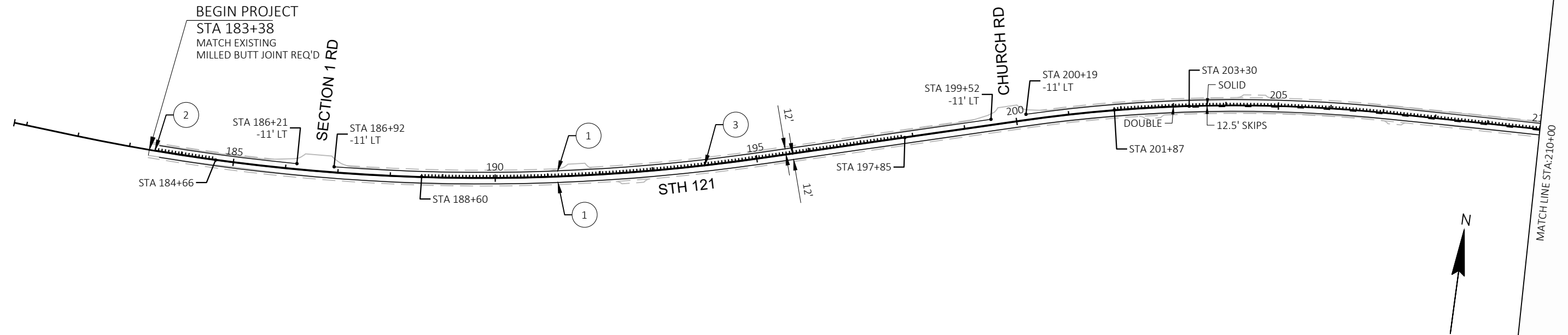
- SURFACE WATER FLOW
- RIPRAP MEDIUM
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE B
- EROSION MAT CLASS III TYPE B
- INLET PROTECTION TYPE A
- TEMPORARY DITCH CHECKS
- CULVERT PIPE CHECKS





LEGEND

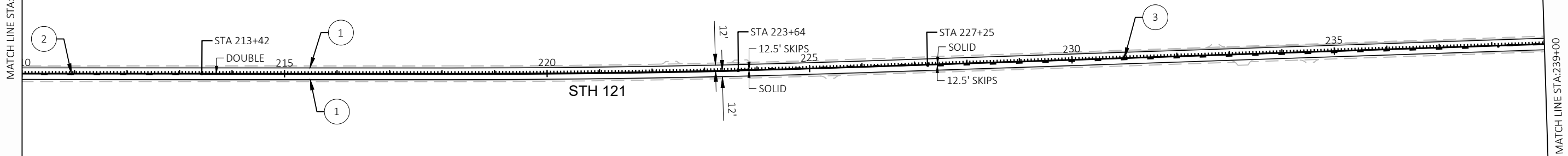
-  SURFACE WATER FLOW
-  RIPRAP MEDIUM
-  SILT FENCE
-  EROSION MAT URBAN CLASS I TYPE B
-  EROSION MAT CLASS III TYPE B
-  INLET PROTECTION TYPE A
-  TEMPORARY DITCH CHECKS
-  CULVERT PIPE CHECKS

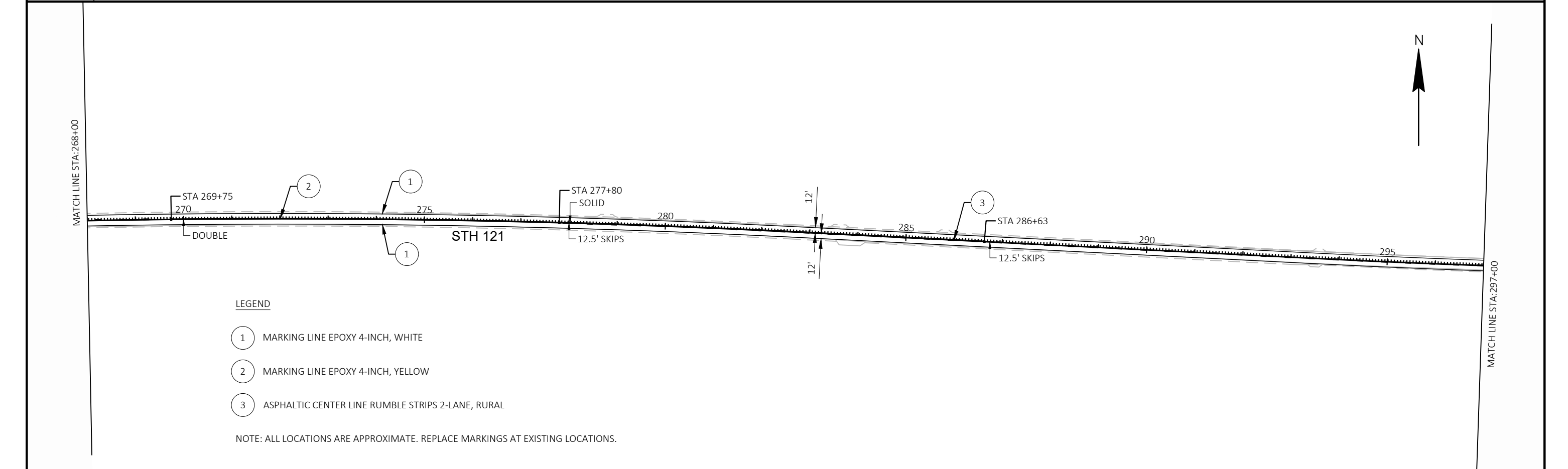
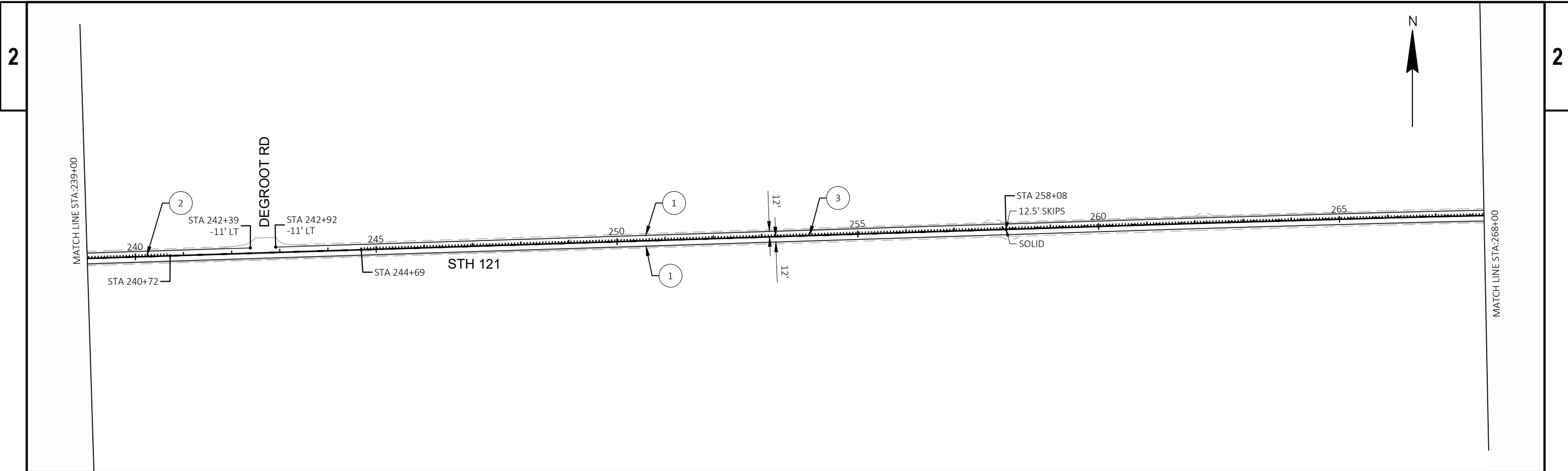


LEGEND

- 1 MARKING LINE EPOXY 4-INCH, WHITE
- 2 MARKING LINE EPOXY 4-INCH, YELLOW
- 3 ASPHALTIC CENTER LINE RUMBLE STRIPS 2-LANE, RURAL

NOTE: ALL LOCATIONS ARE APPROXIMATE. REPLACE MARKINGS AT EXISTING LOCATIONS.

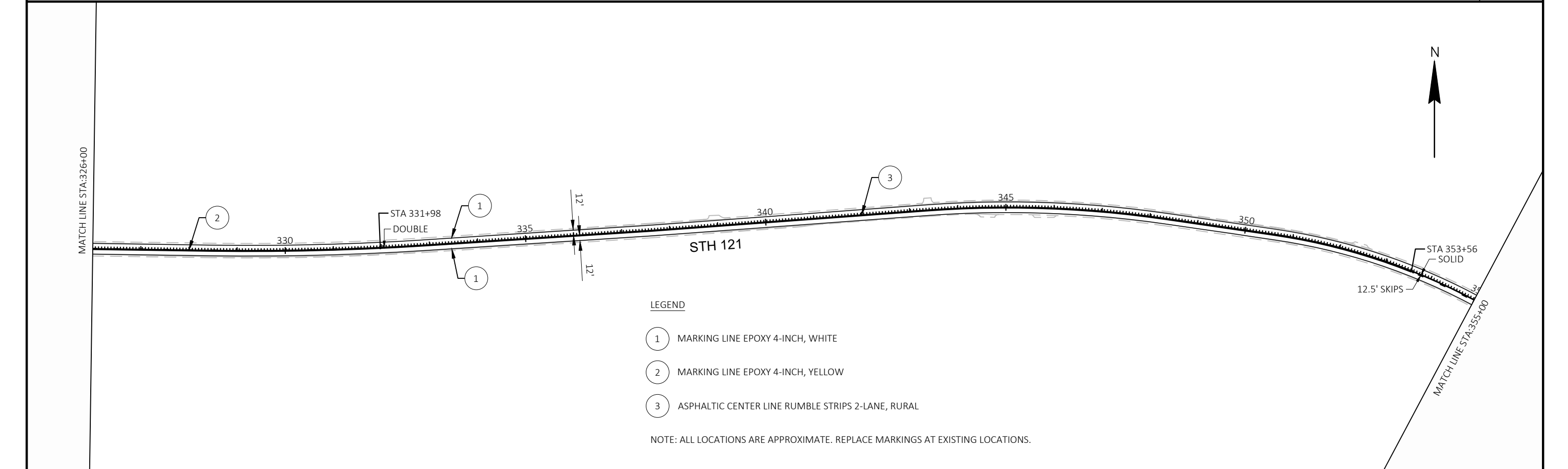
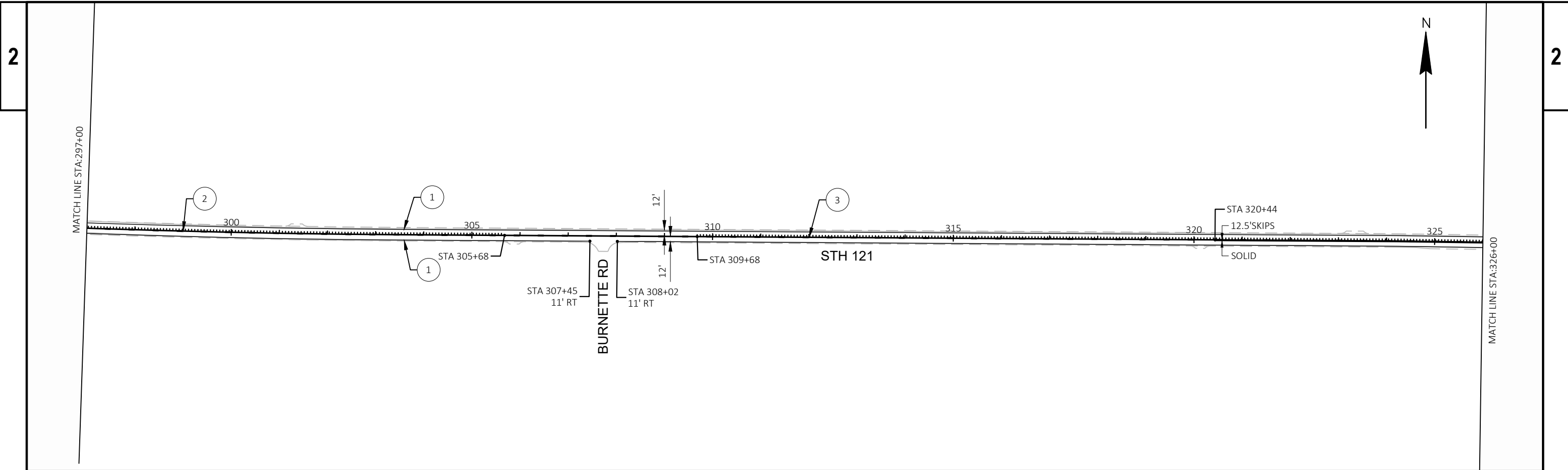




LEGEND

- 1 MARKING LINE EPOXY 4-INCH, WHITE
- 2 MARKING LINE EPOXY 4-INCH, YELLOW
- 3 ASPHALTIC CENTER LINE RUMBLE STRIPS 2-LANE, RURAL

NOTE: ALL LOCATIONS ARE APPROXIMATE. REPLACE MARKINGS AT EXISTING LOCATIONS.



LEGEND

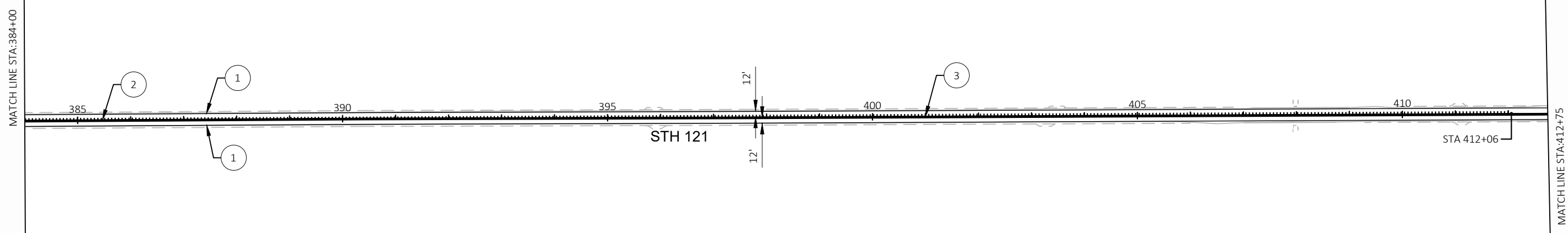
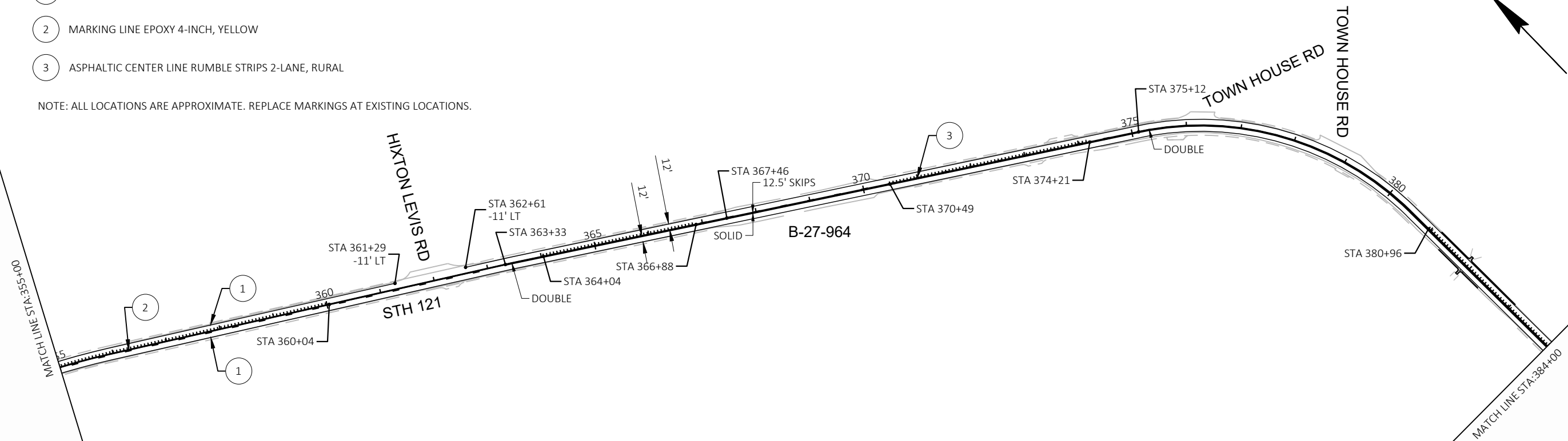
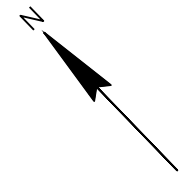
- ① MARKING LINE EPOXY 4-INCH, WHITE
- ② MARKING LINE EPOXY 4-INCH, YELLOW
- ③ ASPHALTIC CENTER LINE RUMBLE STRIPS 2-LANE, RURAL

NOTE: ALL LOCATIONS ARE APPROXIMATE. REPLACE MARKINGS AT EXISTING LOCATIONS.

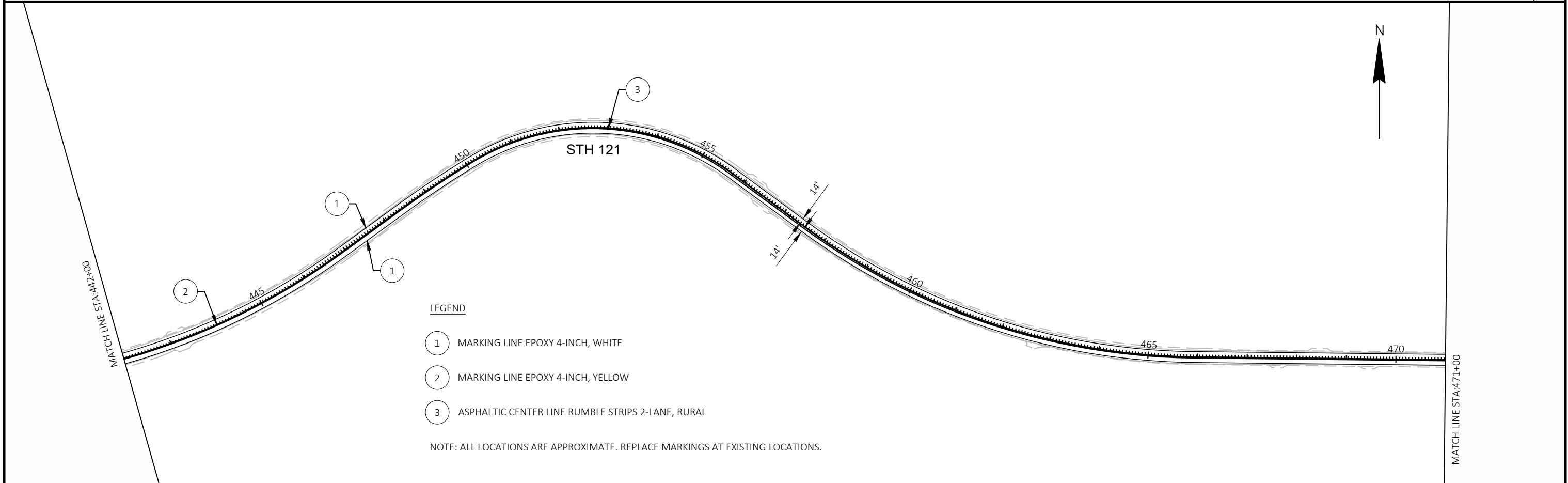
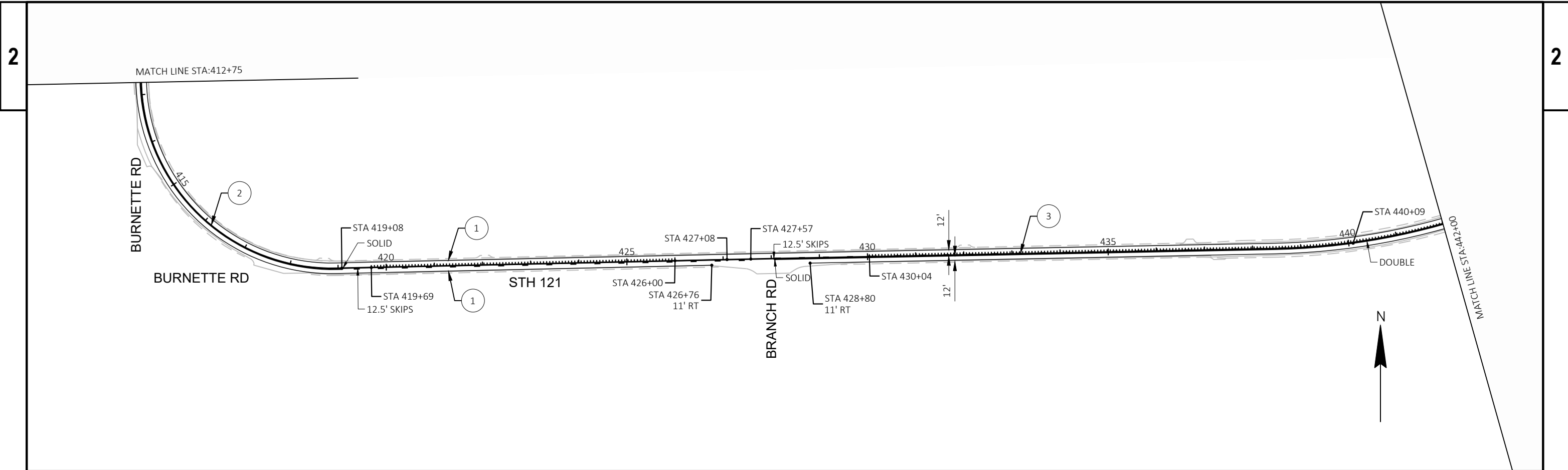
LEGEND

- 1 MARKING LINE EPOXY 4-INCH, WHITE
- 2 MARKING LINE EPOXY 4-INCH, YELLOW
- 3 ASPHALTIC CENTER LINE RUMBLE STRIPS 2-LANE, RURAL

NOTE: ALL LOCATIONS ARE APPROXIMATE. REPLACE MARKINGS AT EXISTING LOCATIONS.



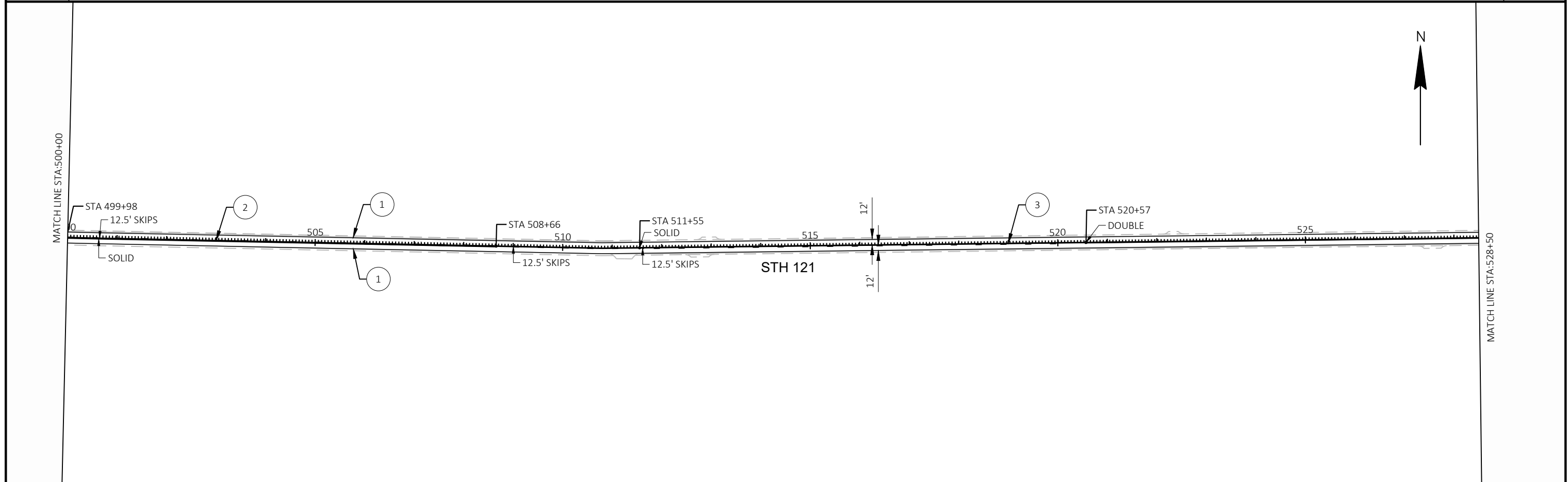
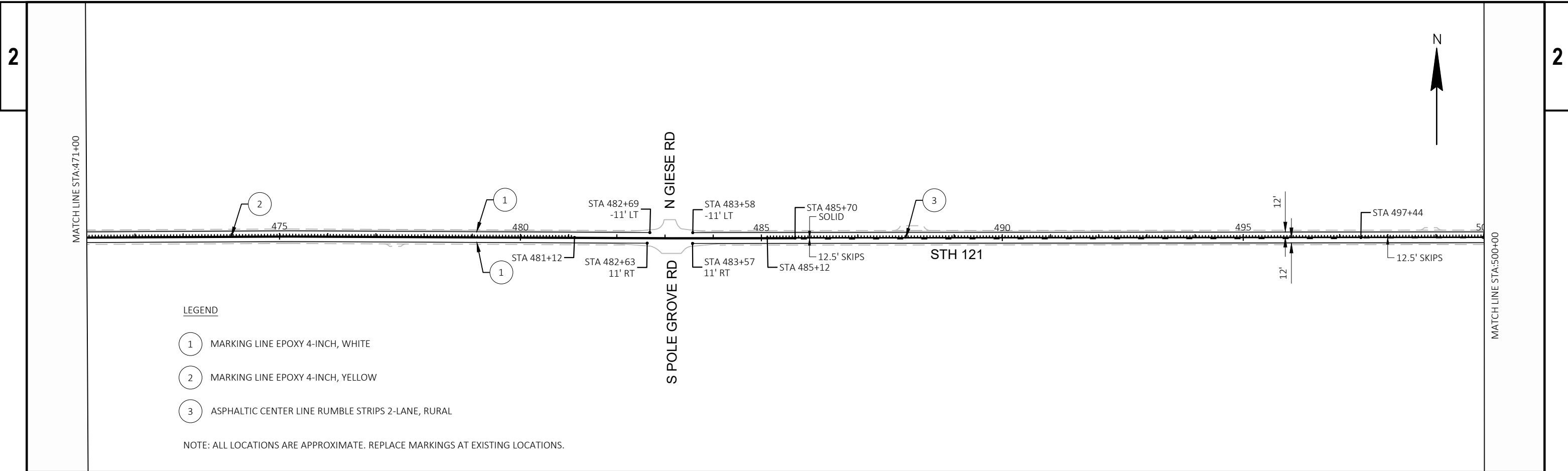
PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	PAVEMENT MARKING	SHEET	E
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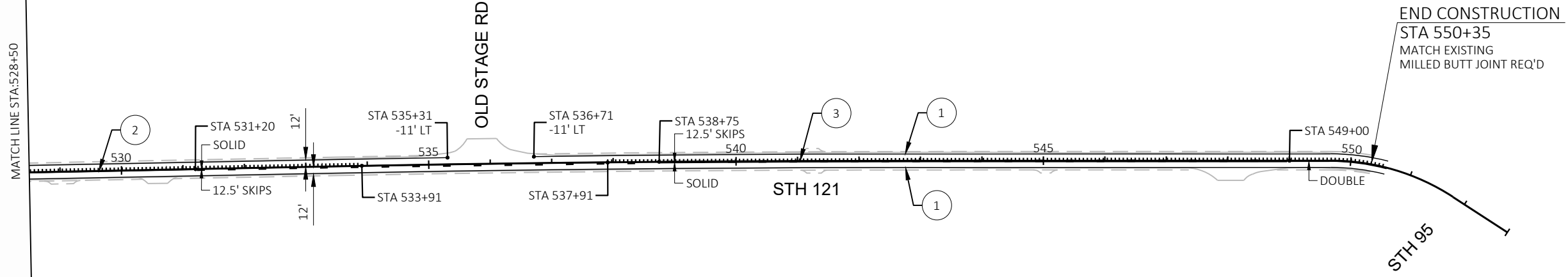
- LEGEND**
- 1 MARKING LINE EPOXY 4-INCH, WHITE
 - 2 MARKING LINE EPOXY 4-INCH, YELLOW
 - 3 ASPHALTIC CENTER LINE RUMBLE STRIPS 2-LANE, RURAL

NOTE: ALL LOCATIONS ARE APPROXIMATE. REPLACE MARKINGS AT EXISTING LOCATIONS.

PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	PAVEMENT MARKING	SHEET	E
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PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	PAVEMENT MARKING	SHEET	E
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LEGEND

- ① MARKING LINE EPOXY 4-INCH, WHITE
- ② MARKING LINE EPOXY 4-INCH, YELLOW
- ③ ASPHALTIC CENTER LINE RUMBLE STRIPS 2-LANE, RURAL

NOTE: ALL LOCATIONS ARE APPROXIMATE. REPLACE MARKINGS AT EXISTING LOCATIONS.



BEGIN CONSTRUCTION
 STA 183+38 Advance Warning Required, see S.D.D. "Traffic Control, Advance Warning Signs 45 M.P.H. or Greater, Two Way Undivided Road Open to Traffic"
 FIXED MESSAGE SIGN REQUIRED PRIOR TO START OF WORK. SEE DETAIL FOR SIGN PLATE G20-57.

END CONSTRUCTION
 STA 550+35 Advance Warning Required, see S.D.D. "Traffic Control, Advance Warning Signs 45 M.P.H. or Greater, Two Way Undivided Road Open to Traffic"
 FIXED MESSAGE SIGN REQUIRED PRIOR TO START OF WORK. SEE DETAIL FOR SIGN PLATE G20-57.

MAINTENANCE OF TRAFFIC

- STH 121
- CLOSED AND DETOURED FOR CULVERT PIPE REPLACEMENTS AS NECESSARY AS NOTED IN THE CONTRACT SPECIAL PROVISIONS.
 SEE S.D.D. "Barricades and Signs for Sideroad Closures"
 SEE S.D.D. "Barricades and Sign for Various Closures"
 - REMAIN OPEN AT ALL TIMES EXCEPT SINGLE LANE CLOSURES WITH FLAGGING OPERATIONS AS ALLOWED BY SPECIAL PROVISIONS AND SHOULDER CLOSURES TO COMPLETE CULVERT PIPE REPAIR.
 SEE S.D.D. "Traffic Control for Lane Closure with Flagging Operation"
 SEE S.D.D. "Traffic Control, Work on Shoulder or Parking Lane, Undivided Roadway"
 SEE S.D.D. "Traffic Control, Signing on Roadways with Milled Surfaces"

LOCAL ROADS

- REMAIN OPEN AT ALL TIMES EXCEPT SINGLE LANE CLOSURES WITH FLAGGING OPERATIONS AS ALLOWED BY SPECIAL PROVISIONS.
 SEE S.D.D. "Traffic Control, Work on Shoulder or Parking Lane, Undivided Roadway"

DRIVEWAYS

- DRIVEWAY ACCESS TO REMAIN OPEN EXCEPT SHORT TERM (<2 HRS) CLOSURES ALLOWED TO COMPLETE WORK THRU THE DRIVEWAY. COORDINATE CLOSURE WITH PROPERTY OWNER.

CONSTRUCTION TO BE COMPLETED

- CULVERT PIPE AND APRON ENDWALL REPLACEMENTS
- GUARDRAIL IMPROVEMENTS
- STRUCTURE REPAIR
- MILL AND OVERLAY STH 121
- PAVEMENT MARKING
 - MILLED SURFACE - TEMPORARY MARKING LINE PAINT 4-INCH (€ & EDGE LINES)
 - LEVELING SURFACE - TEMPORARY MARKING LINE PAINT 4-INCH (€ & EDGE LINES)
 - SURFACE LAYER - EDGE LINES - PAVEMENT MARKING EPOXY 4-INCH
 - € - TEMPORARY MARKING LINE EPOXY 4-INCH PRIOR TO RUMBLE INSTALLATION
 - AFTER RUMBLE APPLY FINAL MARKING LINE EPOXY LINE 4-INCH

TRAFFIC CONTROL GENERAL NOTES:

- ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. REMOVING/REPLACING OR COVERING/UNCOVERING SIGNS WILL BE INCIDENTAL TO OTHER TRAFFIC CONTROL ITEMS.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS REFLECTIVE ORANGE.
- CONSIDER GEOMETRICS WHEN LOCATING SIGNS, ARROW BOARDS, AND SIGN MESSAGE BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE SIGNS, ARROW BOARDS, AND SIGN MESSAGE BOARDS.
- IF SIGNS ARE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS WITH A MINIMUM 5' MOUNTING HEIGHT.
- ALL TRAFFIC CONTROL SIGNING SHALL CONFORM TO: PART IV OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, THE WISCONSIN SUPPLEMENT TO THE MUTCD, TRAFFIC ENGINEERING, OPERATIONS AND SAFETY MANUAL, AND OTHER CONTRACT DOCUMENTS.
- THE TURNING OF TRAFFIC CONTROL DEVICES WHEN NOT IN USE TO OBSCURE THE MESSAGE WILL NOT BE ALLOWED.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- SEE S.D.D. "Channelizing Devices" FOR DEVICES USED DURING THE MAINTENANCE OF TRAFFIC.

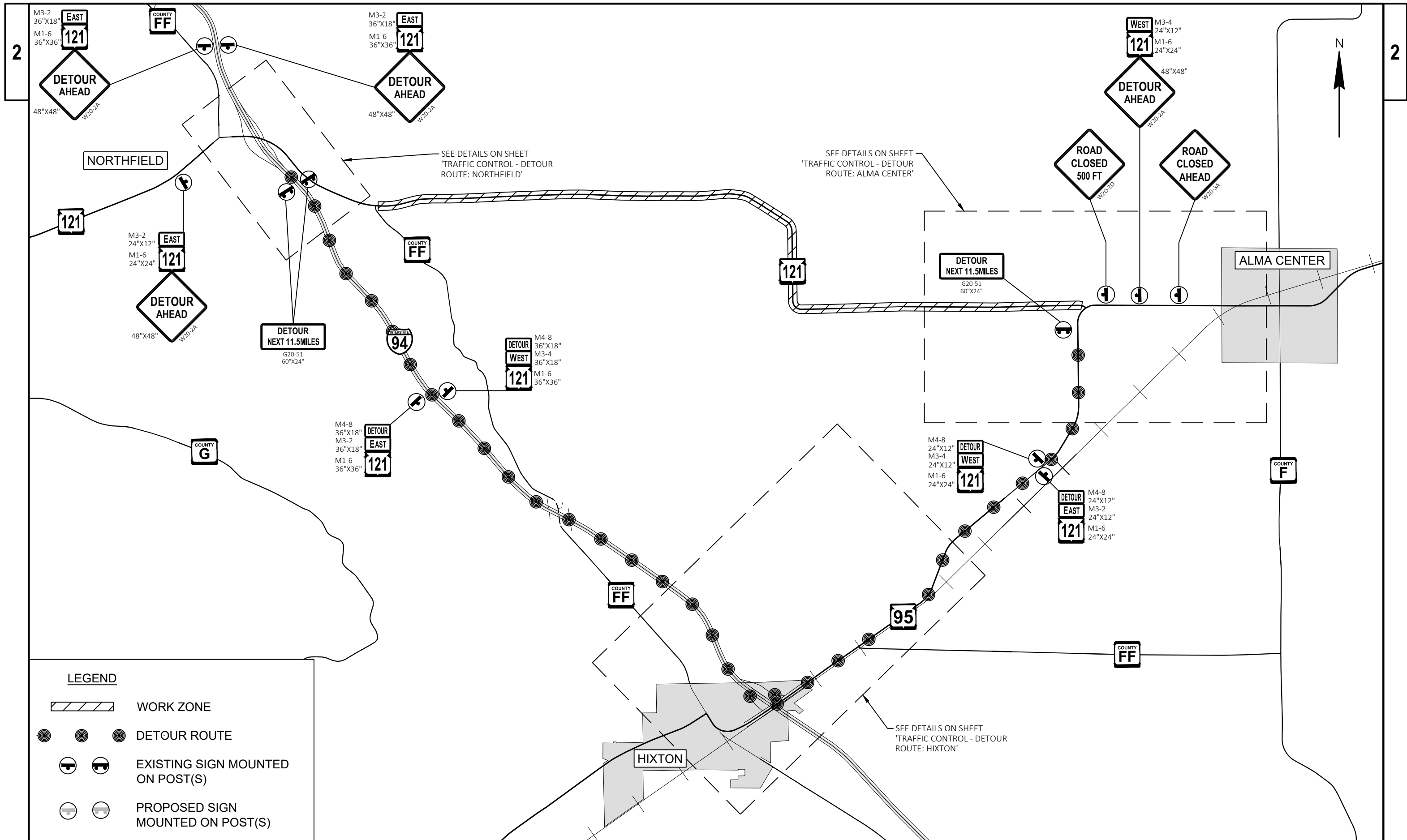
PCMS MESSAGE PLAN

EVENT/LOCATION	PANEL 1	PANEL 2
STH 121 DETOUR	STH 121 TO CLOSE (DATE)	STH 121 TO BE DETOURED

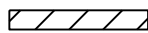



NOTE: PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE PLACED AT THE FOLLOWING LOCATIONS:

- STH 121 WB AT THE STH 95 INTERSECTION
- STH 121 EB AT THE CTH FF INTERSECTION

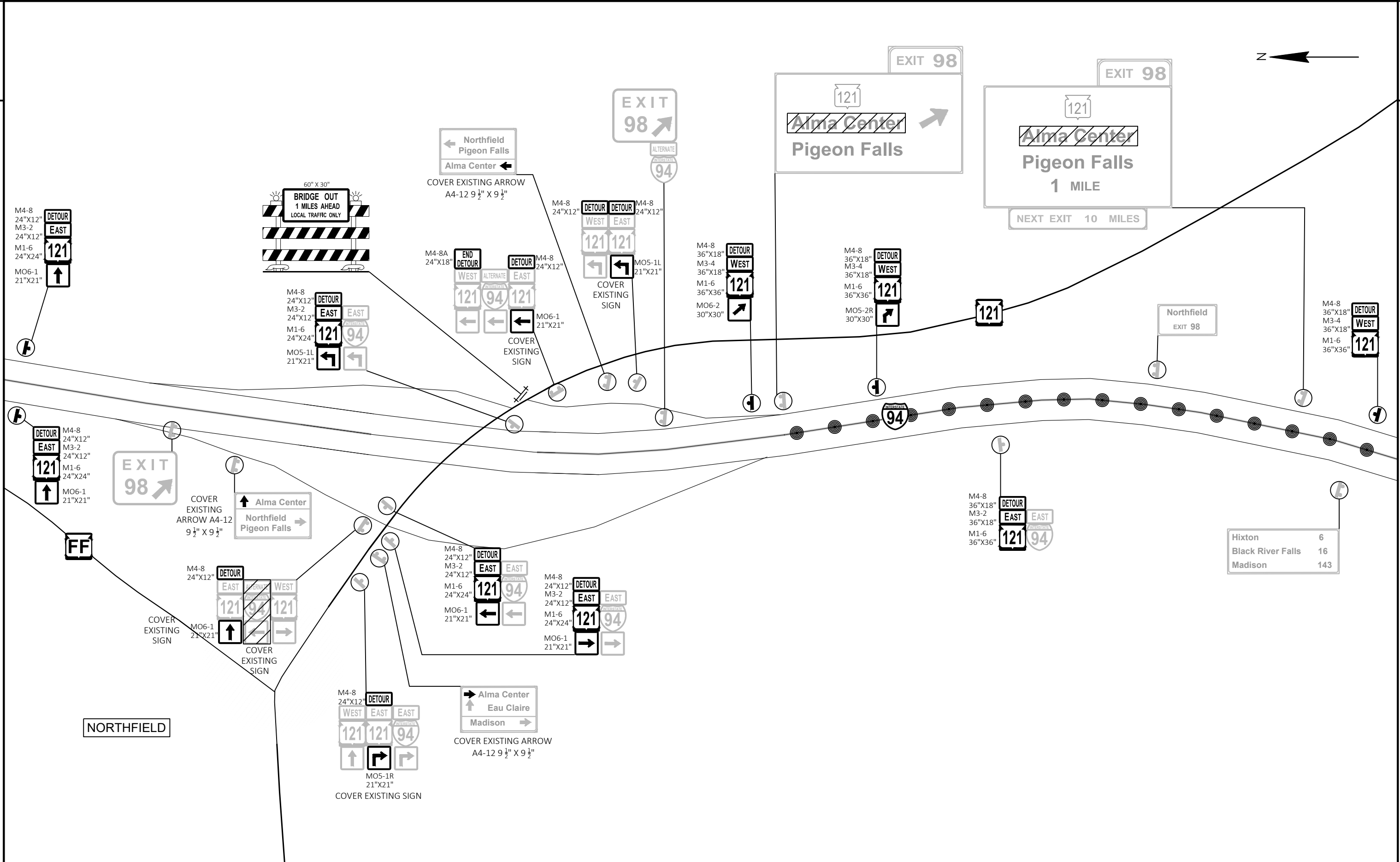
PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE IN PLACE FOR 7 DAYS PRIOR TO, AND DURING, EACH STH 121 DETOUR.



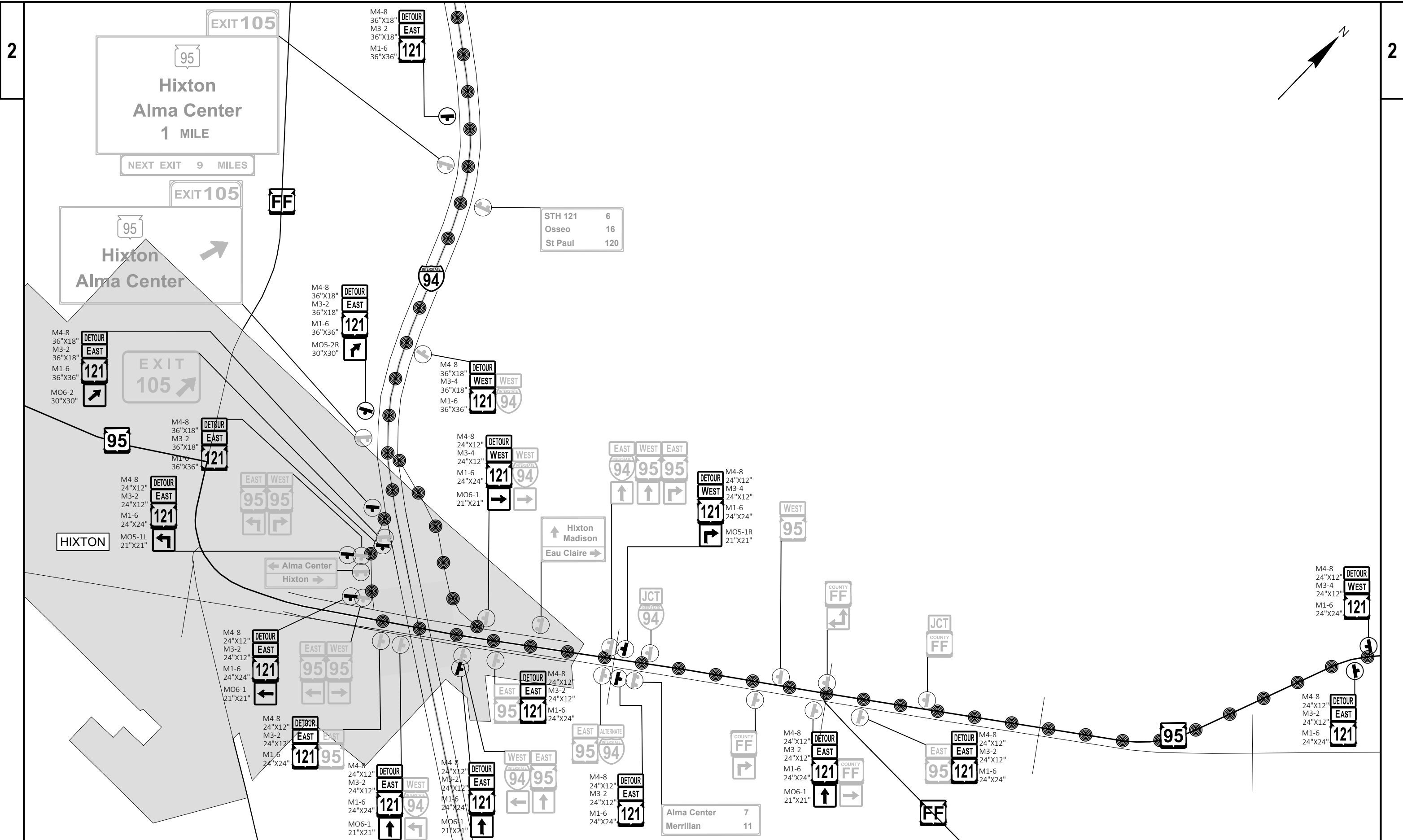
LEGEND

	WORK ZONE
	DETOUR ROUTE
	EXISTING SIGN MOUNTED ON POST(S)
	PROPOSED SIGN MOUNTED ON POST(S)

PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	TRAFFIC CONTROL - DETOUR ROUTE OVERVIEW	SHEET E
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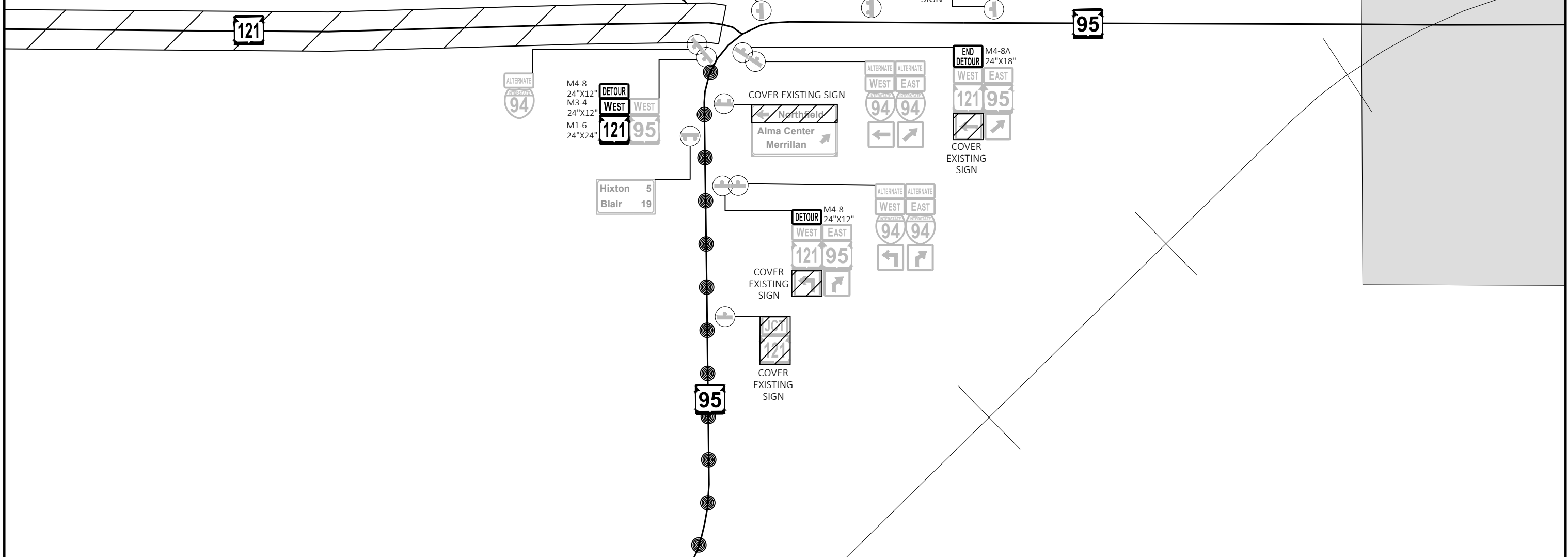
PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON TRAFFIC CONTROL - DETOUR ROUTE: NORTHFIELD SHEET E



PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON TRAFFIC CONTROL - DETOUR ROUTE: HIXTON SHEET E



SEE S.D.D. 'BARRICADES AND SIGN FOR VARIOUS CLOSURES' FOR ROAD CLOSURE BARRICADE DETAIL.



PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	TRAFFIC CONTROL - DETOUR ROUTE: ALMA CENTER	SHEET	E
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Estimate Of Quantities

7505-00-76

Line	Item	Item Description	Unit	Total	Qty
0002	201.0105	Clearing	STA	6.000	6.000
0004	201.0205	Grubbing	STA	6.000	6.000
0006	203.0100	Removing Small Pipe Culverts	EACH	11.000	11.000
0008	203.0220	Removing Structure (structure) 01. (239+37)	EACH	1.000	1.000
0010	203.0220	Removing Structure (structure) 02. (239+47)	EACH	1.000	1.000
0012	204.0110	Removing Asphaltic Surface	SY	940.000	940.000
0014	204.0115	Removing Asphaltic Surface Butt Joints	SY	1,040.000	1,040.000
0016	204.0120	Removing Asphaltic Surface Milling	SY	103,000.000	103,000.000
0018	204.0165	Removing Guardrail	LF	736.000	736.000
0020	204.9060.S	Removing (item description) 01. Apron Endwall	EACH	5.000	5.000
0022	204.9060.S	Removing (item description) 02. Cattle Pass	EACH	1.000	1.000
0024	204.9090.S	Removing (item description) 01. Retaining Wall	LF	20.000	20.000
0026	205.0100	Excavation Common	CY	4,950.000	4,950.000
0028	213.0100	Finishing Roadway (project) 01. 7505-00-76	EACH	1.000	1.000
0030	305.0110	Base Aggregate Dense 3/4-Inch	TON	1,940.000	1,940.000
0032	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	2,210.000	2,210.000
0034	305.0500	Shaping Shoulders	STA	729.000	729.000
0036	455.0605	Tack Coat	GAL	14,390.000	14,390.000
0038	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	1.000	1.000
0040	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	3.000	3.000
0042	460.2005	Incentive Density PWL HMA Pavement	DOL	13,600.000	13,600.000
0044	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	36,700.000	36,700.000
0046	460.2010	Incentive Air Voids HMA Pavement	DOL	13,600.000	13,600.000
0048	460.6645	HMA Pavement 5 MT 58-34 V	TON	14,410.000	14,410.000
0050	460.9000.S	Material Transfer Vehicle (project) 01. 7505-00-76	EACH	1.000	1.000
0052	465.0105	Asphaltic Surface	TON	600.000	600.000
0054	465.0110	Asphaltic Surface Patching	TON	150.000	150.000
0056	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	74.000	74.000
0058	465.0310	Asphaltic Curb	LF	288.000	288.000
0060	465.0315	Asphaltic Flumes	SY	10.000	10.000
0062	465.0475	Asphalt Centerline Rumble Strips 2-Lane Rural	LF	32,615.000	32,615.000
0064	509.9020.S	Epoxy Crack Sealing	LF	16.000	16.000
0066	520.3424	Culvert Pipe Class III-A Non-metal 24-Inch	LF	14.000	14.000
0068	520.3436	Culvert Pipe Class III-A Non-metal 36-Inch	LF	4.000	4.000
0070	520.8000	Concrete Collars for Pipe	EACH	4.000	4.000
0072	520.8700	Cleaning Culvert Pipes	EACH	3.000	3.000
0074	520.9700.S	Culvert Pipe Liners (size) 01. 24-Inch	LF	225.000	225.000
0076	520.9700.S	Culvert Pipe Liners (size) 02. 30-Inch	LF	50.000	50.000
0078	520.9750.S	Cleaning Culvert Pipes for Liner Verification	EACH	5.000	5.000
0080	522.0124	Culvert Pipe Reinforced Concrete Class III 24-Inch	LF	112.000	112.000
0082	522.0130	Culvert Pipe Reinforced Concrete Class III 30-Inch	LF	94.000	94.000
0084	522.0160	Culvert Pipe Reinforced Concrete Class III 60-Inch	LF	94.000	94.000
0086	522.0424	Culvert Pipe Reinforced Concrete Class IV 24-Inch	LF	140.000	140.000
0088	522.0430	Culvert Pipe Reinforced Concrete Class IV 30-Inch	LF	40.000	40.000
0090	522.1024	Apron Endwalls for Culvert Pipe Reinforced Concrete 24-Inch	EACH	9.000	9.000
0092	522.1030	Apron Endwalls for Culvert Pipe Reinforced Concrete 30-Inch	EACH	4.000	4.000
0094	522.1060	Apron Endwalls for Culvert Pipe Reinforced Concrete 60-Inch	EACH	4.000	4.000
0096	522.1500	Pipe Cattle Pass Reinforced Concrete	LF	64.000	64.000
0098	525.0324	Apron Endwalls for Culvert Pipe Aluminum 24-Inch	EACH	8.000	8.000

Estimate Of Quantities

7505-00-76

Line	Item	Item Description	Unit	Total	Qty
0100	525.0330	Apron Endwalls for Culvert Pipe Aluminum 30-Inch	EACH	2.000	2.000
0102	525.0336	Apron Endwalls for Culvert Pipe Aluminum 36-Inch	EACH	2.000	2.000
0104	606.0200	Riprap Medium	CY	208.000	208.000
0106	611.0642	Inlet Covers Type MS	EACH	1.000	1.000
0108	611.3902	Inlets Median 2 Grate	EACH	1.000	1.000
0110	614.0010	Barrier System Grading Shaping Finishing	EACH	4.000	4.000
0112	614.2300	MGS Guardrail 3	LF	112.000	112.000
0114	614.2330	MGS Guardrail 3 K	LF	341.000	341.000
0116	614.2340	MGS Guardrail 3 L	LF	75.000	75.000
0118	614.2610	MGS Guardrail Terminal EAT	EACH	8.000	8.000
0120	618.0100	Maintenance And Repair of Haul Roads (project) 01. 7505-00-76	EACH	1.000	1.000
0122	619.1000	Mobilization	EACH	1.000	1.000
0124	624.0100	Water	MGAL	42.000	42.000
0126	625.0105	Topsoil	CY	120.000	120.000
0128	625.0500	Salvaged Topsoil	SY	3,280.000	3,280.000
0130	628.1504	Silt Fence	LF	3,800.000	3,800.000
0132	628.1520	Silt Fence Maintenance	LF	11,400.000	11,400.000
0134	628.1905	Mobilizations Erosion Control	EACH	14.000	14.000
0136	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0138	628.2002	Erosion Mat Class I Type A	SY	1,320.000	1,320.000
0140	628.2008	Erosion Mat Urban Class I Type B	SY	4,820.000	4,820.000
0142	628.2033	Erosion Mat Class III Type B	SY	1,320.000	1,320.000
0144	628.7005	Inlet Protection Type A	EACH	2.000	2.000
0146	628.7504	Temporary Ditch Checks	LF	30.000	30.000
0148	628.7555	Culvert Pipe Checks	EACH	118.000	118.000
0150	629.0210	Fertilizer Type B	CWT	1.000	1.000
0152	630.0130	Seeding Mixture No. 30	LB	59.000	59.000
0154	630.0500	Seed Water	MGAL	280.000	280.000
0156	633.5200	Markers Culvert End	EACH	29.000	29.000
0158	638.2102	Moving Signs Type II	EACH	10.000	10.000
0160	642.5001	Field Office Type B	EACH	1.000	1.000
0162	643.0300	Traffic Control Drums	DAY	22,680.000	22,680.000
0164	643.0420	Traffic Control Barricades Type III	DAY	805.000	805.000
0166	643.0705	Traffic Control Warning Lights Type A	DAY	1,330.000	1,330.000
0168	643.0900	Traffic Control Signs	DAY	24,900.000	24,900.000
0170	643.0910	Traffic Control Covering Signs Type I	EACH	70.000	70.000
0172	643.0920	Traffic Control Covering Signs Type II	EACH	245.000	245.000
0174	643.1050	Traffic Control Signs PCMS	DAY	100.000	100.000
0176	643.5000	Traffic Control	EACH	1.000	1.000
0178	645.0120	Geotextile Type HR	SY	420.000	420.000
0180	646.1020	Marking Line Epoxy 4-Inch	LF	124,500.000	124,500.000
0182	649.0105	Temporary Marking Line Paint 4-Inch	LF	249,000.000	249,000.000
0184	649.0120	Temporary Marking Line Epoxy 4-Inch	LF	52,000.000	52,000.000
0186	650.4500	Construction Staking Subgrade	LF	1,740.000	1,740.000
0188	650.5000	Construction Staking Base	LF	1,740.000	1,740.000
0190	650.6000	Construction Staking Pipe Culverts	EACH	11.000	11.000
0192	650.8000	Construction Staking Resurfacing Reference	LF	37,600.000	37,600.000
0194	650.9910	Construction Staking Supplemental Control (project) 01. 7505-00-76	LS	1.000	1.000
0196	650.9920	Construction Staking Slope Stakes	LF	1,740.000	1,740.000

Estimate Of Quantities

7505-00-76

Line	Item	Item Description	Unit	Total	Qty
0198	690.0150	Sawing Asphalt	LF	810.000	810.000
0200	740.0440	Incentive IRI Ride	DOL	13,888.000	13,888.000
0202	999.2000.S	Installing and Maintaining Bird Deterrent System (station) 01. 407+98.50	EACH	1.000	1.000
0204	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	1,200.000	1,200.000
0206	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	600.000	600.000
0208	SPV.0060	Special 01. Box Culvert Crack Repair C-27-1003	EACH	1.000	1.000
0210	SPV.0090	Special 01. Cleaning Ditch	LF	413.000	413.000

3

CLEARING AND GRUBBNG

201.0105 201.0205

PROJECT CATEGORY	LOCATION	STATION - STATION	CLEARING (STA)	GRUBBING (STA)
7505-00-76				
0010	CTH FF - CHURCH RD	183+38 - 199+86	2	2
	CHURCH RD - DEGROOT RD	199+86 - 242+72	1	1
	DEGROOT RD - BURNETTE RD	242+72 - 307+74	-	-
	BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	-	-
	HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	-	-
	TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	1	1
	SE BURNETTE RD - BRANCH RD	417+69 - 428+06	1	1
	BRANCH RD - N GIESE RD	428+06 - 483+15	1	1
	N GIESE RD - OLD STAGE RD	483+15 - 535+83	-	-
	OLD STAGE RD - STH 95	535+83 - 550+35	-	-
PROJECT 7505-00-76 TOTAL			6	6

3

REMOVING SMALL PIPE CULVERT

203.0100

PROJECT CATEGORY	LOCATION	STATION - STATION	(EACH)	REMARKS
7505-00-76				
0010	CTH FF - CHURCH RD	190+01	1	30" CMCP
	CHURCH RD - DEGROOT RD	239+37	1	72"H x 48"W CMCP
	CHURCH RD - DEGROOT RD	239+47	1	72"H x 48"W CMCP
	DEGROOT RD - BURNETTE RD	248+49	1	24" CMCP
	DEGROOT RD - BURNETTE RD	294+78	1	24" CMCP
	BURNETTE RD - HIXTON LEVIS RD	314+08	1	30" CMCP
	BURNETTE RD - HIXTON LEVIS RD	347+13	1	2FT OF 24" CMCP
	SE BURNETTE RS - BRANCH RD	422+14	1	30" CMCP
	BRANCH RD - N GIESE RD	454+15	1	24" CMCP
	BRANCH RD - N GIESE RD	468+83	1	24" CMCP
	OLD STAGE RD - STH 95	547+32	1	24" CMCP
PROJECT 7505-00-76 TOTAL			11	

REMOVING ASPHALTIC SURFACE

204.0110 204.0115 204.0120

PROJECT CATEGORY	LOCATION	STATION - STATION	(SY)	BUTT JOINTS (SY)	MILLING (SY)
7505-00-76					
0010	CTH FF - CHURCH RD	183+38 - 199+86	-	97	4,597
	CHURCH RD - DEGROOT RD	199+86 - 242+72	-	53	11,800
	DEGROOT RD - BURNETTE RD	242+72 - 307+74	-	59	17,587
	BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	-	28	14,601
	HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	940	97	3,368
	TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	-	257	12,318
	SE BURNETTE RD - BRANCH RD	417+69 - 428+06	-	105	3,240
	BRANCH RD - N GIESE RD	428+06 - 483+15	-	100	16,529
	N GIESE RD - OLD STAGE RD	483+15 - 535+83	-	46	14,793
	OLD STAGE RD - STH 95	535+83 - 550+35	-	199	4,166
PROJECT 7505-00-76 TOTAL			940	1,040	103,000

REMOVING GUARDRAIL

204.0165

PROJECT CATEGORY	LOCATION	STATION - STATION	OFFSET	(LF)
7505-00-76				
0010	TOWNHOUSE RD - SE BURNETTE RD	380+59 - 382+87	LT	228
	TOWNHOUSE RD - SE BURNETTE RD	380+63 - 382+90	RT	227
	TOWNHOUSE RD - SE BURNETTE RD	407+29 - 408+68	LT	140
	TOWNHOUSE RD - SE BURNETTE RD	407+29 - 408+70	RT	141
PROJECT 7505-00-76 TOTAL				736

REMOVING STRUCTURE

203.0220.01 203.0220.02

PROJECT	CATEGORY	LOCATION	STATION - STATION	(239+37) (EACH)	(239+47) (EACH)
7505-00-76					
0010		CHURCH RD - DEGROOT RD	239+37	1	-
		CHURCH RD - DEGROOT RD	239+47	-	1
		BRANCH RD - N GIESE RD	465+12	-	-
PROJECT 7505-00-76 TOTAL				1	1

REMOVING APRON ENDWALL

204.9060.S.01

PROJECT	CATEGORY	LOCATION	STATION	(EACH)	REMARKS
7505-00-76					
0010		STH 121	190+01	1	30-INCH
		STH 121	206+60	2	36-INCH
		STH 121	342+92	1	24-INCH
		STH 121	347+13	1	24-INCH
PROJECT 7505-00-76 TOTAL				5	

EARTHWORK

205.0100 625.0105

PROJECT	CATEGORY	LOCATION	STATION - STATION	EXCAVATION COMMON (CY)	TOPSOIL (CY)	NOTES
7505-00-76						
0010		STH 121	190+01	781	-	CP REPLACEMENT
		STH 121	239+37	818	-	CP REPLACEMENT
		STH 121	248+49	331	-	CP REPLACEMENT
		STH 121	262+00 - 263+00	-	5	SLOPE REPAIR
		STH 121	294+78	566	-	CP REPLACEMENT
		STH 121	314+08	342	-	CP REPLACEMENT
		STH 121	324+85	-	5	SLOPE REPAIR
		STH 121	354+38	-	30	SLOPE REPAIR
		STH 121	451+12	-	26	SLOPE REPAIR
		STH 121	454+15	602	3	SLOPE REPAIR/CP REPLACEMENT
		STH 121	465+12	714	-	CP REPLACEMENT
		STH 121	468+79	-	20	SLOPE REPAIR
		STH 121	468+83	452	-	CP REPLACEMENT
		STH 121	547+32	344	-	CP REPLACEMENT
		UNDISTRIBUTED		-	31	
PROJECT 7505-00-76 TOTAL				4,950	120	

REMOVING CATTLE PASS

204.9060.S.02

PROJECT	CATEGORY	LOCATION	STATION	(EACH)
7505-00-76				
0010		STH 121	465+12	1
PROJECT 7505-00-76 TOTAL				1

REMOVING RETAINING WALL

204.9090.S.01

PROJECT	CATEGORY	LOCATION	STATION - STATION	(LF)
7505-00-76				
0010		BRANCH RD - N GIESE RD	468+67	20
PROJECT 7505-00-76 TOTAL				20

BASE AGGREGATE DENSE

305.0110 305.0120 624.0100

PROJECT	CATEGORY	LOCATION	STATION - STATION	3/4-INCH (TON)	1 1/4-INCH (TON)	WATER (MGAL)	NOTES
7505-00-76							
0010		CTH FF - CHURCH RD	183+38 - 199+86	40	252	3	-
		CHURCH RD - DEGROOT RD	199+86 - 242+72	42	270	3	-
		DEGROOT RD - BURNETTE RD	242+72 - 307+74	74	450	5	-
		BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	36	218	3	-
		HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	-	-	-	-
		TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	171	-	2	-
		SE BURNETTE RD - BRANCH RD	417+69 - 428+06	-	-	-	-
		BRANCH RD - N GIESE RD	428+06 - 483+15	112	785	9	-
		N GIESE RD - OLD STAGE RD	483+15 - 535+83	-	-	-	-
		OLD STAGE RD - STH 95	535+83 - 550+35	37	235	3	-
		UNDISTRIBUTED		1,428	-	14	TO BE USED IN SHAPING SHOULDER AREAS WITH INSUFFICIENT BASE
PROJECT 7505-00-76 TOTAL				1,940	2,210	42	

PWL Mixture Use Table								
Location	Station	Mixture Use	Underlying Surface	Bid Item	Tons	Thickness	Quality Management Program to be used for:	
							Mixture Acceptance	Density Acceptance
12 FT DRIVING LANES, LT AND RT	183+38 - 189+60	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	233	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
CULVERT REPLACEMENT 12FT DRIVING LANES, LT AND RT	189+60 - 190+73	UPPER AND LOWER LAYERS	ASPHALTIC SURFACE	HMA PAVEMENT 5 MT 58-34 V	43	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	190+73 - 238+81	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	1795	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
CULVERT REPLACEMENT 12FT DRIVING LANES, LT AND RT	238+81 - 240+02	UPPER AND LOWER LAYERS	ASPHALTIC SURFACE	HMA PAVEMENT 5 MT 58-34 V	46	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	240+02 - 247+96	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	297	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
CULVERT REPLACEMENT 12FT DRIVING LANES, LT AND RT	247+96 - 249+01	UPPER AND LOWER LAYERS	ASPHALTIC SURFACE	HMA PAVEMENT 5 MT 58-34 V	40	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	249+01 - 294+26	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	1690	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
CULVERT REPLACEMENT 12FT DRIVING LANES, LT AND RT	294+26 - 295+30	UPPER AND LOWER LAYERS	ASPHALTIC SURFACE	HMA PAVEMENT 5 MT 58-34 V	39	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	295+30 - 313+55	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	682	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
CULVERT REPLACEMENT 12FT DRIVING LANES, LT AND RT	313+55 - 314+59	UPPER AND LOWER LAYERS	ASPHALTIC SURFACE	HMA PAVEMENT 5 MT 58-34 V	39	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	314+59 - 367+17	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	1963	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	370+17 - 453+61	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	3116	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
CULVERT REPLACEMENT 12FT DRIVING LANES, LT AND RT	453+61 - 454+65	UPPER AND LOWER LAYERS	ASPHALTIC SURFACE	HMA PAVEMENT 5 MT 58-34 V	39	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	454+65 - 464+56	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	370	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
CULVERT REPLACEMENT 12FT DRIVING LANES, LT AND RT	464+56 - 465+68	UPPER AND LOWER LAYERS	ASPHALTIC SURFACE	HMA PAVEMENT 5 MT 58-34 V	42	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	465+68 - 468+29	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	98	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
CULVERT REPLACEMENT 12FT DRIVING LANES, LT AND RT	468+29 - 469+34	UPPER AND LOWER LAYERS	ASPHALTIC SURFACE	HMA PAVEMENT 5 MT 58-34 V	40	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	469+34 - 546+80	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	2892	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
CULVERT REPLACEMENT 12FT DRIVING LANES, LT AND RT	546+80 - 547+84	UPPER AND LOWER LAYERS	ASPHALTIC SURFACE	HMA PAVEMENT 5 MT 58-34 V	39	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005
12 FT DRIVING LANES, LT AND RT	547+84 - 550+35	UPPER AND LOWER LAYERS	EXISTING MILLED SURFACE	HMA PAVEMENT 5 MT 58-34 V	94	2.5"	PWL Incentive Air Voids HMA Pavement 460.2010	Incentive Density PWL HMA Pavement 460.2005

3

SHAPING SHOULDERS

305.0500

PROJECT CATEGORY	LOCATION	STATION - STATION	(STA)	
			RT	LT
7505-00-76				
0010	CTH FF - CHURCH RD	183+38 - 199+86	15	15
	CHURCH RD - DEGROOT RD	199+86 - 242+72	42	42
	DEGROOT RD - BURNETTE RD	242+72 - 307+74	62	62
	BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	53	53
	HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	14	14
	TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	50	50
	SE BURNETTE RD - BRANCH RD	417+69 - 428+06	10	10
	BRANCH RD - N GIESE RD	428+06 - 483+15	52	52
	N GIESE RD - OLD STAGE RD	483+15 - 535+83	52	52
	OLD STAGE RD - STH 95	535+83 - 550+35	14	14
PROJECT 7505-00-76 TOTAL			364	365
			729	

HMA ITEMS

PROJECT CATEGORY	LOCATION	STATION - STATION	455.0605	460.6645	465.0105	465.0110	465.0120	465.0310	465.0315
			TACK COAT (GAL)	HMA PAVEMENT 5 MT 58-34 V (TON)	ASPHALTIC SURFACE (TON)	ASPHALTIC SURFACE PATCHING (TON)	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES (TON)	ASPHALTIC CURB (LF)	ASPHALTIC FLUMES (SY)
7505-00-76									
0010	CTH FF - CHURCH RD	183+38 - 199+86	641	643	11	-	7	-	-
	CHURCH RD - DEGROOT RD	199+86 - 242+72	1,649	1,651	11	-	5	-	-
	DEGROOT RD - BURNETTE RD	242+72 - 307+74	2,455	2,461	19	-	7	-	-
	BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	2,041	2,043	9	-	10	-	-
	HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	470	470	-	-	6	-	-
	TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	1,723	1,723	-	-	-	288	10
	SE BURNETTE RD - BRANCH RD	417+69 - 428+06	452	452	-	-	-	-	-
	BRANCH RD - N GIESE RD	428+06 - 483+15	2,307	2,313	33	-	8	-	-
	N GIESE RD - OLD STAGE RD	483+15 - 535+83	2,068	2,070	-	-	11	-	-
	OLD STAGE RD - STH 95	535+83 - 550+35	582	583	10	-	21	-	-
UNDISTRIBUTED			-	-	507	150	-	-	-
PROJECT 7505-00-76 TOTAL			14,390	14,410	600	150	74	288	10

3

MATERIAL TRANSFER VEHICLE

460.9000.S

PROJECT CATEGORY	LOCATION	(EACH)
7505-00-76		
0010	STH 121	1
PROJECT 7505-00-76 TOTAL		
		1

CULVERT PIPE LINER

520.9700.S.01 520.9700.S.02 520.9750.S

CLEANING CULVERT PIPES FOR LINEAR VERIFICATION (EACH)

PROJECT CATEGORY	LOCATION	STATION - STATION	(24-INCH) (LF)	(30-INCH) (LF)	(EACH)
7505-00-76					
0010	CTH FF - CHURCH RD	183+38 - 199+86	-	-	-
	CHURCH RD - DEGROOT RD	199+86 - 242+72	-	-	-
	DEGROOT RD - BURNETTE RD	242+72 - 307+74	116	-	2
	BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	-	-	-
	HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	-	-	-
	TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	-	-	-
	SE BURNETTE RD - BRANCH RD	417+69 - 428+06	109	50	3
	BRANCH RD - N GIESE RD	428+06 - 483+15	-	-	-
	N GIESE RD - OLD STAGE RD	483+15 - 535+83	-	-	-
	OLD STAGE RD - STH 95	535+83 - 550+35	-	-	-
PROJECT 7505-00-76 TOTAL			225	50	5

3

3

CULVERT PIPES

		520.3424						520.3436		520.8000		522.1500		525.0324		525.0330		525.0336		633.5200 *	
CATEGORY	ROADWAY	INLET			OUTLET			SKEW	SLOPE %	CULVERT PIPE CLASS III-A NON-METAL		CONCRETE COLLARS FOR PIPE (EACH)	PIPE CATTLE PASS REINFORCED CONCRETE (LF)	APRON ENDWALLS FOR CULVERT PIPE ALUMINUM			MARKERS CULVERT END (EACH)				
		STATION	OFFSET	** ELEVATION	STATION	OFFSET	** ELEVATION			(24-INCH) (LF)	(36-INCH) (LF)			24-INCH (EACH)	30-INCH (EACH)	36-INCH (EACH)					
STH 121	206+60	28.4' RT		1,016.34	206+60	40.4' LT		-	-	4	1	-	-	-	-	2			2		
STH 121	305+74	19.7' RT		1,047.35	305+75	28.2' LT		-	-	-	-	-	-	2	-	-			2		
STH 121	-	-		-	342+92	24.5' RT		-	-	-	-	-	-	1	-	-			1		
STH 121	-	-		-	347+13	41.9' RT		-	-	-	-	-	-	1	-	-			1		
STH 121	354+38	22.7' LT		1,028.57	354+42	25.8' RT		-	-	5	1	-	-	2	-	-			2		
STH 121	422+14	26.4' LT		976.20	422+14	30.7' LT		-	-	5	1	-	-	1	-	-			1		
STH 121	427+36	22.0' LT		990.75	-	-		-	-	-	-	-	-	-	1	-			1		
STH 121	432+14	22.1' LT		1,002.26	-	-		-	-	-	-	-	-	-	1	-			1		
STH 121	451+12	21.9' RT		1,047.00	451+12	25.9' RT		-	-	4	1	-	-	1	-	-			1		
STH 121	465+12	28.2' LT		1,001.86	465+11	34.5' RT	1° LHF	4.03	-	-	-	64	-	-	-	-			-		
CATEGORY 0010 SUBTOTAL										14	4	4	64	8	2	2			12		
PROJECT 7505-00-76 TOTAL										14	4	4	64	8	2	2			12		

* ADDITIONAL QUNATITY FOUND IN "CULVERT PIPES CONT."
 ** MATCH TO EXISTING PIPE ELEVATION AND SLOPE

CULVERT PIPES CONT.

		522.0124						522.0130		522.0160		522.0424		522.0430		522.1024		522.1030		522.1060		611.0642		611.3902		633.5200 *	
CATEGORY	ROADWAY	INLET			OUTLET			SKEW	SLOPE %	CULVERT PIPE REINFORCED CONCRETE CLASS III			CULVERT PIPE REINFORCED CONCRETE CLASS IV		APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE			INLET COVERS (EACH)	INLETS MEDIAN 2 GRATE (EACH)	MARKERS CULVERT END (EACH)	NOTES						
		STATION	OFFSET	** ELEVATION	STATION	OFFSET	** ELEVATION			24-INCH (LF)	30-INCH (LF)	60-INCH (LF)	24-INCH (LF)	30-INCH (LF)	24-INCH (EACH)	30-INCH (EACH)	60-INCH (EACH)					TYPE MS					
0010	STH 121	190+33	44.0' RT	1038.78	190+01	43.1' LT	20° RHF	2.57	-	94	-	-	-	-	2	-	-	-	-	-	-	-	-	-	-	2	
	STH 121	236+36	23.0' RT	1,001.20	239+37	24.4' LT	1° LHF	0.66	-	-	47	-	-	-	-	2	-	-	-	-	-	-	-	-	-	2	
	STH 121	239+46	22.9' RT	1,001.17	239+47	24.6' LT	1° LHF	0.66	-	-	47	-	-	-	-	2	-	-	-	-	-	-	-	-	-	2	
	STH 121	248+49	15.9' LT	1,017.10	248+48	16.4' RT	3° LHF	1.25	-	-	-	32	-	-	2	-	-	-	-	-	-	-	-	-	-	2	
	STH 121	294+78	19.4' RT	1,053.92	294+80	41.8' LT	3° LHF	6.10	61	-	-	-	-	-	2	-	-	-	-	-	-	-	-	-	-	2	
	STH 121	314+08	24.4' LT	1,035.42	314+06	15.3' RT	3° LHF	3.05	-	-	-	-	40	-	2	-	-	-	-	-	-	-	-	-	-	2	
	STH 121	454+12	18.7' RT	1,038.36	454+15	32.2' LT	3° LHF	3.69	51	-	-	-	-	-	2	-	-	-	-	-	-	-	-	-	-	2	
	STH 121	468+83	27.7' LT	1,005.10	468+79	33.3' RT	4° LHF	2.21	-	-	-	61	-	-	1	-	-	1	-	1	-	-	-	-	1	MATCH INLET TO SURFACE AT SLOPE 4.0:1	
	STH 121	547+32	22.9' RT	1,036.99	547+32	22.63' LT	1° RHF	0.74	-	-	-	47	-	-	2	-	-	-	-	-	-	-	-	-	-	2	
CATEGORY 0010 SUBTOTAL										112	94	94	140	40	9	4	4	1	1			17					
PROJECT 7505-00-76 TOTAL										112	94	94	140	40	9	4	4	1	1			17					

* ADDITIONAL QUNATITY FOUND IN "CULVERT PIPES"
 ** MATCH TO EXISTING PIPE ELEVATION AND SLOPE

CLEANING CULVERTS

PROJECT	CATEGORY	LOCATION	STATION - STATION	CLEANING CULVERT PIPES (EACH)	520.8700
7505-00-76					
0010		CHURCH RD - DEGROOT RD	206+60	1	
		SE BURNETTE RD - BRANCH RD	422+14	1	
		BRANCH RD - N GIESE RD	432+14	1	
PROJECT 7505-00-76 TOTAL				3	

BARRIER SYSTEM GRADING SHAPING FINISHING

PROJECT	CATEGORY	LOCATION	STATION - STATION	OFFSET	FOR INFORMATION ONLY, ITEMS PAID UNDER ITEM 614.0010					614.0010	NOTES
					EXCAVATION COMMON (CY)	BORROW (CY)	SALVAGED TOPSOIL (SY)	FERTILIZER TYPE B (CWT)	SEEDING MIXTURE NO. 30 (LB)		
7505-00-76											
0010		STH 121	380+34 - 384+02	LT	6	30	168	0.03	3	1	C-27-1002
		STH 121	380+34 - 384+02	RT	6	30	153	0.03	3	1	C-27-1002
		STH 121	406+00 - 409+95	LT	6	30	258	0.05	5	1	C-27-1003
		STH 121	406+00 - 409+95	RT	6	30	203	0.04	4	1	C-27-1003
PROJECT 7505-00-76 TOTAL										4	

MGS GUARDRAIL

PROJECT	CATEGORY	LOCATION	STATION - STATION	OFFSET	614.2300	614.2330	614.2340	614.2610
					3 (LF)	3 K (LF)	3 L (LF)	TERMINAL EAT (EACH)
7505-00-76								
0010	C-27-1002		380+34 - 384+02	LT	-	163	25	2
	C-27-1002		380+34 - 384+02	RT	-	75	25	2
	C-27-1003		406+00 - 409+95	LT	-	104	12.5	2
	C-27-1003		406+00 - 409+95	RT	112	-	12.5	2
PROJECT 7505-00-76 TOTAL					112	341	75	8

3

3

LANDSCAPING

		625.0500 629.0210 630.0130 630.0500					
PROJECT	CATEGORY	LOCATION	STATION - STATION	SALVAGED TOPSOIL (SY)	FERTILIZER TYPE B (CWT)	SEEDING MIXTURE NO. 30 (LB)	SEED WATER (MGAL)
7505-00-76							
0010		CTH FF - CHURCH RD	183+38 - 199+86	400	0.07	7	34
		CHURCH RD - DEGROOT RD	199+86 - 242+72	192	0.03	3	16
		DEGROOT RD - BURNETTE RD	242+72 - 307+74	257	0.05	5	22
		BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	136	0.02	2	11
		HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	-	-	-	-
		TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	779	0.14	14	66
		SE BURNETTE RD - BRANCH RD	417+69 - 428+06	78	0.01	1	7
		BRANCH RD - N GIESE RD	428+06 - 483+15	660	0.12	12	56
		N GIESE RD - OLD STAGE RD	483+15 - 535+83	-	-	-	-
		OLD STAGE RD - STH 95	535+83 - 550+35	121	0.02	2	10
		UNDISTRIBUTED		656	0.54	12	59
PROJECT 7505-00-76 TOTAL				3,280	1.00	59	280

EROSION CONTROL

		606.0200 628.1504 628.1520 628.2002 628.2008 628.2033 628.7005 628.7504 628.7555 645.0120											
PROJECT	CATEGORY	LOCATION	STATION - STATION	EROSION MAT									GEOTEXTILE TYPE HR (SY)
				RIPRAP MEDIUM (CY)	SILT FENCE (LF)	SILT FENCE MAINTENANCE (LF)	EROSION MAT CLASS I TYPE A (SY)	EROSION MAT CLASS I TYPE B (SY)	EROSION MAT CLASS III TYPE B (SY)	INLET PROTECTION TYPE A (EACH)	TEMPORARY DITCH CHECKS (LF)	CULVERT PIPE CHECKS (EACH)	
7505-00-76													
0010		CTH FF - CHURCH RD	183+38 - 199+86	-	262	786	-	512	-	-	-	5	-
		CHURCH RD - DEGROOT RD	199+86 - 242+72	13	230	690	212	584	212	-	-	33	26
		DEGROOT RD - BURNETTE RD	242+72 - 307+74	-	385	1,155	246	345	246	-	6	12	-
		BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	-	215	645	129	385	129	-	-	8	-
		HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	-	-	-	-	-	-	-	-	-	-
		TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	-	1,183	3,544	151	606	151	-	12	-	-
		SE BURNETTE RD - BRANCH RD	417+69 - 428+06	-	-	-	-	358	-	-	-	11	-
		BRANCH RD - N GIESE RD	428+06 - 483+15	-	562	1,681	314	855	314	1	6	22	-
		N GIESE RD - OLD STAGE RD	483+15 - 535+83	-	2	1	-	-	-	-	-	-	-
		OLD STAGE RD - STH 95	535+83 - 550+35	-	191	568	-	206	-	-	-	3	-
		UNDISTRIBUTED		195	770	2,330	268	970	268	1	6	24	394
PROJECT 7505-00-76 TOTAL				208	3,800	11,400	1,320	4,820	1,320	2	30	118	420

EROSION CONTROL MOBILIZATIONS

		628.1905 628.1910	
PROJECT	CATEGORY	LOCATION	MOBILIZATIONS
			EROSION CONTROL (EACH)
7505-00-76			
0010		STH 121	14
			EROSION CONTROL (EACH)
PROJECT 7505-00-76 TOTAL			14
			EROSION CONTROL (EACH)
			3

TRAFFIC CONTROL

PROJECT	CATEGORY	LOCATION	638.2102	643.0300	643.0420	643.0705	643.0900	FOR INFORMATION ONLY, PAID FOR UNDER ITEM 643.0910		643.0910	FOR INFORMATION ONLY, PAID FOR UNDER ITEM 643.0920		643.0920	643.1050
			MOVING SIGNS TYPE II (EACH)	DRUMS (DAY)	BARRICADES TYPE III (DAY)	WARNING LIGHTS TYPE A (DAY)	SIGNS (DAY)	NUMBER OF CYCLES	NUMBER OF SIGNS	COVERING SIGNS TYPE I (DAY)	NUMBER OF CYCLES	NUMBER OF SIGNS	COVERING SIGNS TYPE II (DAY)	SIGNS PCMS (DAY)
7505-00-76	0010	STAGE 1 DETOUR	-	22,680	-	-	20,168	-	-	-	-	-	-	15
			-	-	805	1,330	4,732	1	70	70	1	245	245	85
		UNDISTRIBUTED	10	-	-	-	-	-	-	-	-	-	-	-
PROJECT 7505-00-76 TOTAL			10	22,680	805	1,330	24,900			70			245	100

PAVEMENT MARKING

PROJECT	CATEGORY	LOCATION	STATION - STATION	465.0475	646.1020	
				ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL (LF)	MARKING LINE EPOXY 4-INCH	
				WHITE (LF)	YELLOW (LF)	
7505-00-76	0010	CTH FF - CHURCH RD	183+38 - 199+86	1,042	3,154	3,278
		CHURCH RD - DEGROOT RD	199+86 - 242+72	3,885	8,502	5,919
		DEGROOT RD - BURNETTE RD	242+72 - 307+74	7,000	12,959	5,081
		BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	5,036	10,746	7,116
		HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	656	2,766	2,106
		TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	3,110	8,311	8,312
		SE BURNETTE RD - BRANCH RD	417+69 - 428+06	631	1,961	1,320
		BRANCH RD - N GIESE RD	428+06 - 483+15	5,108	10,853	10,119
		N GIESE RD - OLD STAGE RD	483+15 - 535+83	4,879	10,414	6,751
		OLD STAGE RD - STH 95	535+83 - 550+35	1,268	2,868	1,965
PROJECT 7505-00-76 TOTAL				32,615	72,534	51,966
				124,500		

TEMPORARY PAVEMENT MARKING

PROJECT	CATEGORY	LOCATION	STATION - STATION	649.0105		649.0120
				TEMPORARY MARKING LINE PAINT 4-INCH		TEMPORARY MARKING LINE EPOXY 4-INCH
				WHITE (LF)	YELLOW (LF)	YELLOW (LF)
7505-00-76	0010	CTH FF - CHURCH RD	183+38 - 199+86	6,308	6,556	3,278
		CHURCH RD - DEGROOT RD	199+86 - 242+72	17,004	11,838	5,919
		DEGROOT RD - BURNETTE RD	242+72 - 307+74	25,918	10,161	5,081
		BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	21,492	14,232	7,116
		HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	5,532	4,213	2,106
		TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	16,629	16,632	8,320
		SE BURNETTE RD - BRANCH RD	417+69 - 428+06	3,920	2,650	1,330
		BRANCH RD - N GIESE RD	428+06 - 483+15	21,696	20,248	10,129
		N GIESE RD - OLD STAGE RD	483+15 - 535+83	20,811	13,505	6,754
		OLD STAGE RD - STH 95	535+83 - 550+35	5,721	3,935	1,968
PROJECT 7505-00-76 TOTAL				145,031	103,969	52,000
				249,000		

3

3

CONSTRUCTION STAKING

		650.4500	650.5000	650.6000	650.8000	650.9910	650.9920		
PROJECT	CATEGORY	LOCATION	STATION - STATION	SUBGRADE (LF)	BASE (LF)	PIPE CULVERTS (EACH)	RESURFACING REFERENCE (LF)	SUPPLEMENTAL CONTROL 7505-00-76 (LS)	SLOPE STAKES (LF)
7505-00-76									
0010		CTH FF - CHURCH RD	183+38 - 199+86	113	113	1	1,648	-	113
		CHURCH RD - DEGROOT RD	199+86 - 242+72	121	121	3	4,286	-	121
		DEGROOT RD - BURNETTE RD	242+72 - 307+74	210	210	2	6,502	-	210
		BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	104	104	1	5,414	-	104
		HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	-	-	-	1,449	-	-
		TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	763	763	-	5,032	-	763
		SE BURNETTE RD - BRANCH RD	417+69 - 428+06	-	-	-	1,037	-	-
		BRANCH RD - N GIESE RD	428+06 - 483+15	321	321	3	5,509	-	321
		N GIESE RD - OLD STAGE RD	483+15 - 535+83	-	-	-	5,268	-	-
		OLD STAGE RD - STH 95	535+83 - 550+35	108	108	1	1,455	-	108
		UNSDISTRIBUTED		-	-	-	-	1	-
PROJECT 7505-00-76 TOTAL				1,740	1,740	11	37,600	1	1,740

SAWING ASPHALT

690.0150

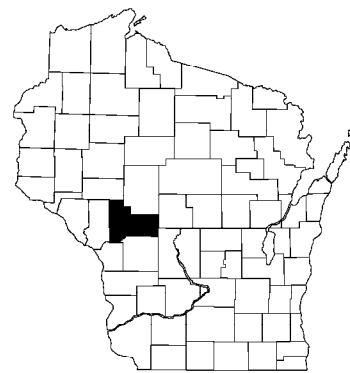
PROJECT	CATEGORY	LOCATION	STATION - STATION	(LF)
7505-00-76				
0010		CTH FF - CHURCH RD	183+38 - 199+86	82
		CHURCH RD - DEGROOT RD	199+86 - 242+72	116
		DEGROOT RD - BURNETTE RD	242+72 - 307+74	141
		BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	98
		HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	36
		TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	-
		SE BURNETTE RD - BRANCH RD	417+69 - 428+06	-
		BRANCH RD - N GIESE RD	428+06 - 483+15	182
		N GIESE RD - OLD STAGE RD	483+15 - 535+83	64
		OLD STAGE RD - STH 95	535+83 - 550+35	91
PROJECT 7505-00-76 TOTAL				810

CLEANING DITCH

SPV.0090.01

PROJECT	CATEGORY	LOCATION	STATION - STATION	(LF)
7505-00-76				
0010		CTH FF - CHURCH RD	183+38 - 199+86	-
		CHURCH RD - DEGROOT RD	199+86 - 242+72	10
		DEGROOT RD - BURNETTE RD	242+72 - 307+74	45
		BURNETTE RD - HIXTON LEVIS RD	307+74 - 361+88	156
		HIXTON LEVIS RD - TOWNHOUSE RD	361+88 - 376+37	-
		TOWNHOUSE RD - SE BURNETTE RD	376+37 - 417+69	-
		SE BURNETTE RD - BRANCH RD	417+69 - 428+06	60
		BRANCH RD - N GIESE RD	428+06 - 483+15	142
		N GIESE RD - OLD STAGE RD	483+15 - 535+83	-
		OLD STAGE RD - STH 95	535+83 - 550+35	-
PROJECT 7505-00-76 TOTAL				413

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
TRANSPORTATION PROJECT PLAT TITLE SHEET
7505-00-26
NORTHFIELD - ALMA CENTER
 (CTH FF TO STH 95)
STH 121
JACKSON COUNTY



CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT		OFF-PREMISE SIGN	
NEW R/W LINE	---	SIGN		COMPENSABLE	
EXISTING R/W OR HE LINE	---	OFF-PREMISE SIGN		NON-COMPENSABLE	
PROPERTY LINE	---	ELECTRIC POLE		TELEPHONE POLE	
LOT, TIE & OTHER MINOR LINES	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)		ACCESS RESTRICTED BY ACQUISITION	
SLOPE INTERCEPT	---	NO ACCESS (BY STATUTORY AUTHORITY)		NO ACCESS (BY PREVIOUS PROJECT OR CONTROL)	
CORPORATE LIMITS	---	NO ACCESS (NEW HIGHWAY)		PARCEL NUMBER (25)	UTILITY NUMBER (40)
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	---	PARALLEL OFFSETS			
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING TO BE REMOVED					
BRIDGE					

CONVENTIONAL ABBREVIATIONS

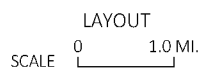
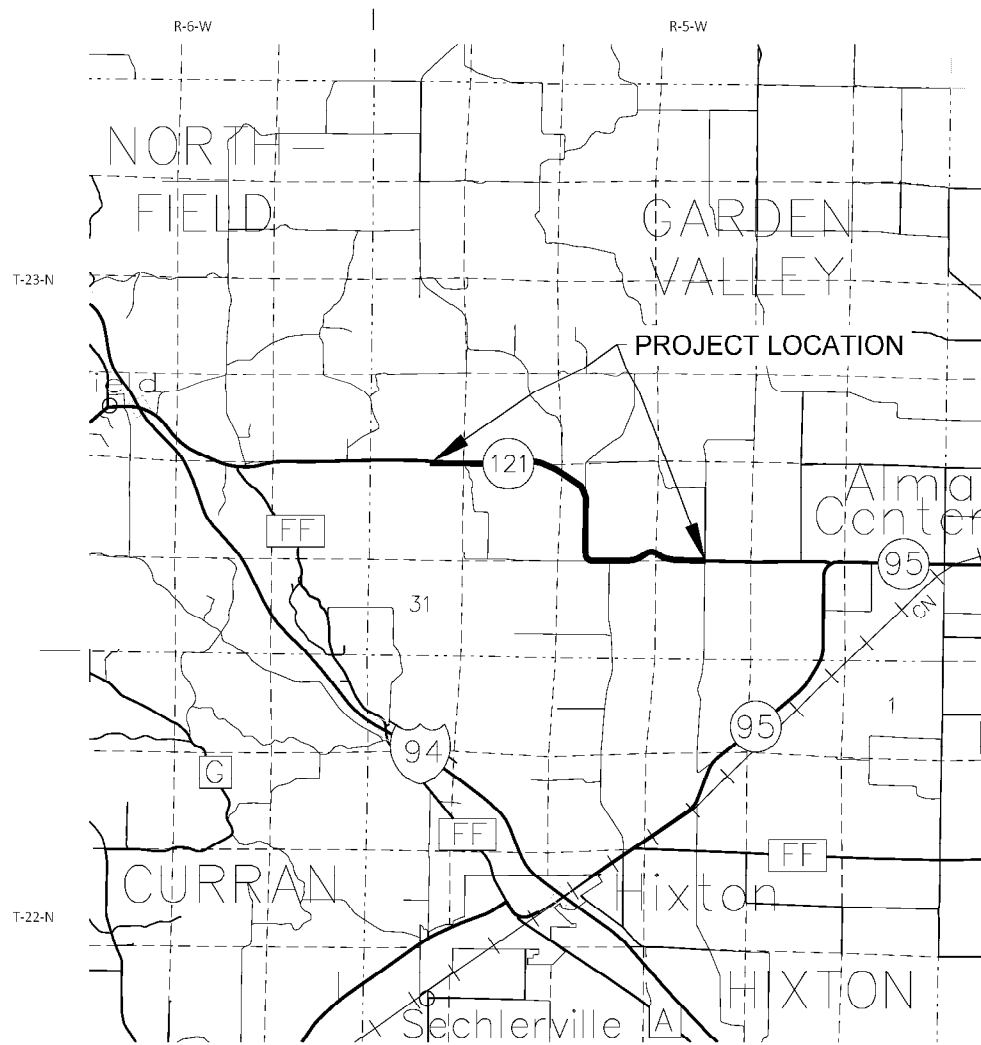
ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT	RDE
CENTERLINE	C/L	EASEMENT	
CERTIFIED SURVEY MAP	CSM	RIGHT	RT
CONCRETE	CONC	RIGHT OF WAY	R/W
COUNTY	CO	SECTION	SEC
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV
DISTANCE	DIST	SQUARE FEET	SF
CORNER	COR	STATE TRUNK HIGHWAY	STH
DOCUMENT NUMBER	DOC	STATION	STA
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED EASEMENT	TLE
GAS VALVE	GV		
GRID NORTH	GN	TRANSPORTATION PROJECT	TPP
HIGHWAY EASEMENT	HE	PLAT	
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

W	WATER
G	GAS
T	TELEPHONE
OH	OVERHEAD TRANSMISSION LINES
E	ELECTRIC
TV	CABLE TELEVISION
FO	FIBER OPTIC
SAN	SANITARY SEWER
SS	STORM SEWER



THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PLAT FOR PROJECT 7505-00-26.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), JACKSON COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLES) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NORTHWEST REGION.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

PROJECT NUMBER 7505-00-26 - 4.01
 SHEET 2 OF 2
 AMENDMENT NO:

TRANSPORTATION PROJECT PLAT NO: 7505-00-26 - 4.01

THAT PART OF THE SE 1/4 OF THE SE 1/4 OF SECTION 19, T 23 N, R 5 W, TOWN OF GARDEN VALLEY, JACKSON COUNTY, WISCONSIN.

RELOCATION ORDER STH 121, NORTHFIELD - ALMA CENTER (CTH FF TO STH 95), JACKSON COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE-NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE-NAMED PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), JACKSON COUNTY, NAD 83(2011) IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT. FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NORTHWEST REGION.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2.

UTILITY INTERESTS REQUIRED

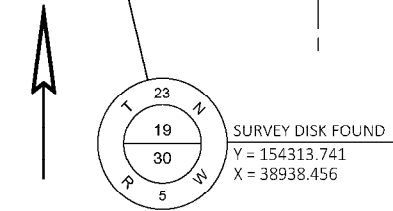
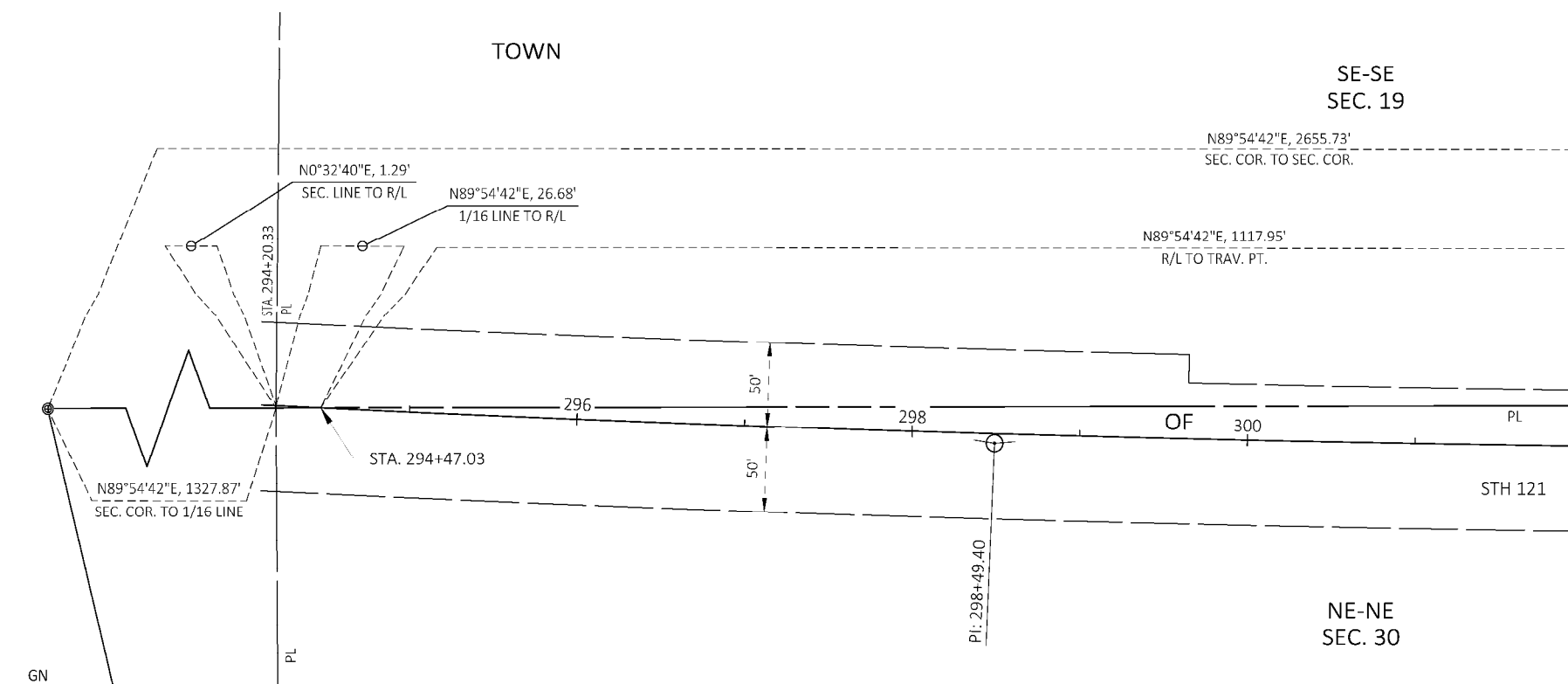
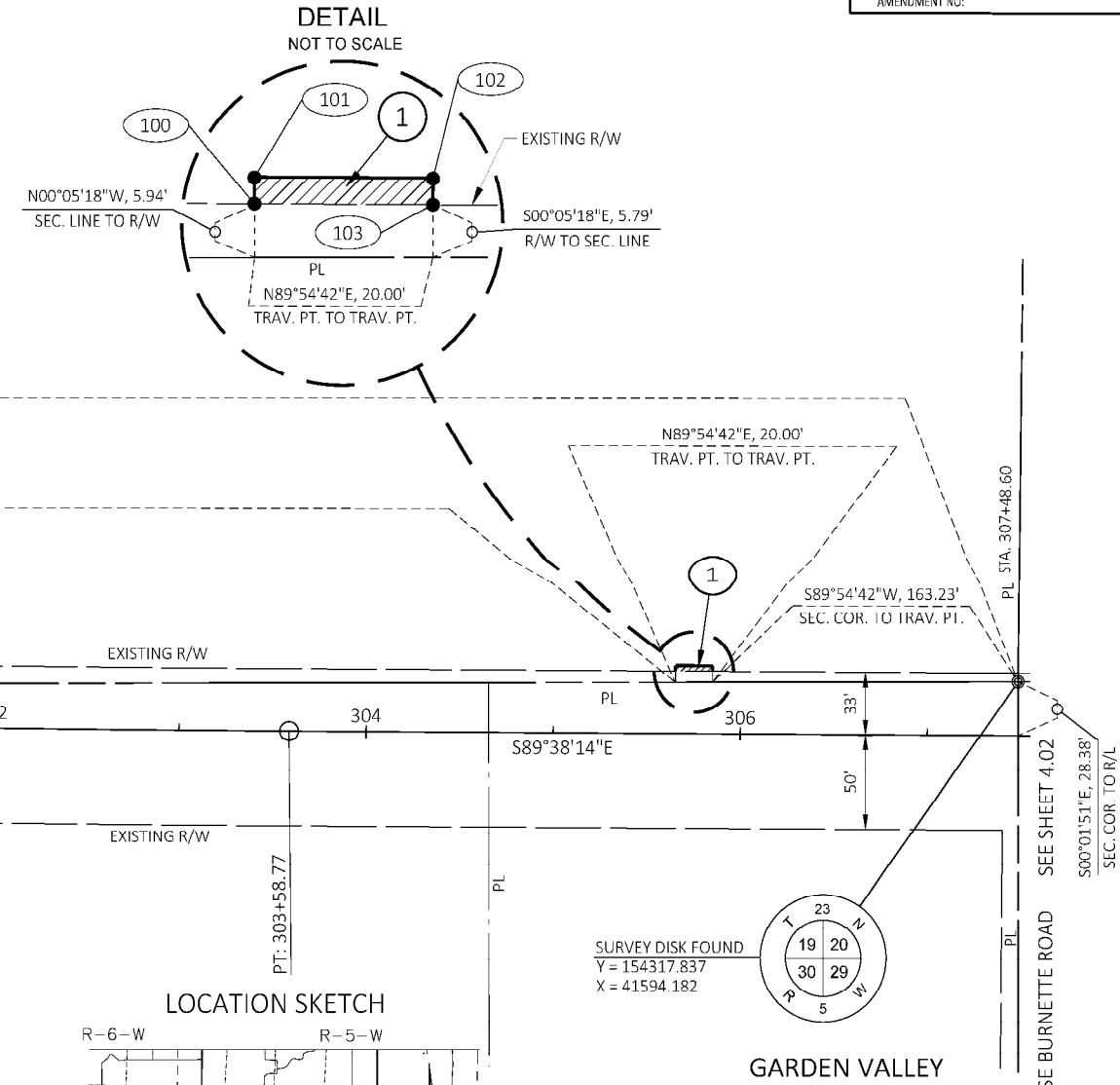
UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
100	JACKSON ELECTRIC COOPERATIVE	RELEASE OF RIGHTS

EASEMENT TABLE

UTILITY NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL NO.
100	JACKSON ELECTRIC COOPERATIVE	BLANKET EASEMENT V. 121, P. 13, DOC. 132027 BLANKET EASEMENT V. 121, P. 14, DOC. 132028	1

Document Number: 403086
Volume: 1 Page: 12
Shari Marg
Register of Deeds
Jackson County, WI
Recorded: 02/02/2021
at: 10:49 AM
Transfer Tax Paid: \$0.00
Transfer Tax Exempt #
Recording Fee Paid: \$25.00
Number of Pages: 2

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 7505-00-26 - 4.01
SHEET 1 OF 2
AMENDMENT NO:



HIGHWAY	BASIS
STH 121	PREVIOUS PROJECT T0178(3) R/W CONVEYANCE V. 132, P. 20, DOC. 155433 R/W CONVEYANCE V. 132, P. 22, DOC. 155462

STA. 294+20.33 TO STA. 294+47.03
L=26.70'
LCH=26.70'
LCB=S87°19'28"E
R=22918.31'

PI STA = 298+49.40
Y = 154295.153
X = 40694.843
DELTA = 2°32'50" LT
D = 0°15'00"
T = 509.54'
L = 1018.91'
R = 22918.31'

STA. 294+47.03 TO PC
L=107.17'
LCH=107.17'
LCB=N87°13'26"W
R=22918.31'

STA. 294+47.03 TO PT
L=911.74'
LCH=911.68'
LCB=S88°29'51"E
R=22918.31'

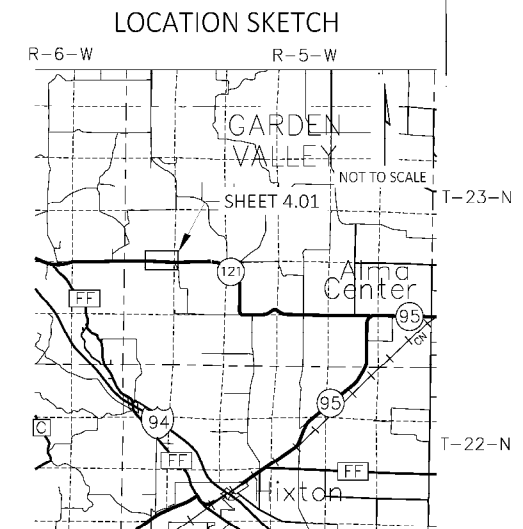
PT: 303+58.77
DB = S87°05'24"E

R/W COURSE TABLE			
FROM POINT	TO POINT	BEARING	DISTANCE
100	101	N00° 21' 34"E	3.00'
101	102	S89° 38' 26"E	20.00'
102	103	S00° 21' 34"W	3.00'

R/W Station & Offset Table		
Point No.	Station	Offset
100	305+65.14	32.88'
101	305+65.14	35.88'
102	305+85.14	35.88'
103	305+85.14	32.88'

SCHEDULE OF LANDS & INTERESTS REQUIRED						
PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W SF REQUIRED			TLE S.F.
			NEW	EXISTING	TOTAL	
1	PAUL W. GIESE	FEE	60	118	178	---

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTEREST TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION. ALL AREAS SHOWN IN SQUARE FEET UNLESS OTHERWISE NOTED.



I, TIMOTHY M. HELD, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



SIGNATURE: *T. M. Held* DATE: 01/22/2021
PRINT NAME: TIMOTHY M. HELD
REGISTRATION NUMBER: 5-2591

SIGNATURE: *Debra B. Stensland* DATE: 1/22/2021
PRINT NAME: DEBRA B. STENSLAND

TRANSPORTATION PROJECT PLAT NO: 7505-00-26 - 4.02

THAT PART OF THE SW 1/4 OF THE SW 1/4 OF SECTION 20, AND PART OF THE NW 1/4 OF SECTION 29, ALL IN T 23 N, R 5 W, TOWN OF GARDEN VALLEY, JACKSON COUNTY, WISCONSIN.

RELOCATION ORDER STH 121, NORTHFIELD - ALMA CENTER (CTH FF TO STH 95), JACKSON COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE-NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE-NAMED PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), JACKSON COUNTY, NAD 83(2011) IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT. FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NORTHWEST REGION.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN VOLUME 1 OF TRANSPORTATION PROJECT PLATS, PAGE 12, AS DOCUMENT NO. 403086.

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
100	JACKSON ELECTRIC COOPERATIVE	RELEASE OF RIGHTS

Document Number: 404512
 Volume: 1 Page: 12
 Shari Marg
 Register of Deeds
 Jackson County, WI
 Recorded: 05/20/2021
 at: 08:56 AM
 Transfer Tax Paid: \$0.00
 Transfer Tax Exempt #
 Recording Fee Paid: \$25.00
 Number of Pages: 1

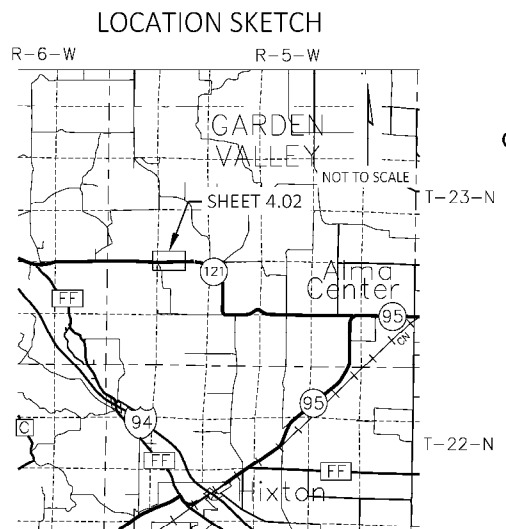
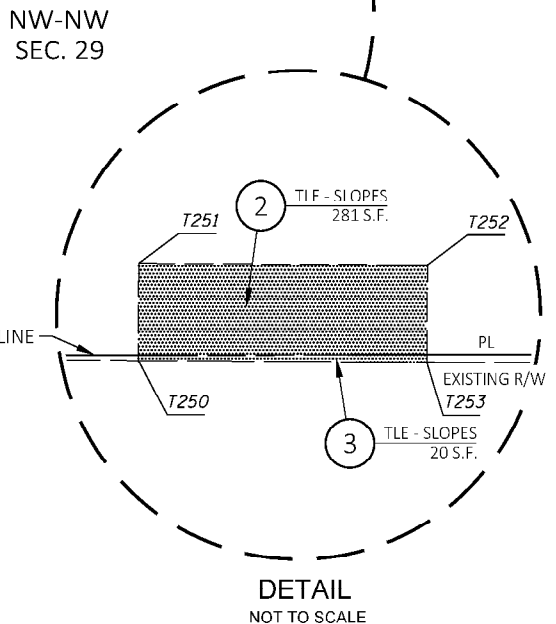
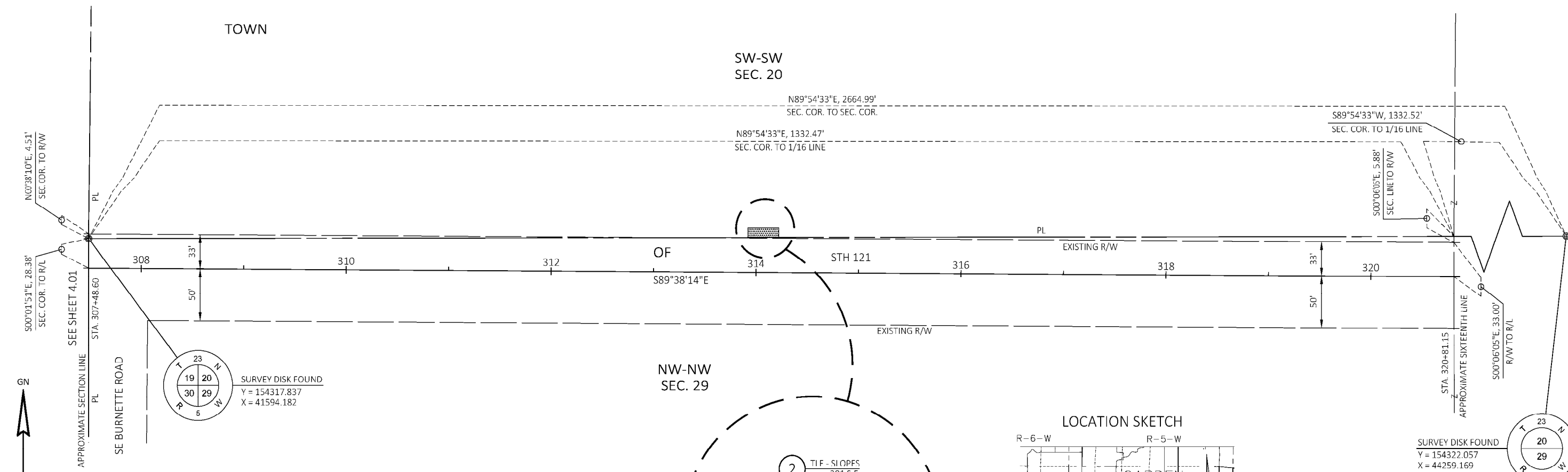
RESERVED FOR REGISTER OF DEEDS
 PROJECT NUMBER 7505-00-26 - 4.02
 SHEET 1 OF 1
 AMENDMENT NO:

EASEMENT TABLE

UTILITY NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL NO.
100	JACKSON ELECTRIC COOPERATIVE	BLANKET EASEMENT V. 121, P. 33, DOC. 132047	2
100	JACKSON ELECTRIC COOPERATIVE	BLANKET EASEMENT V. 121, P. 34, DOC. 132048	3

4

4



HIGHWAY	BASIS
STH 121	PREVIOUS PROJECT T0178(3) R/W CONVEYANCE V. 132, P. 21, DOC. 155455

SCHEDULE OF LANDS & INTERESTS REQUIRED		OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTEREST TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION ALL AREAS SHOWN IN SQUARE FEET UNLESS OTHERWISE NOTED				
PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W SF REQUIRED NEW	EXISTING	TOTAL	TLE S.F.
2	TONI M LARSON	TLE	---	---	---	281
3	TONI DAVIDSON	TLE	---	---	---	20

TLE Station & Offset Table		
Point No.	Station	Offset
T250	313+91.89	32.93'
T251	313+91.89	42.93'
T252	314+21.89	42.93'
T253	314+21.89	32.93'

KL Engineering
 [A] Better Experience

I, TIMOTHY M. HELD, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: *T. M. Held* DATE: 05/18/2021
 PRINT NAME: TIMOTHY M. HELD
 REGISTRATION NUMBER: S-2591

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE DEPARTMENT.

SIGNATURE: *Heather L. Dreisel* DATE: 5/18/2021
 PRINT NAME: HEATHER L. DREISEL

WISCONSIN LAND SURVEYOR
 TIMOTHY M. HELD
 S-2591
 MADISON, WI

TRANSPORTATION PROJECT PLAT NO: 7505-00-26 - 4.03

THAT PART OF THE NW 1/4 OF THE NE 1/4 OF SECTION 29, AND PART OF THE SW 1/4 OF THE SE 1/4 OF SECTION 20, ALL IN T 23 N, R 5 W, TOWN OF GARDEN VALLEY, JACKSON COUNTY, WISCONSIN.

RELOCATION ORDER STH 121, NORTHFIELD - ALMA CENTER (CTH FF TO STH 95), JACKSON COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE-NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE-NAMED PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), JACKSON COUNTY, NAD 83(2011) IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT. FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NORTHWEST REGION.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN VOLUME 1 OF TRANSPORTATION PROJECT PLATS, PAGE 12, AS DOCUMENT NO. 403086.

UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
100	JACKSON ELECTRIC COOPERATIVE	RELEASE OF RIGHTS

EASEMENT TABLE			
UTILITY NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL NO.
100	JACKSON ELECTRIC COOPERATIVE	BLANKET EASEMENT V. 121, P. 88, DOC. 132102	1

Document Number: 404513
Volume: 1 Page: 13
Shari Marg
Register of Deeds
Jackson County, WI
Recorded: 05/20/2021
at: 09:01 AM
Transfer Tax Paid: \$0.00
Transfer Tax Exempt #
Recording Fee Paid: \$25.00
Number of Pages: 1

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 7505-00-26 - 4.03
SHEET 1 OF 1
AMENDMENT NO:

R/W COURSE TABLE			
FROM POINT	TO POINT	BEARING	DISTANCE
300	301	N06° 03' 46"E	2.43'
301	302	SEE CURVE NOTE	
302	303	S06° 42' 50"W	25.00'
303	304	N83° 36' 42"W	21.20'
304	300	N06° 03' 46"E	22.57'

R/W Station & Offset Table		
Point No.	Station	Offset
300	347+02.86	28.37'
301	347+02.91	25.93'
302	347+24.63	26.36'
303	347+24.10	51.35'
304	347+02.42	50.93'

CURVE 301 TO 302
L=21.48'
LCH=21.48'
LCB=S83°36'42"E
R=1890.00'

PC TO STA. 347+50.86
L=415.93'
LCH=415.36'
LCB=S89°01'14"E
R=2291.83'

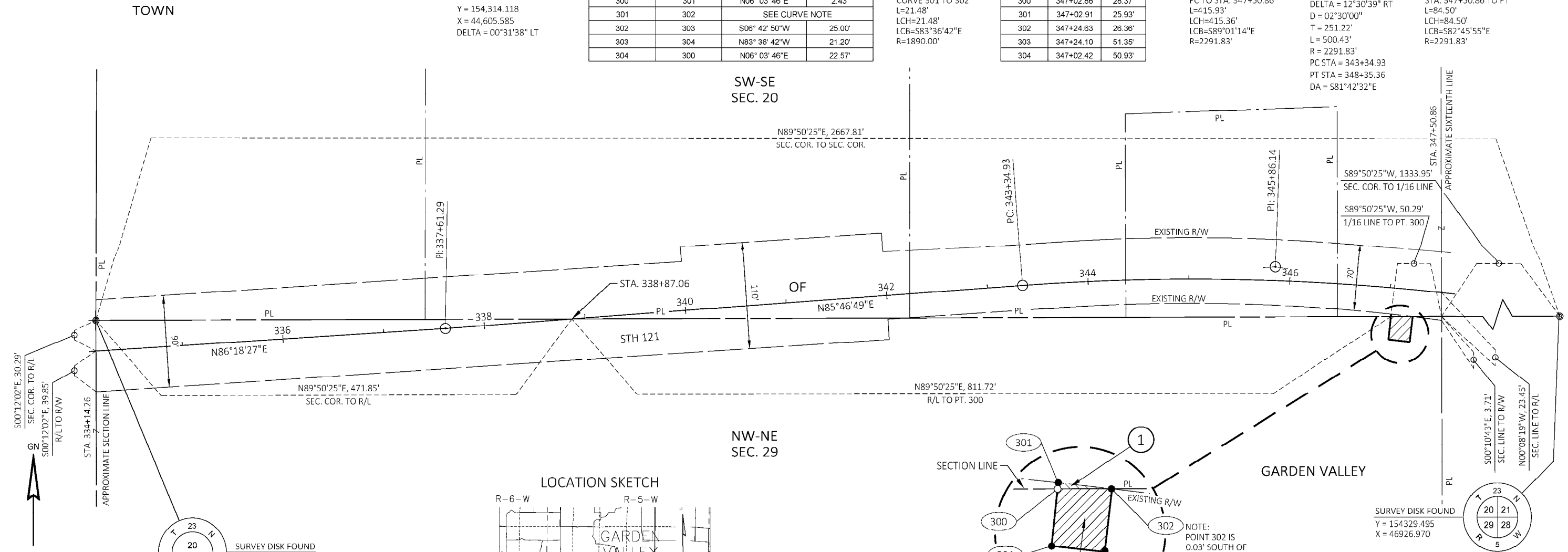
PI STA = 345+86.14
Y = 154,374.812
X = 45,428.203
DELTA = 12°30'39" RT
D = 02°30'00"
T = 251.22'
L = 500.43'
R = 2291.83'
PC STA = 343+34.93
PT STA = 348+35.36
DA = S81°42'32"E

STA. 347+50.86 TO PT
L=84.50'
LCH=84.50'
LCB=S82°45'55"E
R=2291.83'

PI STA = 337+61.29
Y = 154,314.118
X = 44,605.585
DELTA = 00°31'38" LT

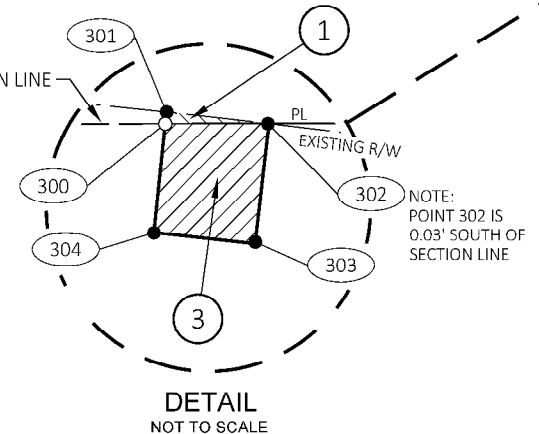
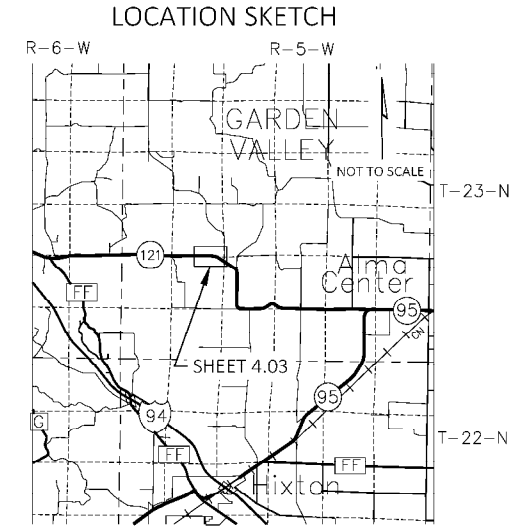
SW-SE
SEC. 20

NW-NE
SEC. 29



HIGHWAY	BASIS
STH 121	R/W CONVEYANCE V. 109, P. 395, DOC. 130721 R/W CONVEYANCE V. 109, P. 391, DOC. 130003

SCHEDULE OF LANDS & INTERESTS REQUIRED						
PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W SF REQUIRED			TLE S.F.
			NEW	EXISTING	TOTAL	
1	PAUL W. GIESE	FEE	27	---	27	---
3	TONI DAVIDSON	FEE	508	---	508	---



KL Engineering
[A] Better Experience

I, TIMOTHY M. HELD, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: *T. M. Held* DATE: 05/18/2021
PRINT NAME: TIMOTHY M. HELD
REGISTRATION NUMBER: S-2591

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE DEPARTMENT.

SIGNATURE: *Heather L. Dreisel* DATE: 5/18/2021
PRINT NAME: HEATHER L. DREISEL

WISCONSIN LAND SURVEYOR
TIMOTHY M. HELD
S-2591
MADISON, WI

TRANSPORTATION PROJECT PLAT NO: 7505-00-26 - 4.04

THAT PART OF THE NE 1/4 OF THE NW 1/4 OF SECTION 33, T 23 N, R 5 W, TOWN OF GARDEN VALLEY, JACKSON COUNTY, WISCONSIN.

RELOCATION ORDER STH 121, NORTHFIELD - ALMA CENTER (CTH FF TO STH 95), JACKSON COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE-NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE-NAMED PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), JACKSON COUNTY, NAD 83(2011) IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NORTHWEST REGION.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN VOLUME 1 OF TRANSPORTATION PROJECT PLATS, PAGE 12, AS DOCUMENT NO. 403086.

UTILITY INTERESTS REQUIRED

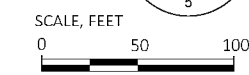
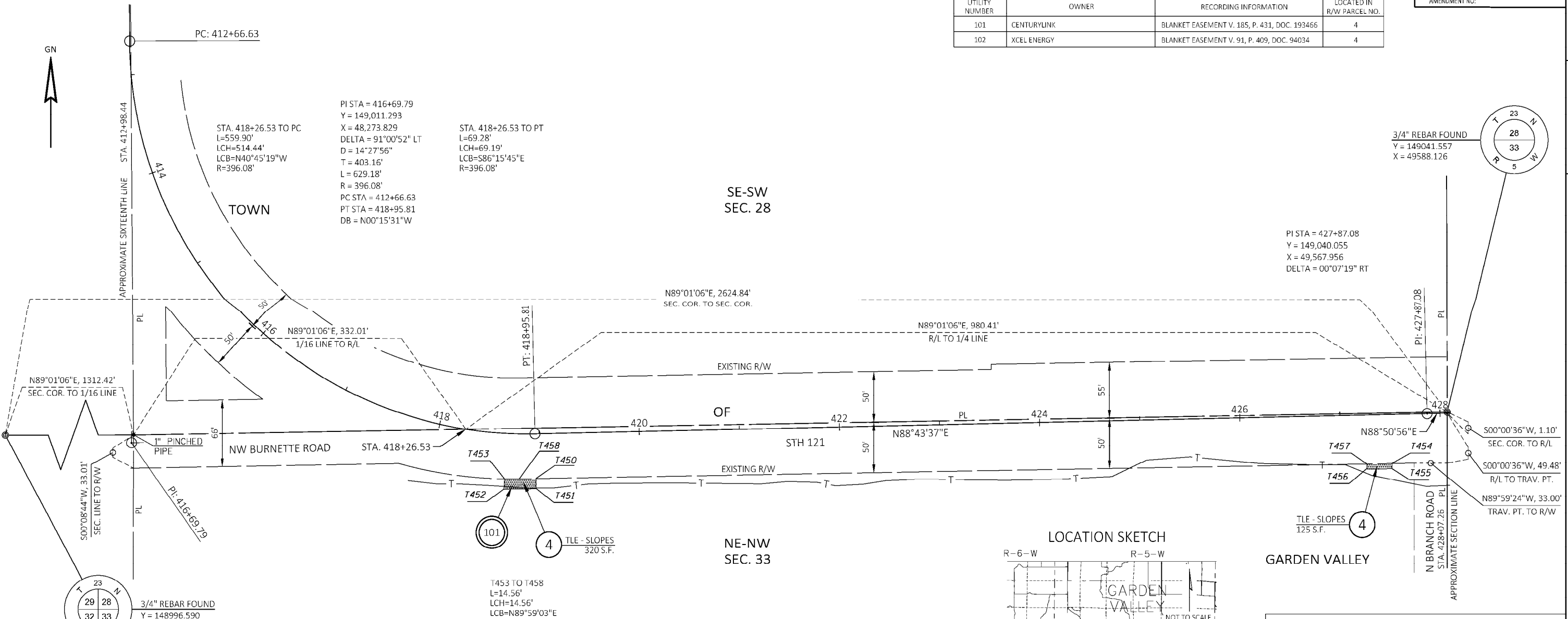
UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
101	CENTURYLINK	RELEASE OF RIGHTS
102	XCEL ENERGY	RELEASE OF RIGHTS

Document Number: 404514
Volume: 1 Page: 14
Shari Marg
Register of Deeds
Jackson County, WI
Recorded: 05/20/2021
at: 09:02 AM
Transfer Tax Paid: \$0.00
Transfer Tax Exempt #
Recording Fee Paid: \$25.00
Number of Pages: 1

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 7505-00-26 - 4.04
SHEET 1 OF 1
AMENDMENT NO:

EASEMENT TABLE

UTILITY NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL NO.
101	CENTURYLINK	BLANKET EASEMENT V. 185, P. 431, DOC. 193466	4
102	XCEL ENERGY	BLANKET EASEMENT V. 91, P. 409, DOC. 94034	4



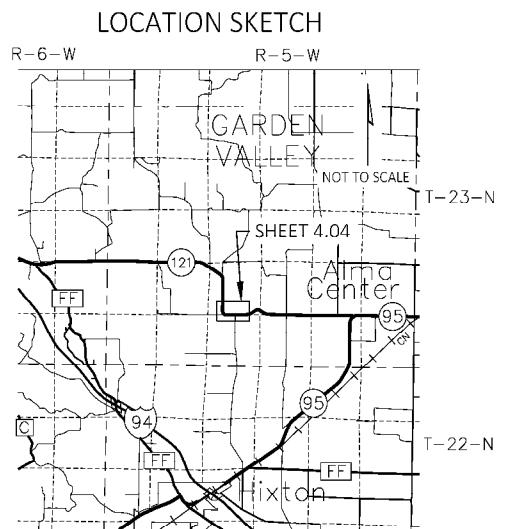
HIGHWAY	BASIS
STH 121	PREVIOUS PROJECT T0178(3) R/W CONVEYANCE V. 132, P. 49, DOC. 156845

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W SF REQUIRED			TLE S.F.
			NEW	EXISTING	TOTAL	
4	CHARLES D JANKE AND BETTY M. JANKE	TLE	---	---	---	445

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTEREST TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION ALL AREAS SHOWN IN SQUARE FEET UNLESS OTHERWISE NOTED

TLE Station & Offset Table		
Point No.	Station	Offset
T450	418+95.81	44.31'
T451	418+95.81	54.31'
T452	418+67.65	55.29'
T453	418+67.65	45.02'
T454	427+50.83	48.66'
T455	427+50.83	53.66'
T456	427+25.83	53.53'
T457	427+25.83	48.53'
T458	418+80.73	44.54'



I, TIMOTHY M. HELD, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



SIGNATURE: *T. M. Held* DATE: 05/18/2021
PRINT NAME: TIMOTHY M. HELD
REGISTRATION NUMBER: S-2591
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE DEPARTMENT.
SIGNATURE: *Heather L. Dresel* DATE: 5/18/2021
PRINT NAME: HEATHER L. DRESEL

TRANSPORTATION PROJECT PLAT NO: 7505-00-26 - 4.05

THAT PART OF THE NW 1/4 OF THE NW 1/4 OF SECTION 34, T 23 N, R 5 W, TOWN OF GARDEN VALLEY, JACKSON COUNTY, WISCONSIN.

RELOCATION ORDER STH 121, NORTHFIELD - ALMA CENTER (CTH FF TO STH 95), JACKSON COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE-NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE-NAMED PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), JACKSON COUNTY, NAD 83(2011) IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES. GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT. FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NORTHWEST REGION.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN VOLUME 1 OF TRANSPORTATION PROJECT PLATS, PAGE 12, AS DOCUMENT NO. 403086.

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
101	CENTURYLINK	RELEASE OF RIGHTS

Document Number: 404515
 Volume: 1 Page: 15
 Shari Marg
 Register of Deeds
 Jackson County, WI
 Recorded: 05/20/2021
 at: 09:03 AM
 Transfer Tax Paid: \$0.00
 Recording Fee Paid: \$25.00
 Number of Pages: 1

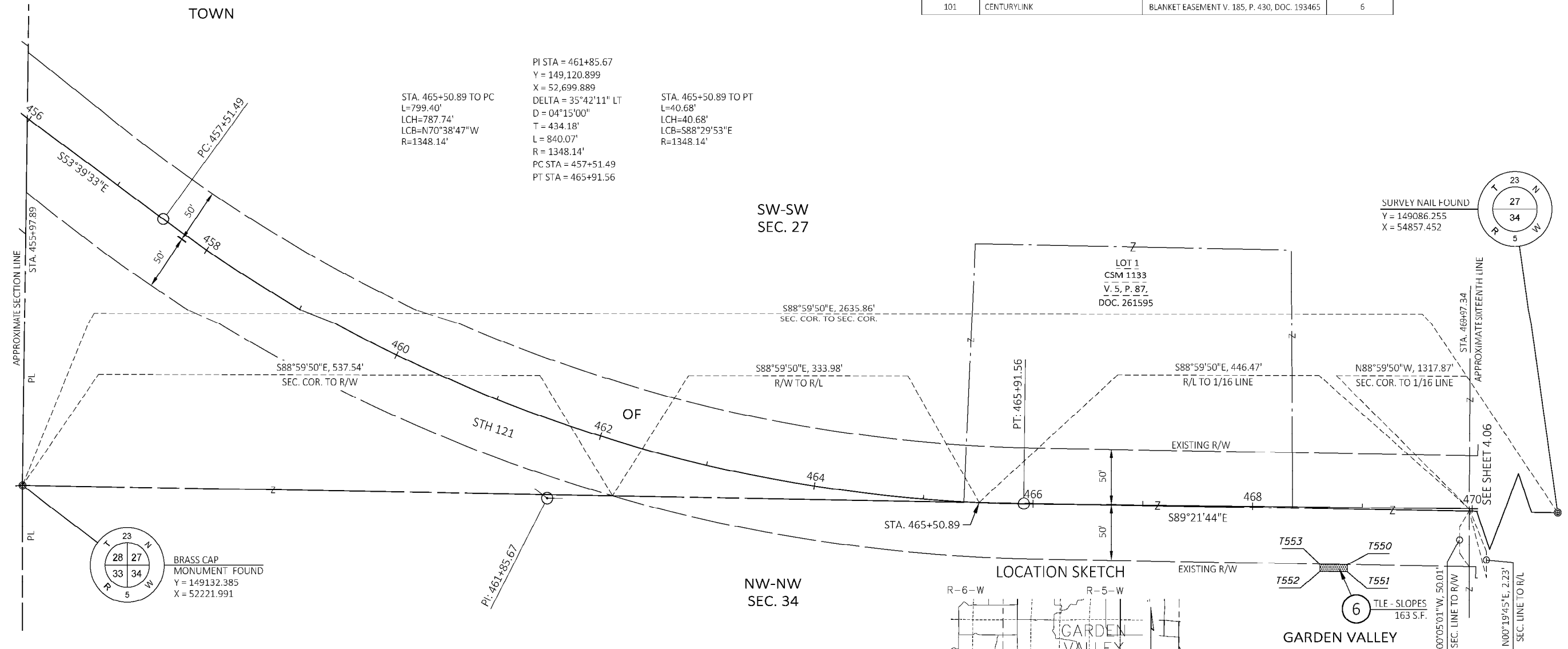
RESERVED FOR REGISTER OF DEEDS
 PROJECT NUMBER 7505-00-26 - 4.05
 SHEET 1 OF 1
 AMENDMENT NO:

EASEMENT TABLE

UTILITY NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL NO.
101	CENTURYLINK	BLANKET EASEMENT V. 185, P. 430, DOC. 193465	6

4

4



PI STA = 461+85.67
 Y = 149,120.899
 X = 52,699.889
 DELTA = 35°42'11" LT
 D = 04°15'00"
 T = 434.18'
 L = 840.07'
 R = 1348.14'
 PC STA = 457+51.49
 PT STA = 465+91.56

STA. 465+50.89 TO PC
 L=799.40'
 LCH=787.74'
 LCB=N70°38'47"W
 R=1348.14'

STA. 465+50.89 TO PT
 L=40.68'
 LCH=40.68'
 LCB=S88°29'53"E
 R=1348.14'

SURVEY NAIL FOUND
 Y = 149086.255
 X = 54857.452

BRASS CAP MONUMENT FOUND
 Y = 149132.385
 X = 52221.991



HIGHWAY	BASIS
STH 121	PREVIOUS PROJECT T0178(3) R/W CONVEYANCE V. 132, P. 37, DOC. 156724

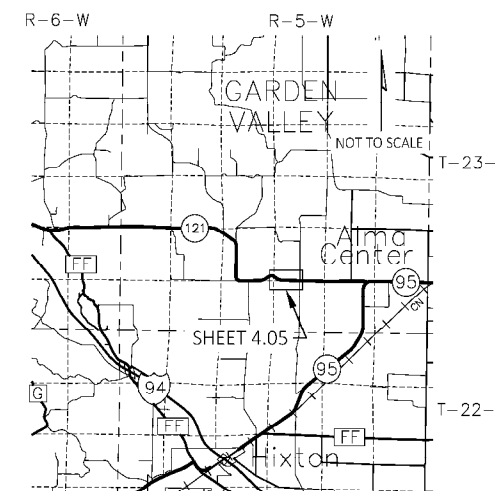
Point No.	Station	Offset
T550	468+86.93	51.53'
T551	468+86.93	58.03'
T552	468+61.93	57.87'
T553	468+61.93	51.37'

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTEREST TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION ALL AREAS SHOWN IN SQUARE FEET UNLESS OTHERWISE NOTED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W SF REQUIRED			TLE S.F.
			NEW	EXISTING	TOTAL	
6	MICHAEL J GIESE	TLE	---	---	---	163

LOCATION SKETCH



I, TIMOTHY M. HELD, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



SIGNATURE: *T. M. Held* DATE: 05/18/2021
 PRINT NAME: TIMOTHY M. HELD
 REGISTRATION NUMBER: S-2591

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE DEPARTMENT.

SIGNATURE: *Heather L. Dresel* DATE: 5/18/2021
 PRINT NAME: HEATHER L. DRESEL

TRANSPORTATION PROJECT PLAT NO: 7505-00-26 - 4.06

THAT PART OF THE SE 1/4 OF THE SW 1/4 OF SECTION 27 AND PART OF THE NE 1/4 OF THE NW 1/4 OF SECTION 34, ALL IN T 23 N, R 5 W, TOWN OF GARDEN VALLEY, JACKSON COUNTY, WISCONSIN

RELOCATION ORDER STH 121, NORTHFIELD - ALMA CENTER (CTH FF TO STH 95), JACKSON COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE-NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE-NAMED PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), JACKSON COUNTY, NAD 83(2011) IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NORTHWEST REGION.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN VOLUME 1 OF TRANSPORTATION PROJECT PLATS, PAGE 12, AS DOCUMENT NO. 403086.

UTILITY INTERESTS REQUIRED

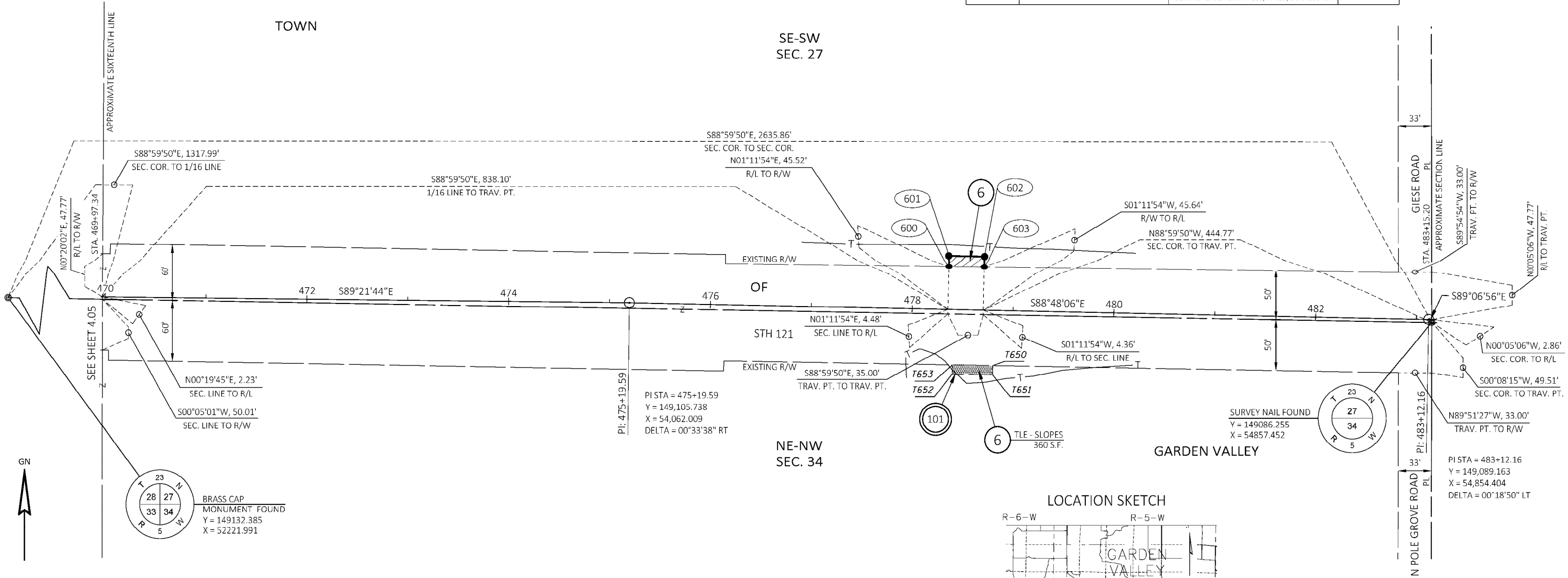
UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
101	CENTURYLINK	RELEASE OF RIGHTS

Document Number: 404516
Volume: 1 Page: 16
Shari Marg
Register of Deeds
Jackson County, WI
Recorded: 05/20/2021
at: 09:03 AM
Transfer Tax Paid: \$0.00
Transfer Tax Exempt #:
Recording Fee Paid: \$25.00
Number of Pages: 1

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 7505-00-26 - 4.06
SHEET 1 OF 1
AMENDMENT NO:

EASEMENT TABLE

UTILITY NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL NO.
101	CENTURYLINK	BLANKET EASEMENT V. 185, P. 443, DOC. 193478	6
		BLANKET EASEMENT V. 185, P. 429, DOC. 193464	6



HIGHWAY	BASIS
STH 121	PREVIOUS PROJECT T0178(3) R/W CONVEYANCE V. 132, P. 46, DOC. 156843 R/W CONVEYANCE V. 132, P. 37, DOC. 156724

R/W COURSE TABLE			
FROM POINT	TO POINT	BEARING	DISTANCE
600	601	N01° 00' 10"E	12.50'
601	602	S88° 59' 50"E	35.00'
602	603	S01° 00' 10"W	12.50'

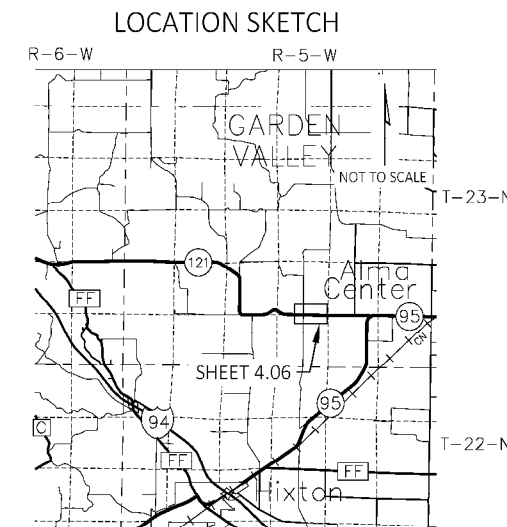
R/W Station & Offset Table		
Point No.	Station	Offset
600	478+35.50	45.52'
601	478+35.45	58.02'
602	478+70.45	58.14'
603	478+70.50	45.64'

TLE Station & Offset Table		
Point No.	Station	Offset
T650	478+81.08	54.33'
T651	478+81.08	63.33'
T652	478+41.08	63.46'
T653	478+41.08	54.46'

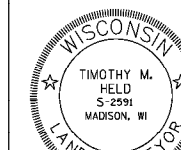
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W SF REQUIRED			TLE S.F.
			NEW	EXISTING	TOTAL	
6	MICHAEL J GIESE	FEE/TLE	438	1750	2188	360

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTEREST TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION ALL AREAS SHOWN IN SQUARE FEET UNLESS OTHERWISE NOTED



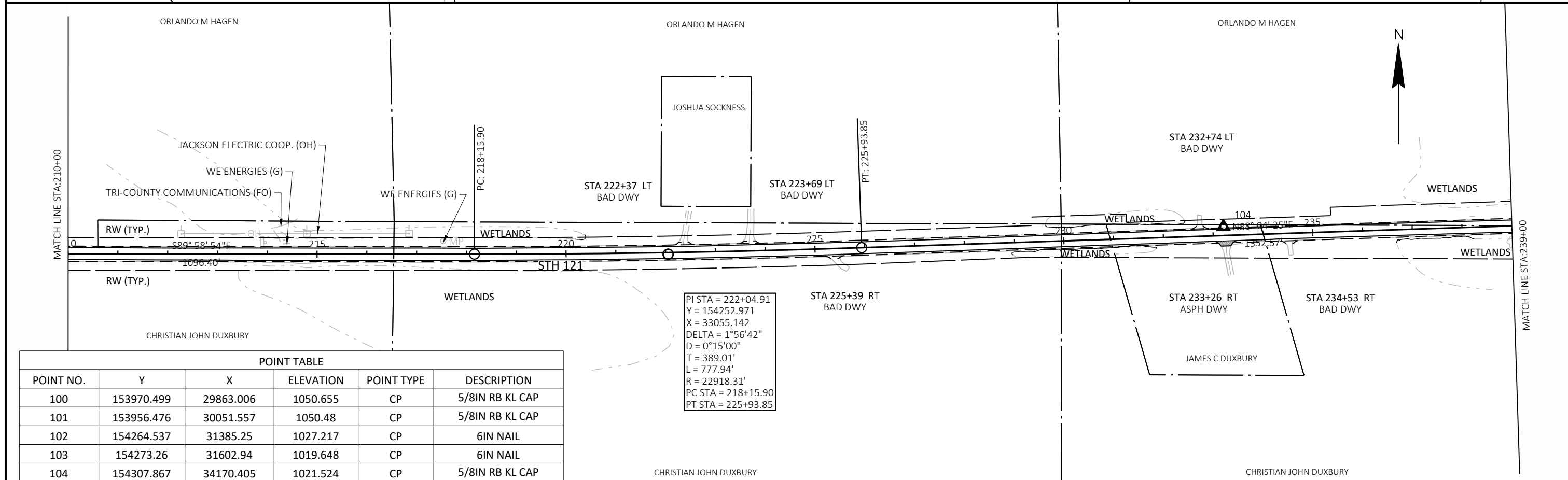
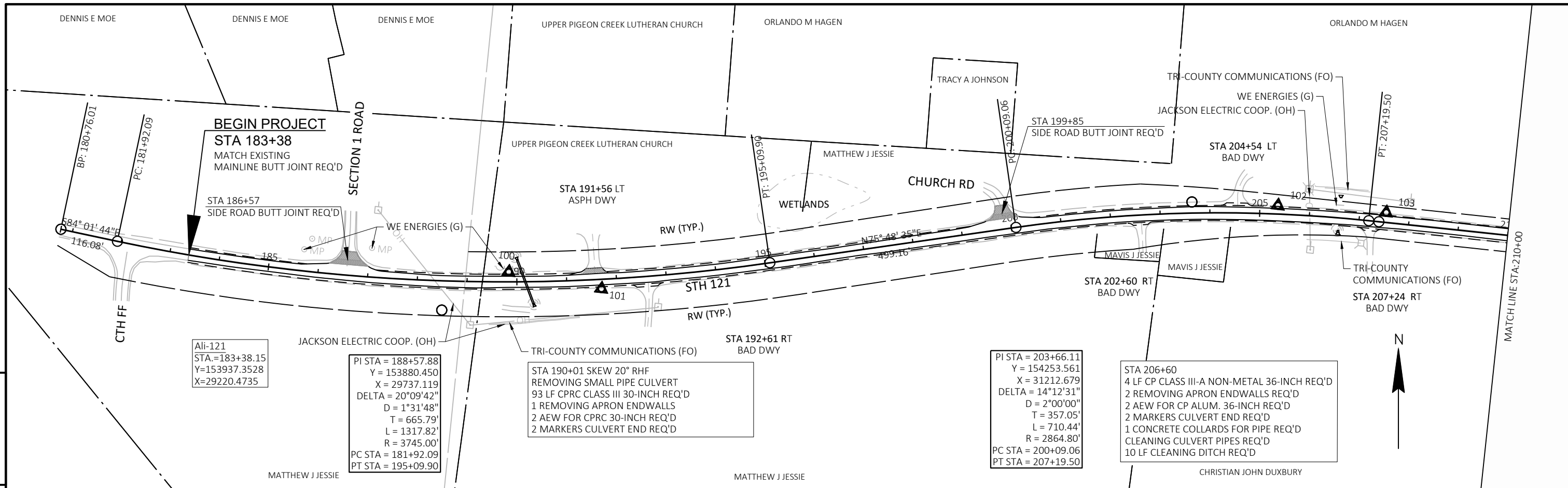
I, TIMOTHY M. HELD, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



SIGNATURE: *T. M. Held* DATE: 05/18/2021
PRINT NAME: TIMOTHY M. HELD
REGISTRATION NUMBER: 5-2591

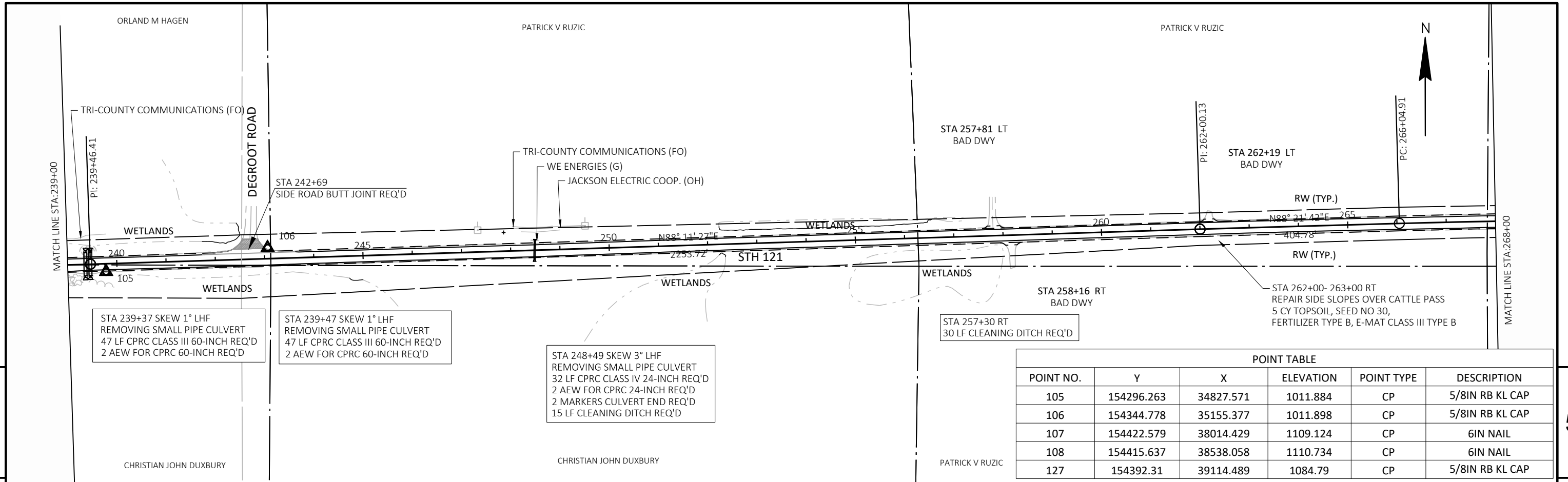
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE DEPARTMENT.

SIGNATURE: *Heather L. Dresel* DATE: 5/18/2021
PRINT NAME: HEATHER L. DRESEL



POINT TABLE					
POINT NO.	Y	X	ELEVATION	POINT TYPE	DESCRIPTION
100	153970.499	29863.006	1050.655	CP	5/8IN RB KL CAP
101	153956.476	30051.557	1050.48	CP	5/8IN RB KL CAP
102	154264.537	31385.25	1027.217	CP	6IN NAIL
103	154273.26	31602.94	1019.648	CP	6IN NAIL
104	154307.867	34170.405	1021.524	CP	5/8IN RB KL CAP

PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON PLAN SHEETS SHEET E



STA 239+37 SKEW 1° LHF
REMOVING SMALL PIPE CULVERT
47 LF CPRC CLASS III 60-INCH REQ'D
2 AEW FOR CPRC 60-INCH REQ'D

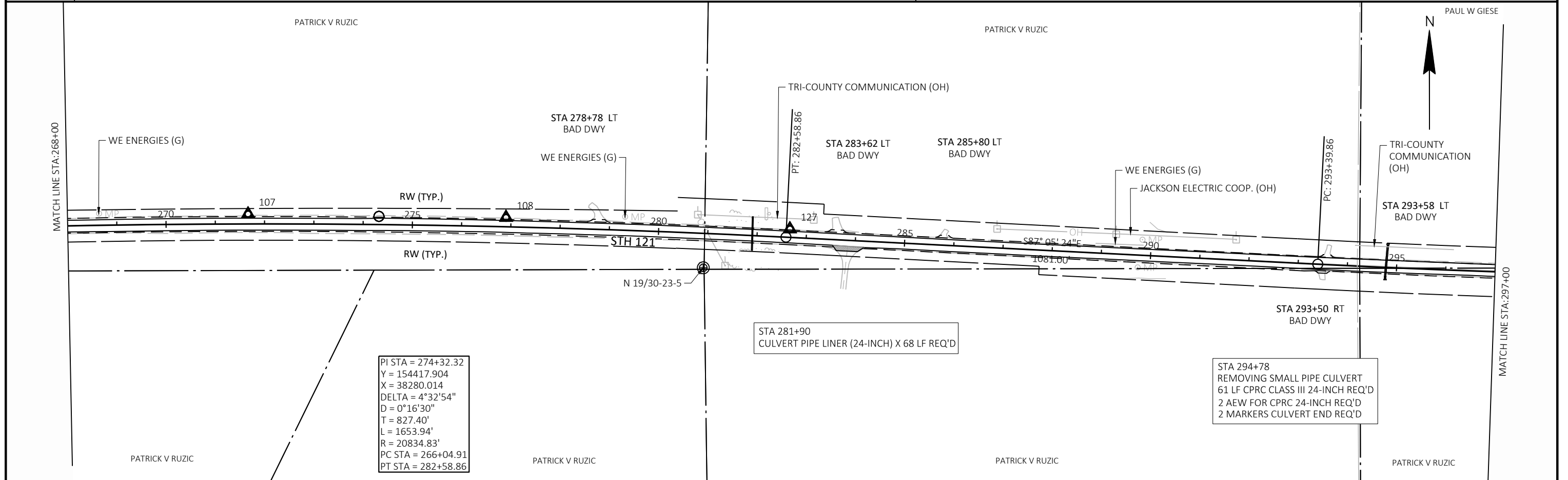
STA 239+47 SKEW 1° LHF
REMOVING SMALL PIPE CULVERT
47 LF CPRC CLASS III 60-INCH REQ'D
2 AEW FOR CPRC 60-INCH REQ'D

STA 248+49 SKEW 3° LHF
REMOVING SMALL PIPE CULVERT
32 LF CPRC CLASS IV 24-INCH REQ'D
2 AEW FOR CPRC 24-INCH REQ'D
2 MARKERS CULVERT END REQ'D
15 LF CLEANING DITCH REQ'D

STA 257+30 RT
30 LF CLEANING DITCH REQ'D

STA 262+00- 263+00 RT
REPAIR SIDE SLOPES OVER CATTLE PASS
5 CY TOPSOIL, SEED NO 30,
FERTILIZER TYPE B, E-MAT CLASS III TYPE B

POINT TABLE					
POINT NO.	Y	X	ELEVATION	POINT TYPE	DESCRIPTION
105	154296.263	34827.571	1011.884	CP	5/8IN RB KL CAP
106	154344.778	35155.377	1011.898	CP	5/8IN RB KL CAP
107	154422.579	38014.429	1109.124	CP	6IN NAIL
108	154415.637	38538.058	1110.734	CP	6IN NAIL
127	154392.31	39114.489	1084.79	CP	5/8IN RB KL CAP



PI STA = 274+32.32
Y = 154417.904
X = 38280.014
DELTA = 4°32'54"
D = 0°16'30"
T = 827.40'
L = 1653.94'
R = 20834.83'
PC STA = 266+04.91
PT STA = 282+58.86

STA 281+90
CULVERT PIPE LINER (24-INCH) X 68 LF REQ'D

STA 294+78
REMOVING SMALL PIPE CULVERT
61 LF CPRC CLASS III 24-INCH REQ'D
2 AEW FOR CPRC 24-INCH REQ'D
2 MARKERS CULVERT END REQ'D

PAUL W GIESE

TONI M LARSON-DAVIDSON

TONI M LARSON-DAVIDSON

POINT TABLE					
POINT NO.	Y	X	ELEVATION	POINT TYPE	DESCRIPTION
109	154370.195	45148.367	1050.558	CP	5/8IN RB KL CAP
110	154335.717	45568.466	1045.181	CP	5/8IN RB KL CAP



MATCH LINE STA:297+00

MATCH LINE STA:326+00

STA 301+36 LT
BAD DWY

STA 323+34 LT
BAD DWY

PI STA = 298+49.40
Y = 154295.153
X = 40694.843
DELTA = 2°32'50"
D = 0°15'00"
T = 509.54'
L = 1018.91'
R = 22918.31'
PC STA = 293+39.86
PT STA = 303+58.77

STA 305+74
CULVERT PIPE LINER (24-INCH) X 48 LF REQ'D
2 AEW FOR CP ALUM 24-INCH REQ'D
2 MARKERS CULVERT END REQ'D

STA 305+88 RT
BAD DWY

STA 307+72
SIDE ROAD
MILLED BUTT JOINT REQ'D

STA 314+08 SKEW 3° LHF
REMOVING SMALL PIPE CULVERT
40 LF CPRC CLASS IV 30-INCH REQ'D
2 AEW FOR CPRC 24-INCH REQ'D
2 MARKERS CULVERT END REQ'D

STA 320+13 RT
BAD DWY

STA. 324+85
REPAIR SLOPE
5 CY TOPSOIL, SEED & EMAT

PATRICK V RUZIC

MICHAEL HARPER

TONI DAVIDSON

TONI DAVIDSON

5

5

TONI M LARSON-DAVIDSON

PAUL W GIESE

PAUL W GIESE

WIS DNR

MATCH LINE STA:326+00



PC: 328+44.45

STA 332+92 LT
ASPH DWY

PT: 332+95.77

STA 338+94 LT
BAD DWY

PT: 337+61.29

STA 343+39 LT
ASPH DWY

PC: 343+34.93

STA 346+50 - STA 347+50 LT
CLEANING DITCH REQ'D

WE ENERGIES (G)

WE ENERGIES (G)

ROGER L JOHNSON

XCEL ENERGY (OH)

JACKSON ELECTRIC COOP. (OH)

PRW

PT: 348+35.36

PC: 350+61.97

STA 352+40 LT
BAD DWY

CENTURYLINK (T)

STA 342+92, 22.2' RT
AEW FOR CP ALUM. 24-INCH
1 REMOVING APRON ENDWALL
1 MARKERS CULVERT END REQ'D

STA 344+62 RT
BAD DWY

STA 345+79 RT
BAD DWY

STA 347+13, 42.0' RT
AEW FOR CP ALUM. 24-INCH
REMOVE 2' OF CP
1 MARKERS CULVERT END REQ'D

STA 328+28 RT
ASPH DWY

PI STA = 330+70.22
Y = 154269.599
X = 43915.720
DELTA = 4°30'47"
D = 1°00'00"
T = 225.77'
L = 451.32'
R = 5729.58'
PC STA = 328+44.45
PT STA = 332+95.77

TONI DAVIDSON

TONI DAVIDSON

PI STA = 345+86.14
Y = 154374.812
X = 45428.203
DELTA = 12°30'39"
D = 2°30'00"
T = 251.22'
L = 500.43'
R = 2291.83'
PC STA = 343+34.93
PT STA = 348+35.36

PI STA = 353+31.88
Y = 154266.987
X = 46168.125
DELTA = 23°56'16"
D = 4°30'00"
T = 269.91'
L = 531.95'
R = 1273.24'
PC STA = 350+61.97
PT STA = 355+93.92

PAUL W GIESE

PROJECT NO: 7505-00-76

HWY: STH 121

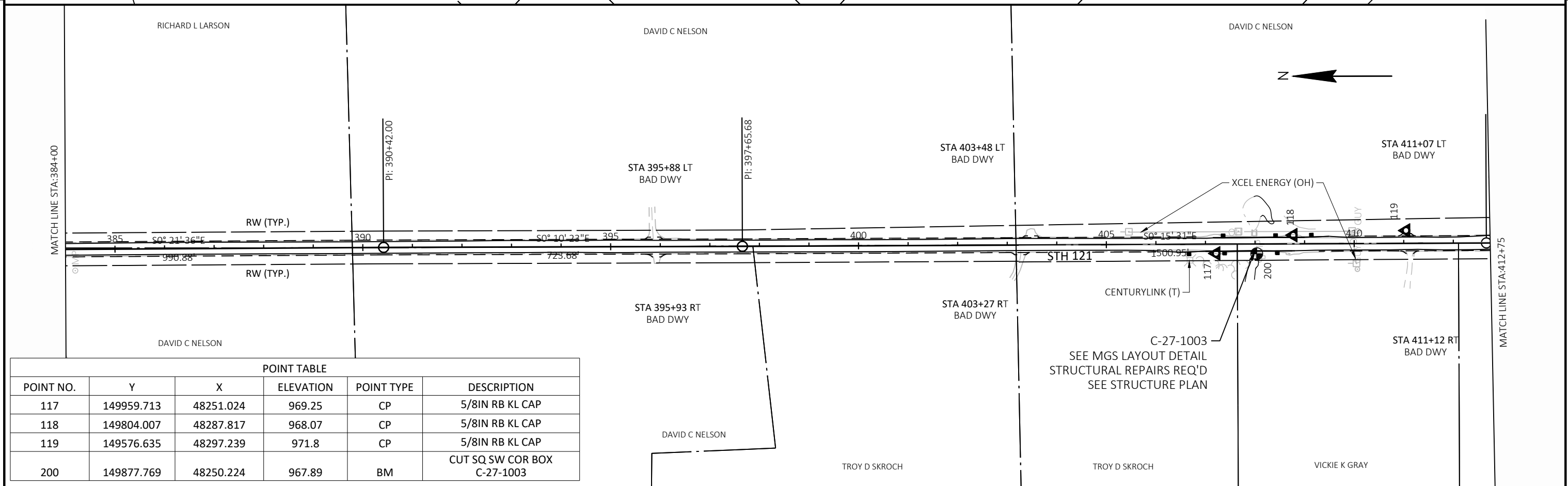
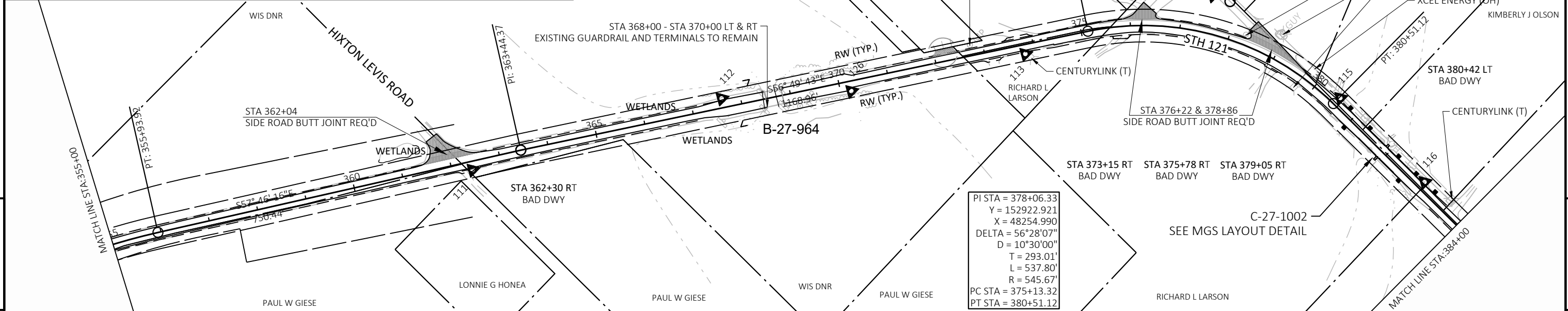
COUNTY: JACKSON

PLAN SHEETS

SHEET

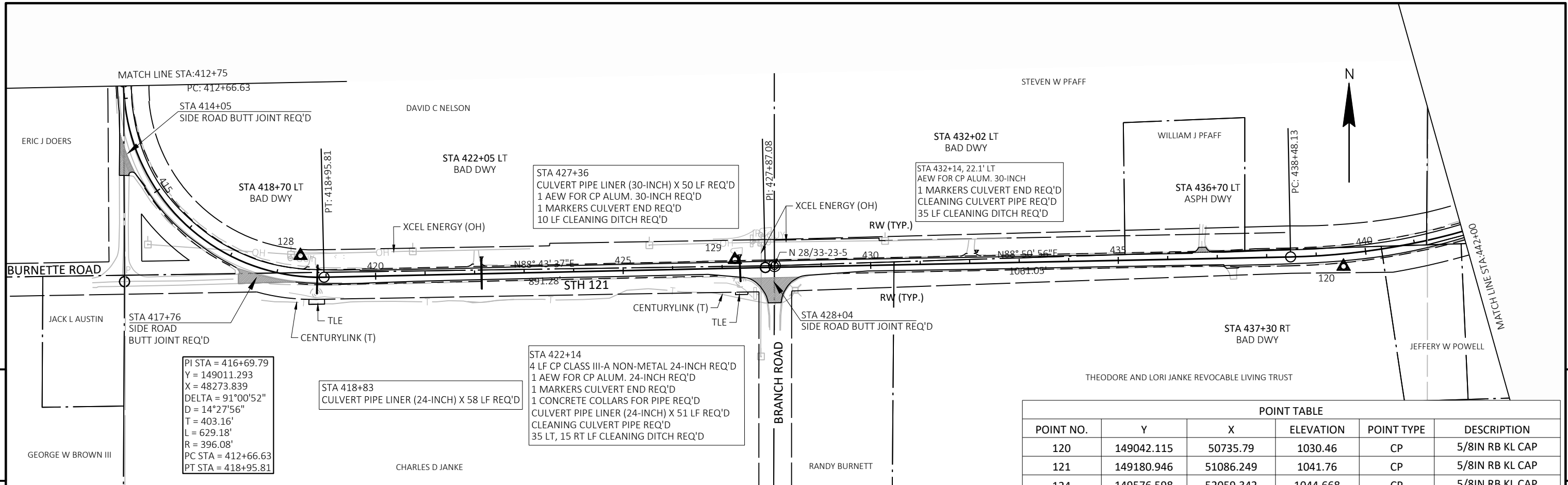
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POINT TABLE					
POINT NO.	Y	X	ELEVATION	POINT TYPE	DESCRIPTION
111	153763.465	46934.621	991.044	CP	5/8IN RB KL CAP
112	153509.987	47391.507	977.082	CP	5/8IN RB KL CAP
113	153140.188	47888.316	984.237	CP	5/8IN RB KL CAP
114	152955.944	48239.148	989.214	CP	5/8IN RB KL CAP
115	152628.081	48274.836	981.915	CP	5/8IN RB KL CAP
116	152388.024	48275.296	979.779	CP	5/8IN RB KL CAP
126	153335.553	47587.085	977.303	CP	5/8IN RB KL CAP

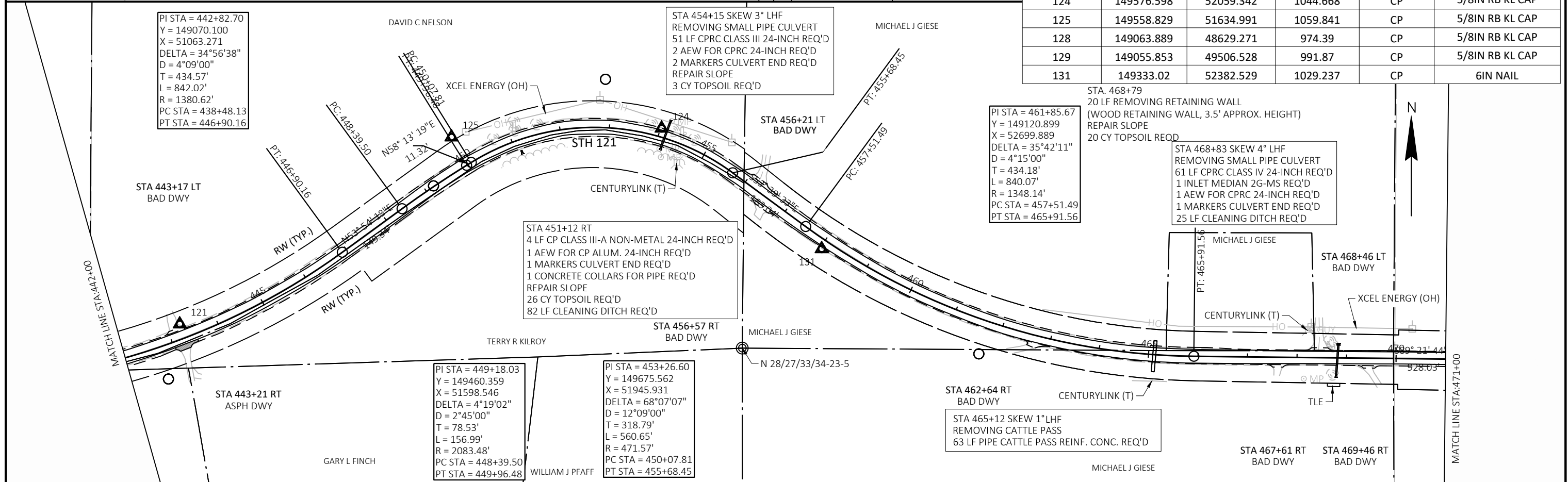


POINT TABLE					
POINT NO.	Y	X	ELEVATION	POINT TYPE	DESCRIPTION
117	149959.713	48251.024	969.25	CP	5/8IN RB KL CAP
118	149804.007	48287.817	968.07	CP	5/8IN RB KL CAP
119	149576.635	48297.239	971.8	CP	5/8IN RB KL CAP
200	149877.769	48250.224	967.89	BM	CUT SQ SW COR BOX C-27-1003

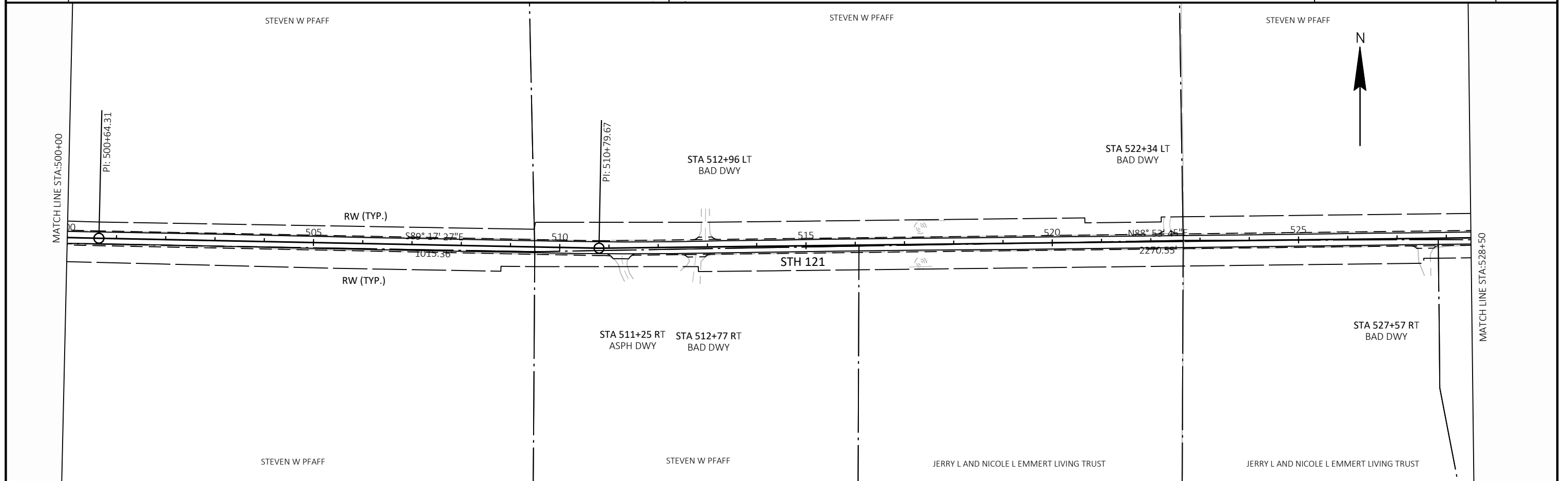
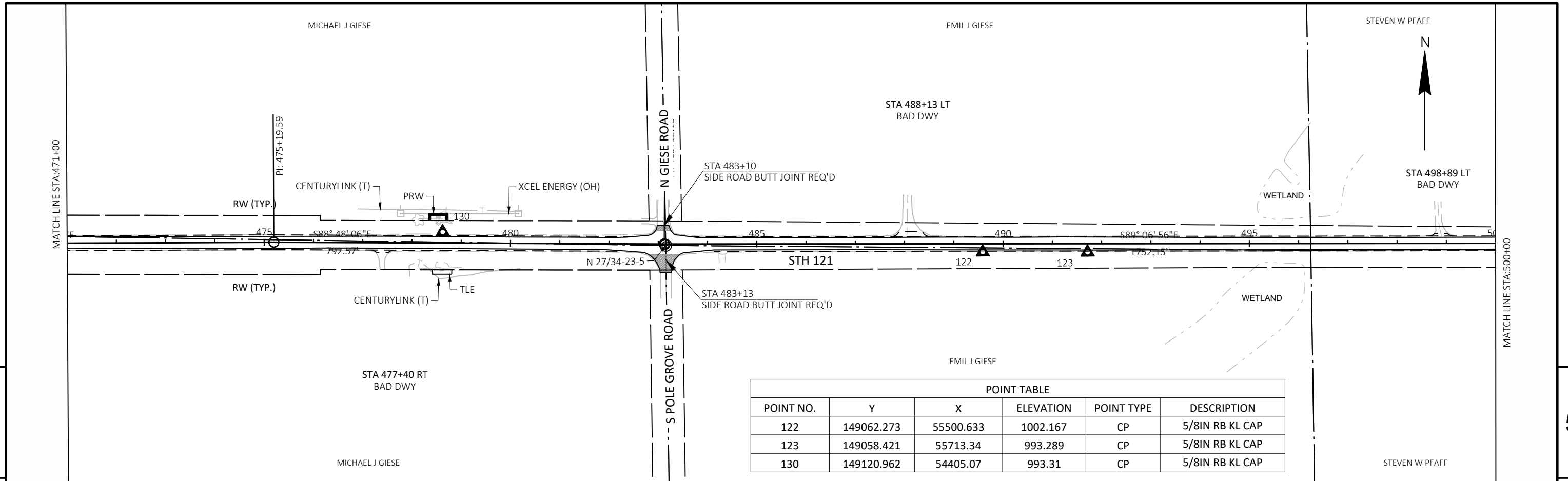
PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON PLAN SHEETS SHEET E



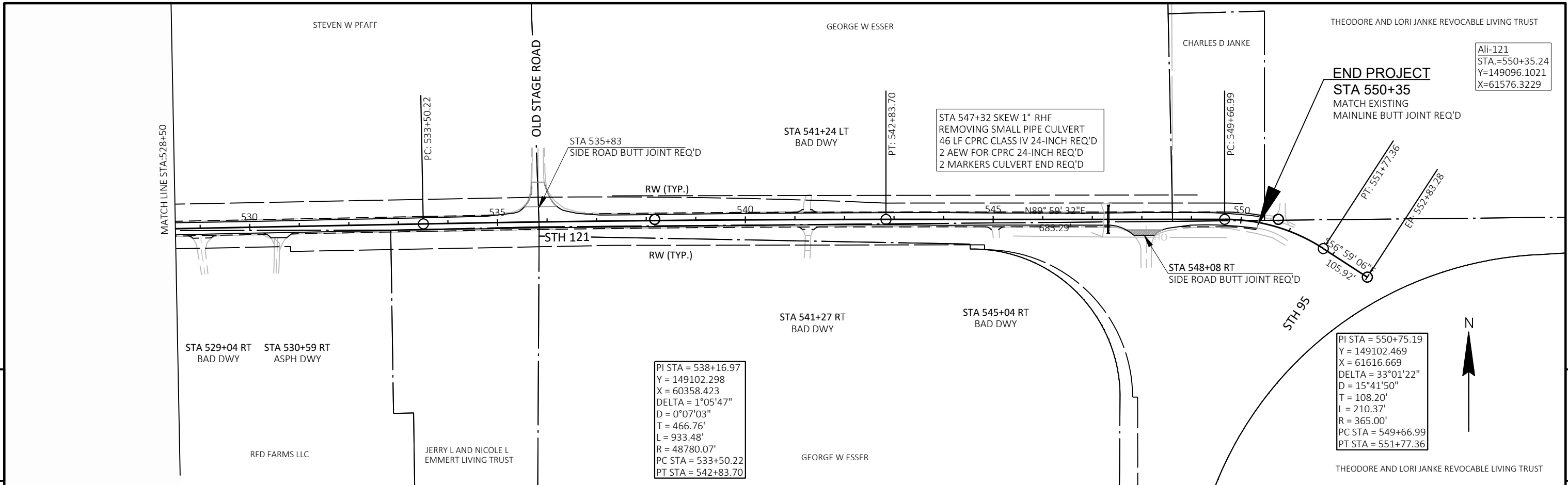
POINT TABLE					
POINT NO.	Y	X	ELEVATION	POINT TYPE	DESCRIPTION
120	149042.115	50735.79	1030.46	CP	5/8IN RB KL CAP
121	149180.946	51086.249	1041.76	CP	5/8IN RB KL CAP
124	149576.598	52059.342	1044.668	CP	5/8IN RB KL CAP
125	149558.829	51634.991	1059.841	CP	5/8IN RB KL CAP
128	149063.889	48629.271	974.39	CP	5/8IN RB KL CAP
129	149055.853	49506.528	991.87	CP	5/8IN RB KL CAP
131	149333.02	52382.529	1029.237	CP	6IN NAIL



PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON PLAN SHEETS SHEET E



PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON PLAN SHEETS SHEET E



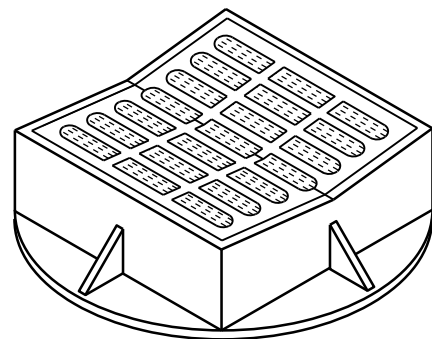
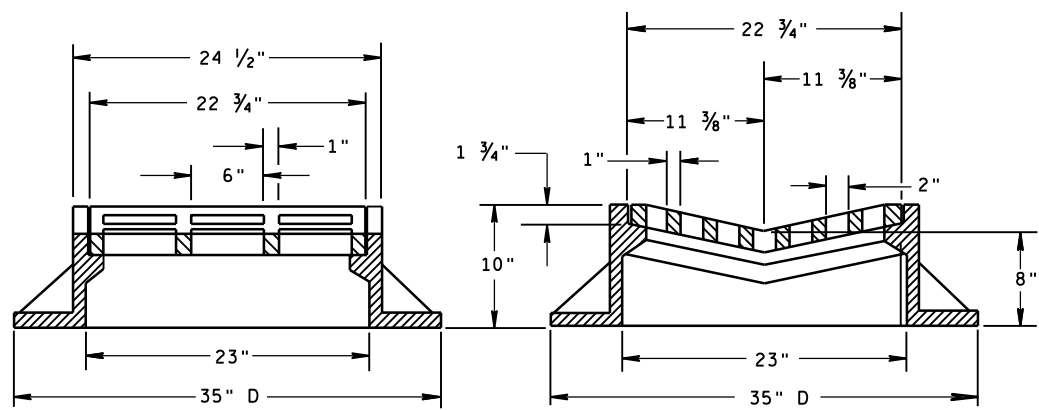
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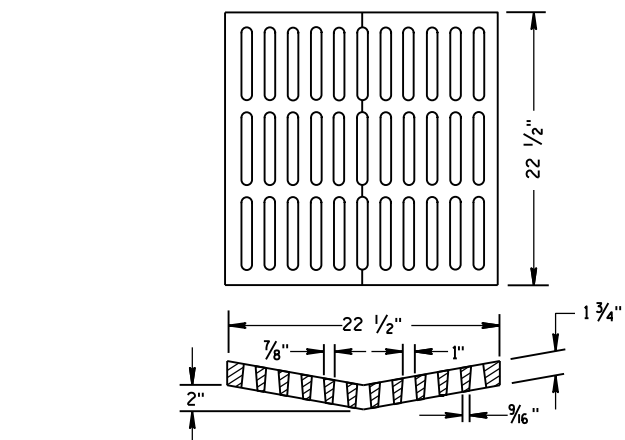
PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	PLAN SHEETS	SHEET	E
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Standard Detail Drawing List

08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08C08-02	INLETS MEDIAN 1 AND 2 GRATE
08D04-06	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E15-01	CULVERT PIPE CHECK
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F03-03	DETAILS FOR PIPE CATTLE PASS, CONCRETE ENDWALLS AND STEPS
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
13A08-01	ASPHALTIC RUMBLE STRIPS AT INTERSECTION
13A11-03A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-03B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13C19-03	HMA LONGITUDINAL JOINTS
14B42-07A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07D	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B43-04A	MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L)
14B43-04B	MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L)
14B43-04C	MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L)
14B44-04A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-08C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C08-20A	LONGITUDINAL MARKING (MAINLINE)
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-08	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-06A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C35-04A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY

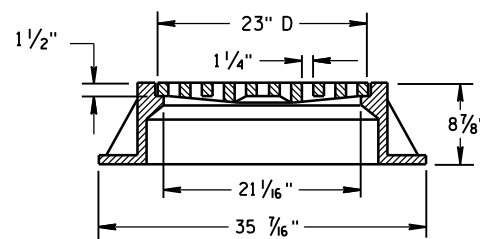
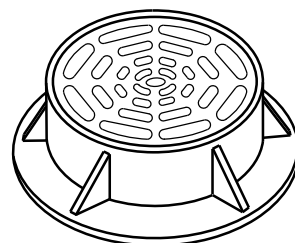
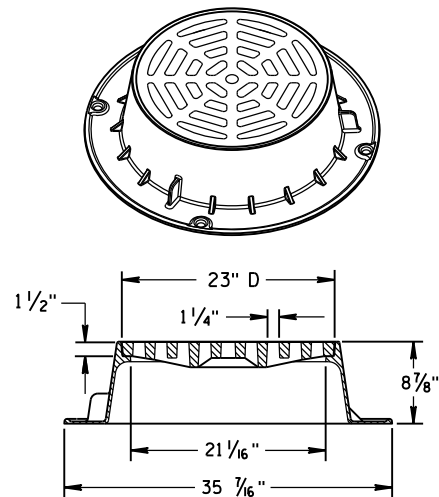


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

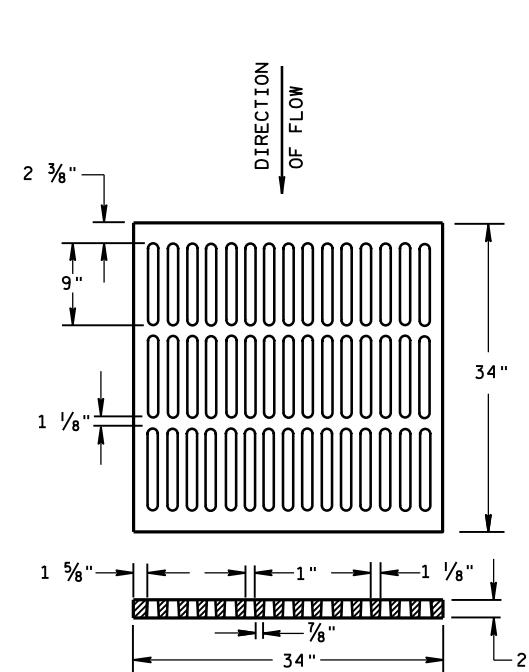
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

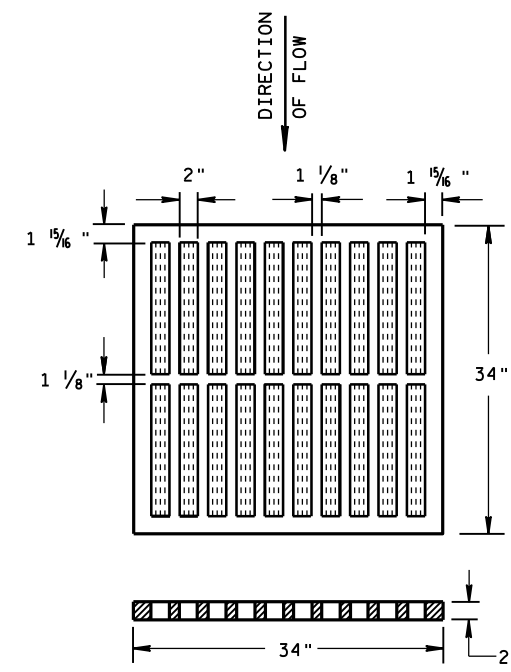
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



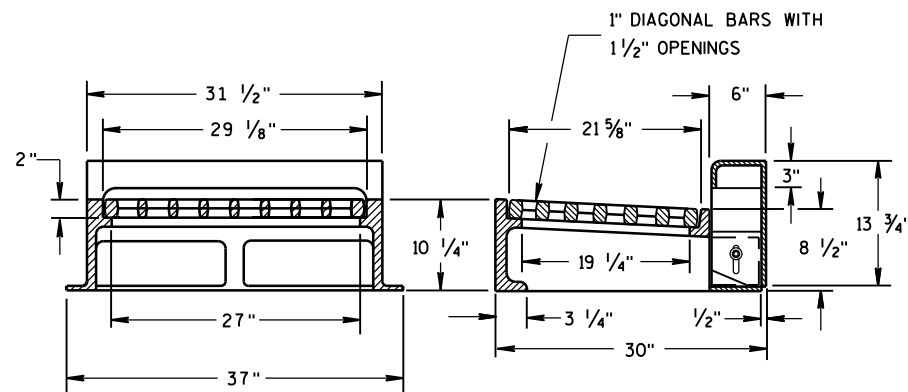
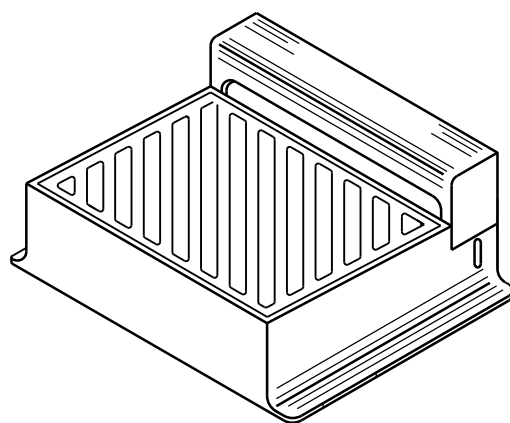
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE



TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

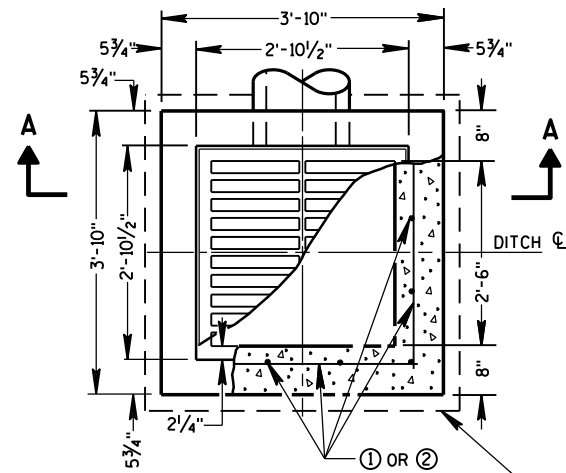
DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.

DIRECTION OF FLOW

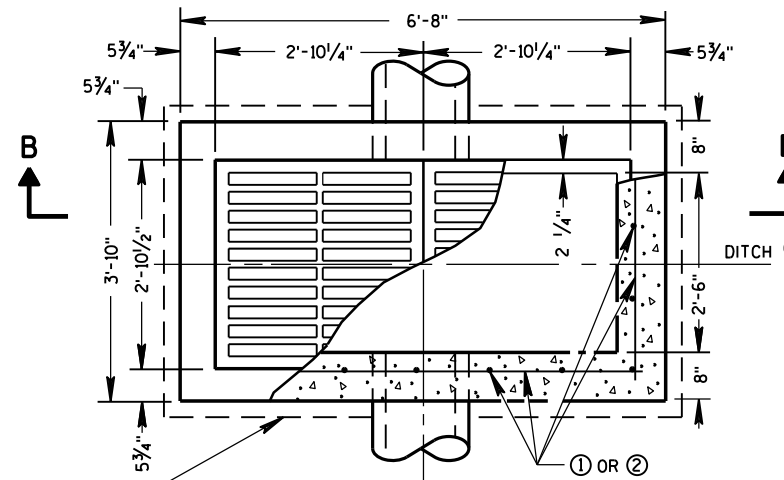
INLET COVERS
TYPE B, B-A, C,
MS, MS-A, & WM

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE 11/27/2013 /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

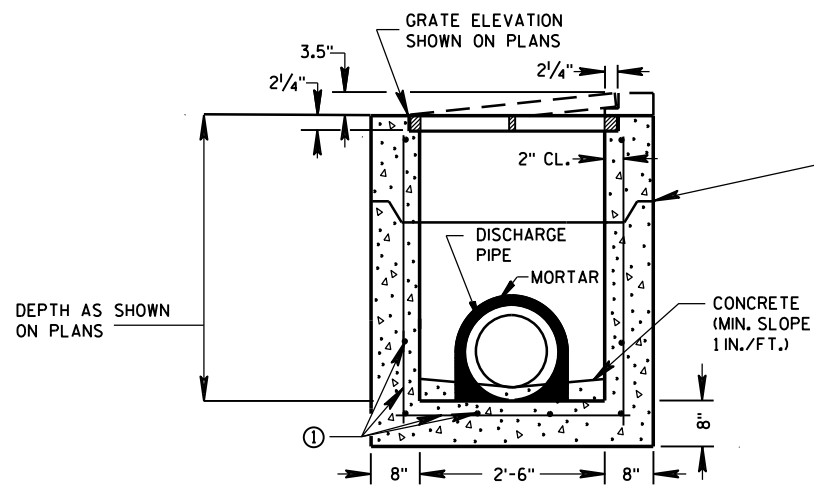


PLAN VIEW

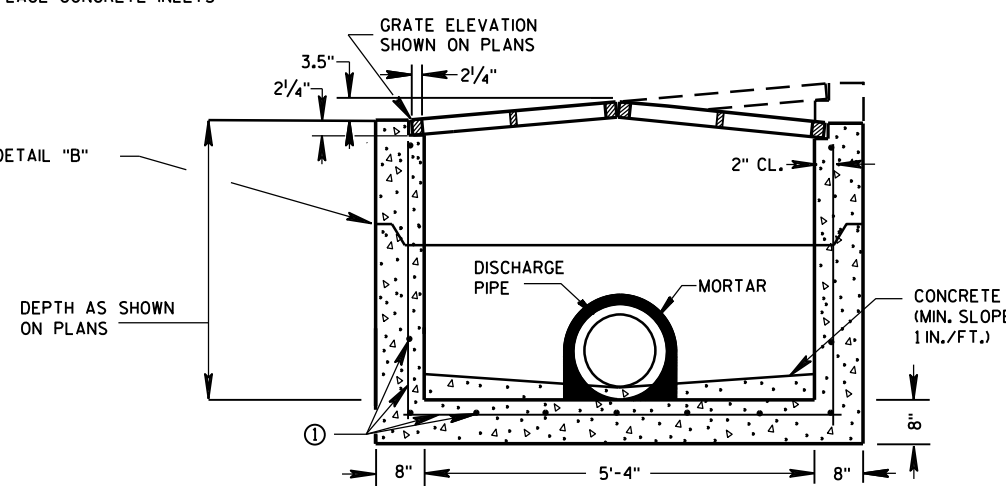


PLAN VIEW

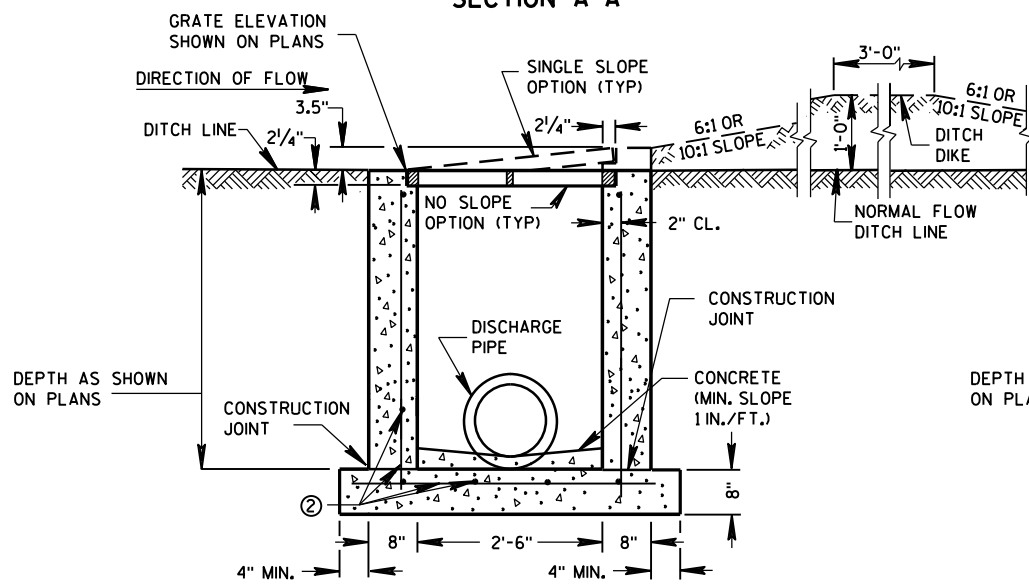
4" OVERHANGING BASE ON REINFORCED CAST-IN-PLACE CONCRETE INLETS



PRECAST REINFORCED CONCRETE SECTION A-A

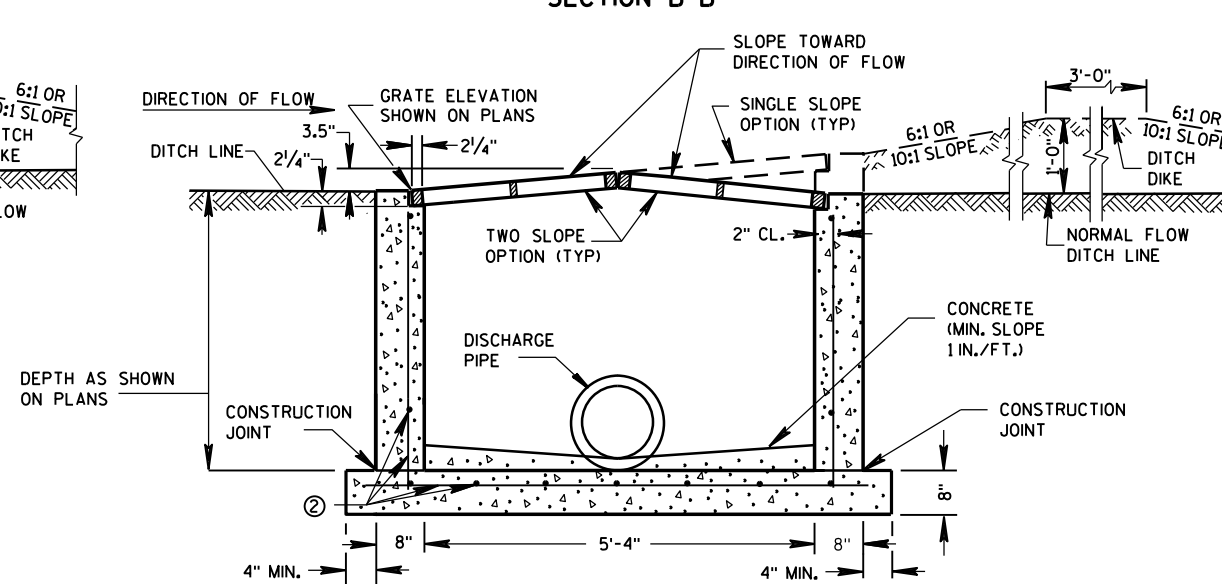


PRECAST REINFORCED CONCRETE SECTION B-B



REINFORCED CAST-IN-PLACE CONCRETE SECTION A-A

INLETS MEDIAN 1 GRATE



REINFORCED CAST-IN-PLACE CONCRETE SECTION B-B

INLETS MEDIAN 2 GRATE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLETS WHICH MAY INCLUDE PRECAST REINFORCED CONCRETE INLETS, SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL MEDIAN INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, IG-MS", ETC. THE FIRST NUMBER AND LETTER DESIGNATE THE TYPE OF STRUCTURE, AND THE FOLLOWING LETTERS DESIGNATE THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT. BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

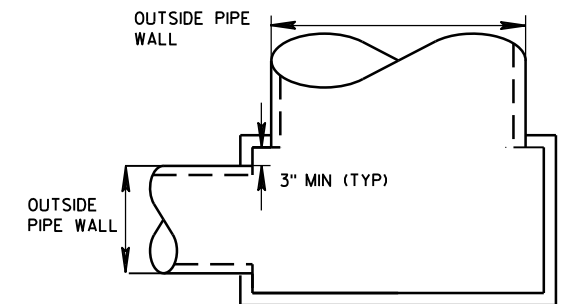
ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

- ① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

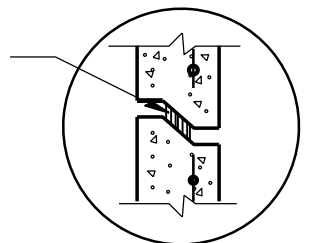
PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
1 GRATE	18	18
2 GRATE	18	42



DETAIL "A"

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



DETAIL "B"

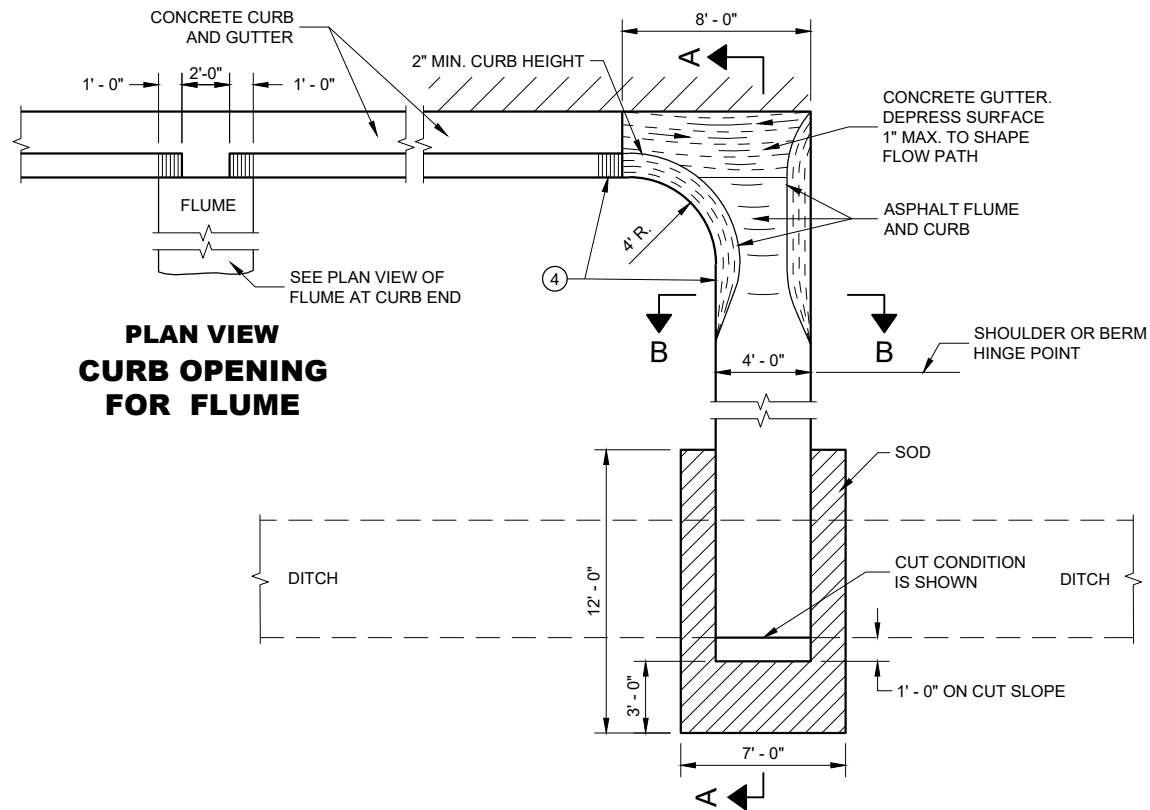
INLETS MEDIAN 1 AND 2 GRATE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept., 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR

NOTE: TAPER CURB ENDS TO GUTTER IN 1' - 0"

ASPHALTIC FLUME



**PLAN VIEW
CURB OPENING
FOR FLUME**

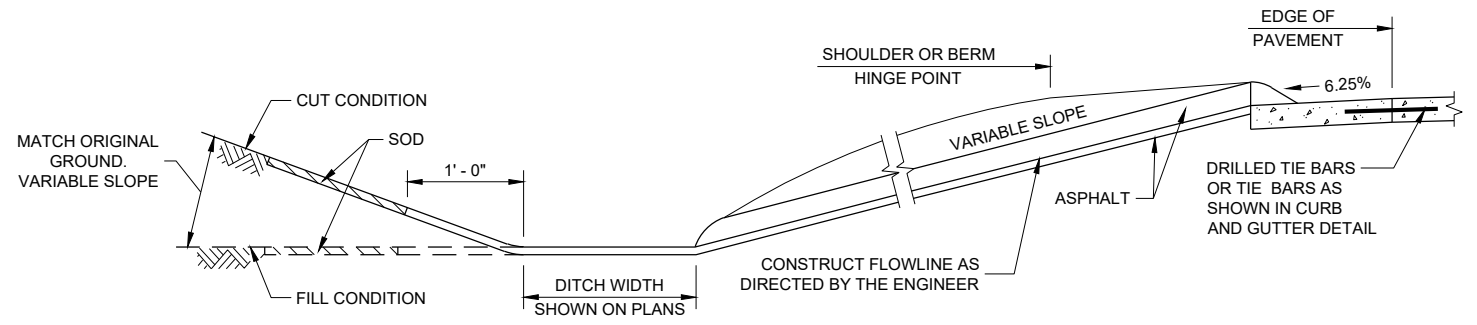
**PLAN VIEW
FLUME AT CURB END**

GENERAL NOTES

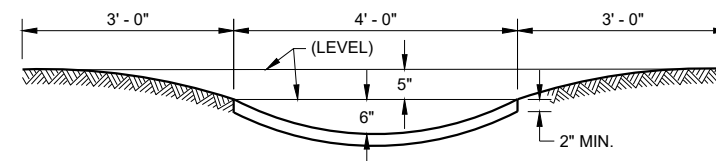
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

4" X 4" - W3.0 X W3.0 CONCRETE REINFORCEMENT SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

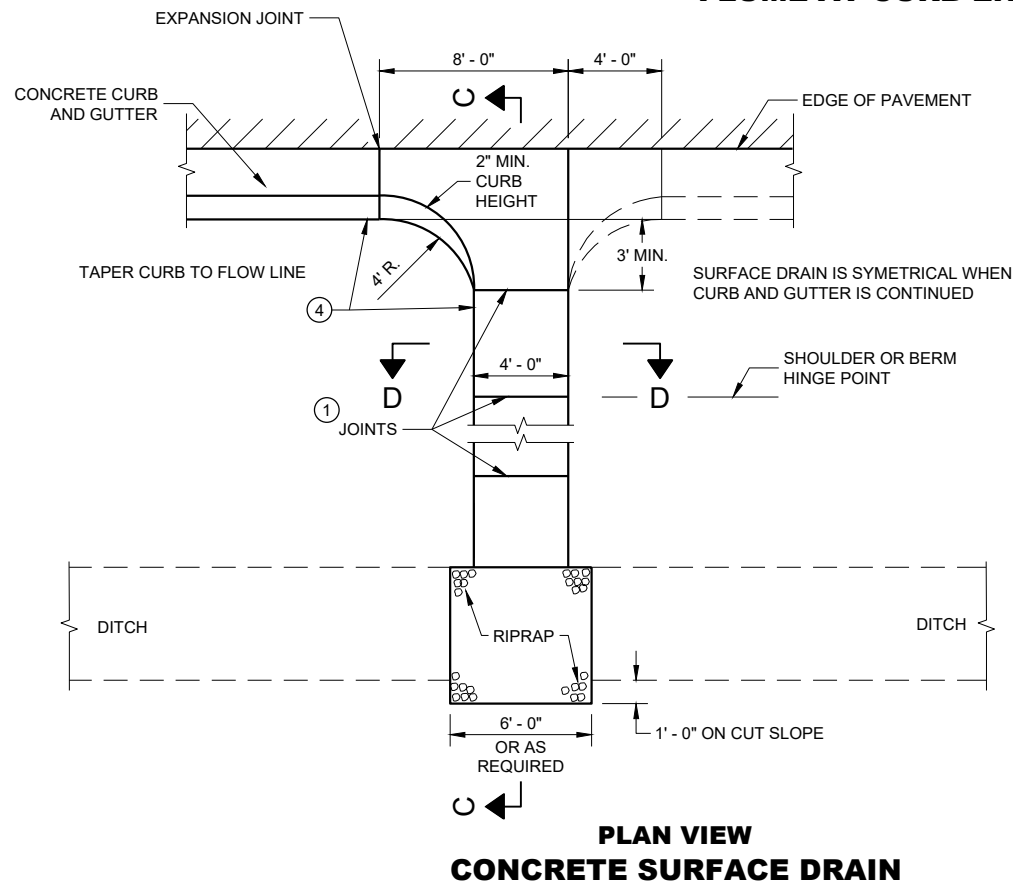
- ① JOINTS SHALL BE 1/8" TO 1/4" WIDE BY 1 1/2" DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
- ② GEOTEXTILE TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
- ③ CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED.
- ④ ANGLE OF FLUME IN RELATION TO BACK OF CURB TO BE CONSTRUCTED PER THE PLAN DETAILS OR AS DIRECTED BY THE ENGINEER. ANGLE OF FLUME MAY BE OTHER THAN 90 DEGREES AS SHOWN.



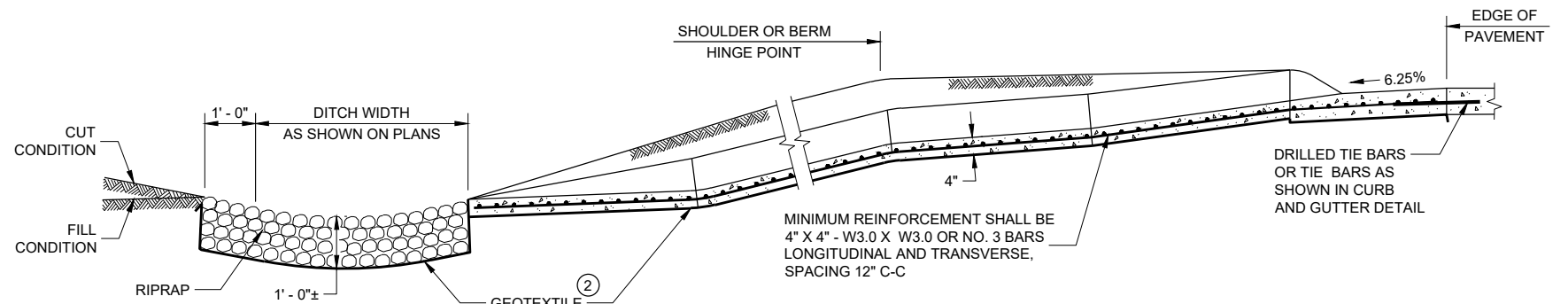
SECTION A - A



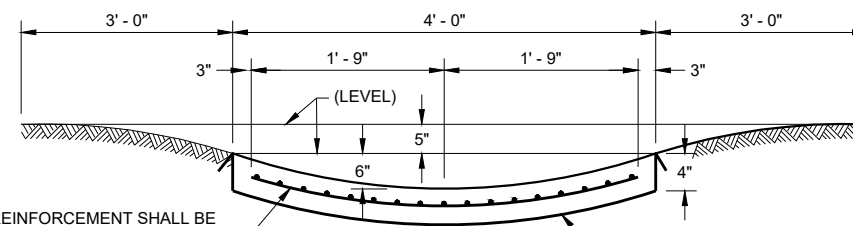
SECTION B - B



**PLAN VIEW
CONCRETE SURFACE DRAIN**



SECTION C - C



SECTION D - D

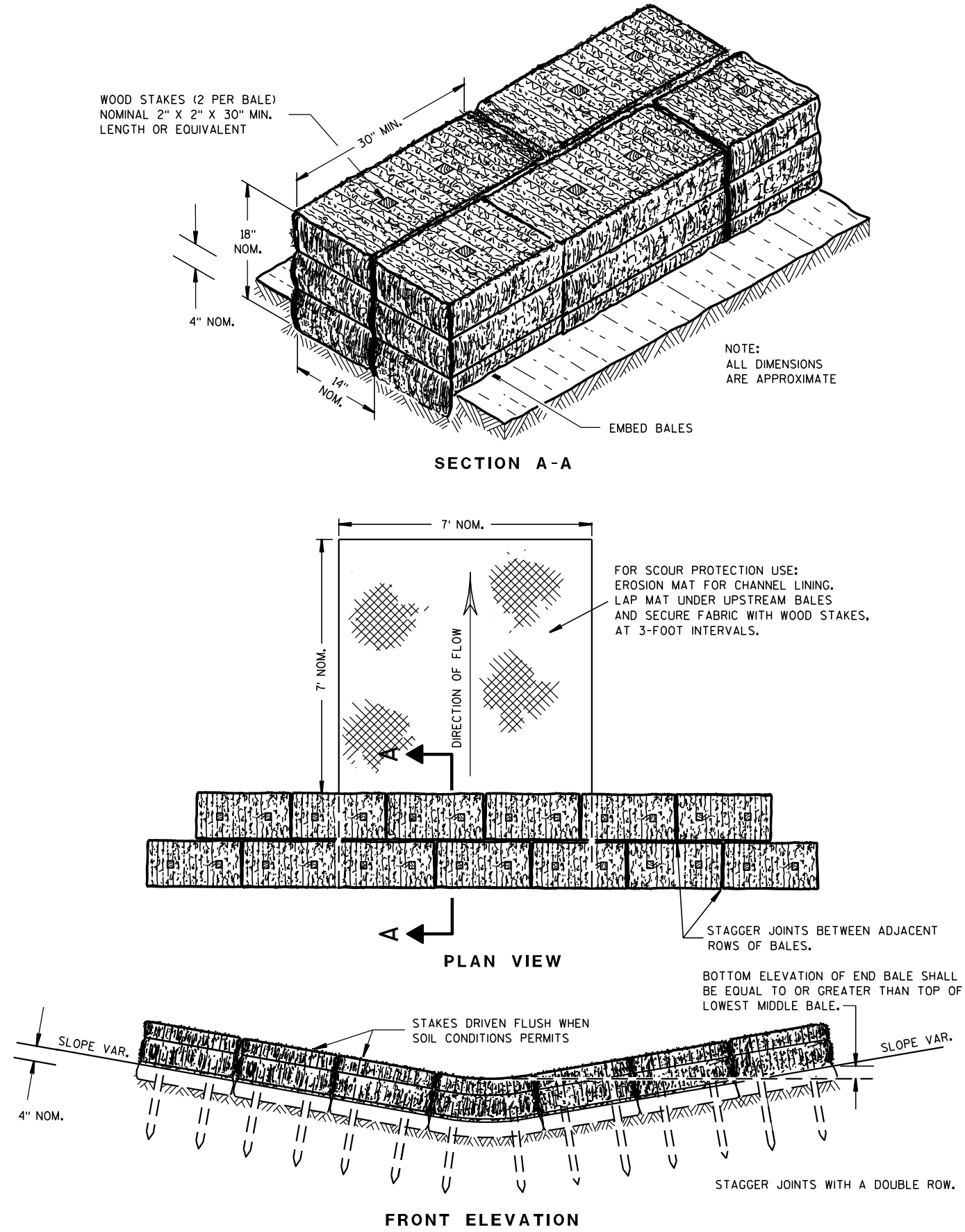
MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE, SPACING 12" C-C

CONCRETE SURFACE DRAINS AND ASPHALTIC FLUMES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA

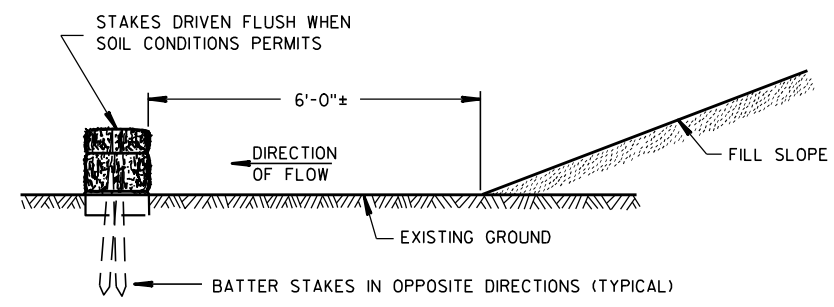
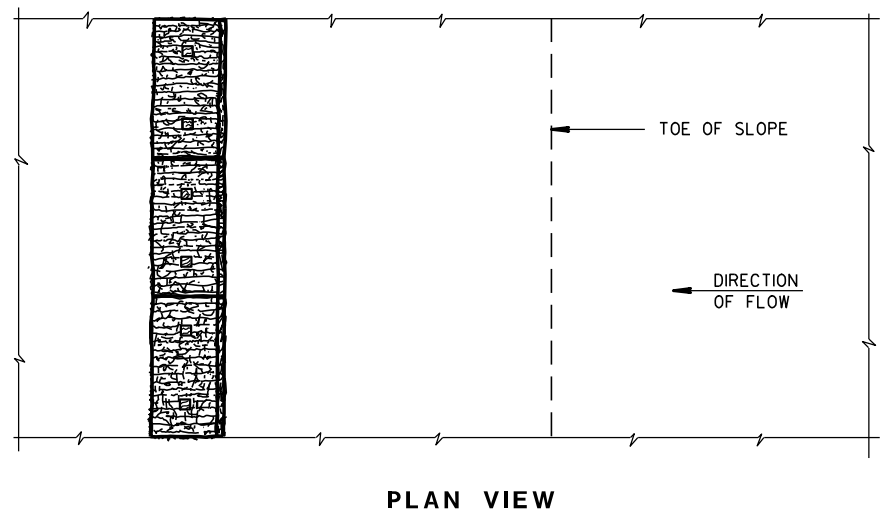
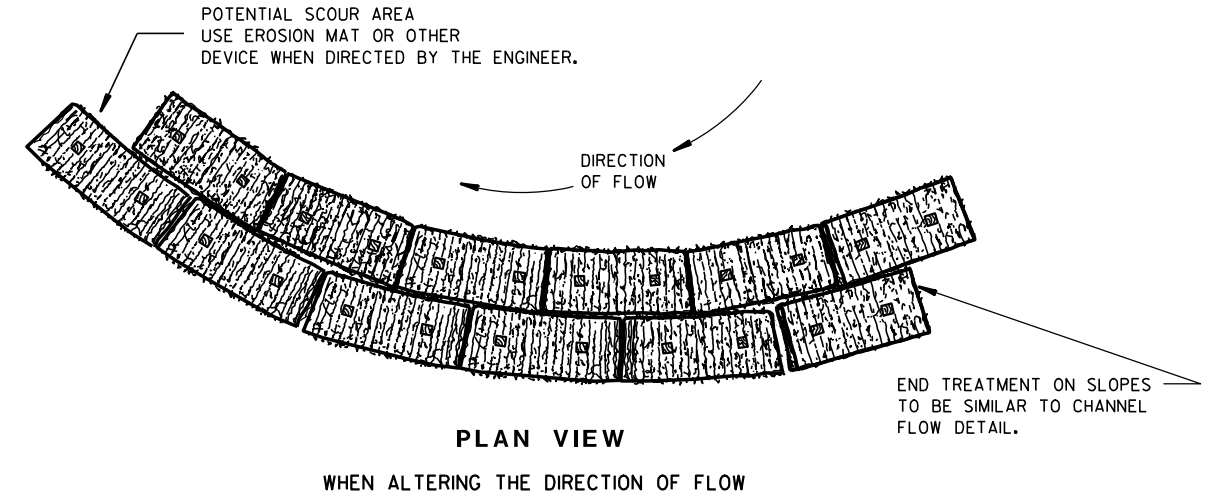


TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

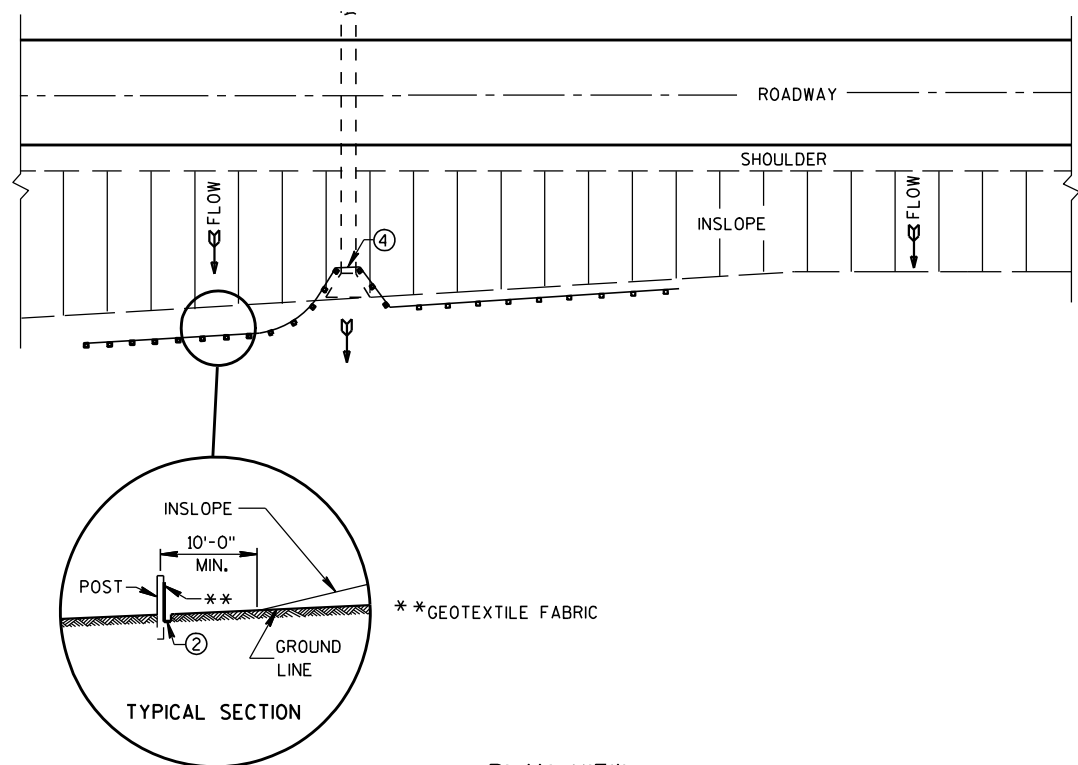


EROSION BALES FOR SHEET FLOW

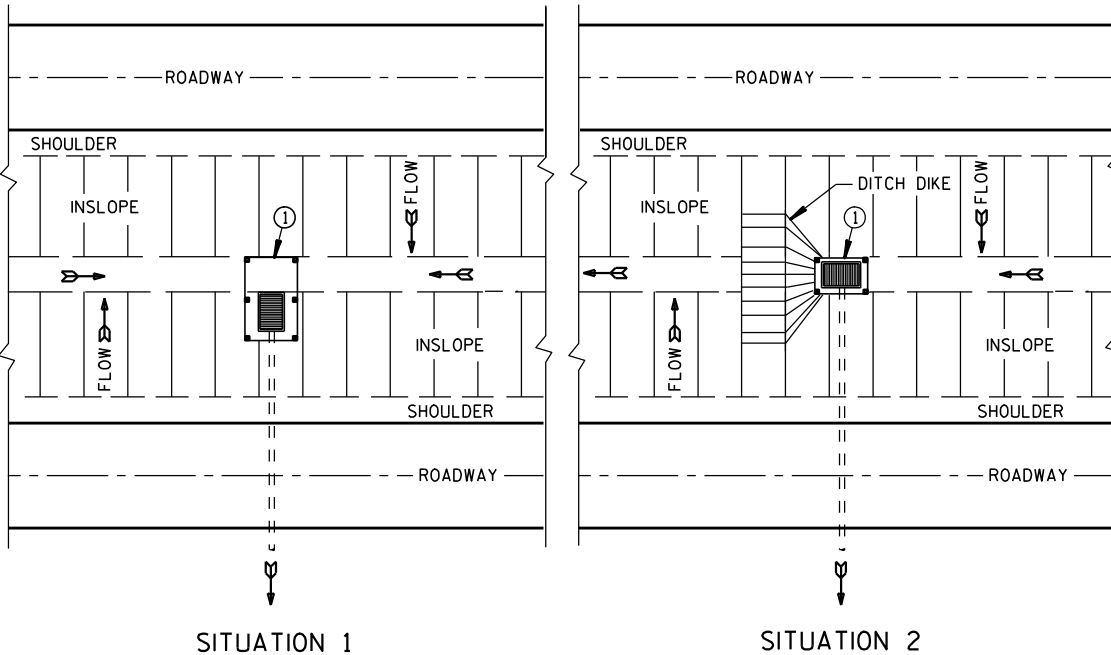
TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED
 6/04/02 /S/ Beth Canestra
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
 FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

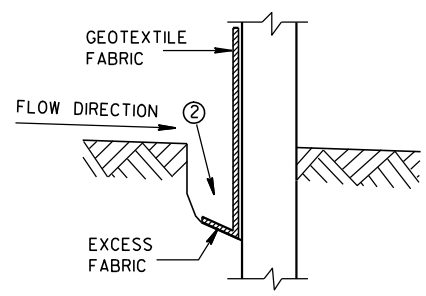


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

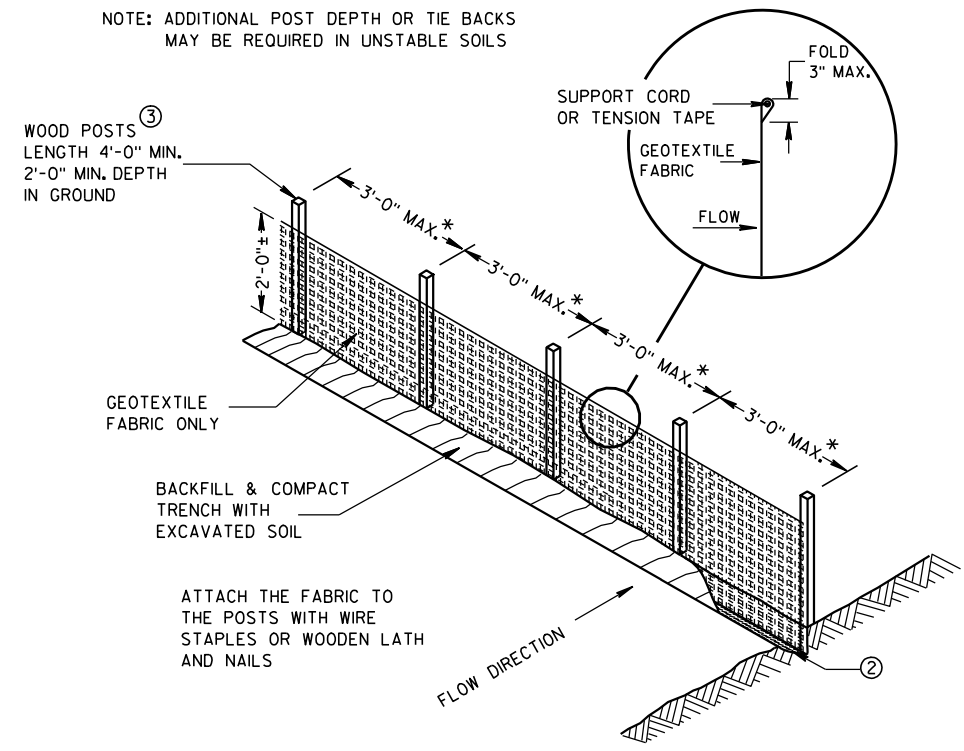
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



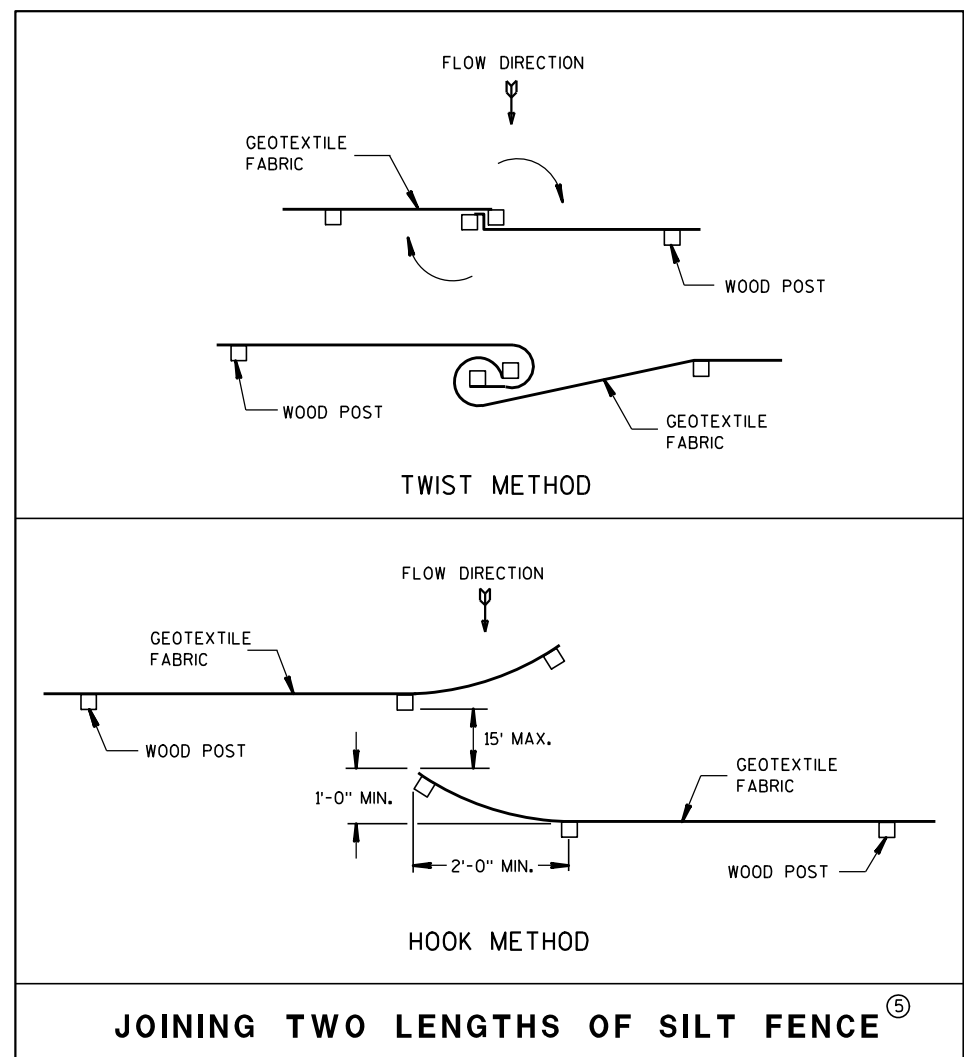
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

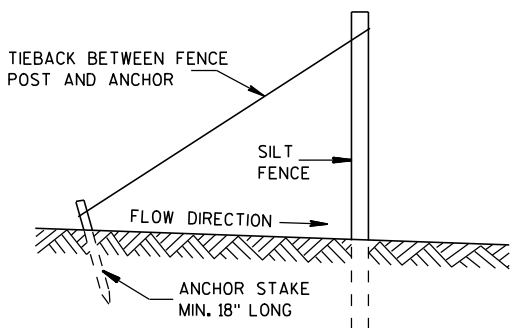


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

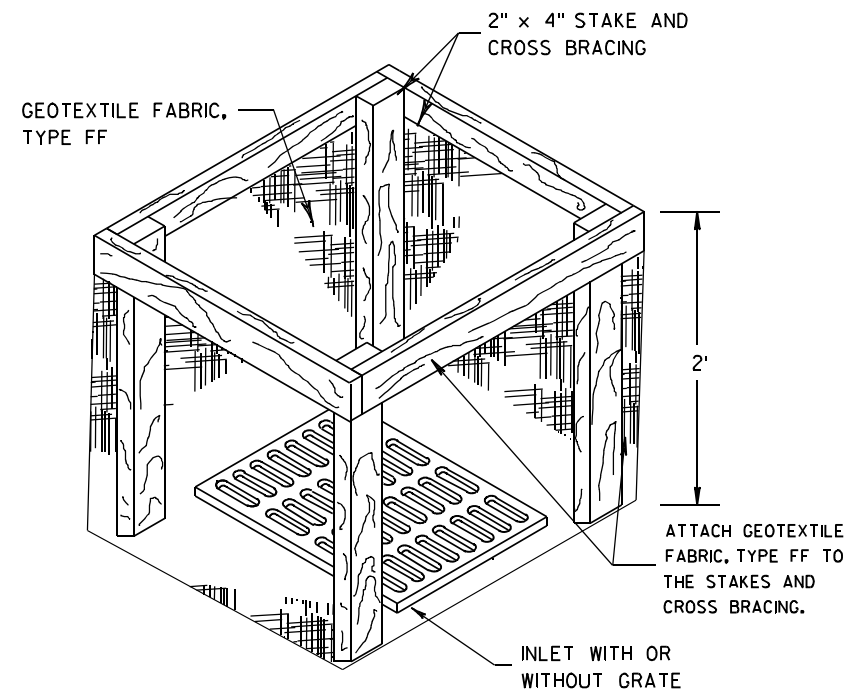
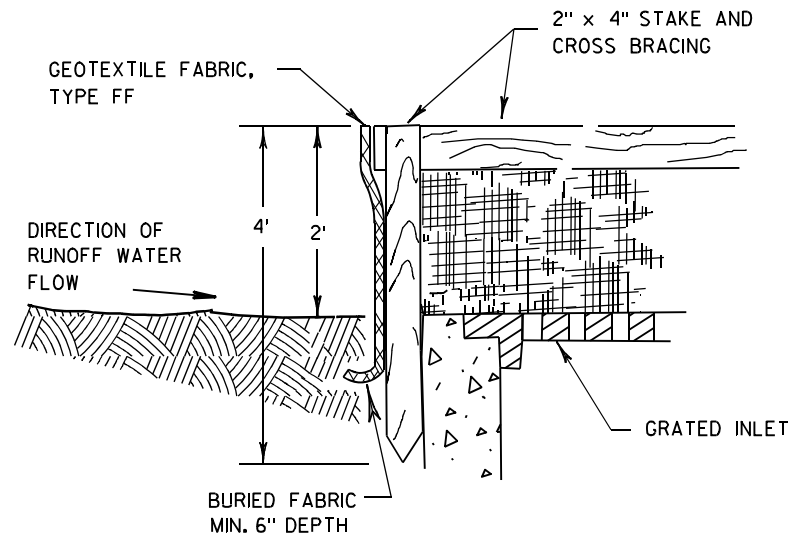


JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Canestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



INLET PROTECTION, TYPE A

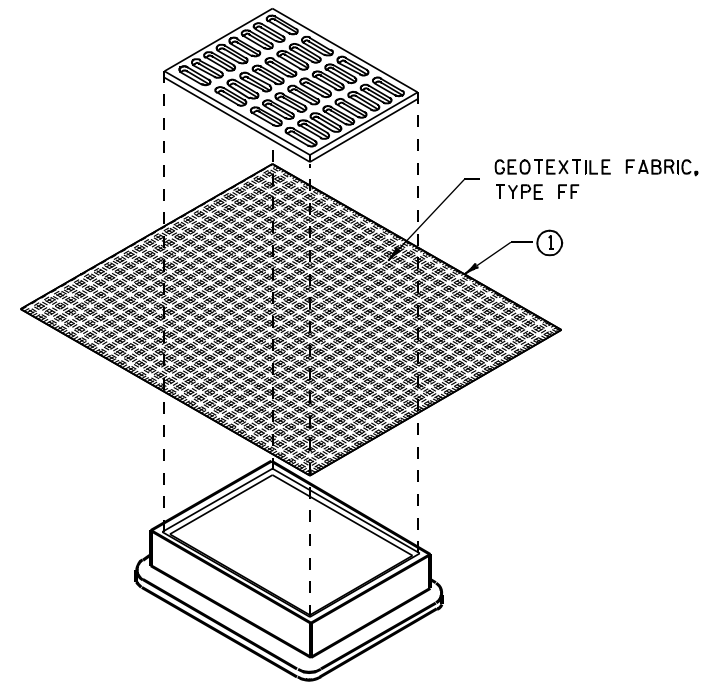
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

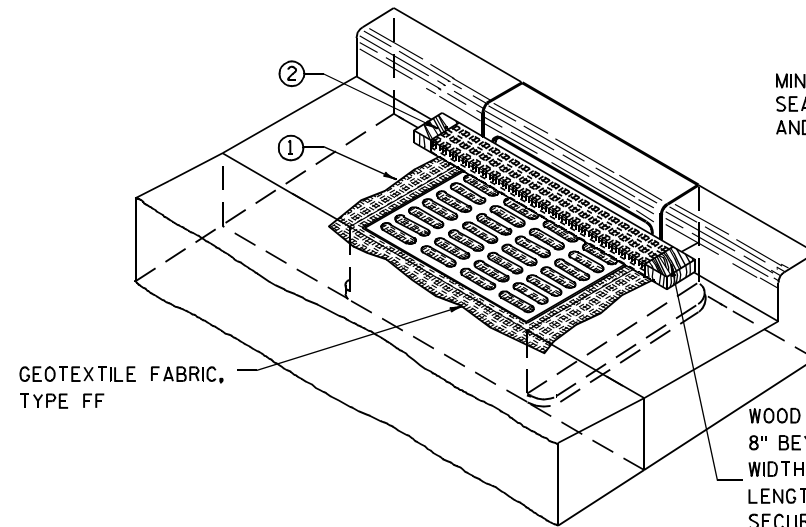
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

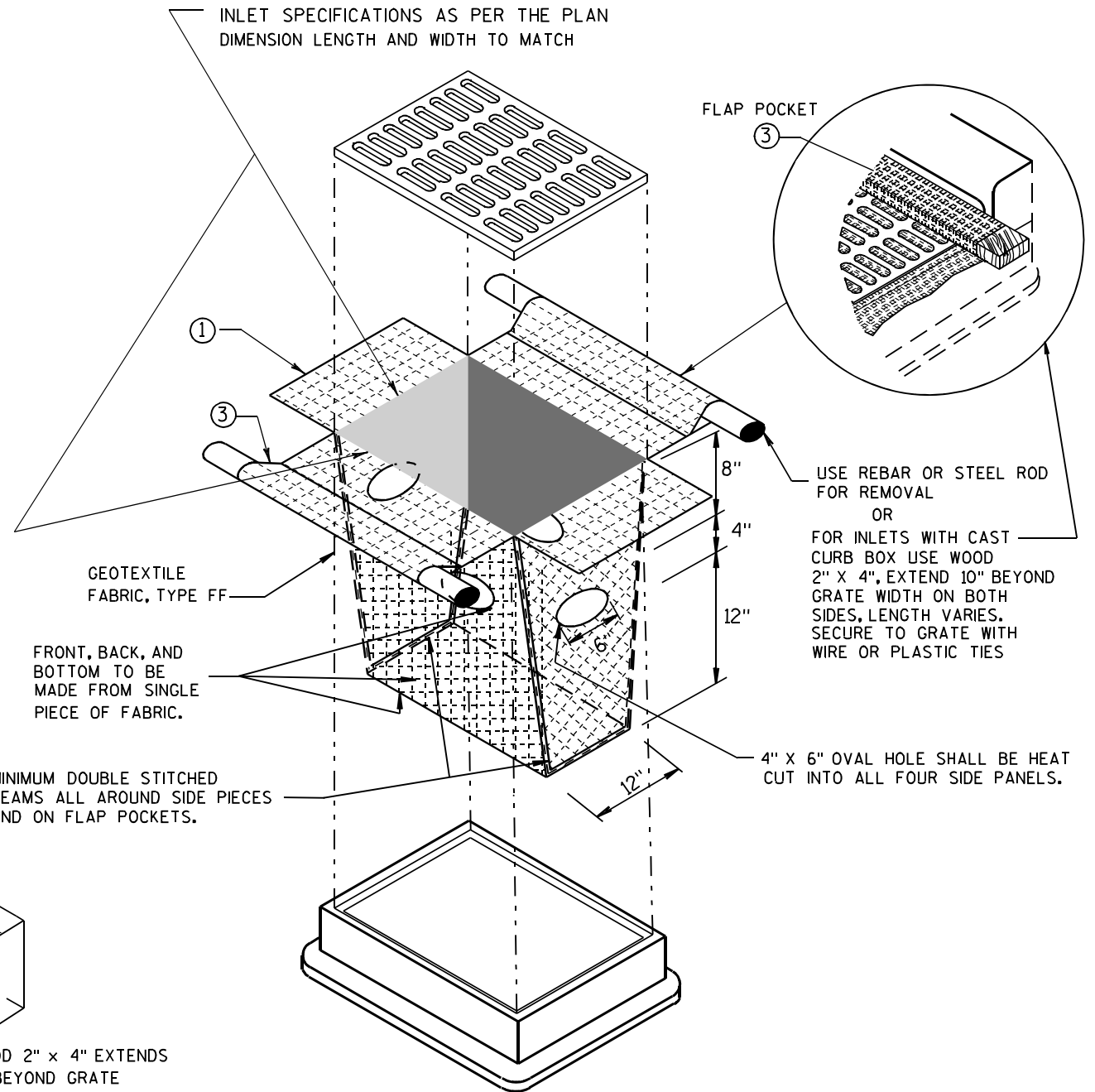
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

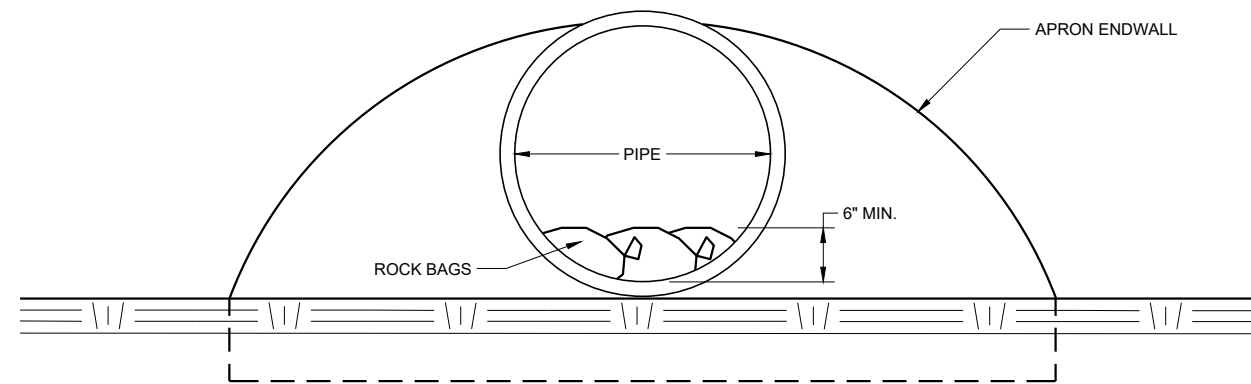
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



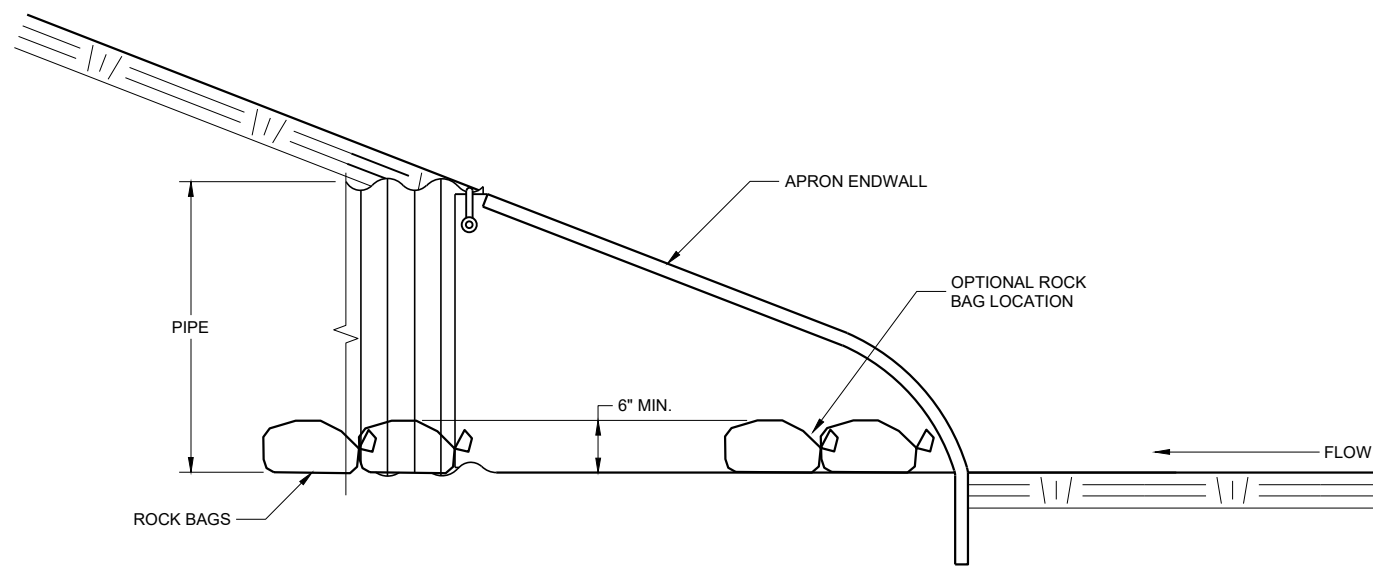
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



END VIEW



SIDE VIEW

CULVERT PIPE CHECK
 (INSTALL ON INLET END ONLY)

CULVERT PIPE CHECK

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 May 2019 /S/ Daniel Schave
 DATE EROSION CONTROL ENGINEER

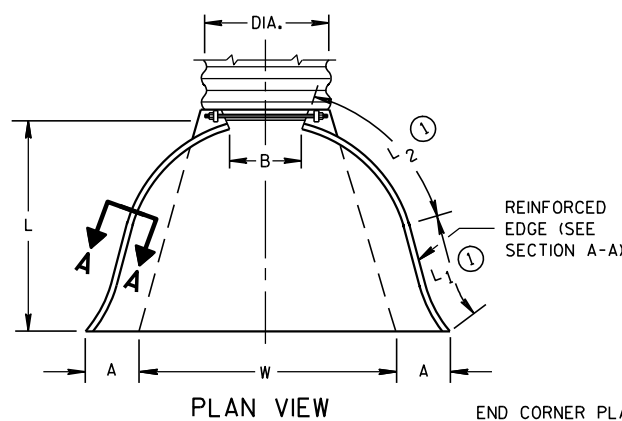
FHWA

METAL APRON ENDWALLS											
PIPE DIA. (IN.)	MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1	L2	W (±2")		
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109x	.105x	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109x	.105x	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109x	.105x	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109x	.105x	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109x	.105x	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109x	.105x	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109x	.105x	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

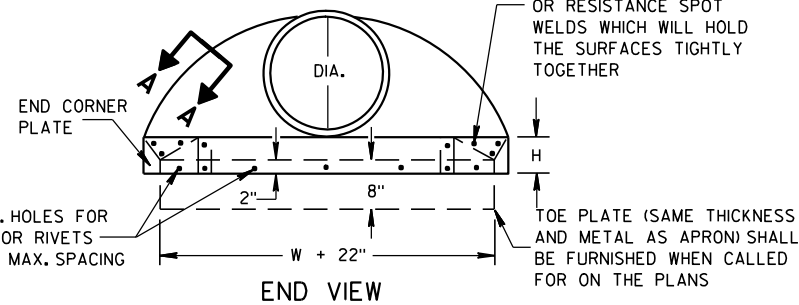
* EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE APRON ENDWALLS									
PIPE DIA. (IN.)	DIMENSIONS (Inches)							APPROX. SLOPE	
	T	A	B	C	D	E	G		
12	2	4	24	48 1/8	72 1/8	24	2	3 to 1	
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1	
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1	
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1	
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1	
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1	
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1	
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1	
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1	
48	5	24	72	26	98	84	5	3 to 1	
54	5 1/2	27	65	33 1/4-35	98 1/4-100	90	5 1/2	2 1/2 to 1	
60	6	30-35	60	39	99	96	5	2 to 1	
66	6 1/2	30-35	72-78	21-27	99	102	5 1/2	2 to 1	
72	7	30-35	78	21	99	108	6	2 to 1	
78	7 1/2	30-35	78	21	99	114	6 1/2	2 to 1	
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1	
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1	

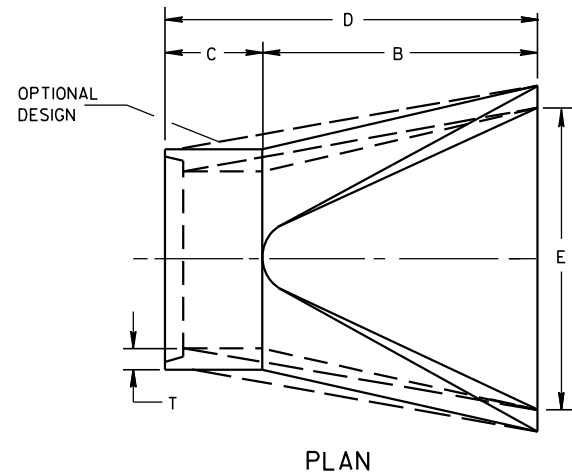
* MINIMUM
** MAXIMUM



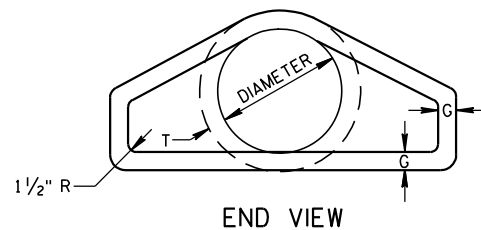
END CORNER PLATES MAY BE FASTENED TO APRON PROPER BY BOLTS, RIVETS, OR RESISTANCE SPOT WELDS WHICH WILL HOLD THE SURFACES TIGHTLY TOGETHER



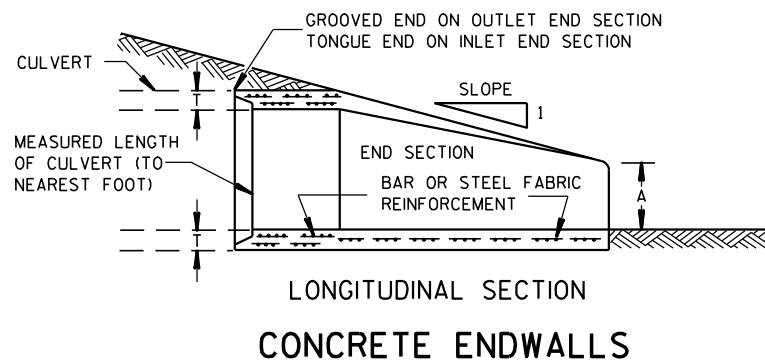
SIDE ELEVATION
METAL ENDWALLS



PLAN

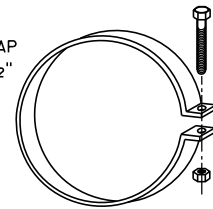


END VIEW

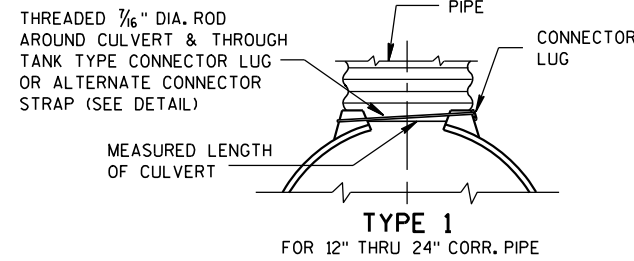


LONGITUDINAL SECTION
CONCRETE ENDWALLS

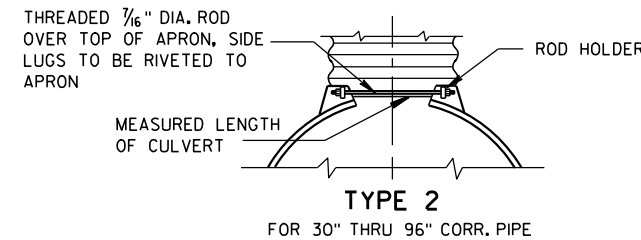
1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



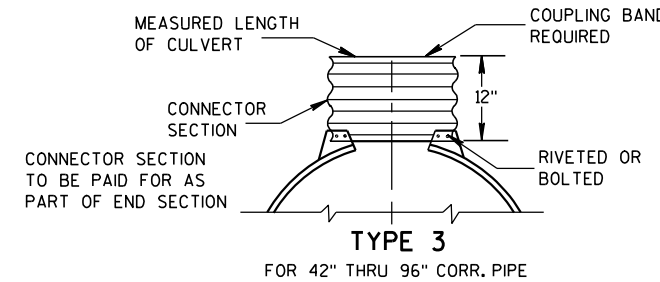
ALTERNATE FOR TYPE 1 CONNECTION
END SECTION CONNECTOR STRAP



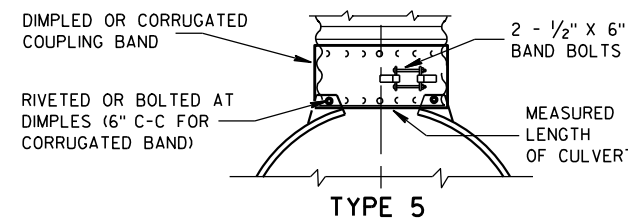
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



TYPE 5
ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

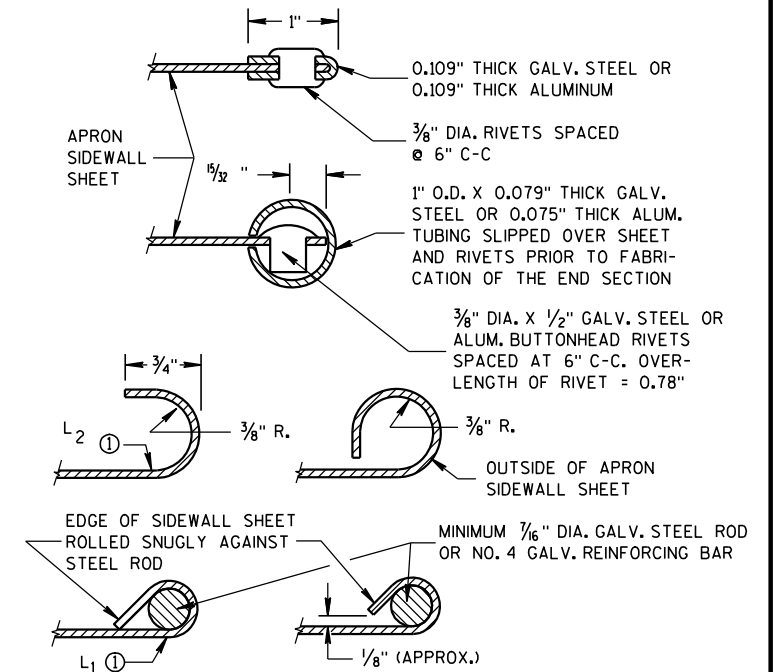
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VICE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

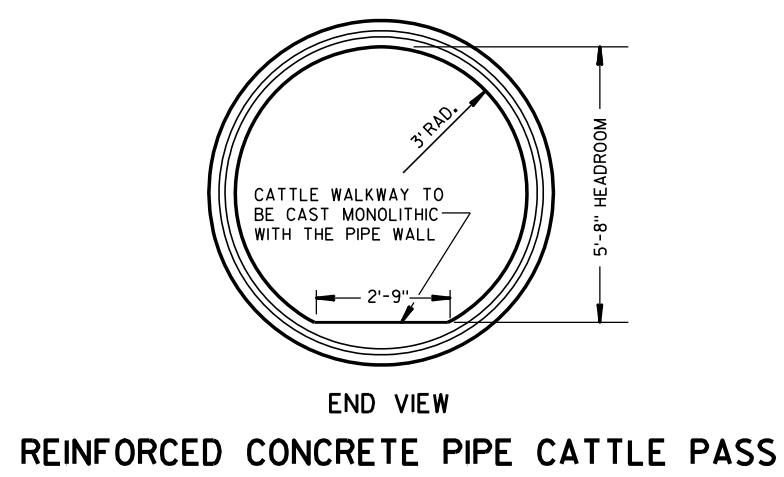
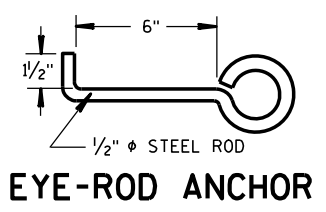
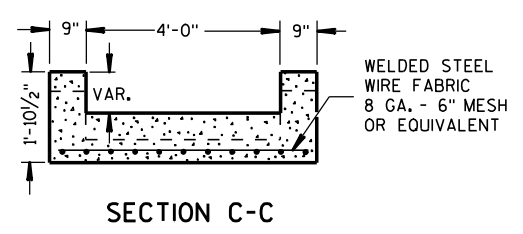
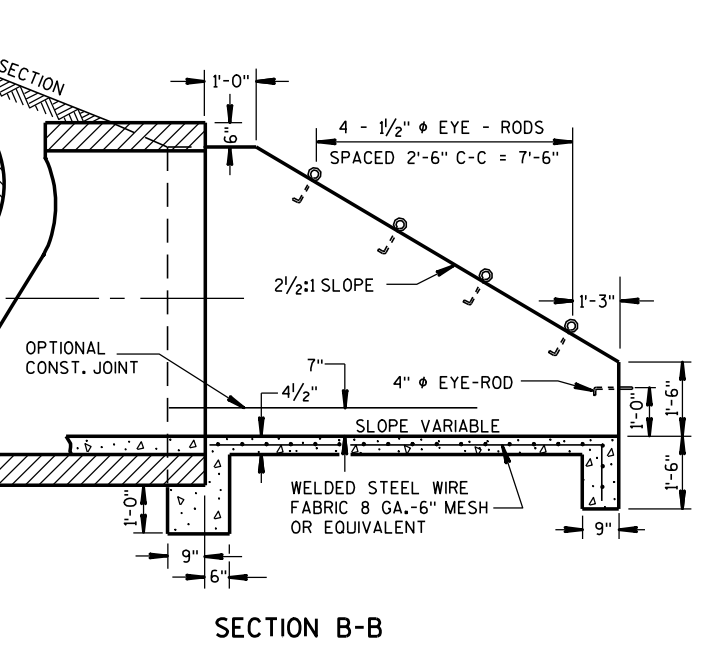
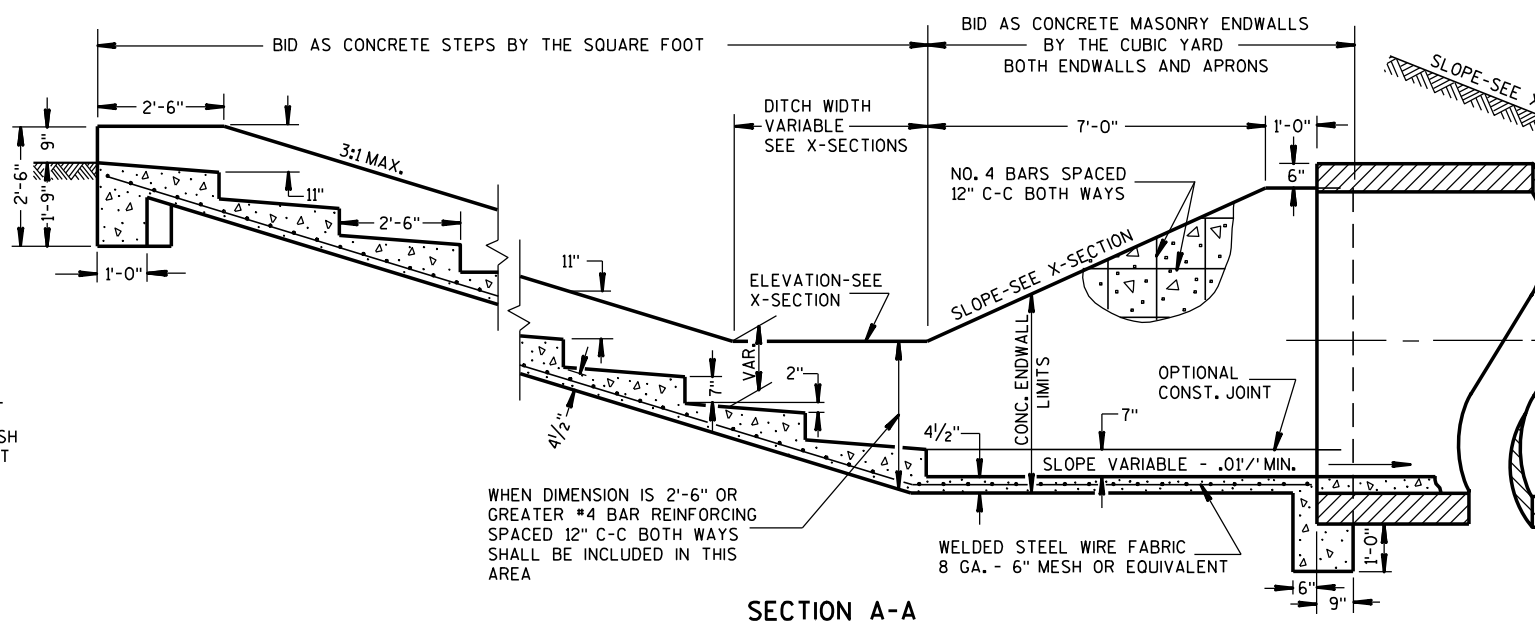
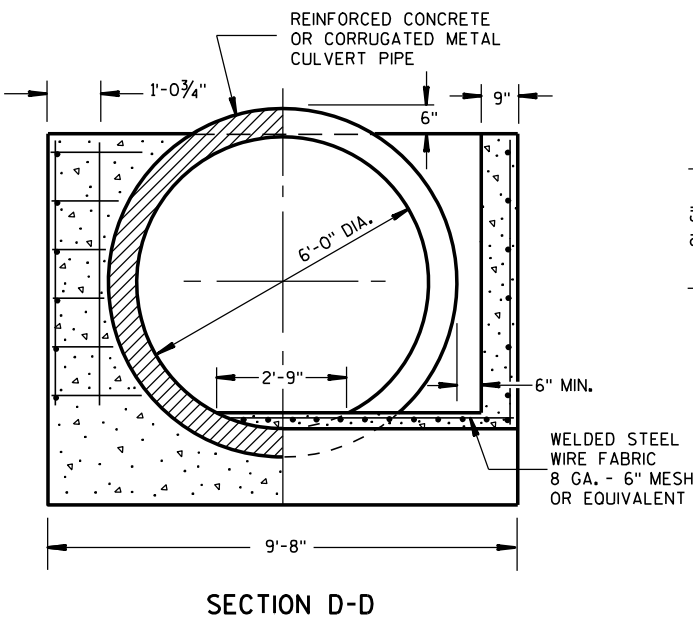
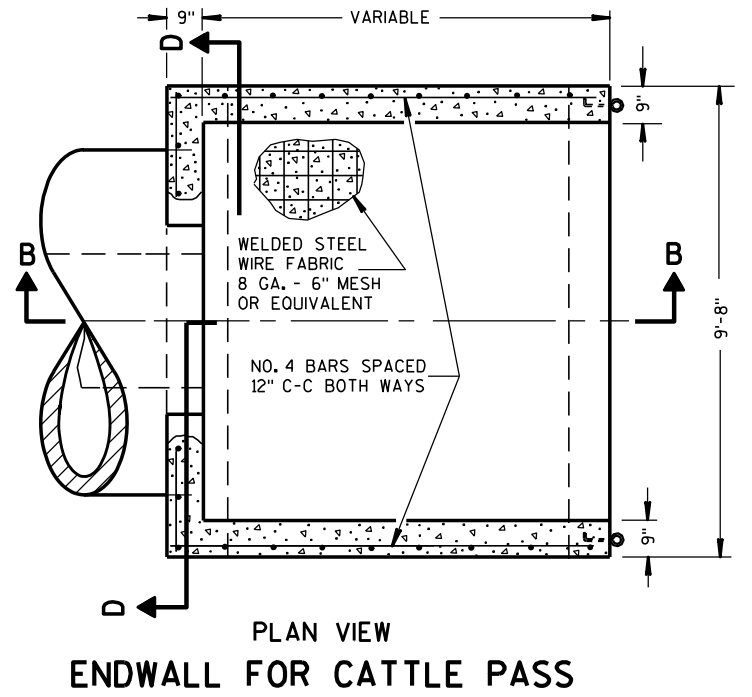
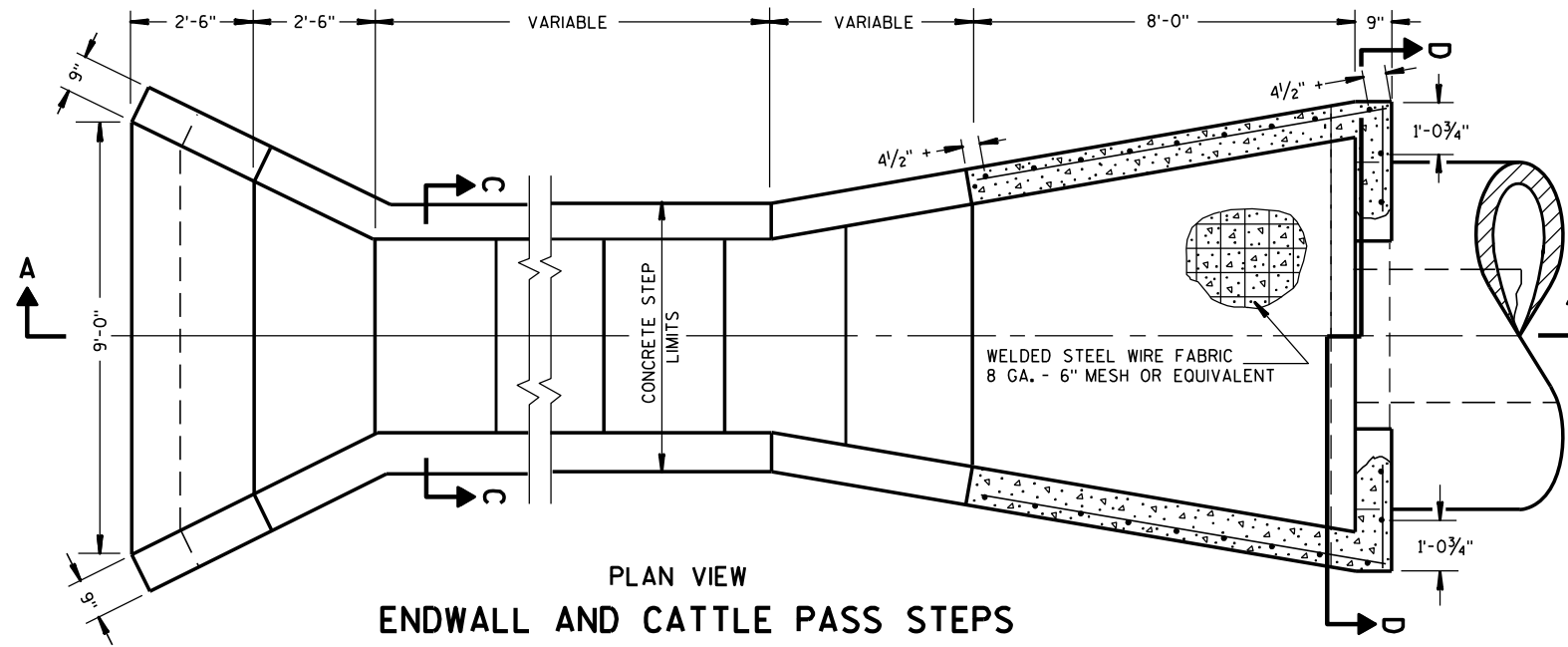
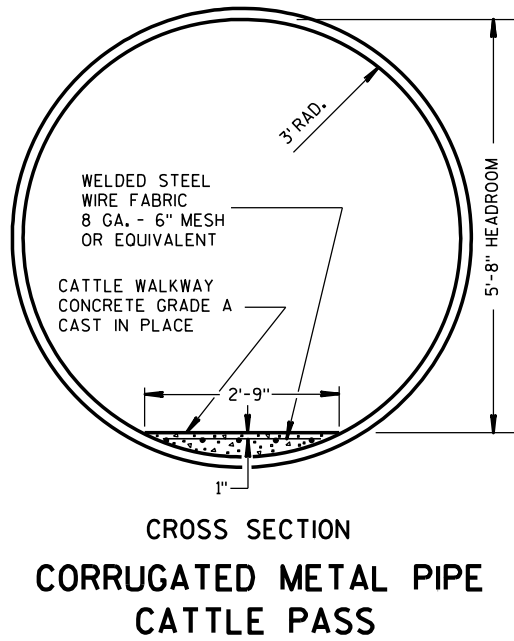
WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR
CULVERT PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 /S/ Rory L. Rhinesmith
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



GENERAL NOTES

- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- ALL STEEL REINFORCEMENT IN ENDWALLS AND CATTLE PASS STEPS SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE NOTED.
- ALL STEEL REINFORCEMENT OR FABRIC USED AS SHOWN ABOVE SHALL BE INCIDENTAL TO THE BID ITEM OF WHICH IT IS AN INTEGRAL PART.
- EYE-RODS FOR FENCE CONNECTIONS SHALL BE PROVIDED BY THE CONTRACTOR AS AN INCIDENTAL TO THE BID ITEM OF CONCRETE MASONRY, ENDWALLS AND SHALL BE GALVANIZED.
- CONCRETE USED FOR THE CATTLE WALKWAY WITHIN THE PIPE SHALL BE INCIDENTAL TO THE BID ITEM OF PIPE CATTLE PASS.

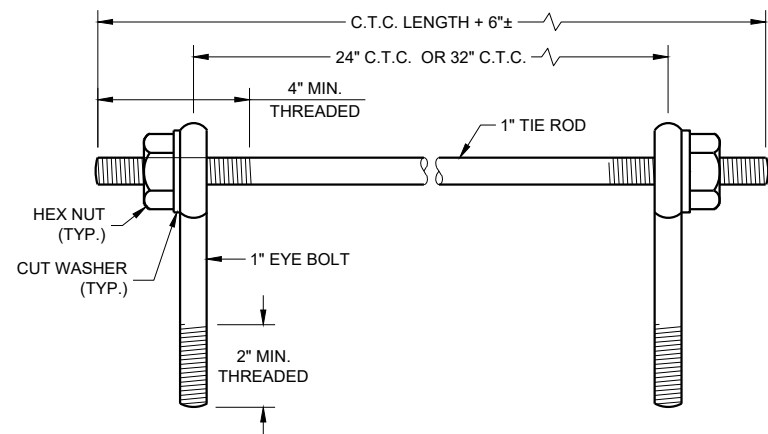
DETAILS FOR PIPE CATTLE PASS, CONCRETE ENDWALL AND STEPS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 6/6/75 DATE	/S/ Harold Fieder STATE DESIGN ENGINEER FOR HWYS
FHWA	

6

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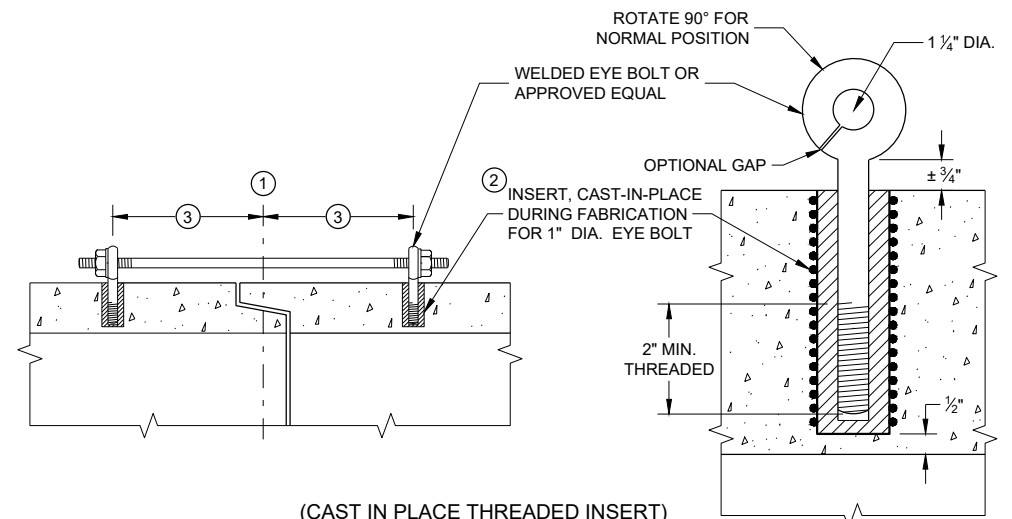
S.D.D. 8 F 3-3

S.D.D. 8 F 3-3



EYE BOLTS AND TIE ROD

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)



(CAST IN PLACE THREADED INSERT)
LONGITUDINAL SECTIONS

GENERAL NOTES

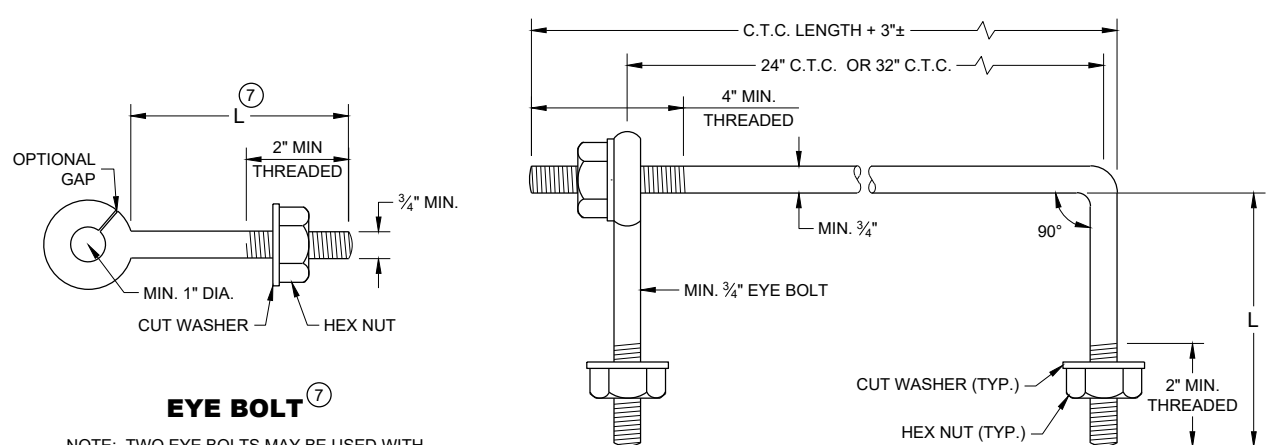
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

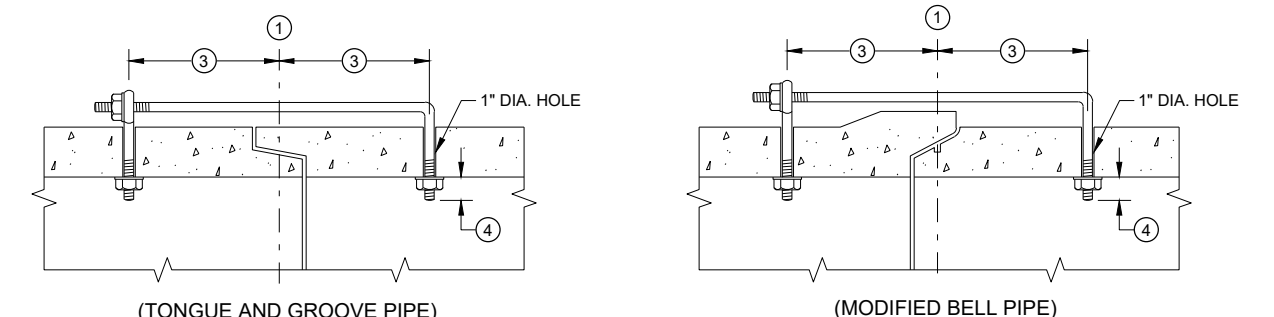
- ① CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.
- ⑦ EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.



EYE BOLT AND TIE ROD

EYE BOLT ⑦

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30\"/>



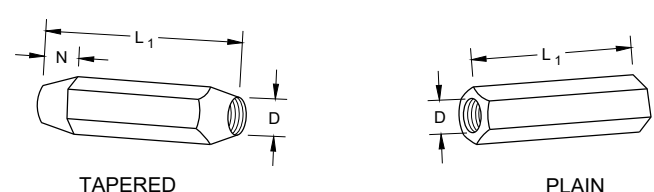
LONGITUDINAL SECTION
(JOINT TIES FOR 18\"/>

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

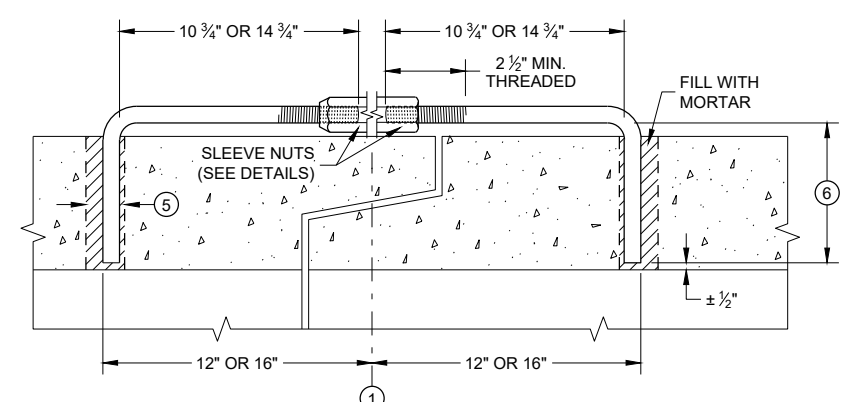
ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L ₁	N
12 - 60	5/8	5/8	5	1/2
66 - 84	3/4	3/4	5	1/2
90 - 144	1	1	7	1 1/16

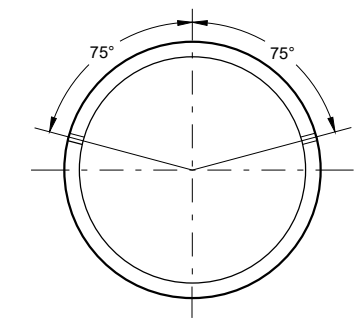
DIMENSIONS SHOWN ARE IN INCHES



RIGHT AND LEFT THREADS SLEEVE NUTS

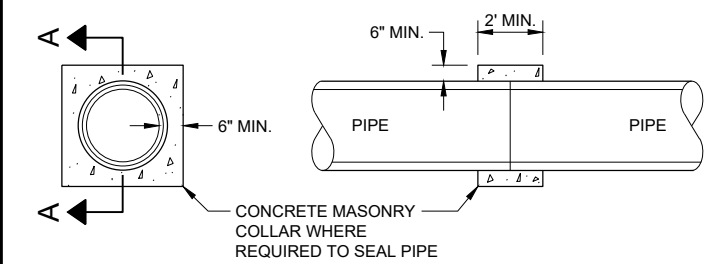


LONGITUDINAL SECTION
ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



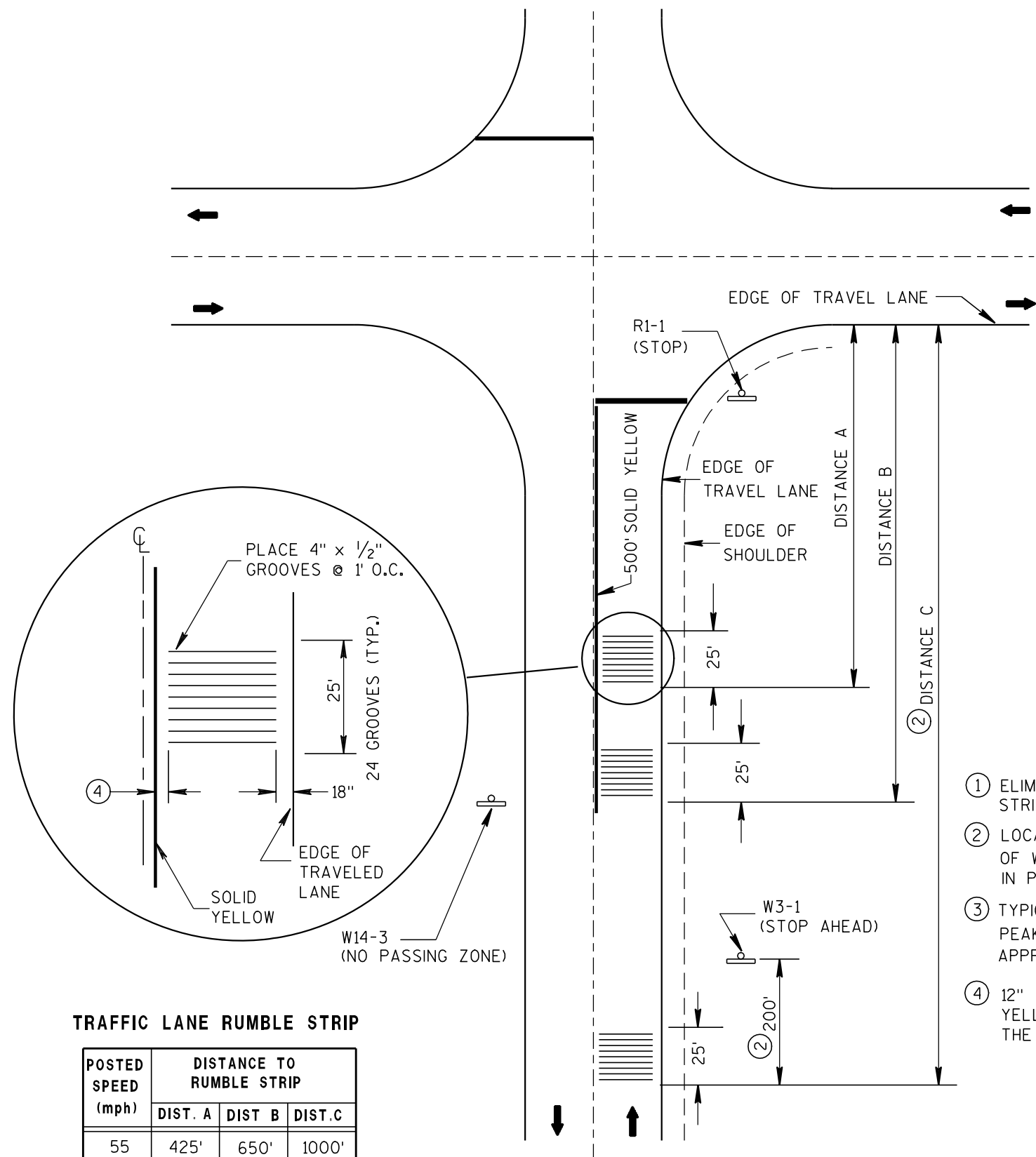
SECTION A - A
CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



TRAFFIC LANE RUMBLE STRIP

POSTED SPEED (mph)	DISTANCE TO RUMBLE STRIP		
	DIST. A	DIST. B	DIST. C
55	425'	650'	1000'
50	325'	450'	800'
45	275'	400'	650'
40	225'	①	550'
35	175'	①	475'
≤ 30	125'	①	425'

ARROW SYMBOL (➔) SHOWS DIRECTION OF TRAVEL

**PLAN VIEW
RUMBLE STRIP LOCATION**

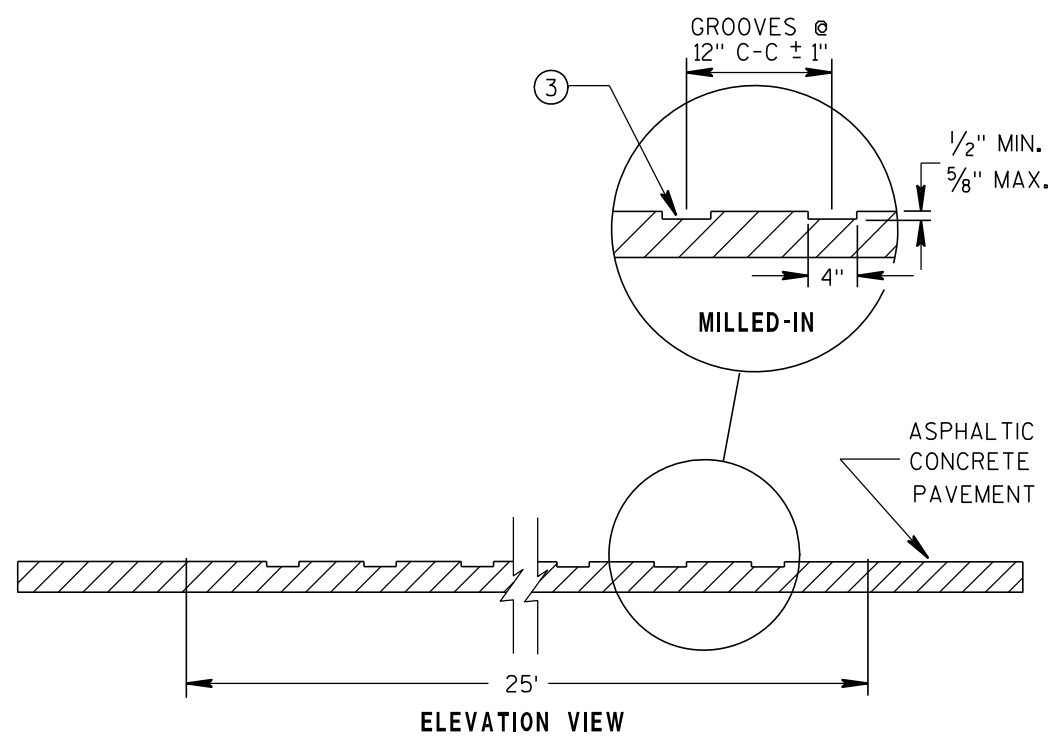
- ① ELIMINATE THE MIDDLE SET OF RUMBLE STRIPS.
- ② LOCATE RUMBLE STRIP 200' IN ADVANCE OF W3-1 SIGN AS SHOWN. IF W3-1 IS NOT IN PLACE, USE DISTANCE C.
- ③ TYPICAL VERTICAL VARIATION BETWEEN PEAKS AND VALLEYS WITHIN THE CUT APPROXIMATELY 1/16"
- ④ 12" CLEAR BETWEEN THE SOLID YELLOW LINE AND THE EDGE OF THE RUMBLE.

GENERAL NOTES

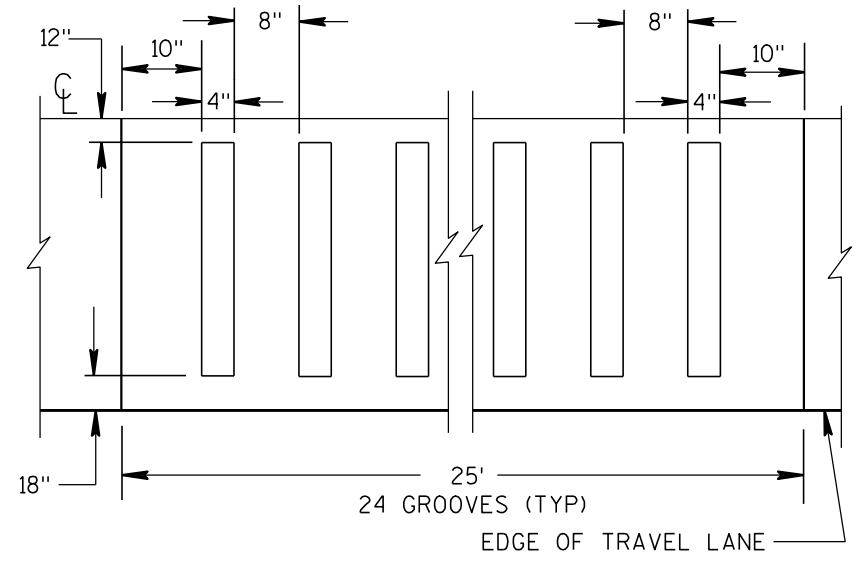
CONTRACTOR SHALL CONFIRM RUMBLE STRIP LOCATION WITH THE ENGINEER PRIOR TO INSTALLATION. THE ENGINEER MAY MODIFY THE RUMBLE STRIP LOCATION AS FIELD CONDITIONS DICTATE.

WHEN ASPHALTIC PAVEMENT IS NEW IN THE RUMBLE AREA THE CONTRACTOR SHALL ALLOW THE PAVEMENT TO CURE A MINIMUM OF 7 DAYS PRIOR TO RUMBLE INSTALLATION.

PAVEMENT MARKING AND SIGNING DETAILS AND SPECIFICATIONS ARE PROVIDED ELSEWHERE IN THE CONTRACT.



ELEVATION VIEW



**PLAN VIEW
ASPHALTIC PAVEMENT
MILLED-IN**

ASPHALTIC RUMBLE STRIPS AT INTERSECTION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/17/2011 DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	

GENERAL NOTES

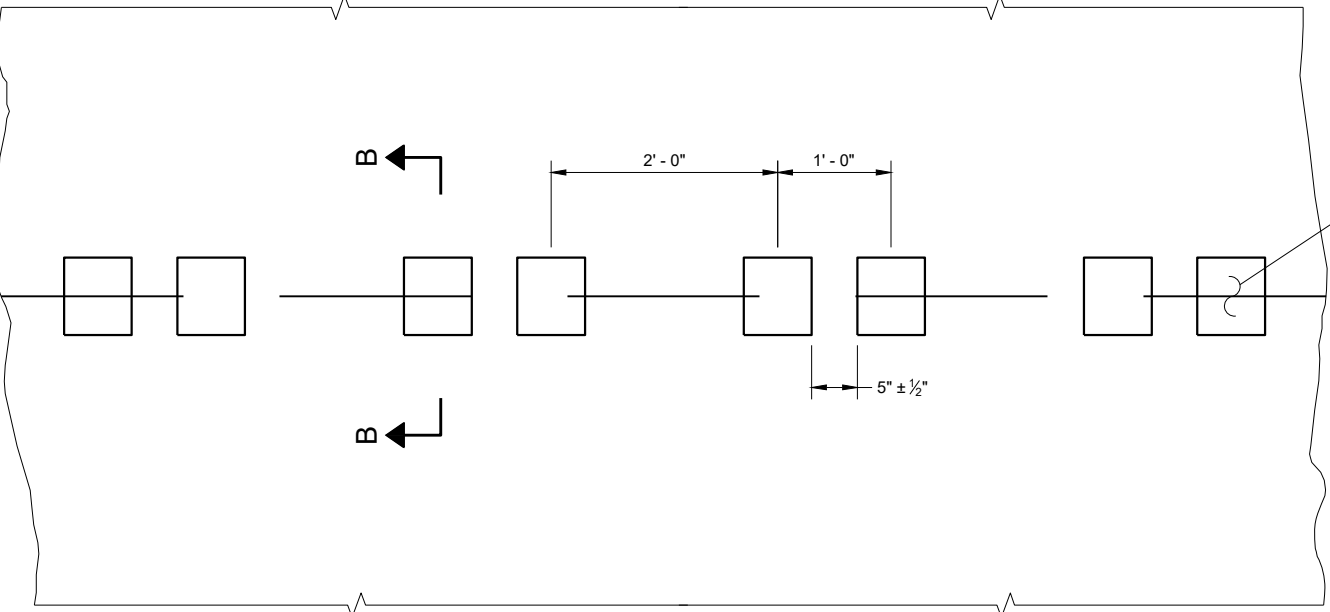
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTERLINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

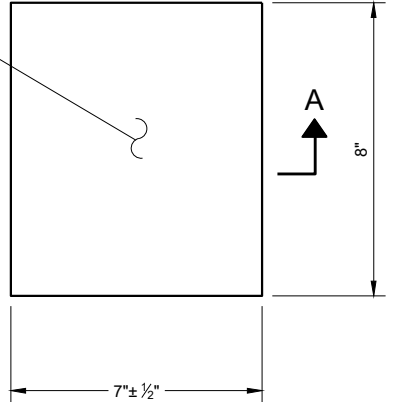
INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

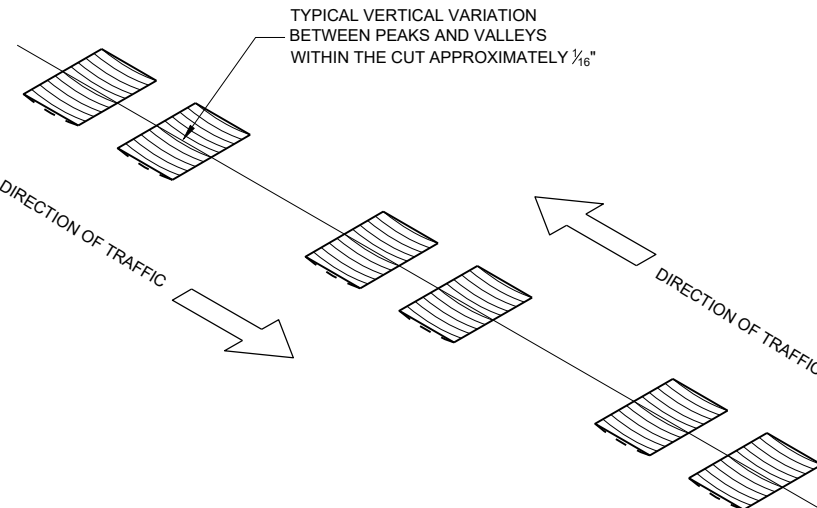
- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



**PLAN VIEW
SHOULDER WITH GROOVES**

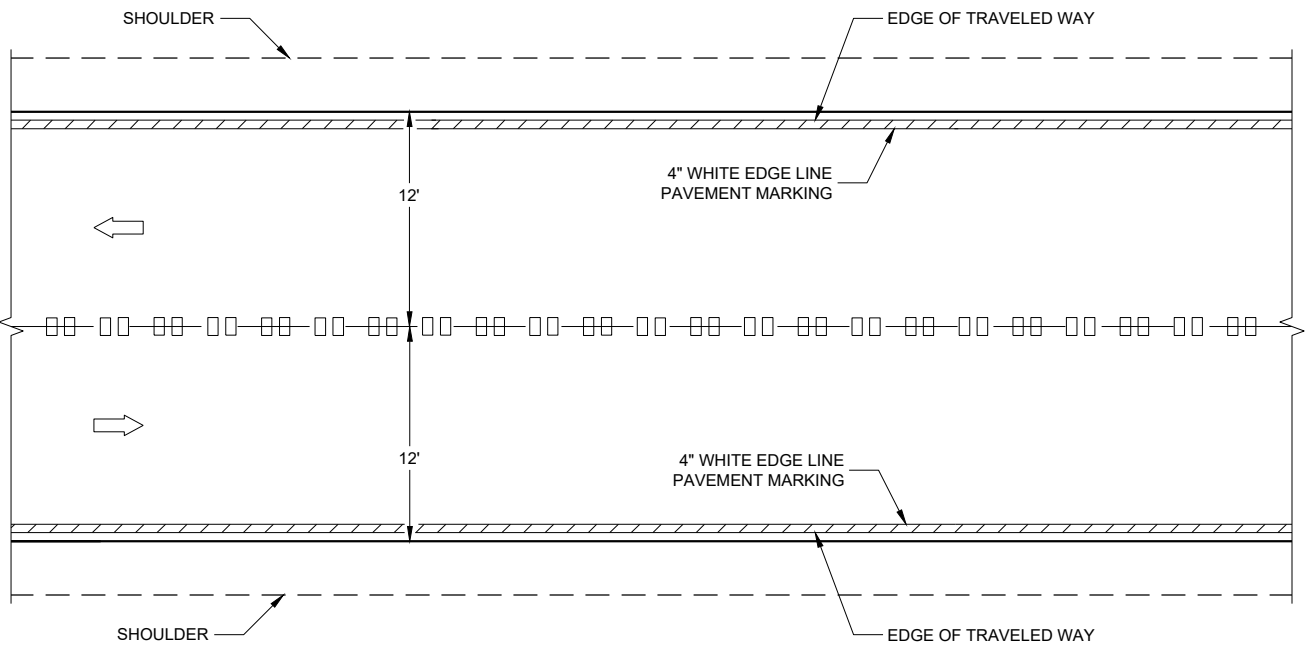


**PLAN VIEW
(SINGLE GROOVE)**

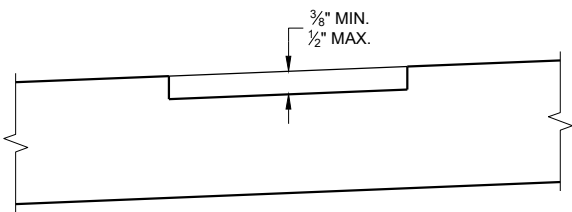


ISOMETRIC

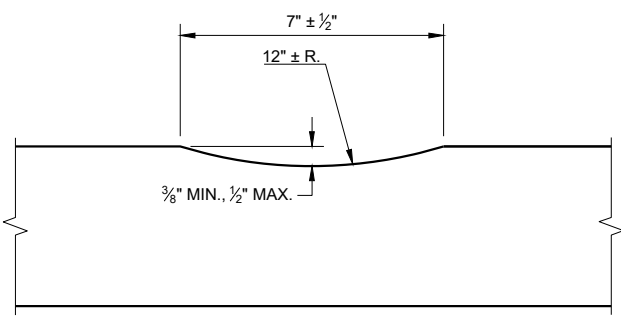
PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP



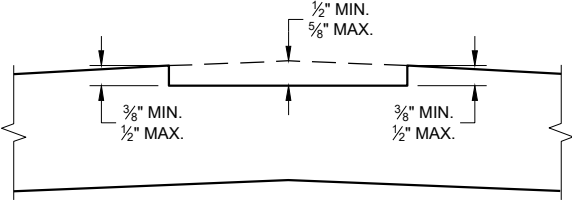
CENTERLINE GROOVES ON TWO-WAY ROADWAYS



**SECTION B - B
SUPERELEVATED ROADWAY**



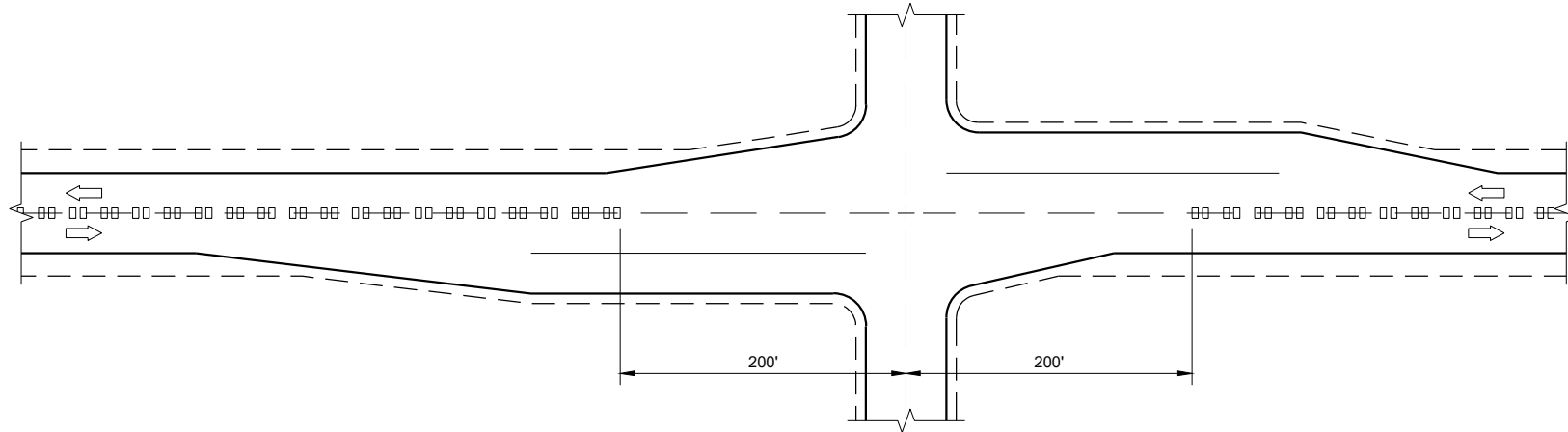
SECTION A - A



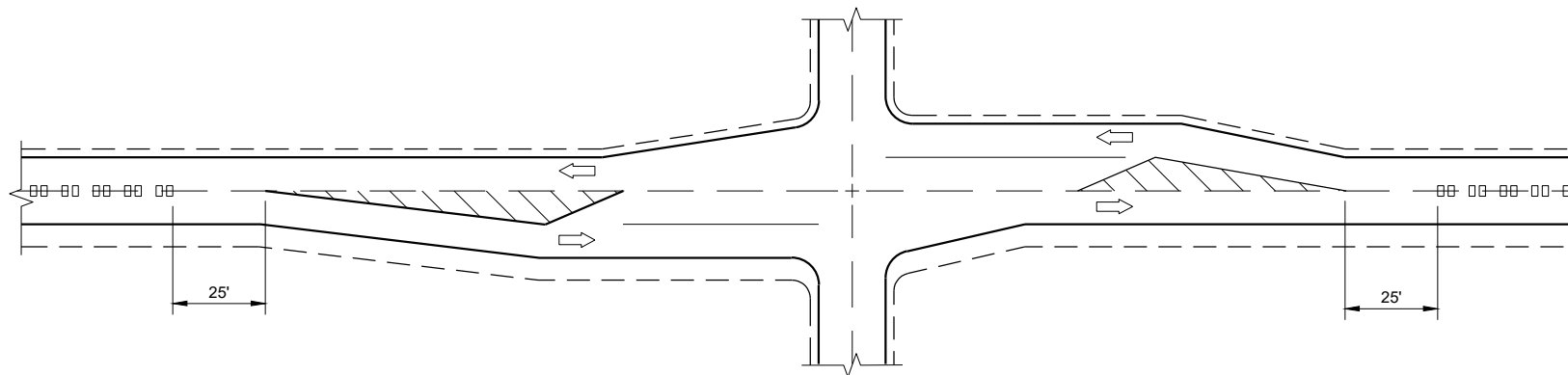
**SECTION B - B
CROWNED ROADWAY**

**2-LANE RURAL
CENTER LINE RUMBLE STRIP,
MILLING**

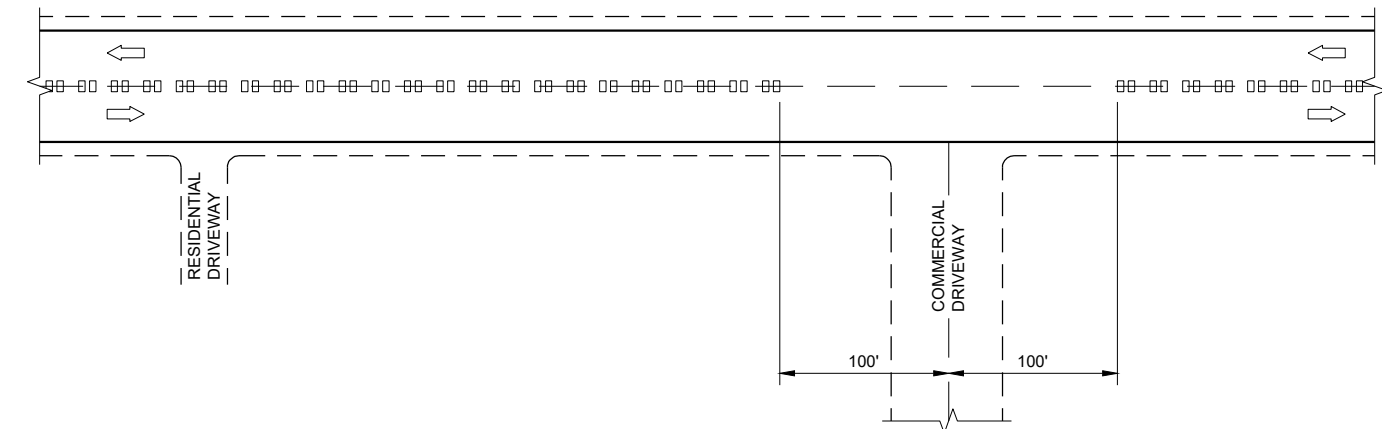
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CENTERLINE GROOVES AT INTERSECTIONS



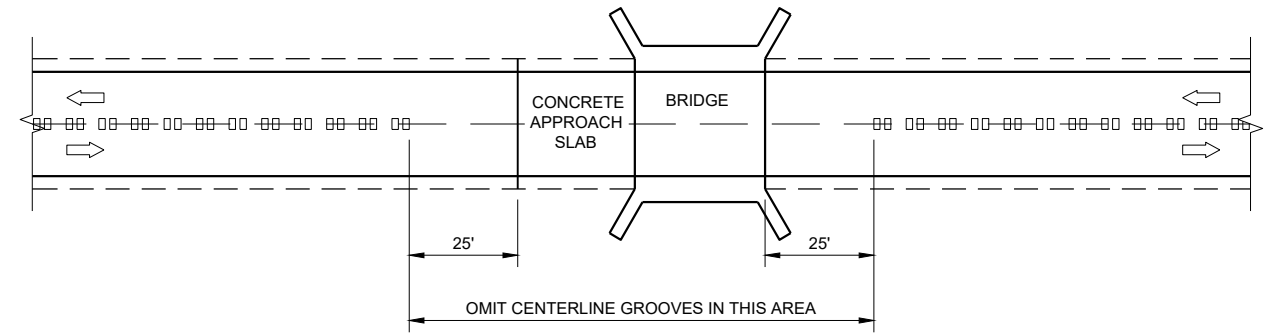
**CENTERLINE GROOVES AT INTERSECTIONS
(WITH LEFT TURN LANES)**



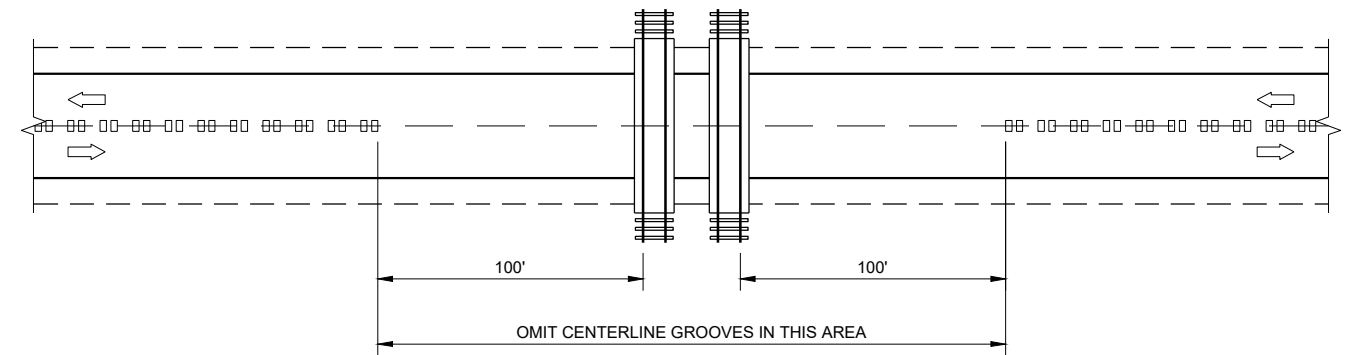
CENTERLINE GROOVES AT DRIVEWAYS^①

GENERAL NOTES

- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



CENTERLINE GROOVES AT BRIDGES



CENTERLINE GROOVES AT RAILROADS

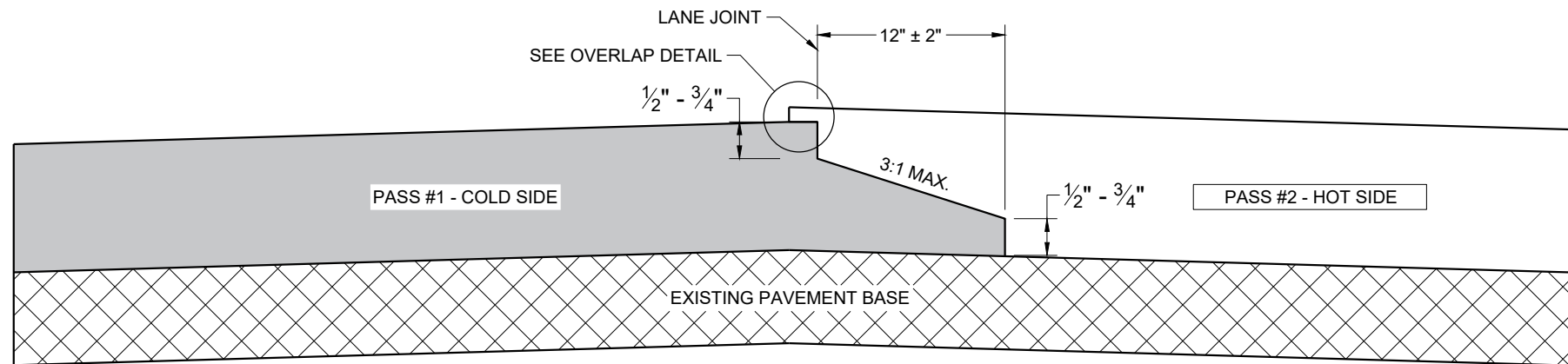
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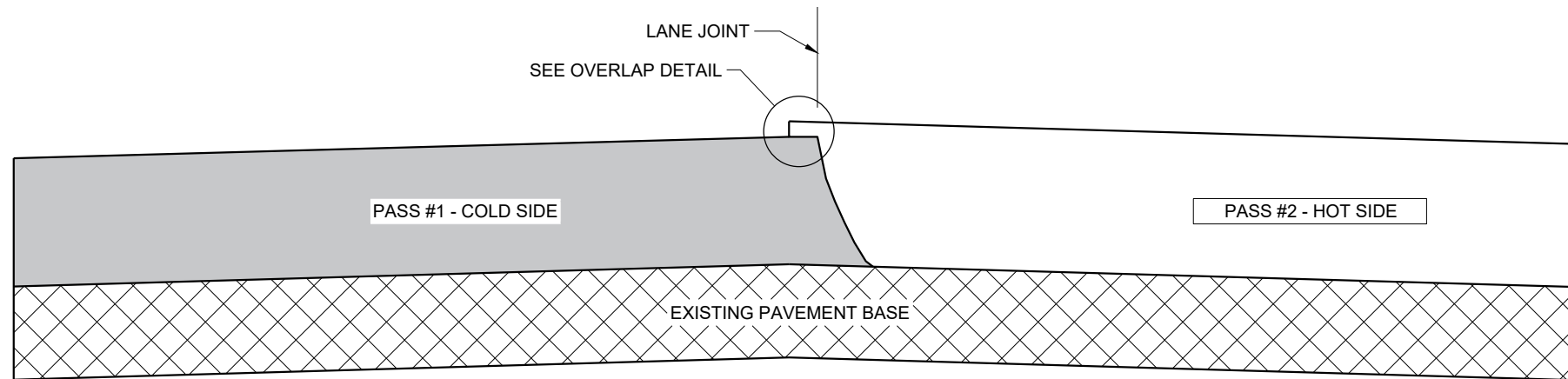
SDD 13A11 - 03b

SDD 13A11 - 03b

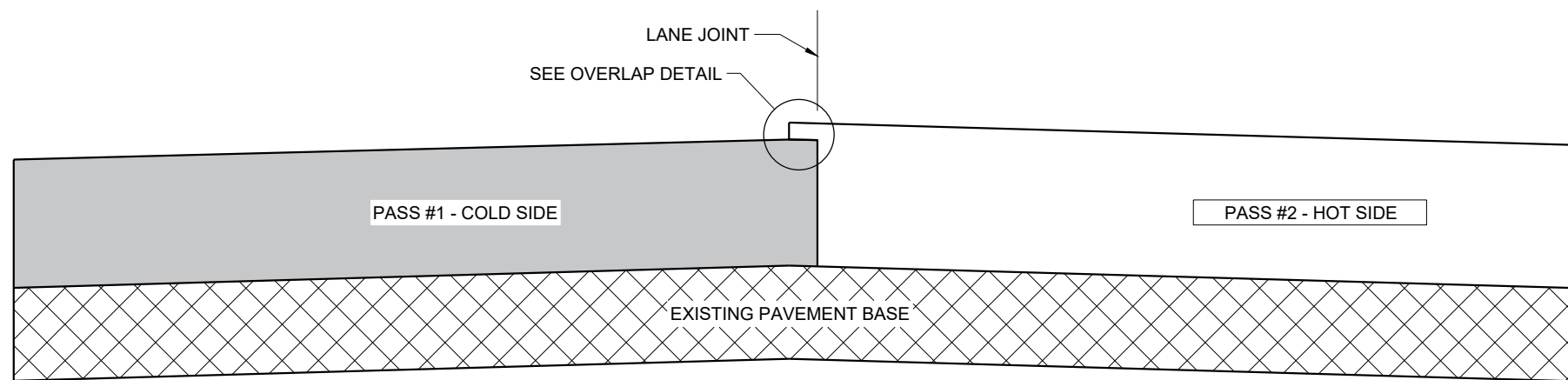
2-LANE RURAL CENTERLINE RUMBLE STRIP, MILLING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE 7/2018	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

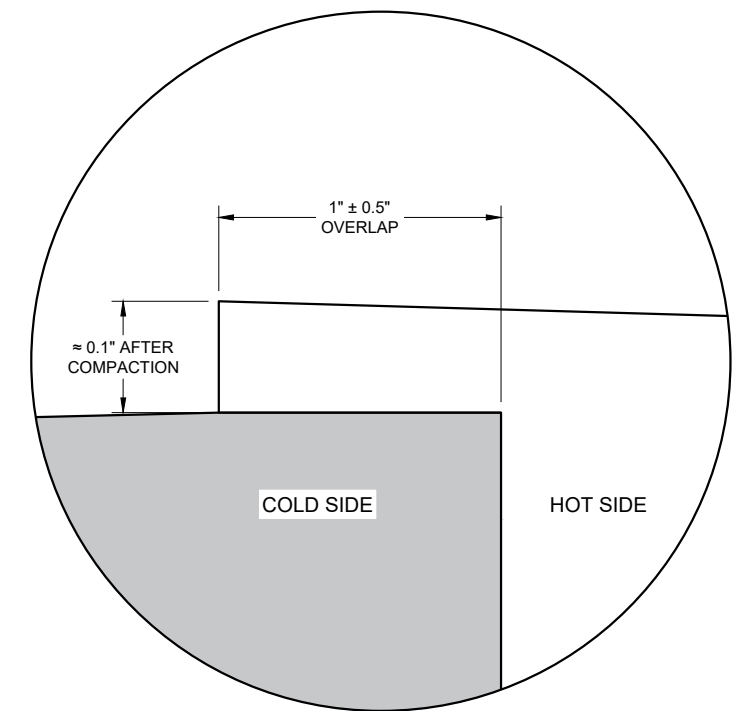
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY $0.1"$ AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO $2"$ FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

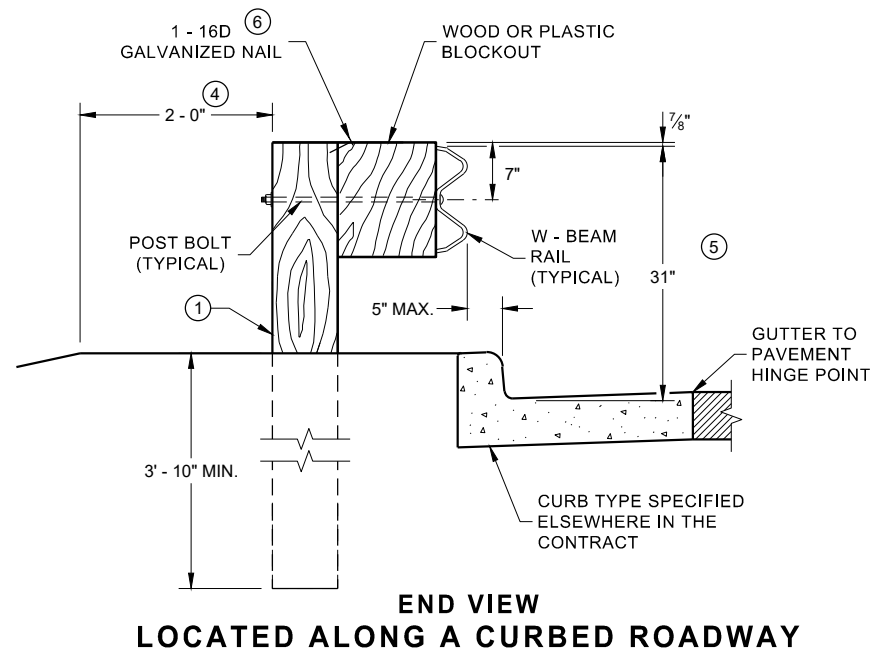
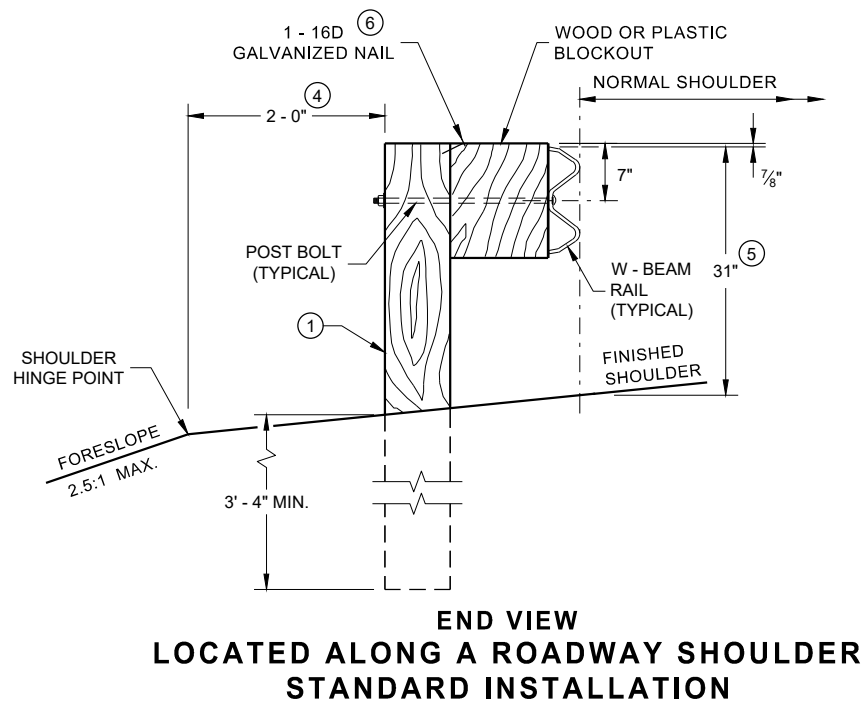
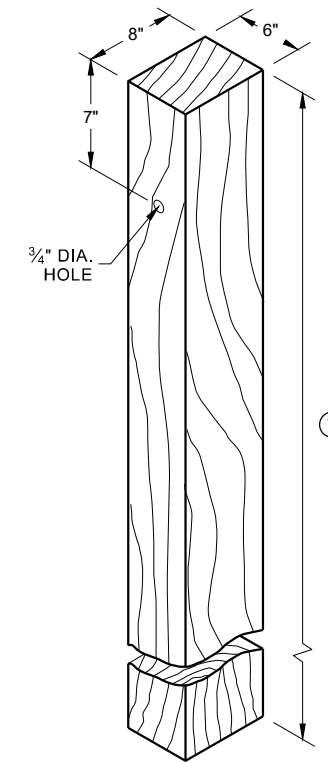
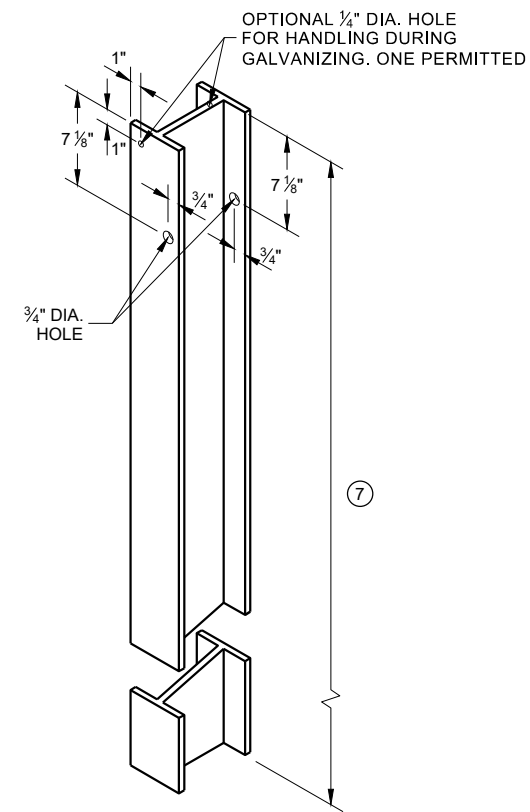
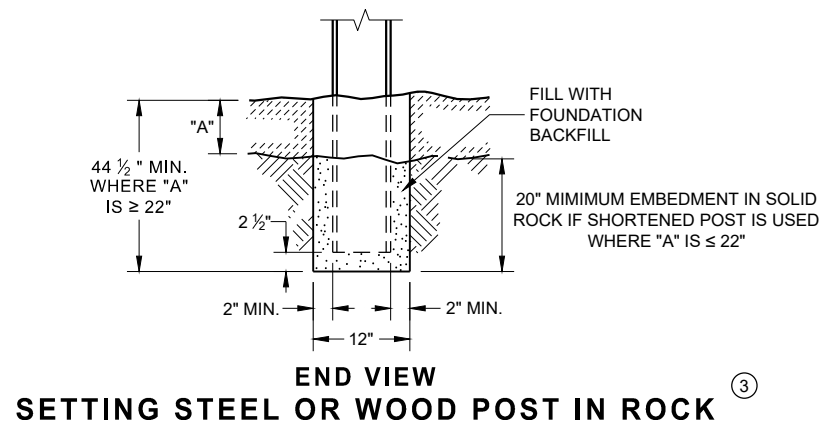
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SDD 13C19 - 03

SDD 13C19 - 03

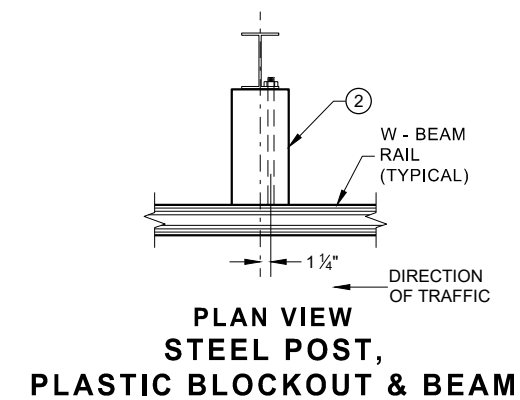
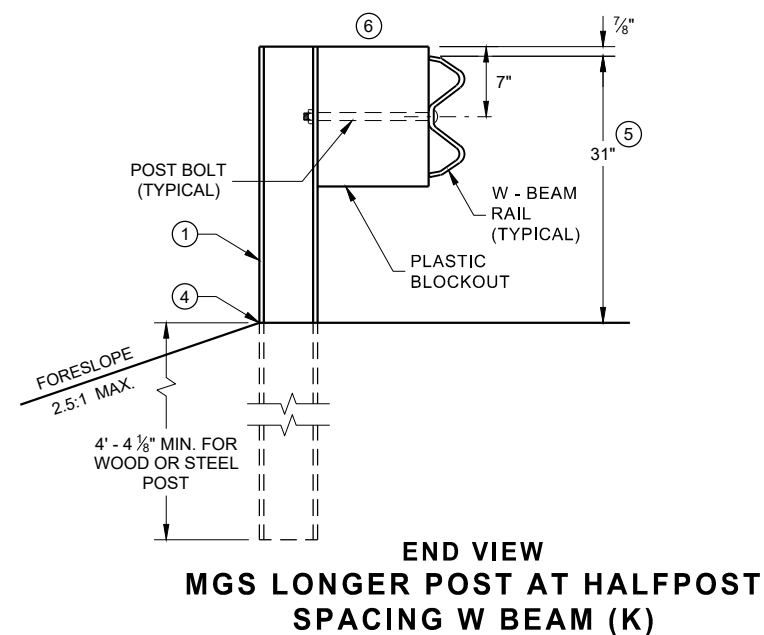
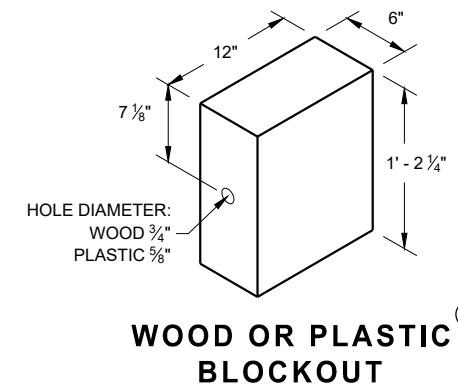
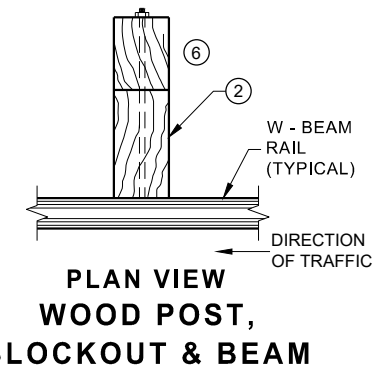
HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	

- ① WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- ② USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.
- ③ IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2" INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AND INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.
- ④ WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).
- ⑤ FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS $\pm 1"$. FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27 3/4" TO 32".
- ⑥ WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.
- ⑦ TOTAL POST LENGTH FOR TYPE K IS 7' - 0". TOTAL POST LENGTH FOR OTHER MGS TYPES IS 6' - 0".



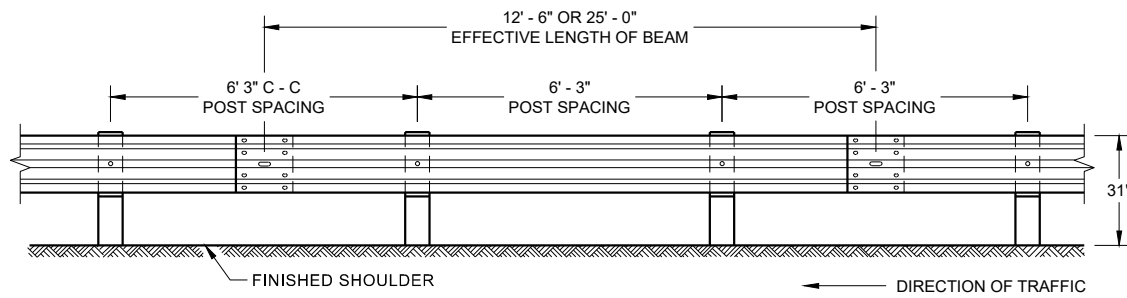
STEEL POST & HOLE PUNCHING DETAIL (W 6 X 9)

WOOD POST (6" X 8") NOMINAL

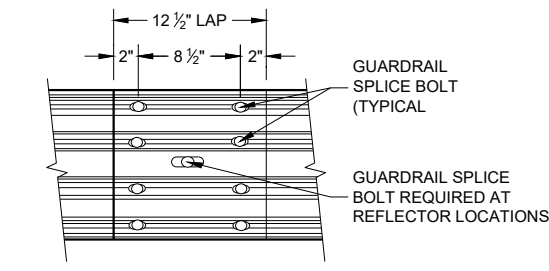


MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



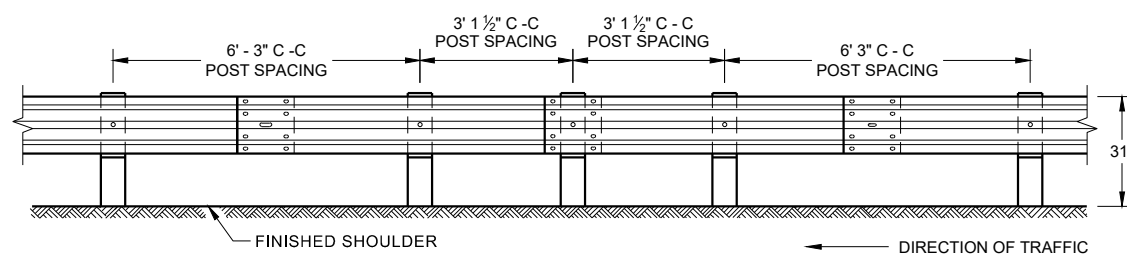
**FRONT VIEW
POST SPACING STANDARD INSTALLATION**



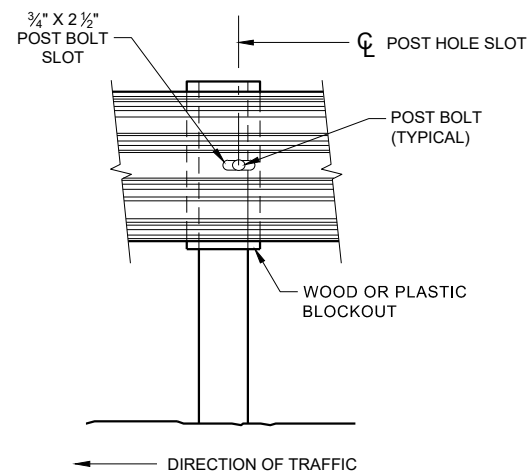
**FRONT VIEW
MID-SPAN BEAM SPLICE**

GENERAL NOTES

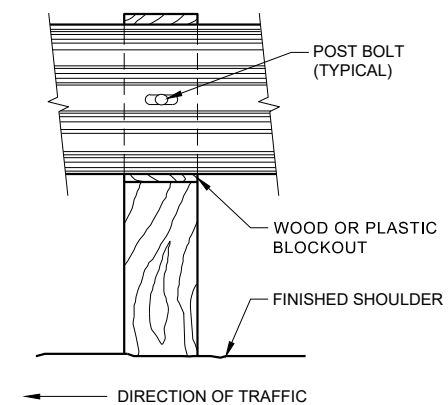
- ⑧ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.
 - ⑨ 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.
- POST BOLTS ARE A 5/8" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES 3/4" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND 5/8" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.
- GUARD RAIL SPLICE BOLTS ARE A 5/8" DIAMETER ASTM A307 GUARDRAIL HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES 3/4" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT.



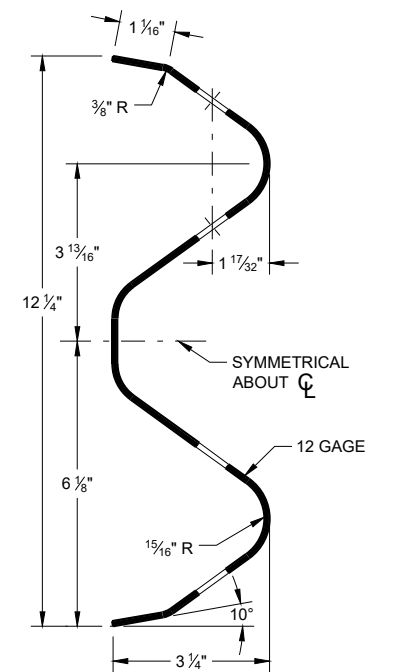
**FRONT VIEW
HALF POST SPACING (HS) AND
HALF POST SPACING WITH LONGER POSTS (K)**



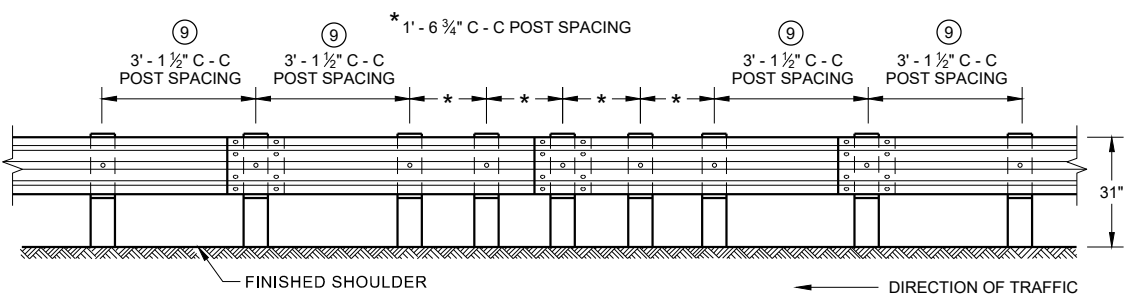
FRONT VIEW AT STEEL POST



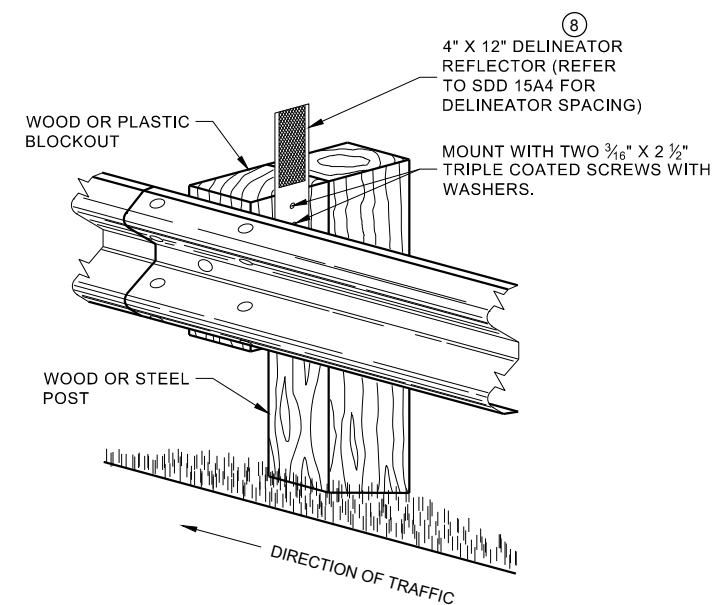
FRONT VIEW AT WOOD POST



SECTION THRU W-BEAM RAIL



**FRONT VIEW
QUARTER POST SPACING (QS)**



**ONE SIDED REFLECTOR DETAIL
AND TYPICAL INSTALLATION**

**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

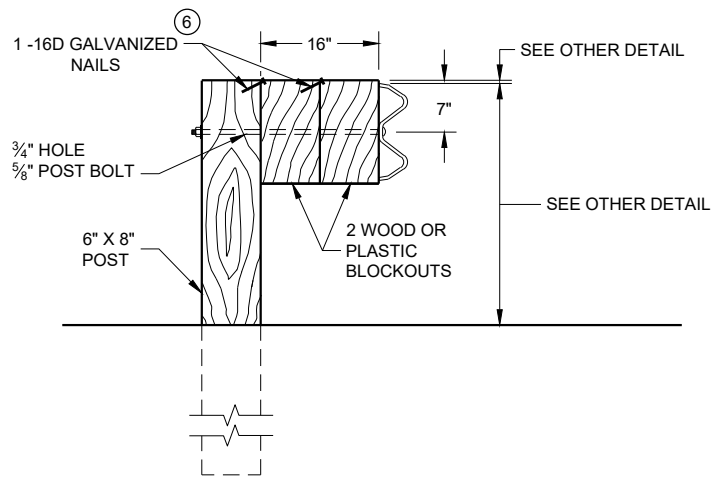
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

6

SDD 14B42 - 07b

SDD 14B42 - 07b

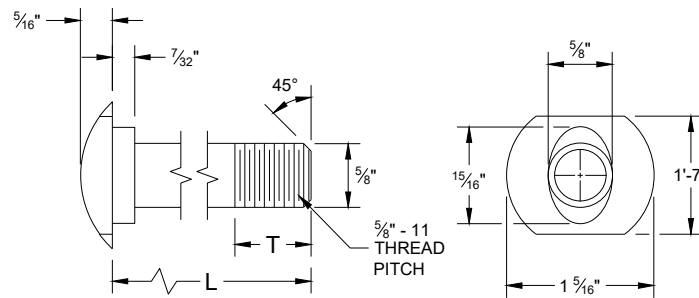


DETAIL FOR 16" BLOCKOUT DEPTH

IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.

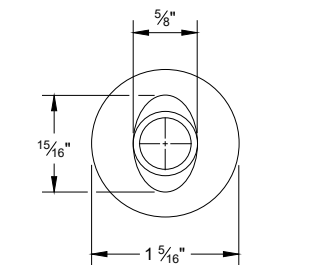
NOTE:

1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF 3/16".
2. IF THE BOLT EXTENDS MORE THAN 1/4" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

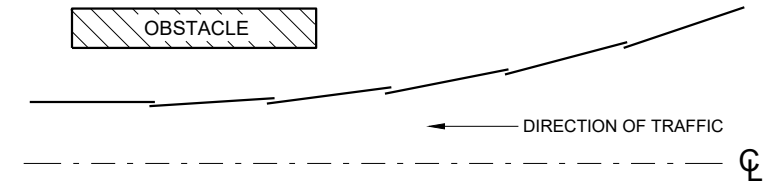


POST BOLT TABLE

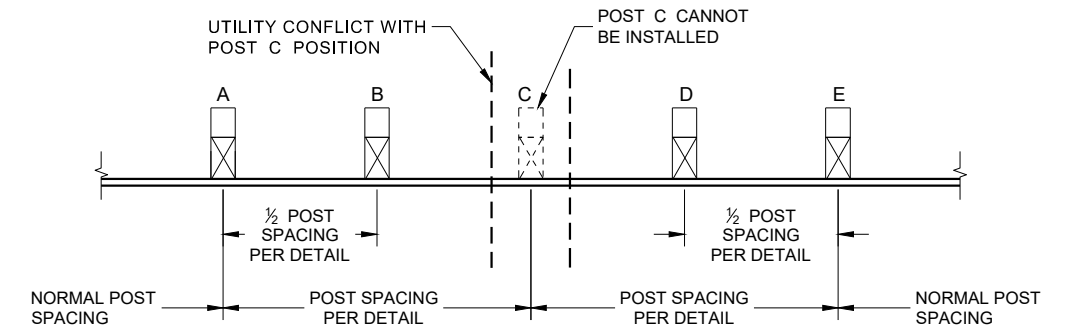
L	T (MIN.)
1 1/4"	1 1/8"
2"	1 3/4"
10"	4"
14"	4 1/16"
18"	4"
21"	4 1/16"
25"	4"



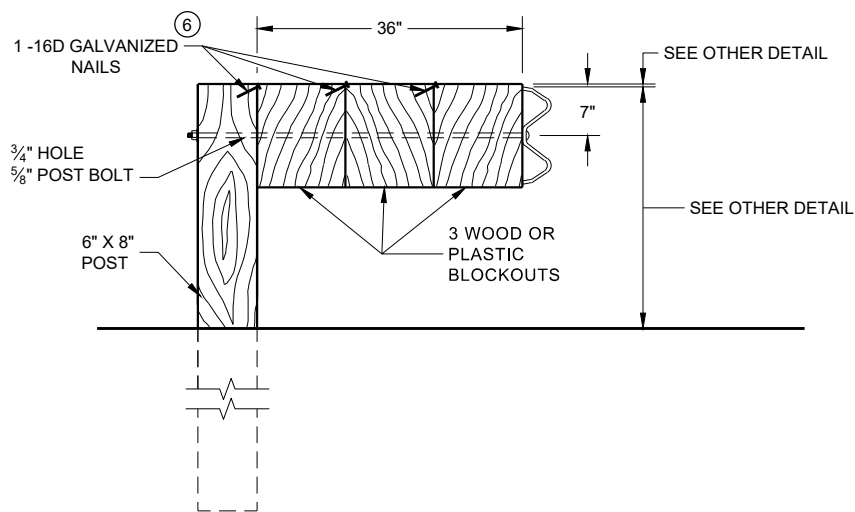
ALTERNATE BOLT HEAD



**PLAN VIEW
BEAM LAPPING DETAIL**

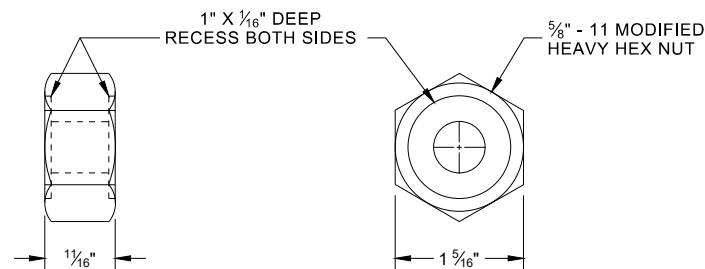


**POST DRIVING FOR CONTINUOUS
UNDERGROUND OBSTRUCTION**

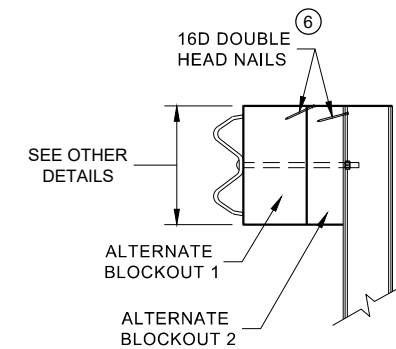


DETAIL FOR 36" BLOCKOUT DEPTH

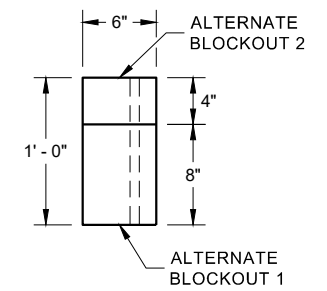
NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.
DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.



**POST BOLT, SPLICE BOLT
AND RECESS NUT**



SIDE VIEW



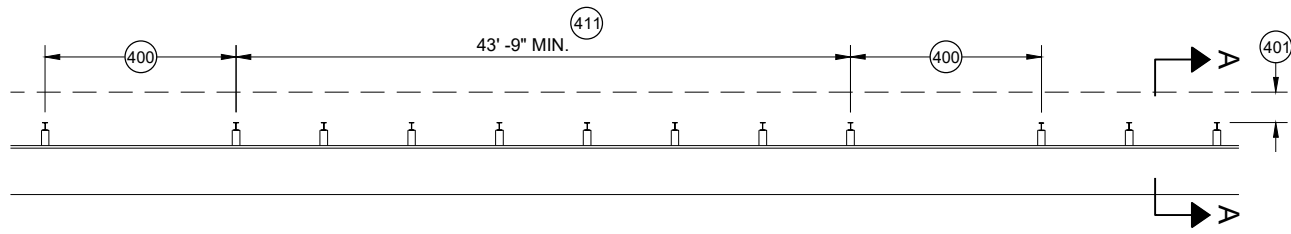
PLAN VIEW

**ALTERNATE WOOD
BLOCKOUT DETAIL**

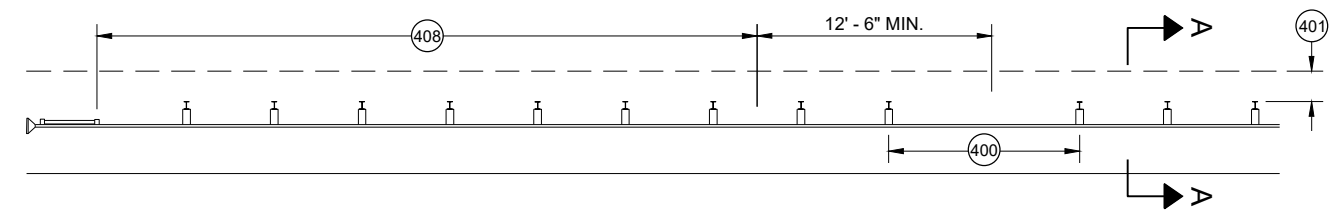
6 WHEN USING STEEL POST AND WOOD BLOCKOUTS, INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

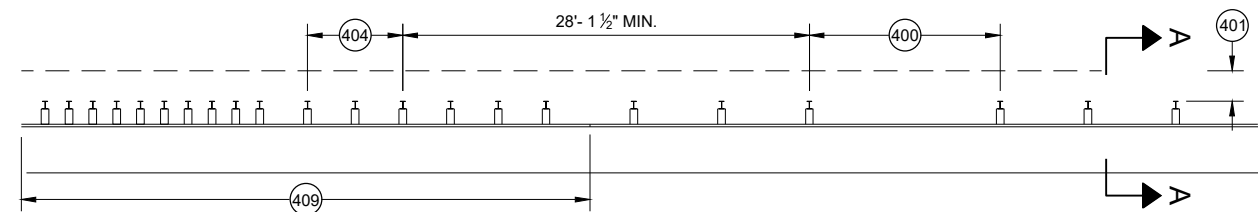
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



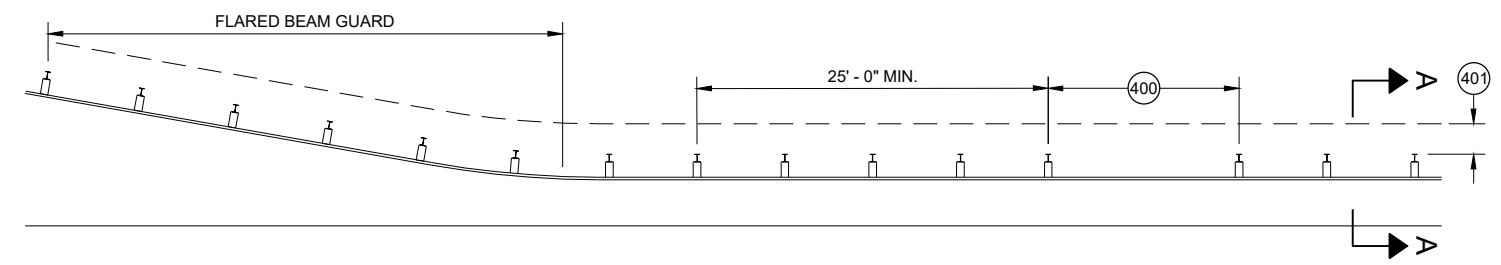
MISSING POST IN MGS GUARDRAIL



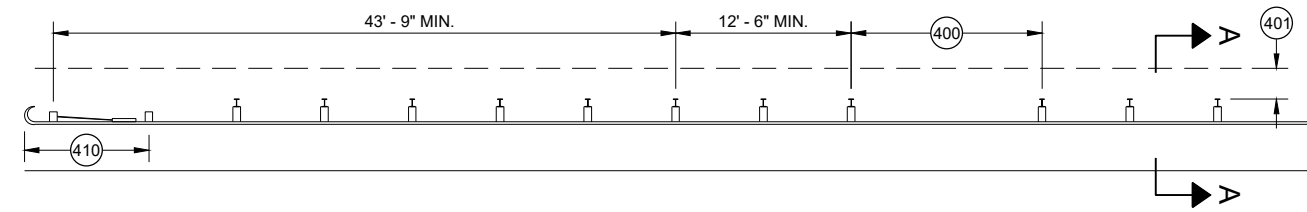
MISSING POST IN MGS GUARDRAIL NEAR EAT



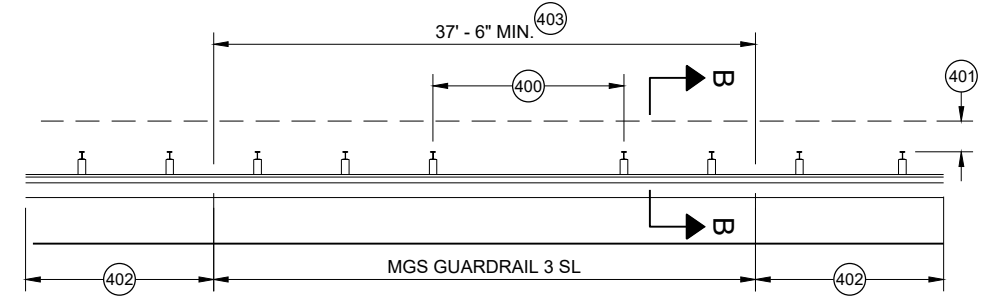
MISSING POST IN MGS GUARDRAIL NEAR AN APPROACH TRANSITION



MISSING POST IN MGS GUARDRAIL NEAR FLARED BEAM GUARD

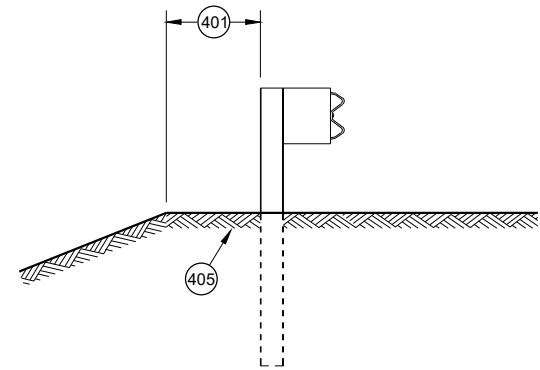


MISSING POST IN MGS GUARDRAIL NEAR A TYPE 2 END TERMINAL

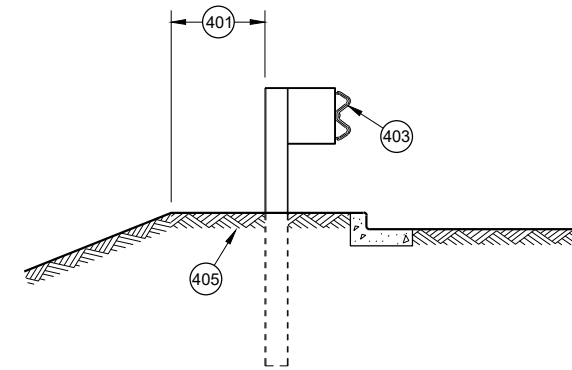


MISSING POST IN SHORT SPAN MGS GUARDRAIL NEAR CURB (SL)

- 400 MAX SPAN 12' - 6"
- 401 2' MIN.
- 402 MGS GUARDRAIL 3
- 403 NESTING BEAM GUARD
- 404 ASYMMETRIC TRANSITION
- 405 SOIL WELL DRAINED AND COMPACTED
- 406 SEE OTHER DRAWINGS IN THIS SDD
- 407 SEE OTHER DRAWINGS FOR MIN. SPACING BETWEEN SPANS
- 408 SEE SDD 14B44
- 409 SEE SDD 14B45
- 410 SEE SDD 14B47
- 411 MINIMUM DISTANCE BETWEEN MISSING POST SPANS.



SECTION A - A



SECTION B - B

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2021 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	

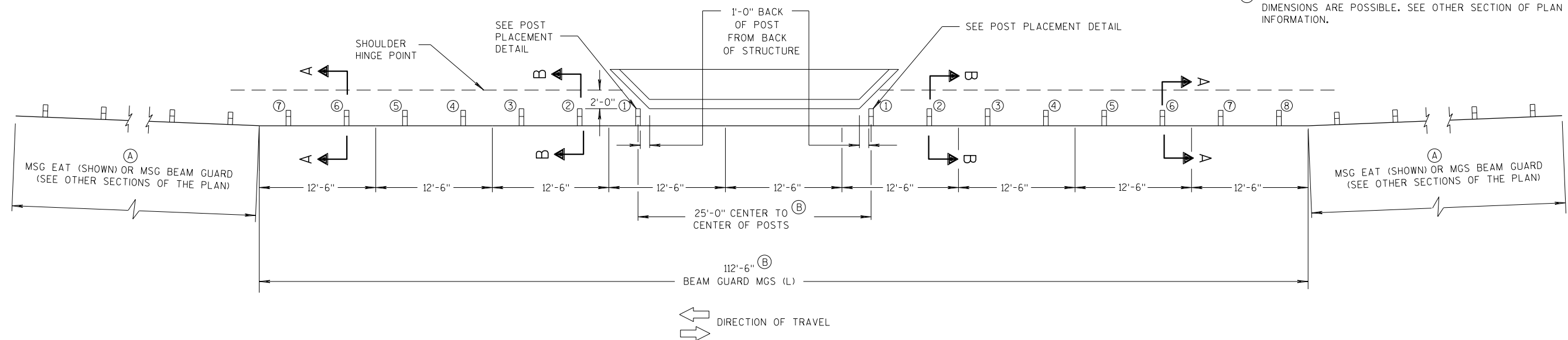
GENERAL NOTES

POSTS 1 THROUGH 3 ARE CRT POSTS.
ALL OTHER POSTS SHALL BE WOOD OR STEEL.

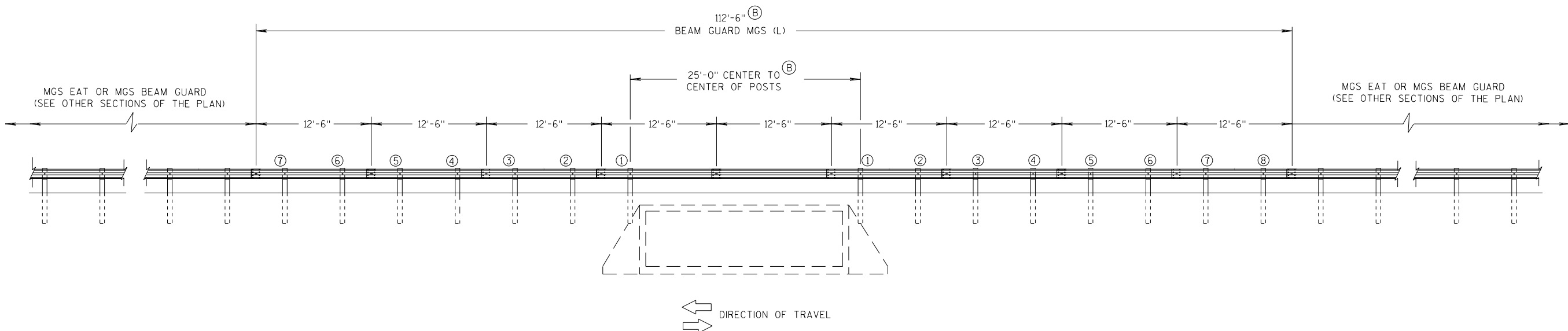
SEE SDD 14 B 42 FOR MORE DETAILS.

(A) FLARE FOR MGS EAT SHOWN, IF INSTALLING MGS NO FLARE NEEDED.

(B) VALUES SHOWN ON DRAWING REPRESENT THE MAXIMUM LENGTH. SHORTER DIMENSIONS ARE POSSIBLE. SEE OTHER SECTION OF PLAN FOR MORE INFORMATION.



PLAN VIEW



ELEVATION VIEW

MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L) TWO-WAY TRAFFIC

**MIDWEST GUARDRAIL SYSTEM
LONG SPAN MGS (L)**

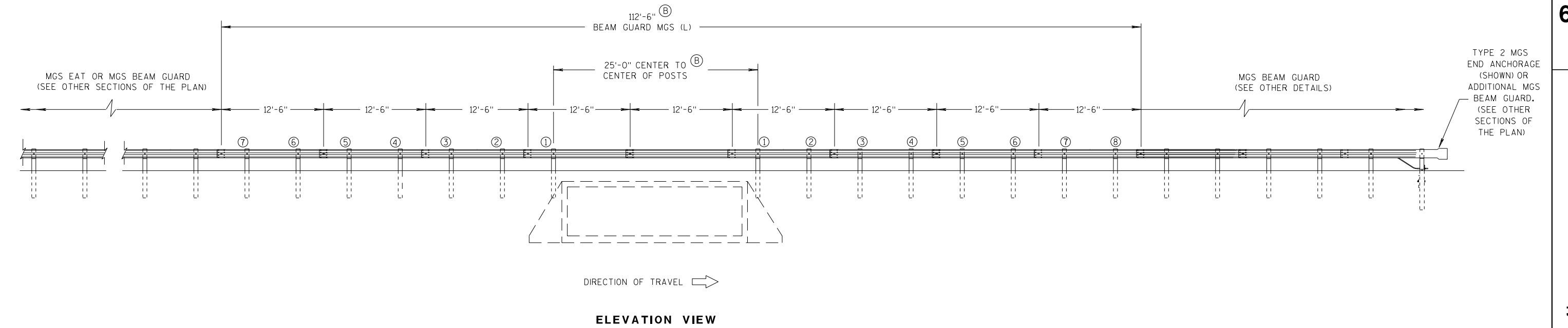
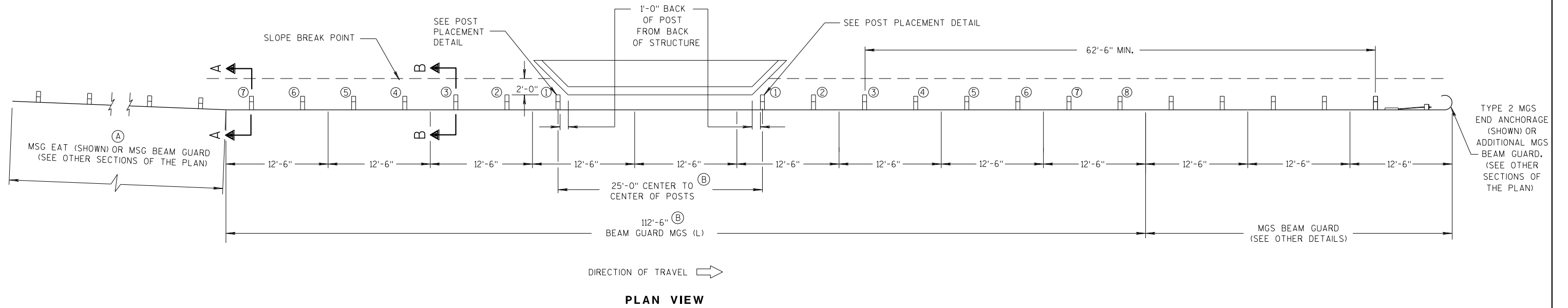
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

POSTS 1 THROUGH 3 ARE CRT POSTS.
ALL OTHER POSTS SHALL BE WOOD OR STEEL.

SEE SDD 14 B 42 FOR MORE DETAILS.

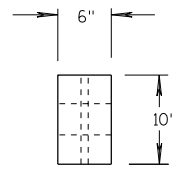
- (A) FLARE FOR MGS EAT SHOWN. IF INSTALLING MGS NO FLARE NEEDED.
- (B) VALUES SHOWN ON DRAWING REPRESENT THE MAXIMUM LENGTH. SHORTER DIMENSIONS ARE POSSIBLE. SEE OTHER SECTION OF PLAN FOR MORE INFORMATION.



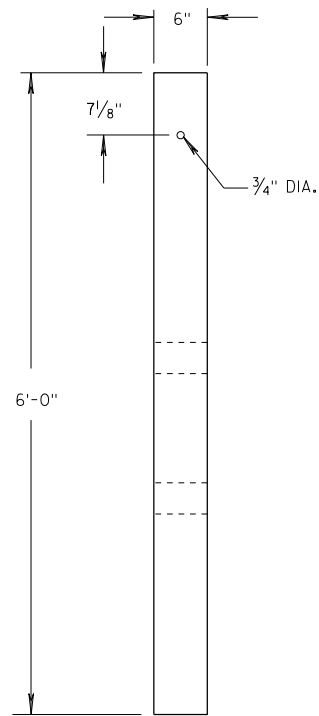
MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L) ONE-WAY TRAFFIC

**MIDWEST GUARDRAIL SYSTEM
LONG SPAN MGS (L)**

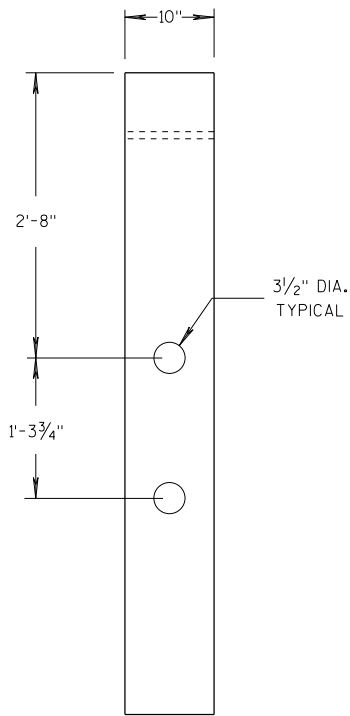
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW

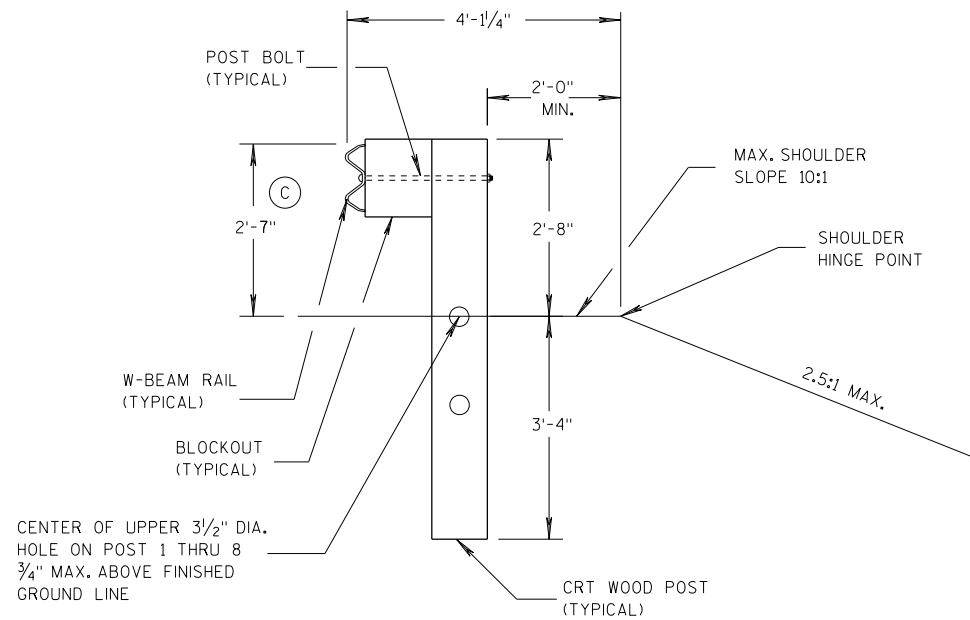


FRONT VIEW

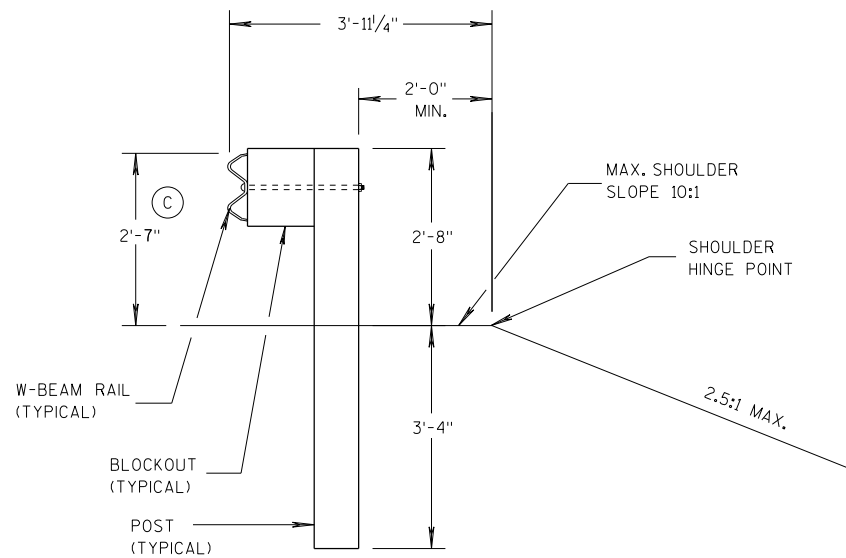


SIDE VIEW

CRT WOOD POST



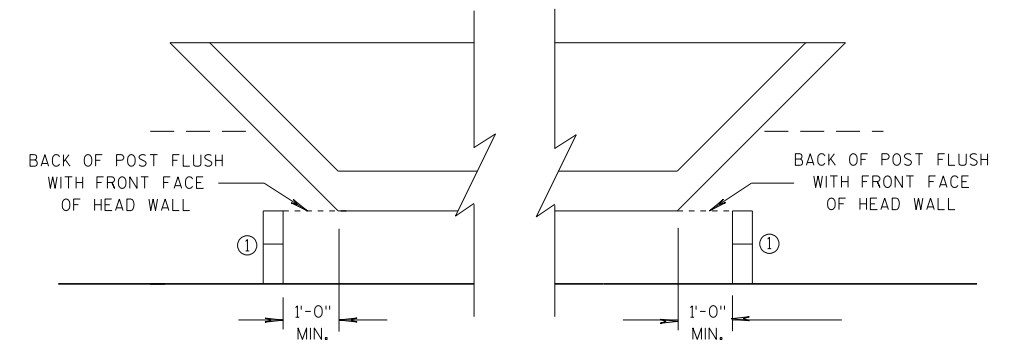
SECTION B-B
POSTS NO. 1-3
SEE OTHER DETAILS



SECTION A-A
POSTS NO. 4-8
SEE OTHER DETAILS

GENERAL NOTES

(C) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".



POST PLACEMENT DETAIL

MIDWEST GUARDRAIL SYSTEM
LONG SPAN MGS (L)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED	/s/ Rodney Taylor
DATE	07/2018
FHWA	ROADWAY STANDARDS DEVELOPMENT ENGINEER

GENERAL NOTES

- (A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.
 - (B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED
 - (C) DIFFERENT MANUFACTURERS REQUIRE DIFFERENT PERFORATED W - BEAM RAIL END PANELS. SEE MANUFACTURER'S INFORMATION.
 - (D) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF - TAPPING SCREWS. ONE SCREW PER CORNER.
 - (E) HARDWARE MAY VARY BETWEEN MANUFACTURER. SEE MANUFACTURER'S DRAWING FOR INFORMATION.
- DIMENSIONS MAY VARY, MANUFACTURER'S INFORMATION.

SEE SDD 14B42 FOR MORE INFORMATION.

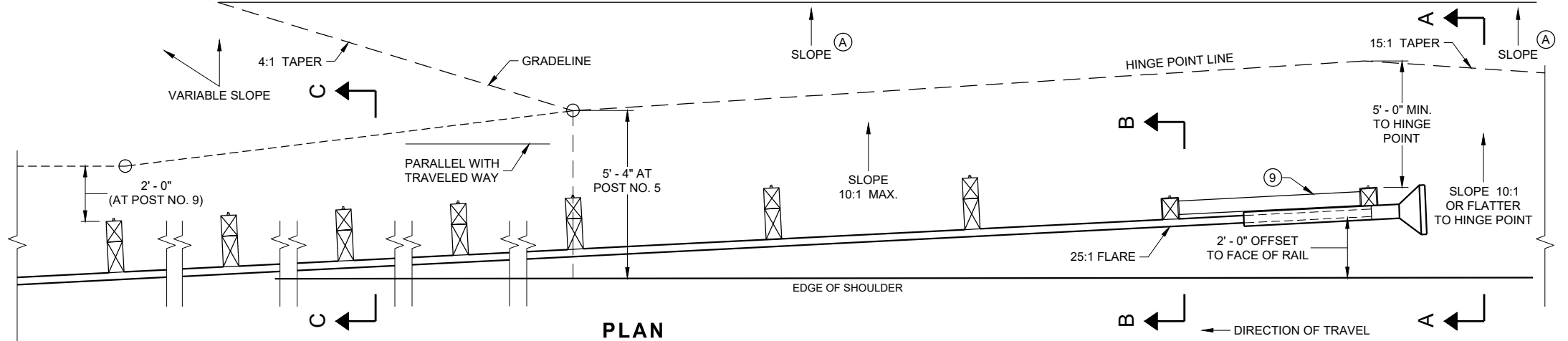
* DO NOT ATTACH BLOCKOUTS TO POST 1 AND 2.

DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

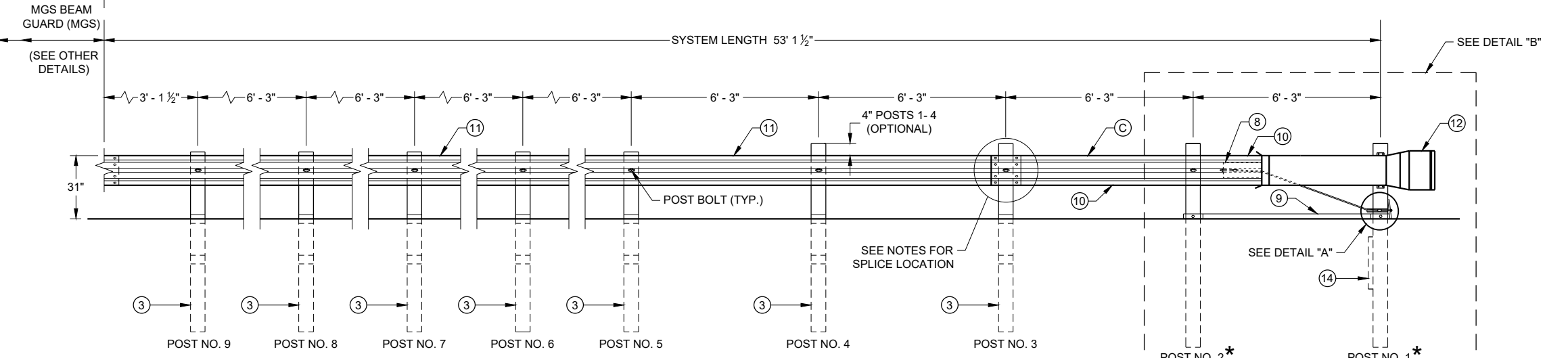
SEE MANUFACTURER'S DRAWING FOR SPLICE LOCATION, HARDWARE DIMENSIONS AND INSTALLATION INSTRUCTIONS.

THE CENTER OF THE UPPER 3 1/2" DIAMETER HOLE ON POST NUMBER 3 THROUGH POST 9 IS TO BE FLUSH WITH THE GROUND LINE UP TO A MAXIMUM OF 2" ABOVE GROUND LINE. WOOD BLOCKS ON POSTS NUMBERED 3 THROUGH 9 MAY BE ADJUSTED UP TO 3" ABOVE THE TOP OF POST.

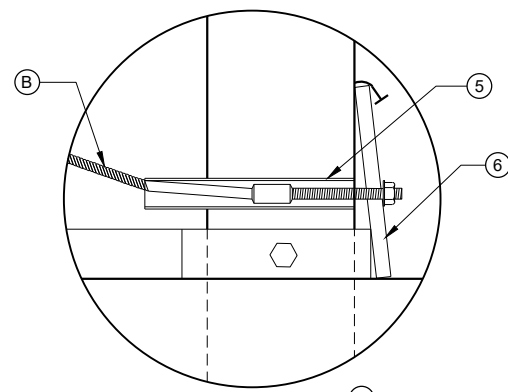
CLEAR ZONE LIMITS, EITHER AS SHOWN ELSEWHERE IN THE PLANS OR, IF NOT SHOWN ELSEWHERE IN THE PLANS, 15 FEET BEYOND THE HINGE POINT LINE



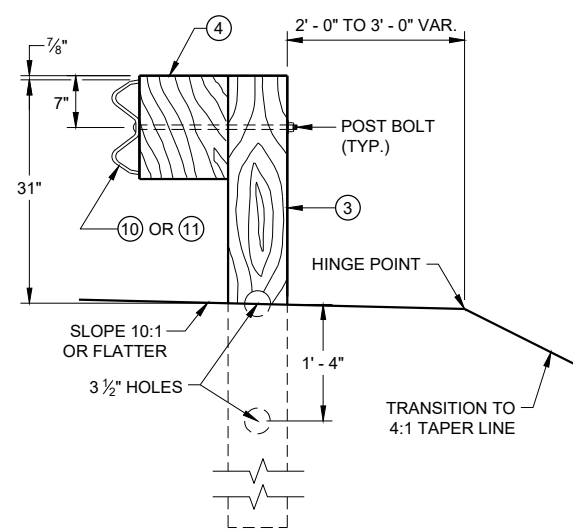
PLAN



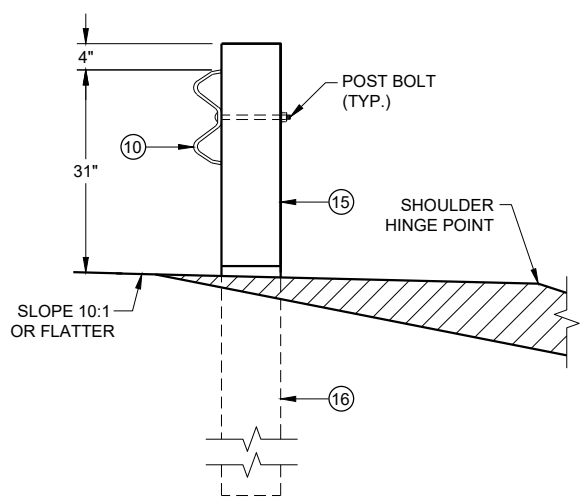
ELEVATION



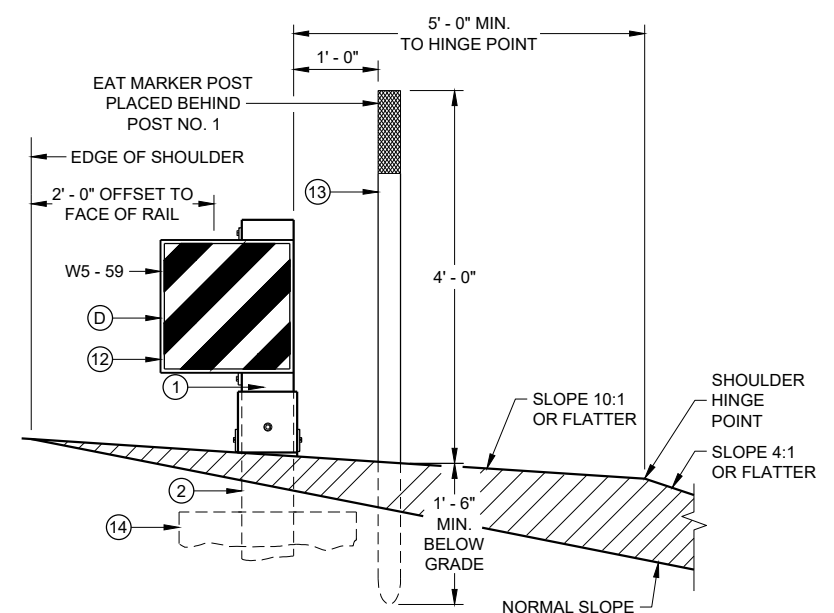
DETAIL "A"



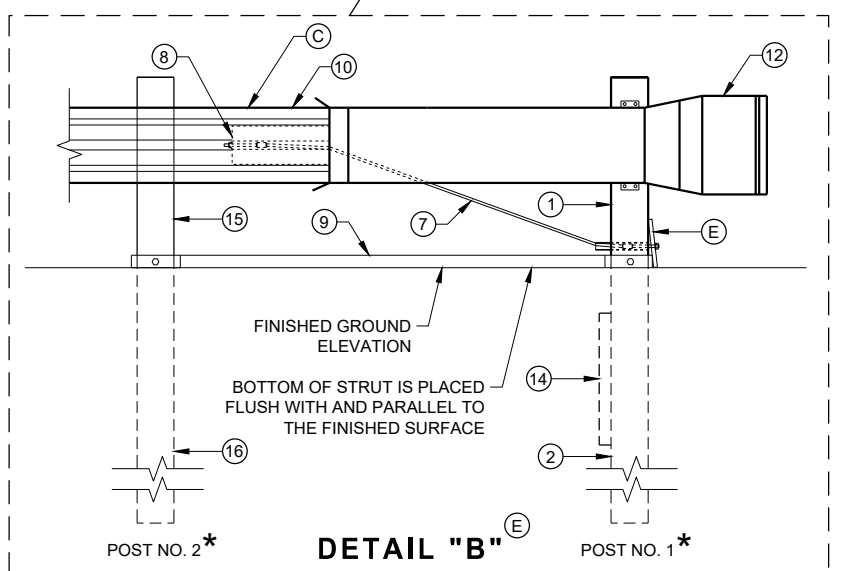
**SECTION C - C
TYPICAL AT POST NOS. 3 - 9**



**SECTION B - B
TYPICAL AT POST NO. 2***



**SECTION A - A
TYPICAL AT POST NO. 1***



DETAIL "B"

**MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

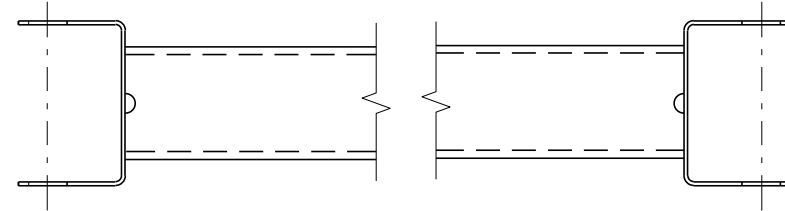
6

SDD 14B44 - 04a

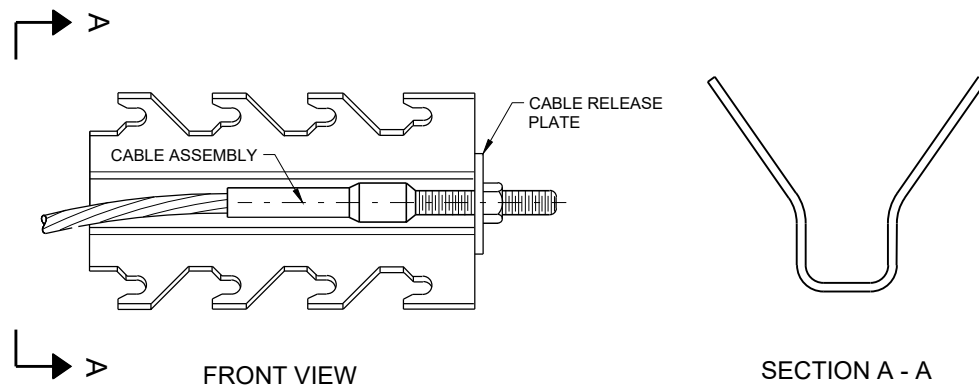
SDD 14B44 - 04a

BILL OF MATERIALS

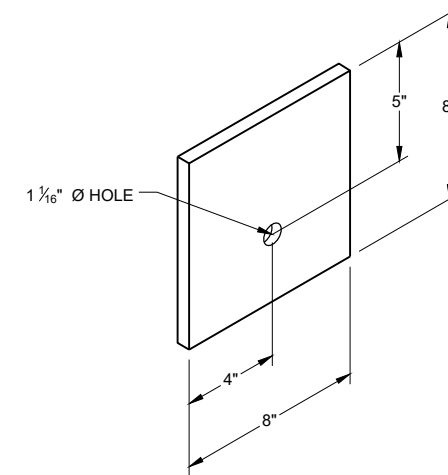
PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUFACTURER'S DETAILS FOR MORE INFORMATION.
①	UPPER POST NO. 1 6" X 6" TUBE
②	LOWER POST NO. 1
③	WOOD CRT
④	WOOD BLOCKOUT
⑤	PIPE SLEEVE
⑥	BEARING PLATE
⑦	BCT CABLE ASSEMBLY
⑧	ANCHOR CABLE BOX
⑨	GROUND STRUT
⑩	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
⑪	STANDARD W-BEAM RAIL. MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
⑫	IMPACT HEAD
⑬	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)
⑭	SOIL PLATE
⑮	UPPER POST NO. 2
⑯	LOWER POST NO. 2



GENERIC GROUND STRUT ⑨ ⑤



GENERIC ANCHOR CABLE BOX ⑨ ⑤



BEARING PLATE ⑥ ⑤

6

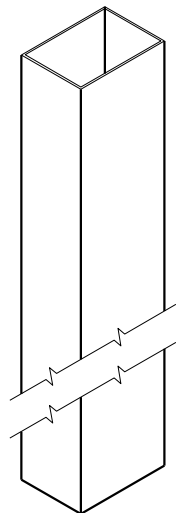
6

SDD 14B44 - 04b

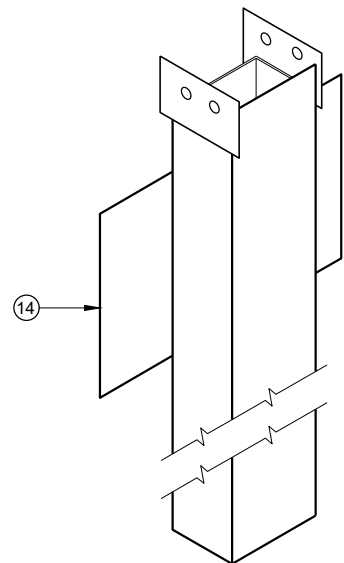
SDD 14B44 - 04b

**MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)**

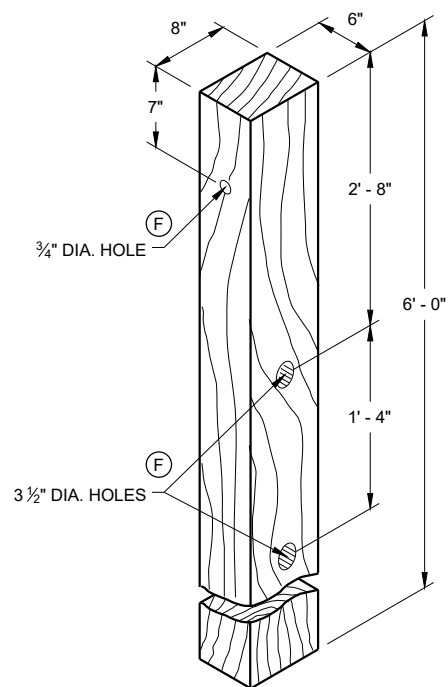
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



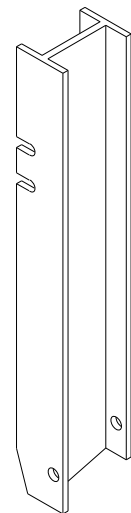
UPPER POST NO. 1 ⁽¹⁾ (E)



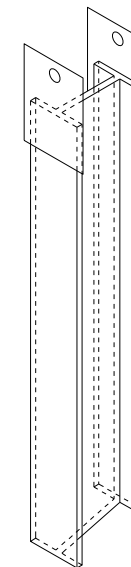
LOWER POST NO. 1 ⁽²⁾ (E)



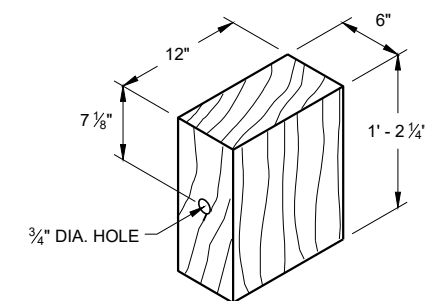
WOOD CRT POST ⁽³⁾ (E)
POSTS NUMBER 3-9



UPPER POST NO. 2 ⁽¹⁵⁾ (E)

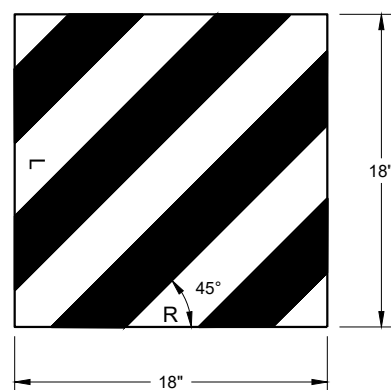


LOWER POST NO. 2 ⁽¹⁶⁾ (E)



WOOD BLOCKOUT ⁽⁴⁾
REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2

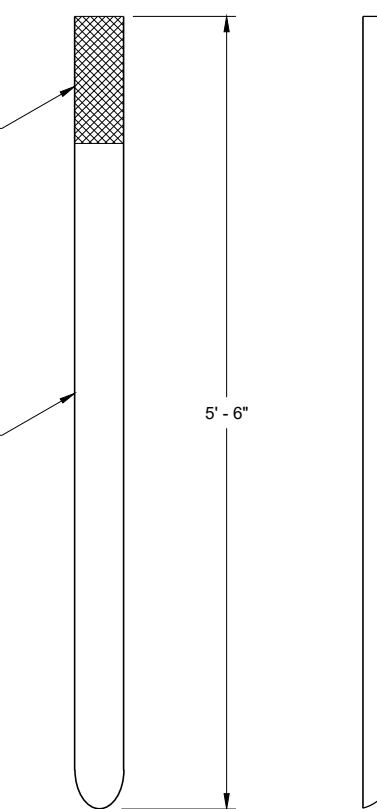
6



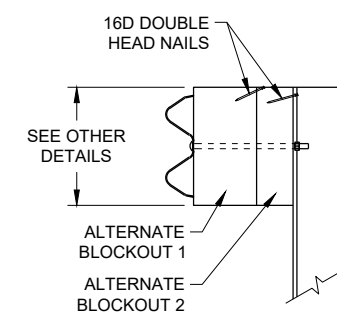
REFLECTIVE SHEETING DETAIL ^(E)

TYPE H
YELLOW REFLECTIVE
SHEETING 3" X 9".
SEE STANDARD
SPECIFICATION 637.

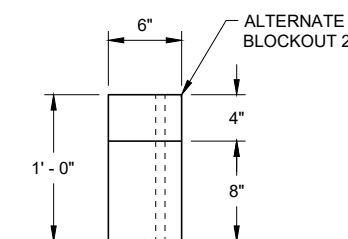
E.A.T. MARKER
POST (YELLOW)



FRONT VIEW SIDE VIEW
E.A.T. MARKER POST ⁽¹³⁾



SIDE VIEW



TOP VIEW

ALTERNATE WOOD
BLOCKOUT DETAIL

6

SDD 14B44 - 04c

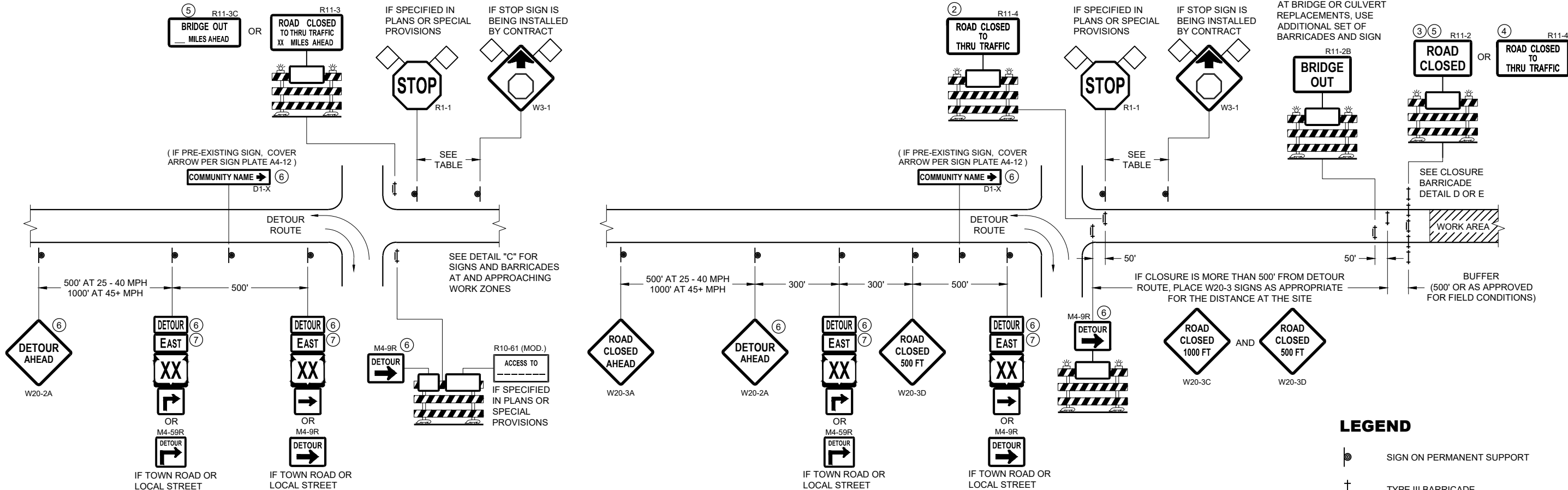
SDD 14B44 - 04c

**MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

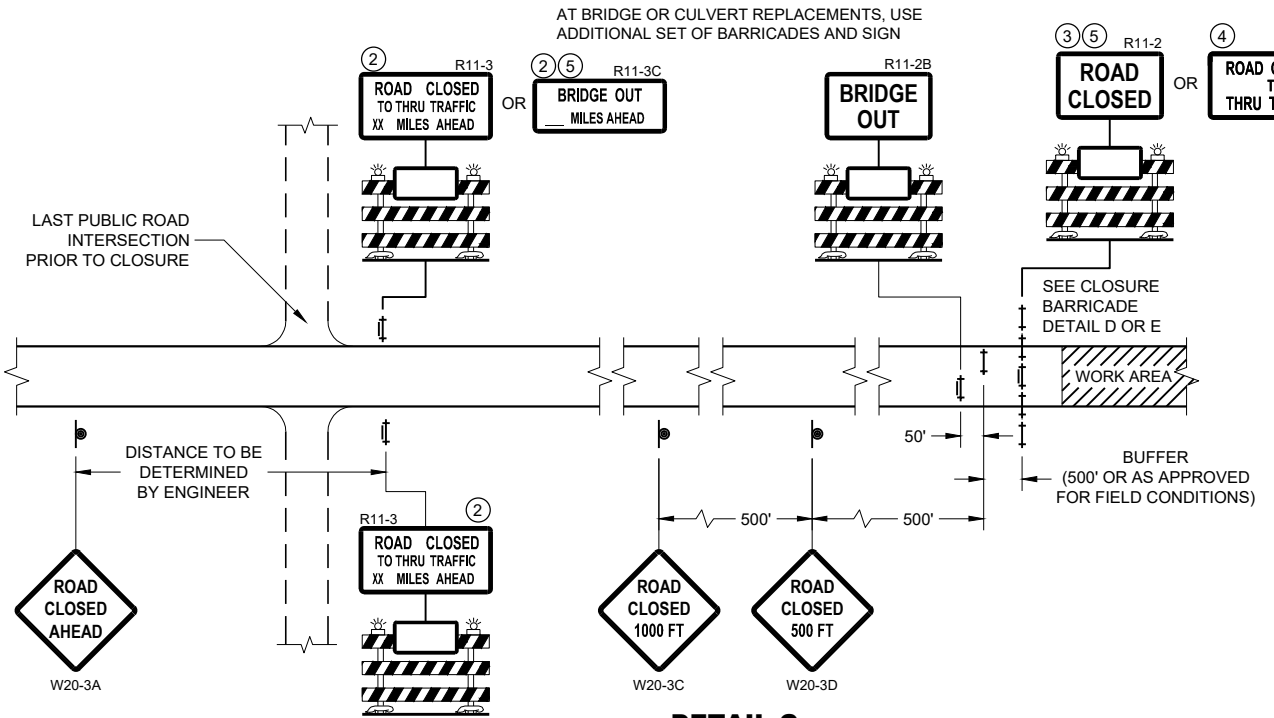
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



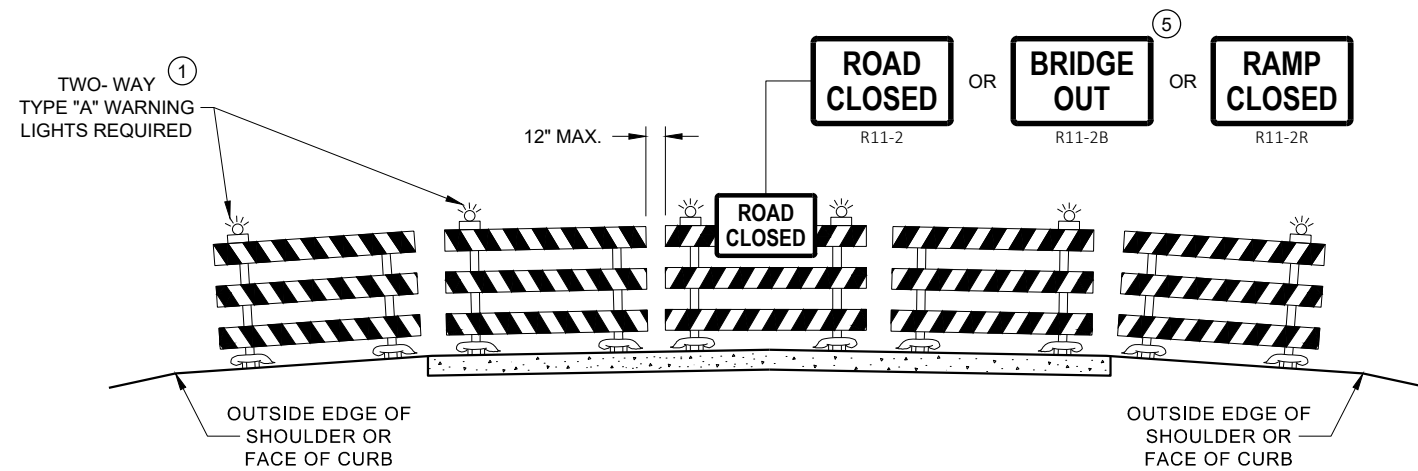
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

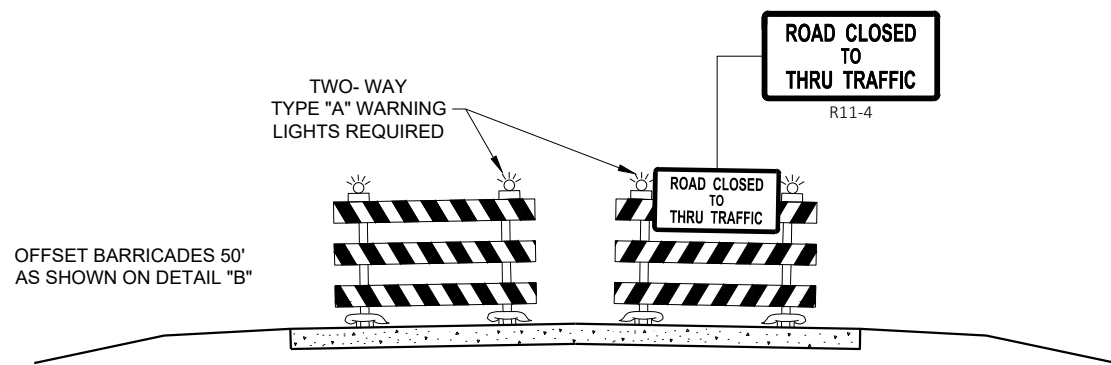
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER
FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

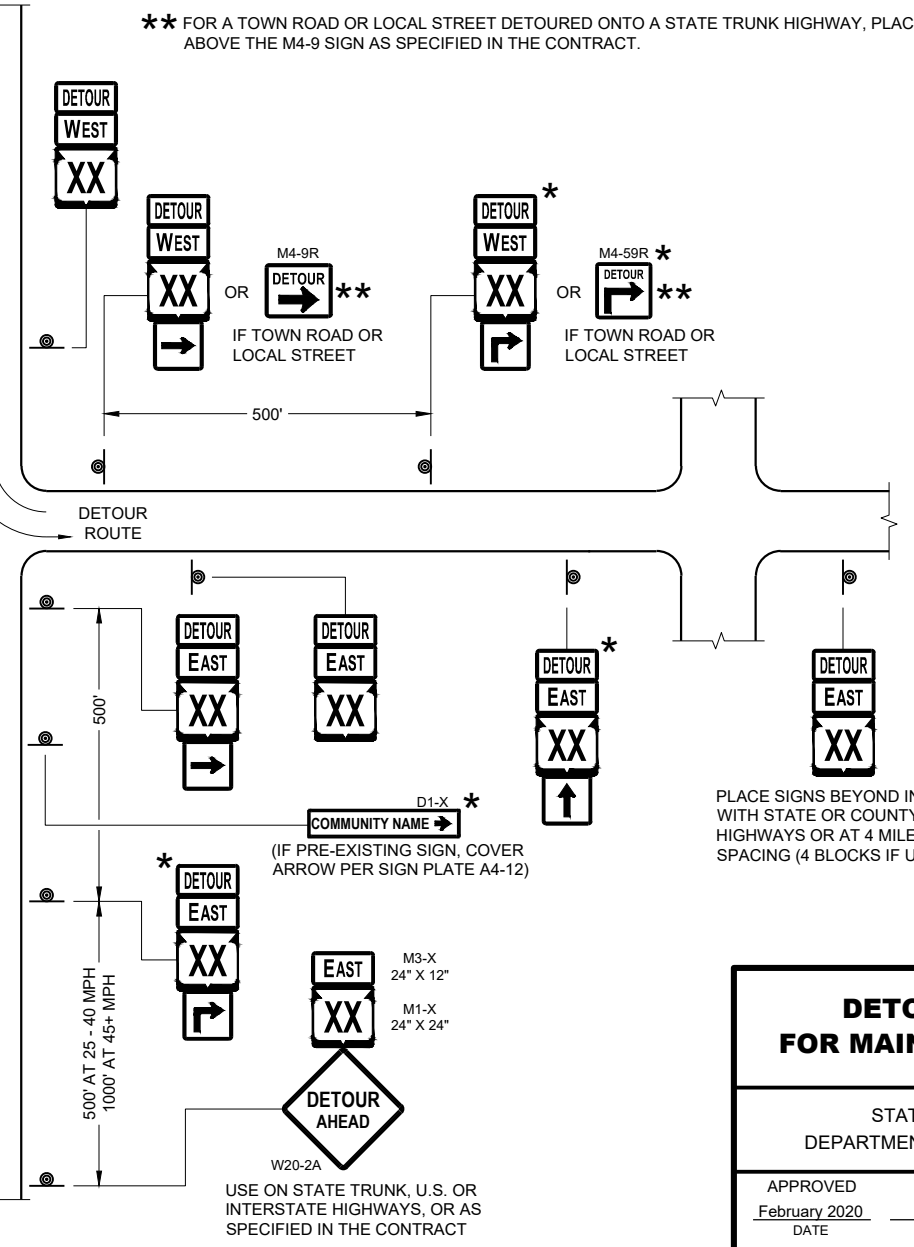
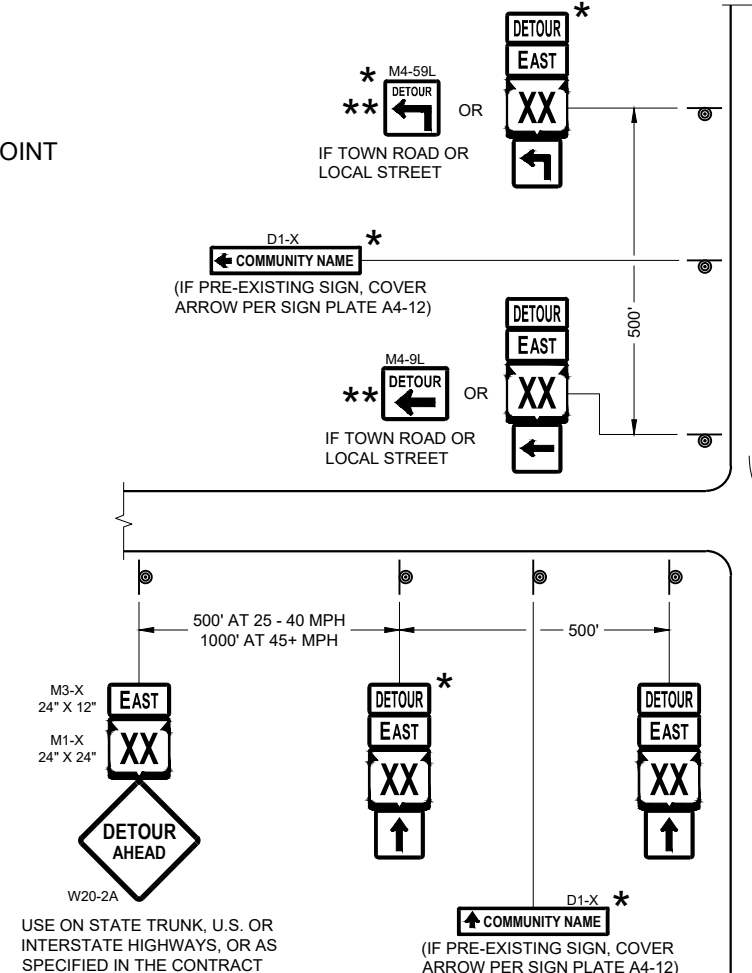
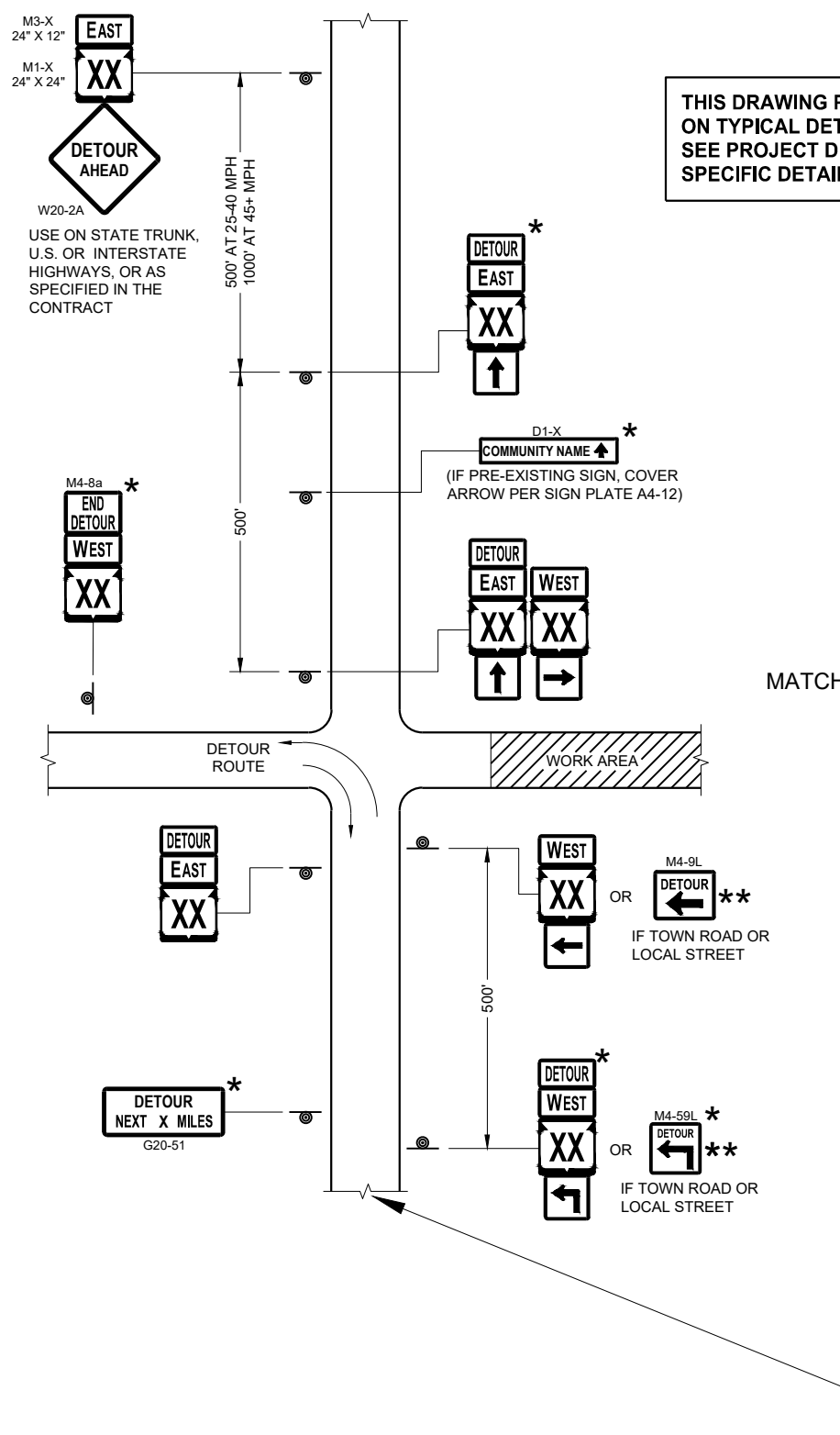
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

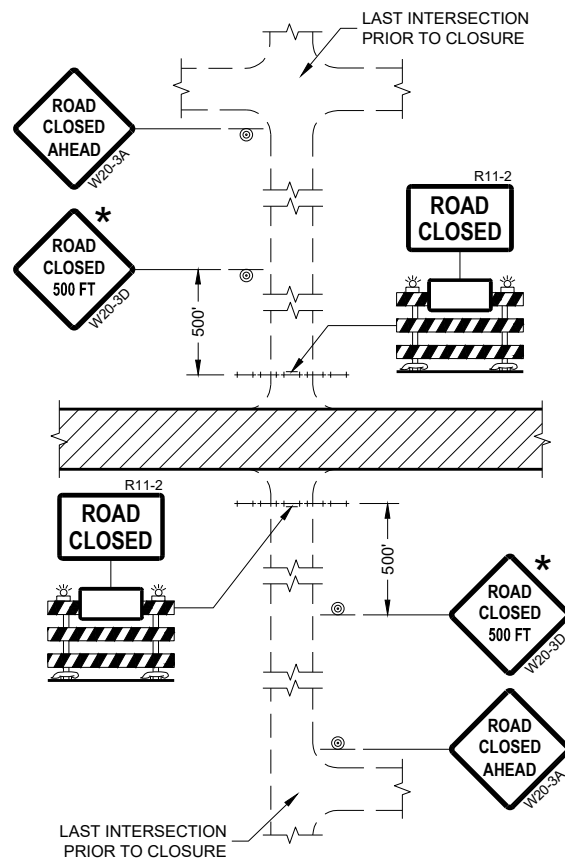
MATCH POINT

DETAIL F DETOUR SIGNING

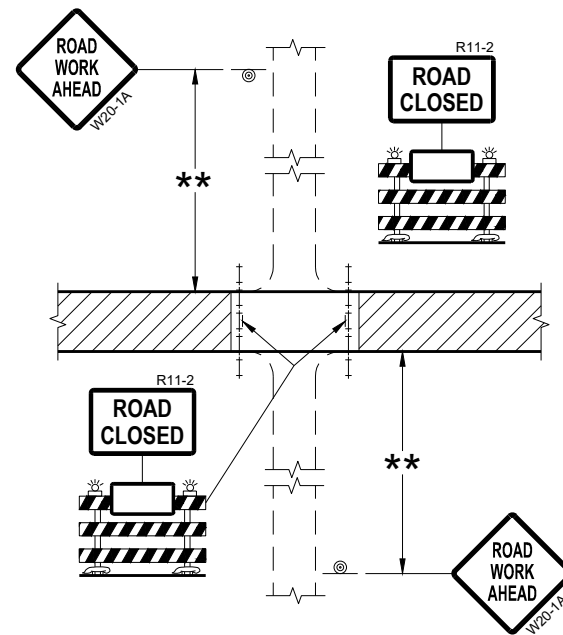


SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

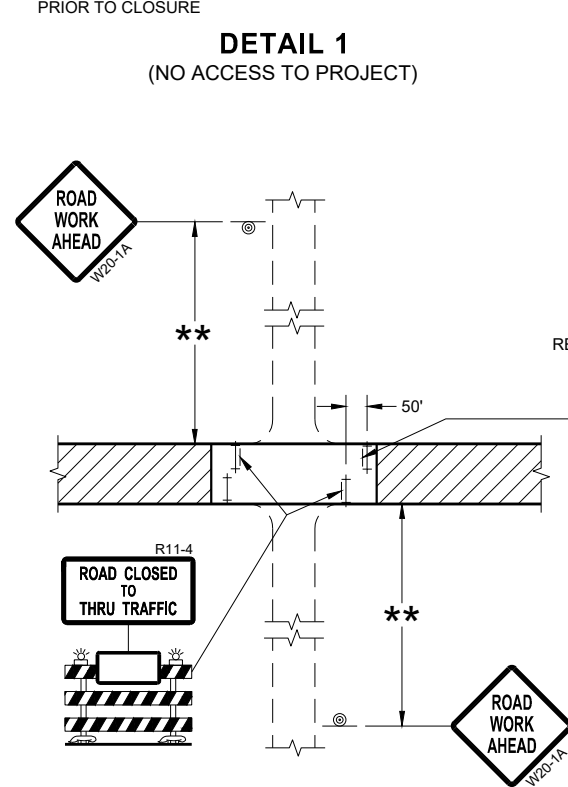
DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2020 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



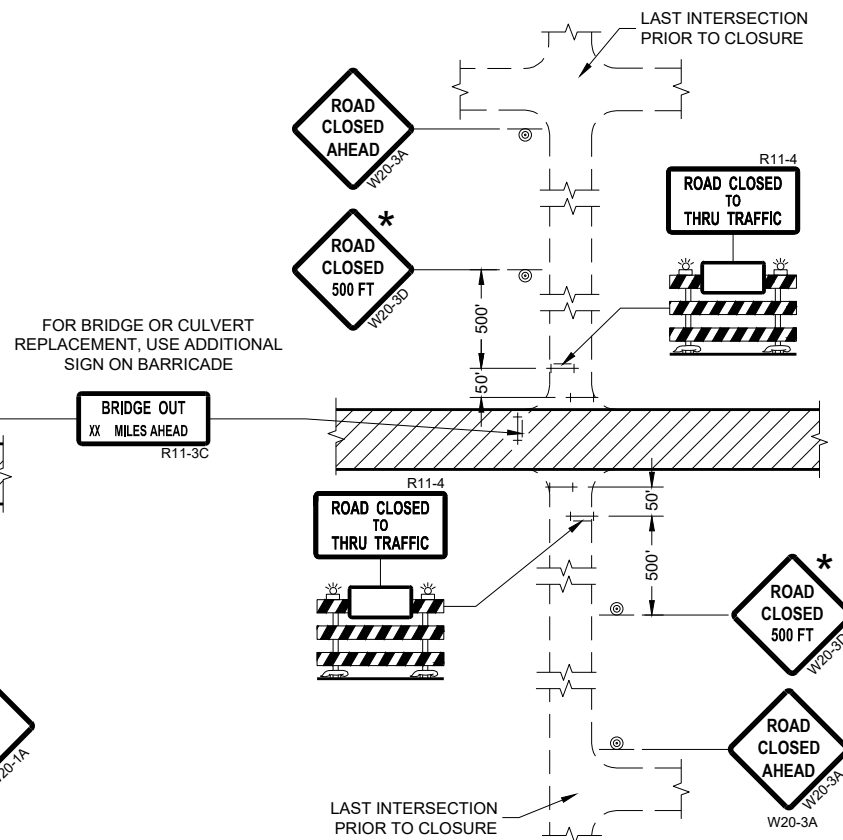
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

* OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.

** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

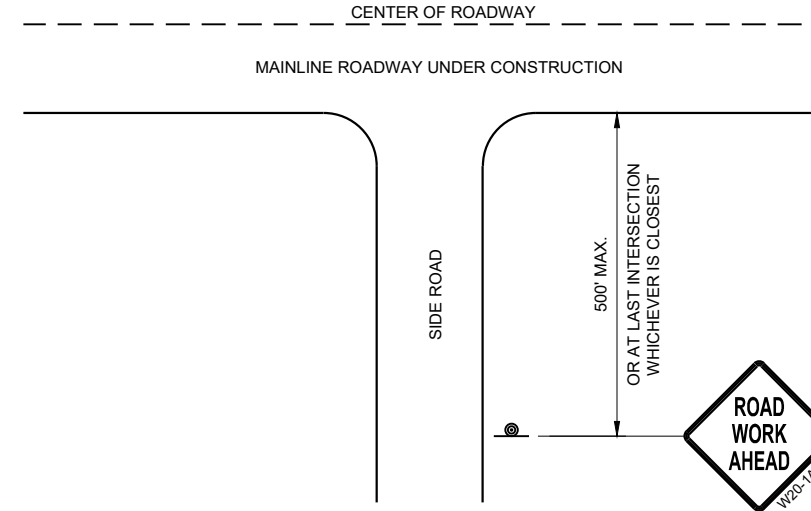
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

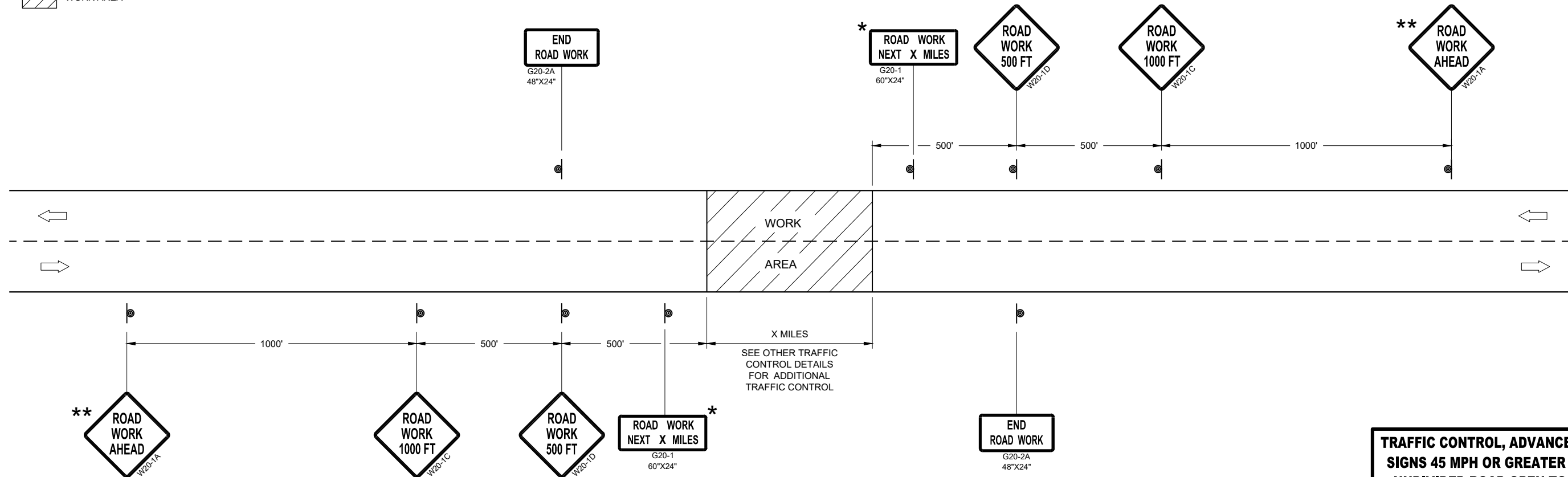
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



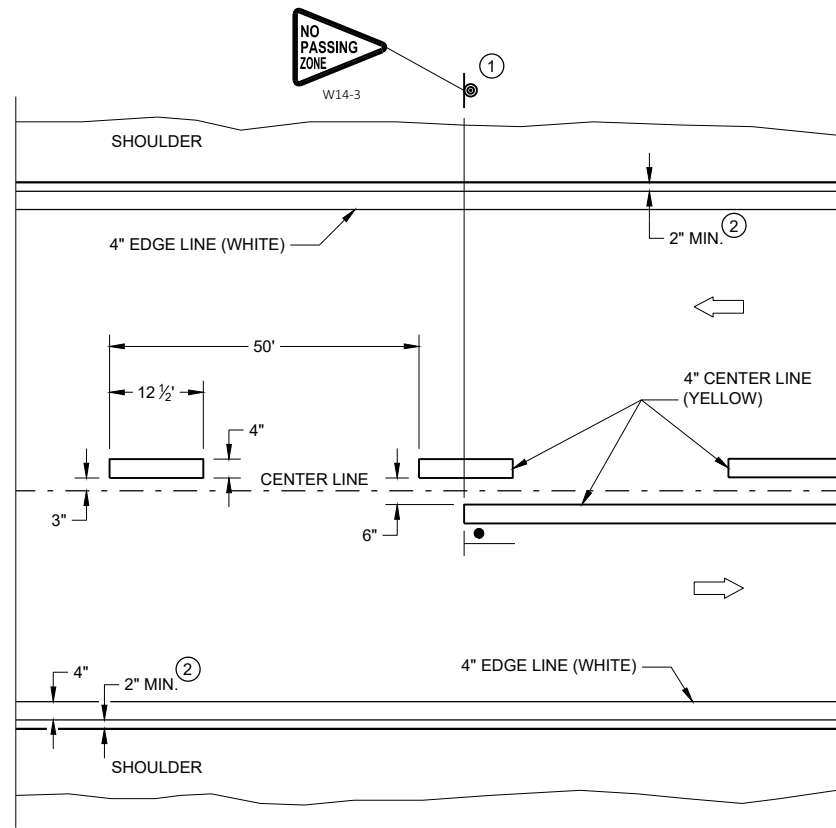
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 45 MPH OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

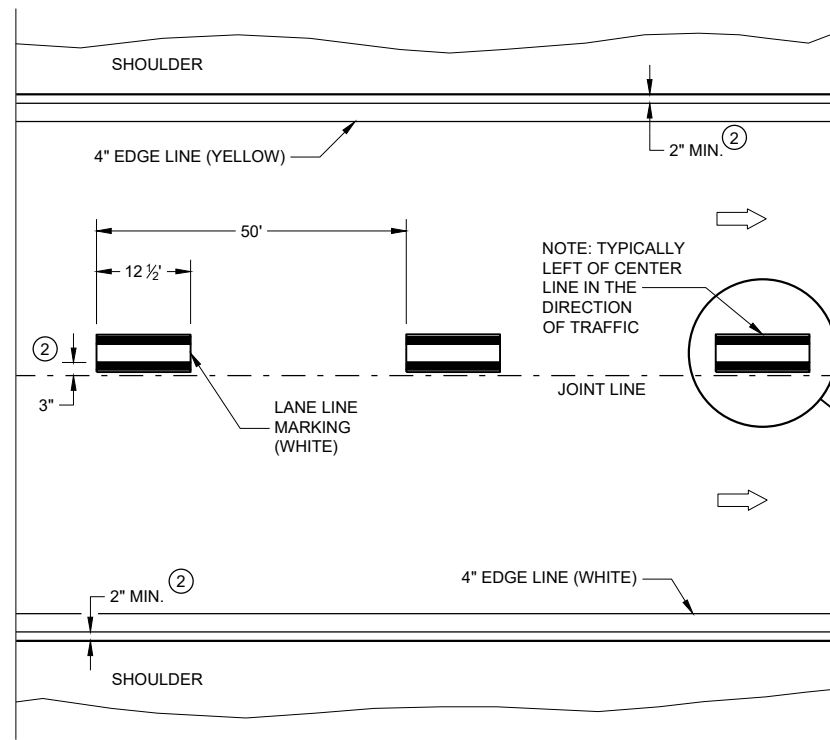
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

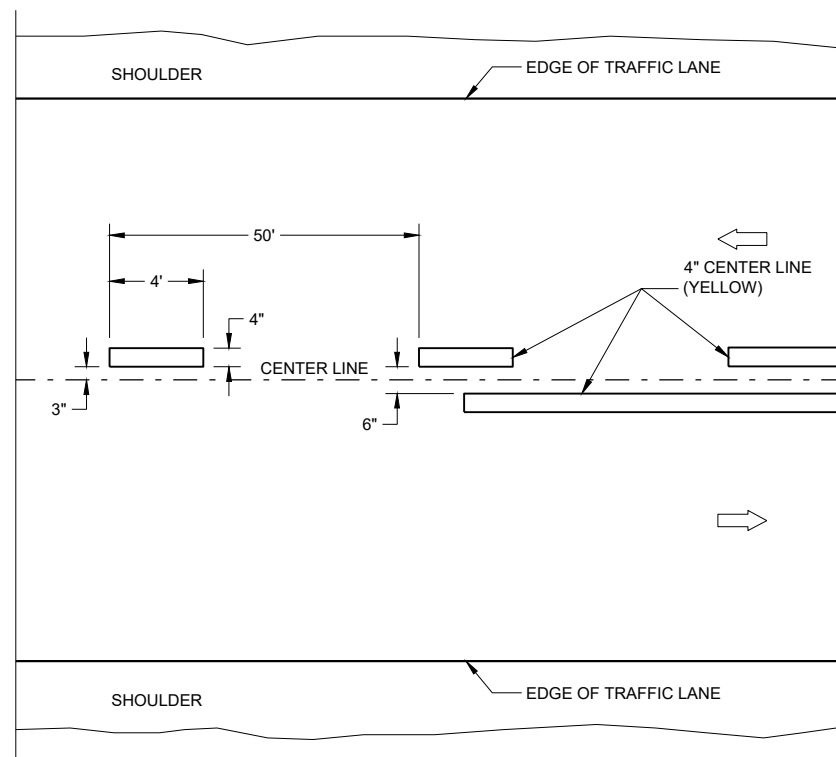


TWO WAY TRAFFIC

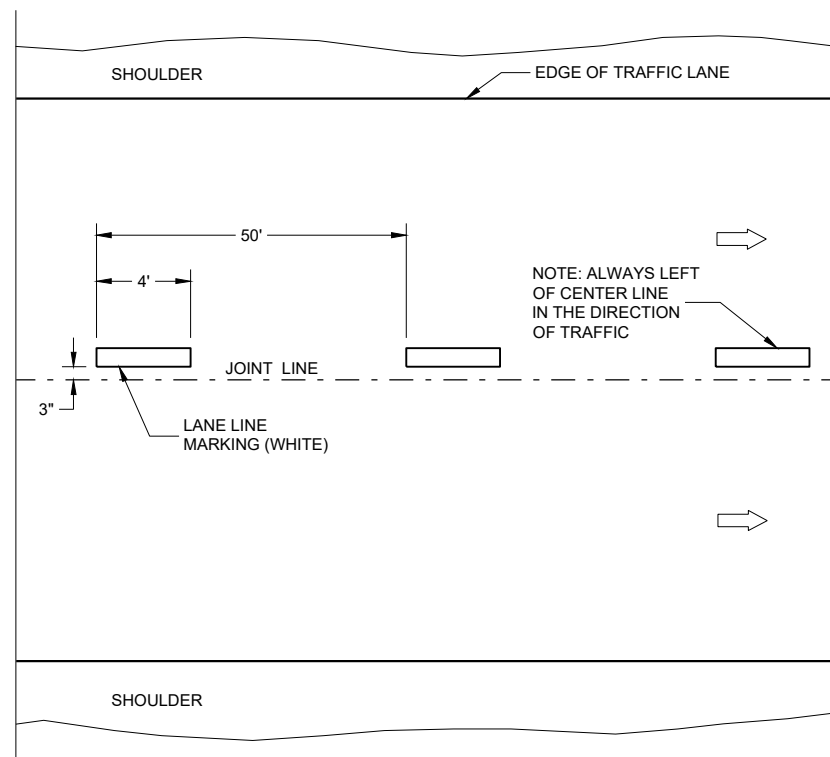


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

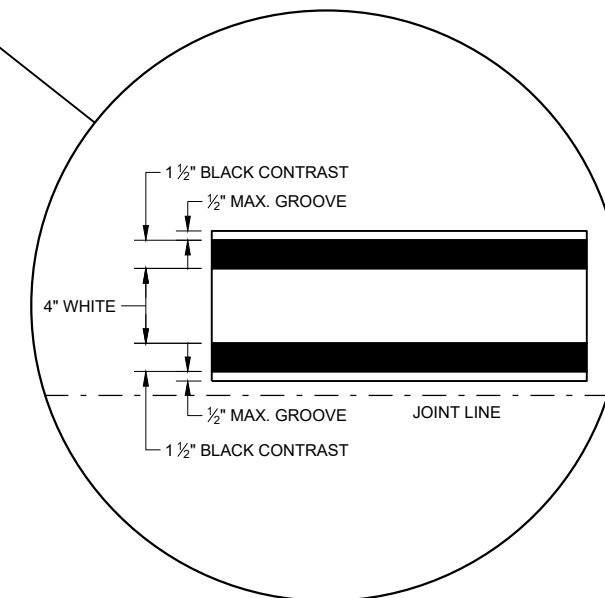
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

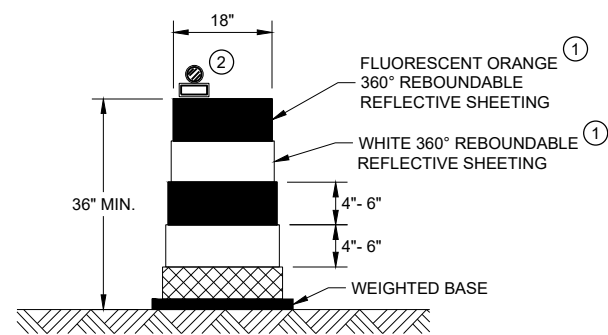
- |— "T" MARKING
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC



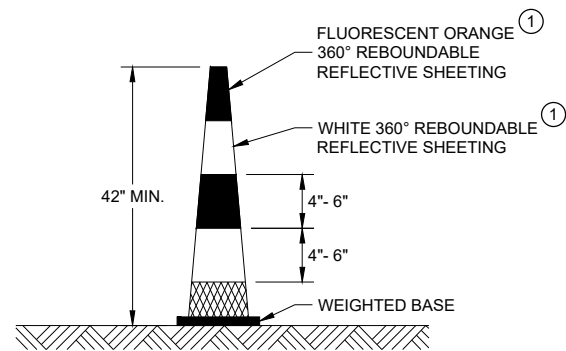
**LONGITUDINAL MARKING
(MAINLINE)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Matthew Rauch
DATE STATEWIDE SIGNING AND MARKING
ENGINEER



DRUM

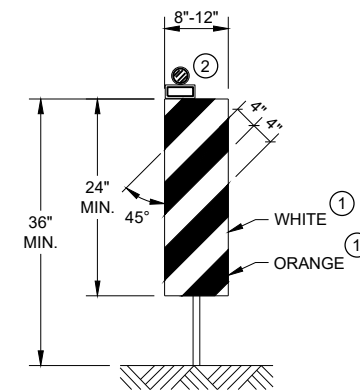


42" CONE

DO NOT USE IN TAPERS
1/2 SPACING OF DRUMS

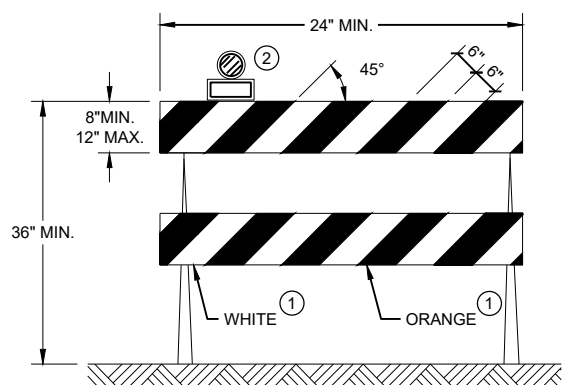
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



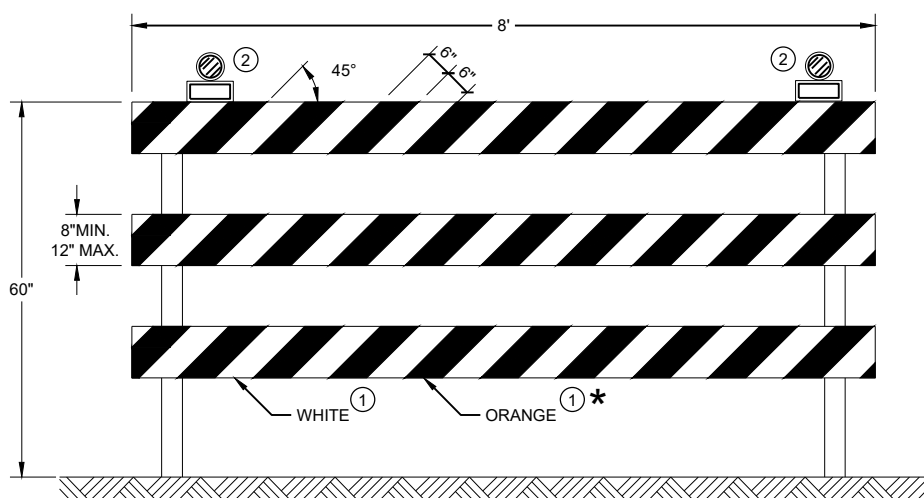
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.






* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

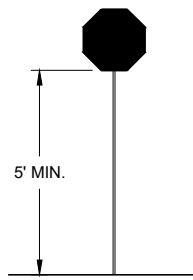
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



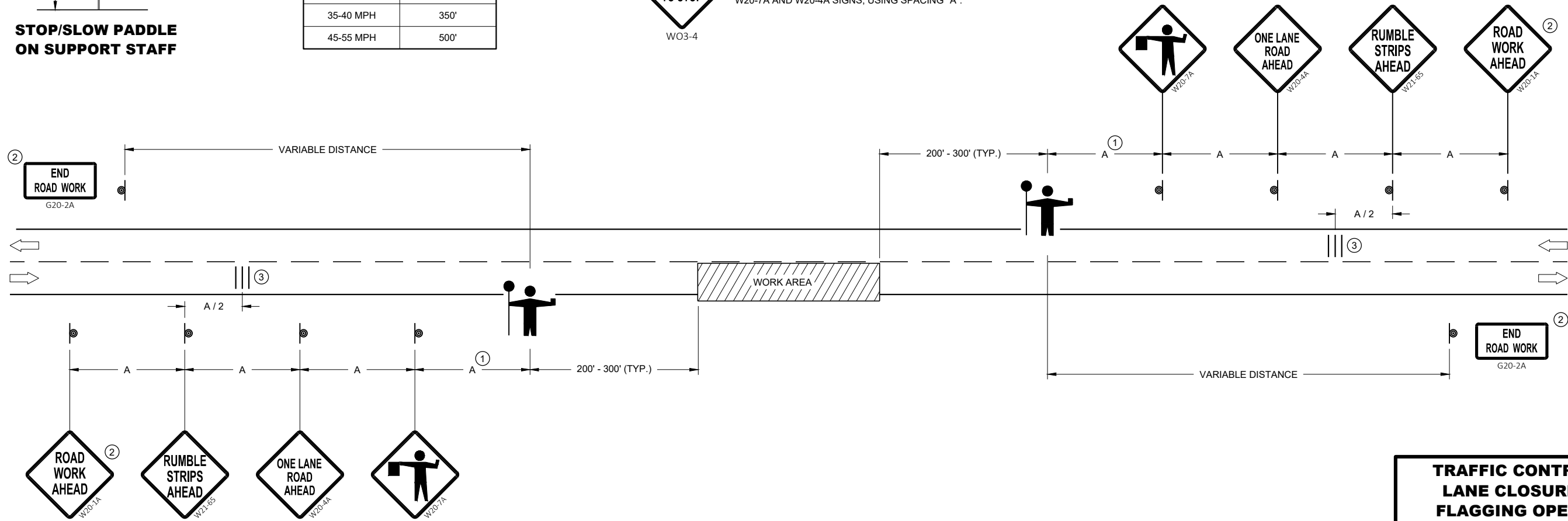
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF WO3-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".







TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

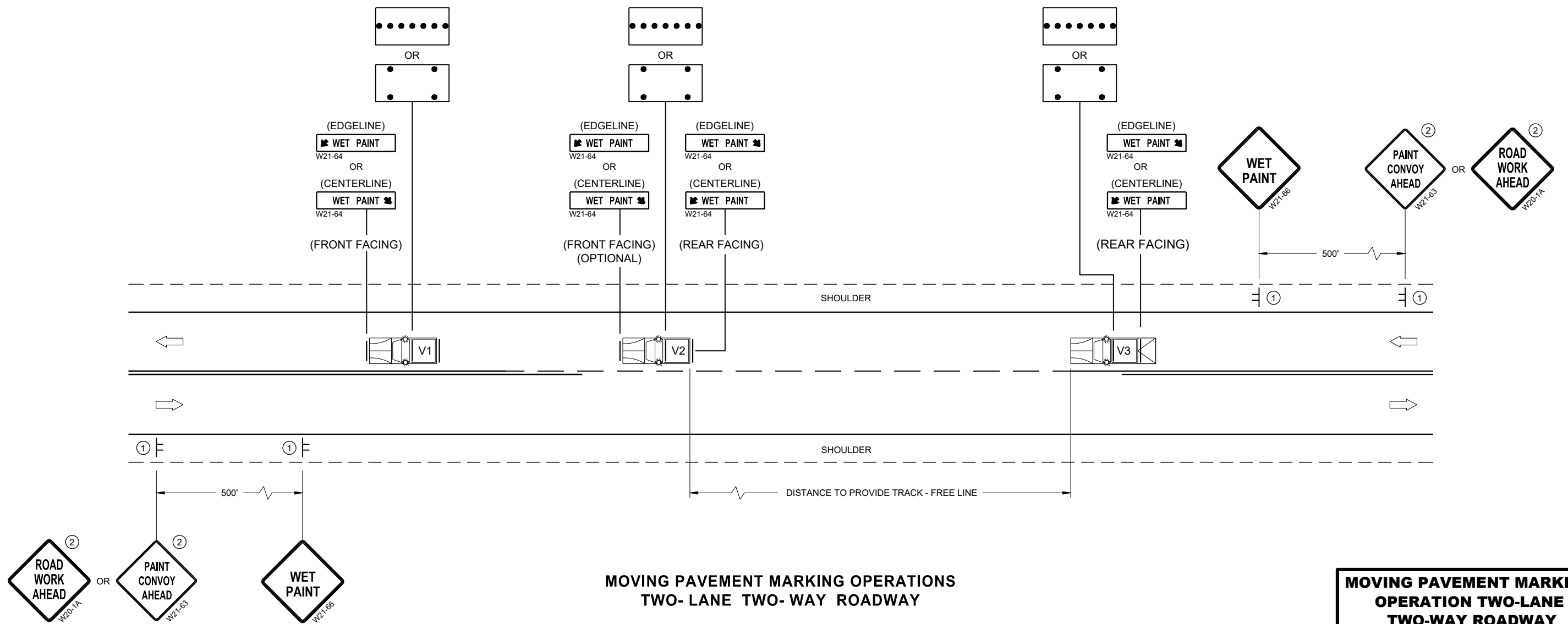
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

6

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**MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY**

SDD 15C19 - 06a

SDD 15C19 - 06a

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

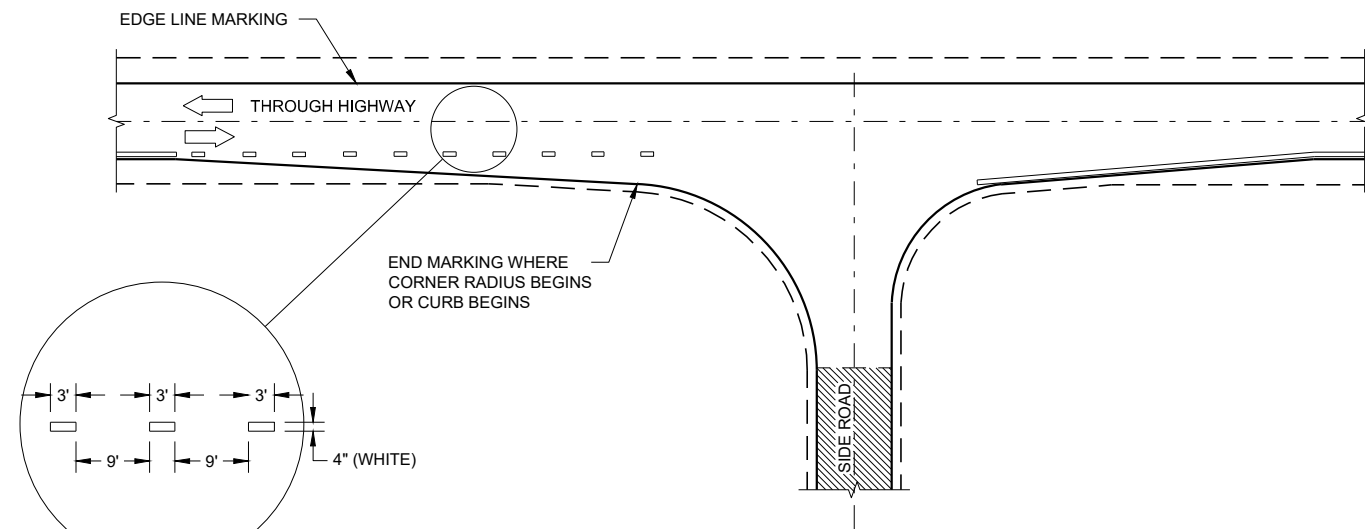
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

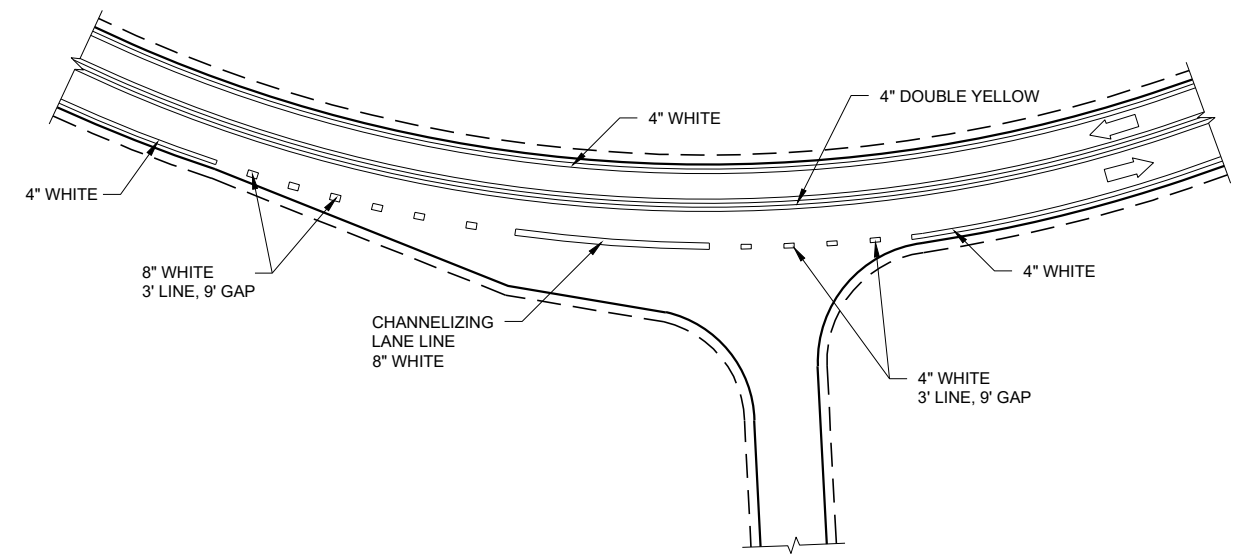
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

LEGEND

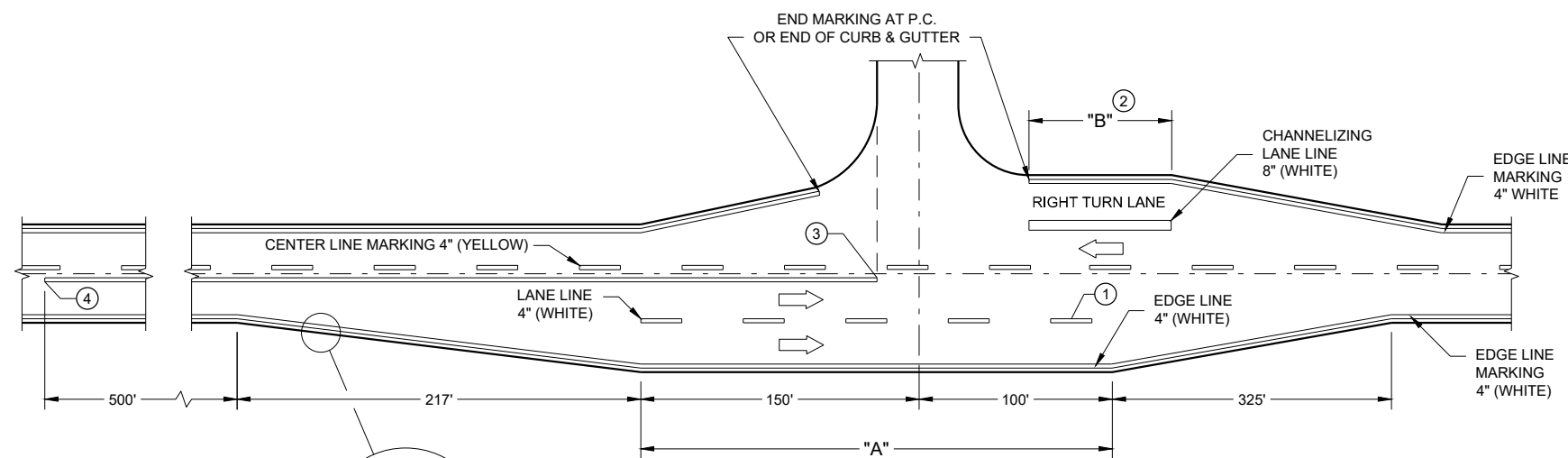
➡ DIRECTION OF TRAVEL



MINOR INTERSECTION



INTERSECTION ON OUTSIDE OF CURVE







**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

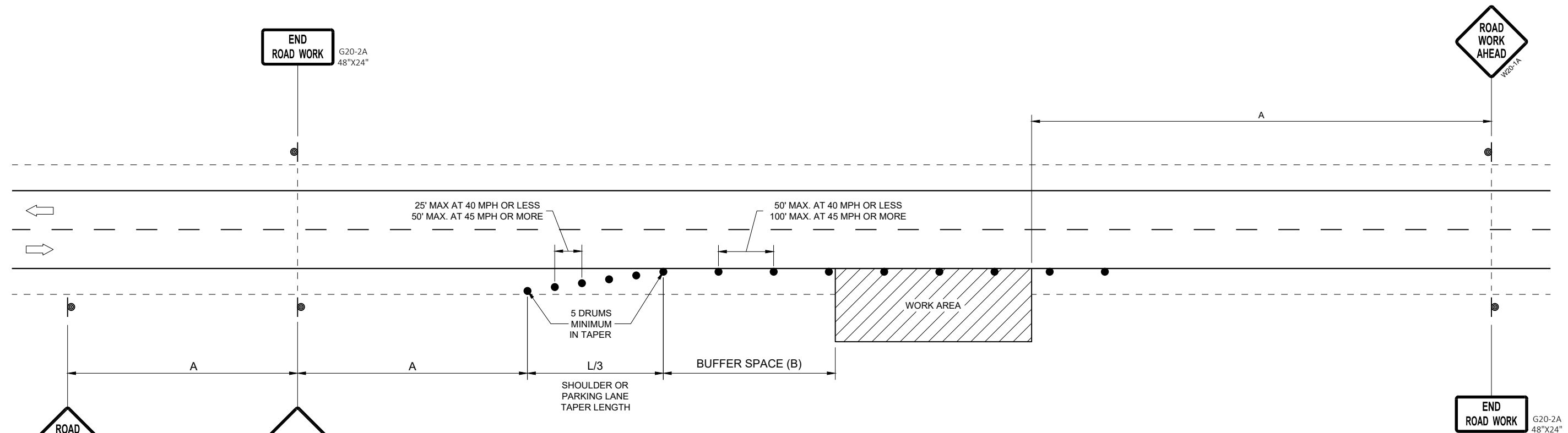
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

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POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

SDD 15D28 - 04

SDD 15D28 - 04

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.



THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

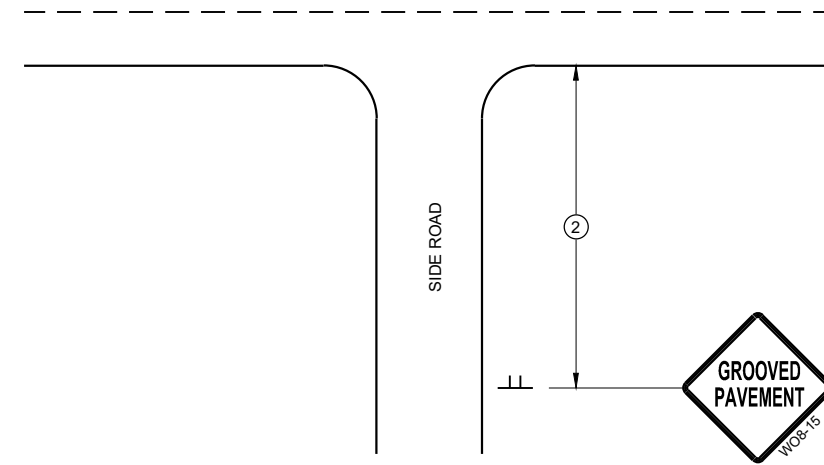
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

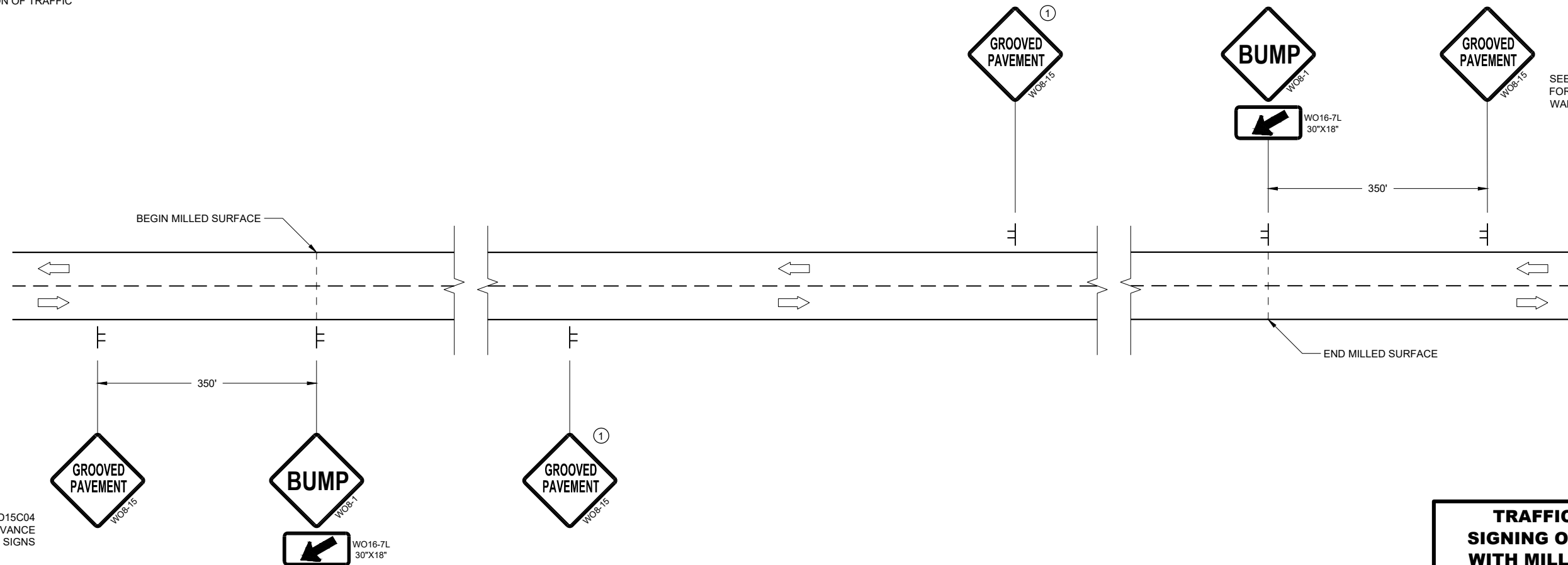
- ① PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



SEE SDD15C04 FOR ADVANCE WARNING SIGNS

SEE SDD15C04 FOR ADVANCE WARNING SIGNS

DETAIL FOR SIGNING ON MILLED SURFACES





TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

- V1 WORK VEHICLE
- V2 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  FLASHING ARROW PANEL (CAUTION)
-  WORK AREA
-  DIRECTION OF TRAFFIC

POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

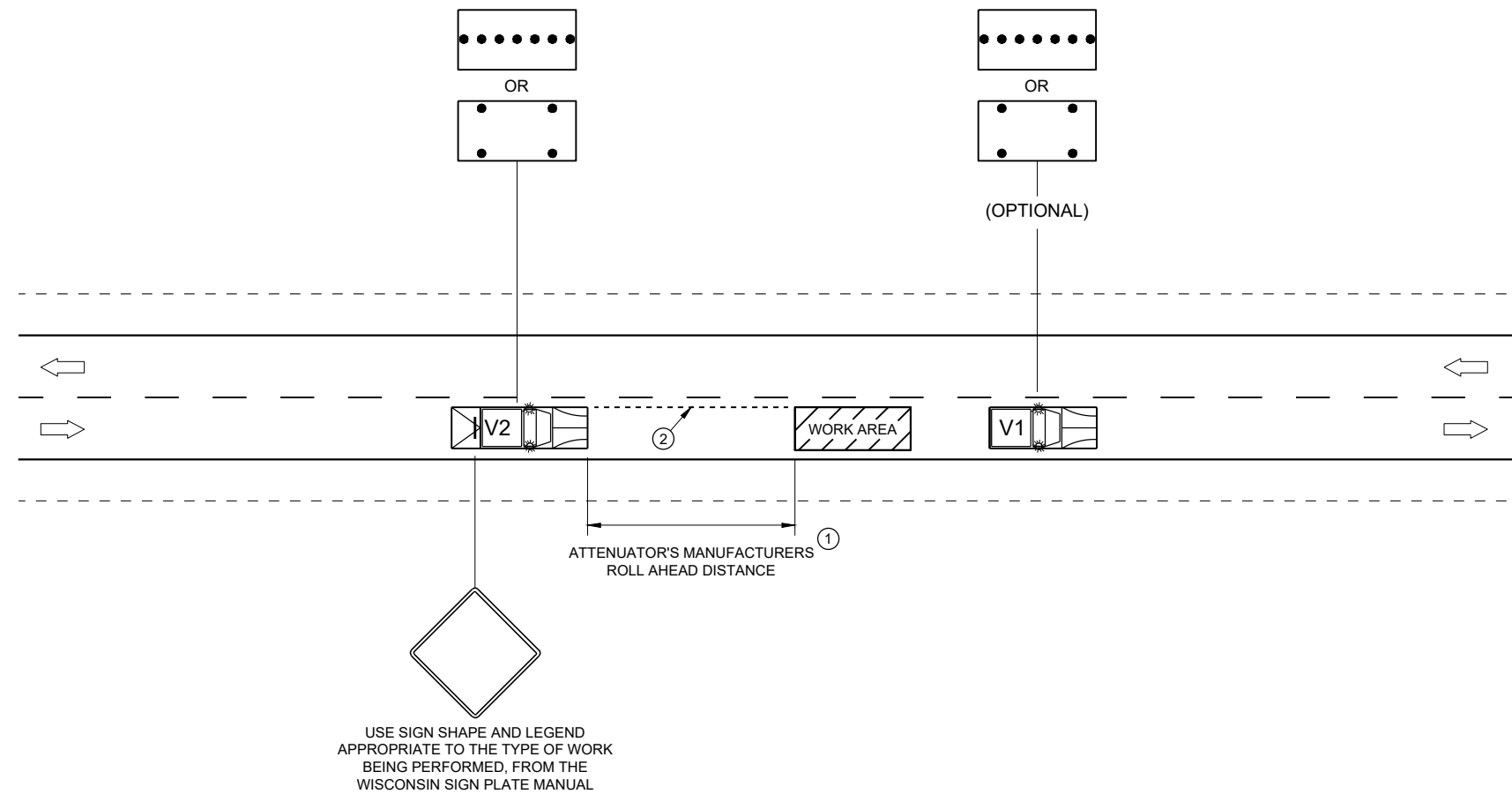
MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

- ① DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ② ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



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SDD 15D51 - 01

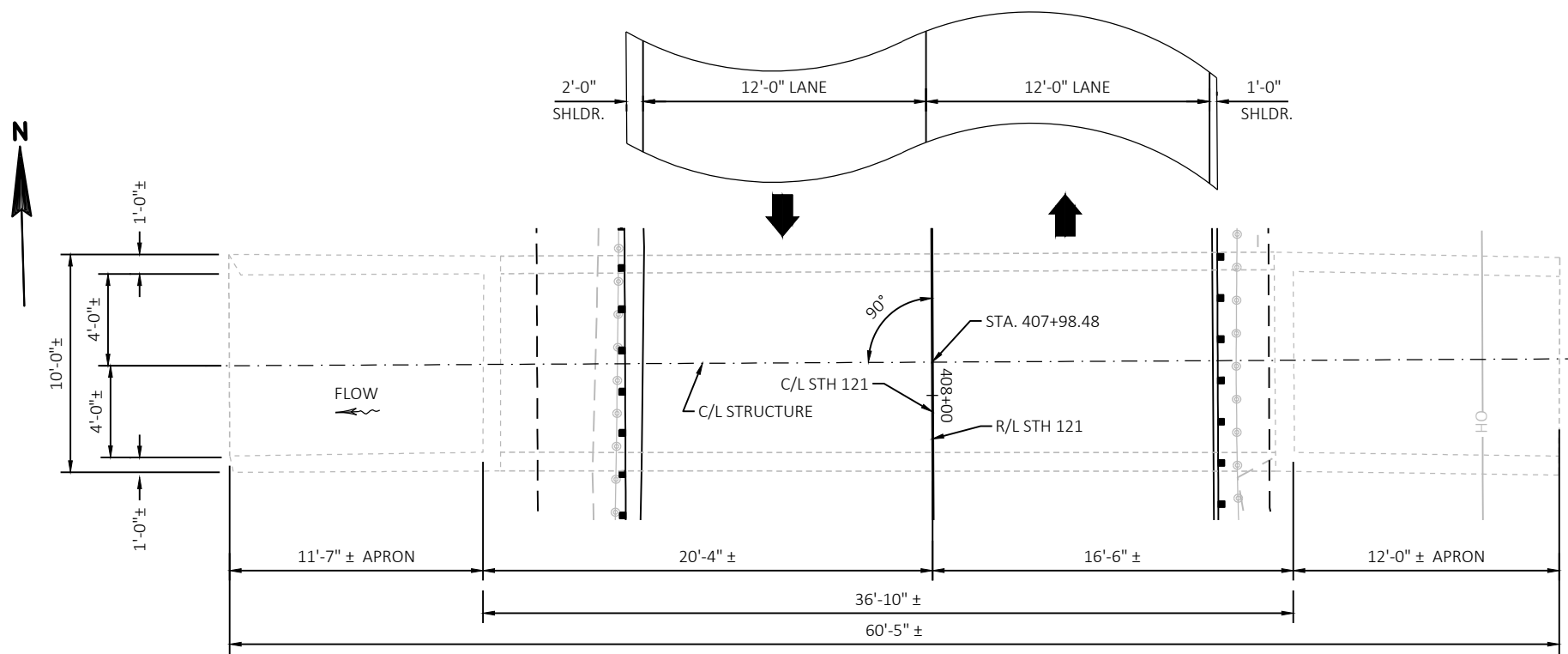
SDD 15D51 - 01

**TRAFFIC CONTROL,
MOBILE OPERATIONS ON
AN UNDIVIDED ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA



TRAFFIC VOLUME

A.A.D.T. = 610 (2010)
 A.A.D.T. = 900 (2023)
 RDS = 55 MPH

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

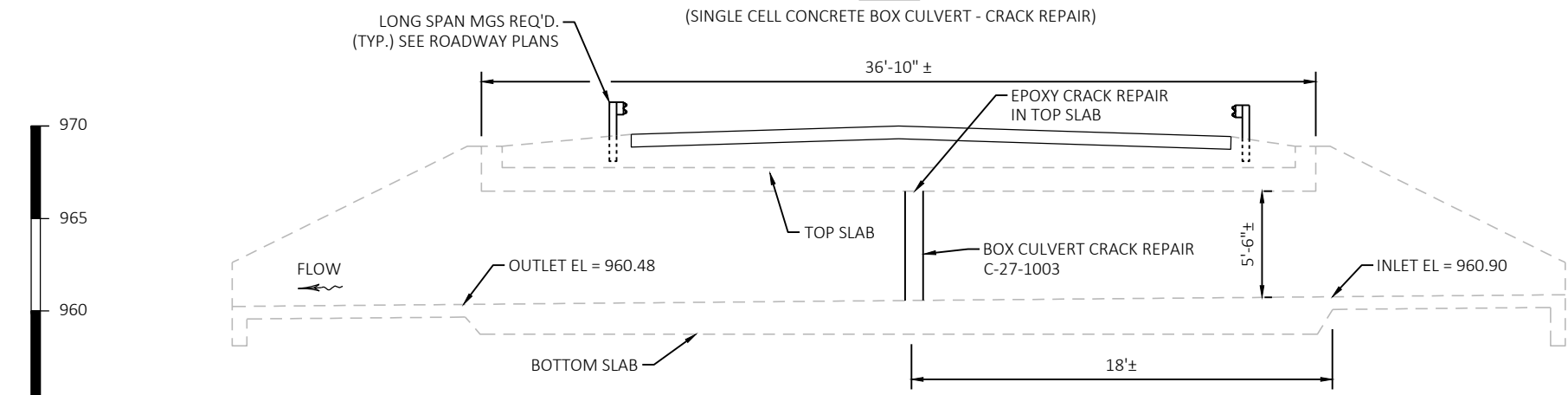
THE EXISTING STRUCTURE IS A SINGLE CELL 8' WIDE X 5'-6" HIGH CONCRETE BOX CULVERT WITH A BARREL LENGTH OF 37-FEET.

DIMENSIONS SHOWN ON THE PLANS ARE BASED ON SURVEY DATA.

TOTAL ESTIMATED QUANTITIES

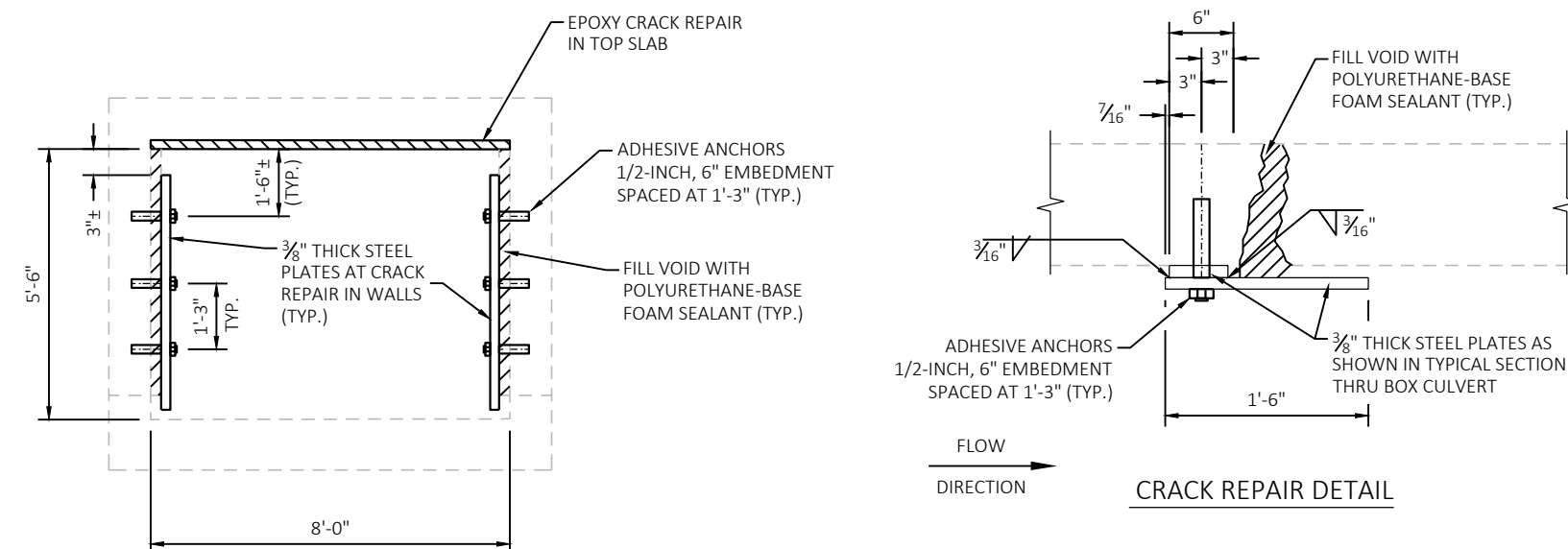
ITEM NUMBER	BID ITEM	QUANTITY	UNIT
509.9020.S	EPOXY CRACK SEALING	16	LF
SPV.0060.01	BOX CULVERT CRACK REPAIR C-27-1003	1	EACH

● PAYMENT FOR BOX CULVERT CRACK REPAIR, INCLUDING STEEL PLATES, ADHESIVE ANCHORS, POLYURETHANE-BASED FOAM SEALANT, AND ANY OTHER MISCELLANEOUS WORK ASSOCIATED WITH THE BOX CULVERT CRACK REPAIRS TO BE INCLUDED IN THE LUMP SUM BID PRICE FOR "BOX CULVERT CRACK REPAIR C-27-1003".



LIST OF DRAWINGS

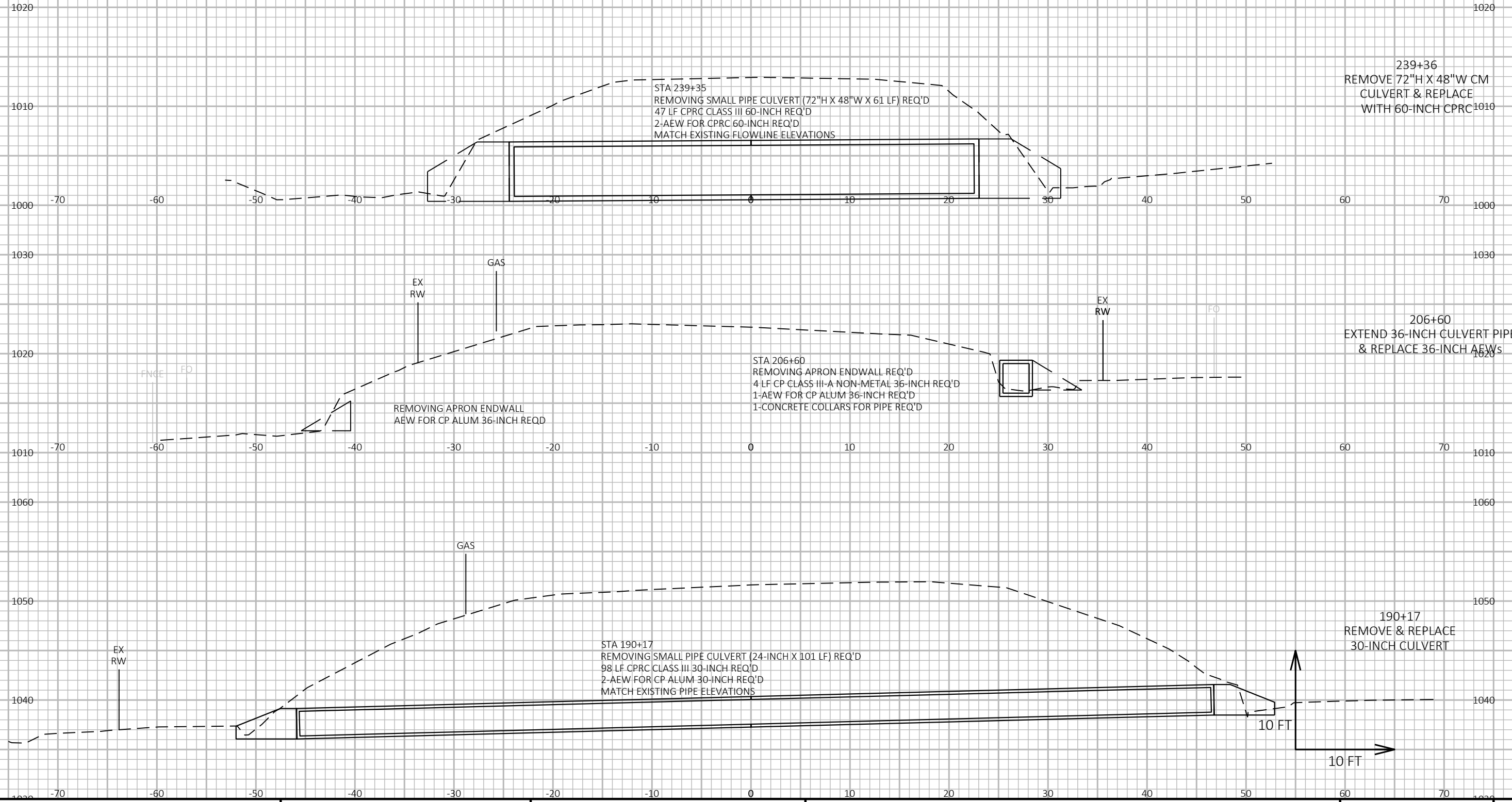
1. GENERAL PLAN



Chad Halverson
 October 18, 2021

STRUCTURE DESIGN CONTACTS
 BUREAU OF STRUCTURES:
 AARON BONK (608) 261-0261
 CONSULTANT:
 CHAD HALVERSON (608) 663-1218

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
ACCEPTED	<i>Chad Halverson</i>	SDR 02/22/22	DATE
STRUCTURE C-27-1003			
STH 121 OVER DRAINAGE WAY			
COUNTY	JACKSON	TOWN/CITY/VILLAGE	GARDEN VALLEY
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	CDH	DESIGN CK'D.	CAH
DRAWN BY	STD	PLANS CK'D.	CAH
GENERAL PLAN			SHEET 1 OF 1



239+36
 REMOVE 72"H X 48"W CM
 CULVERT & REPLACE
 WITH 60-INCH CPRC

206+60
 EXTEND 36-INCH CULVERT PIPE
 & REPLACE 36-INCH AEWs

190+17
 REMOVE & REPLACE
 30-INCH CULVERT

PROJECT NO: 7505-00-76

HWY: STH 121

COUNTY: JACKSON

CROSS SECTIONS: CROSS SECTIONS

SHEET

E

STA. 294+69
 REMOVING SMALL PIPE CULVERT REQ'D
 70 LF CPRC CLASS III 24-INCH REQ'D
 2-AEW FOR CPRC 24-INCH REQ'D
 MATCH EXISTING PIPE ELEVATIONS

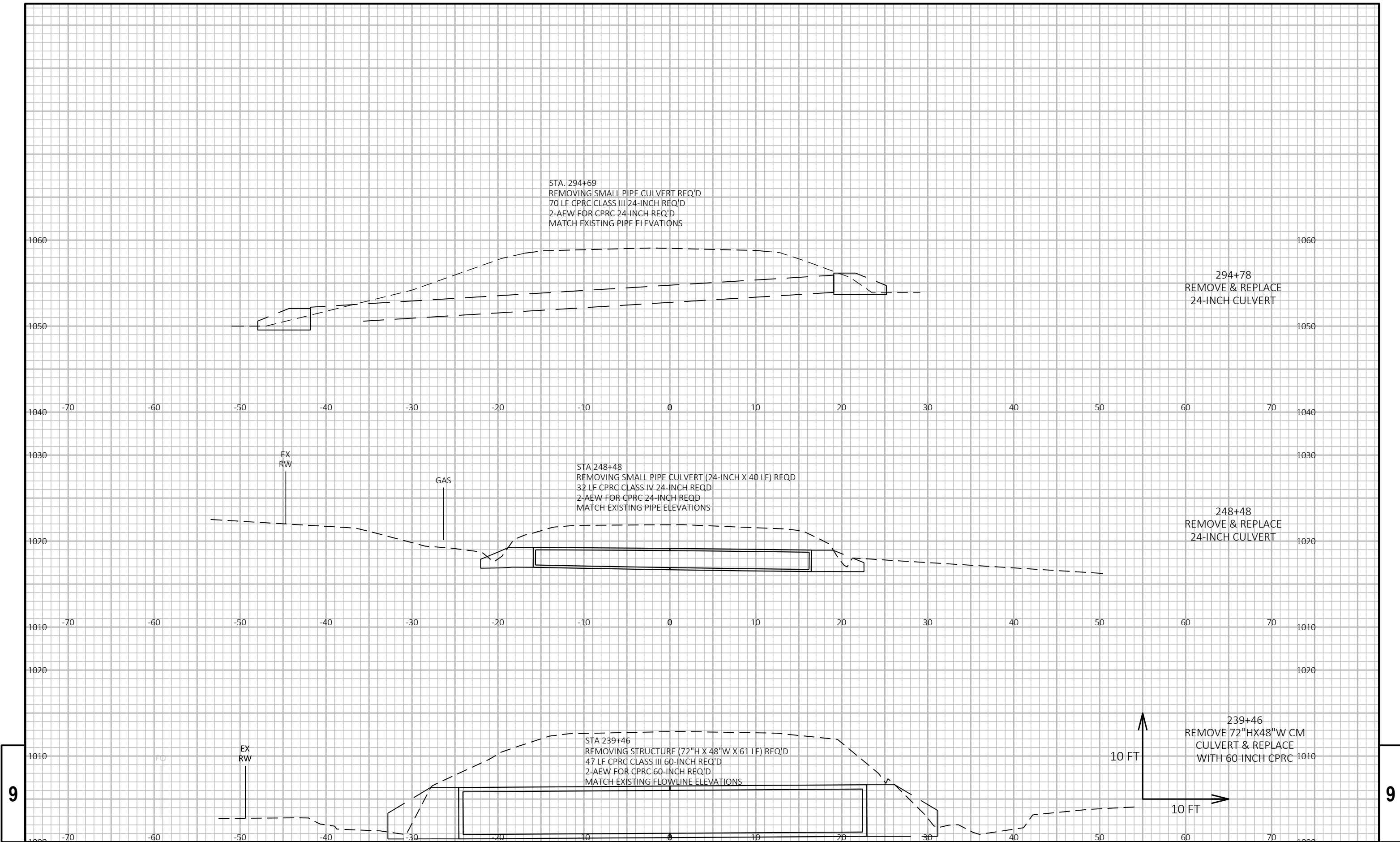
294+78
 REMOVE & REPLACE
 24-INCH CULVERT

STA. 248+48
 REMOVING SMALL PIPE CULVERT (24-INCH X 40 LF) REQ'D
 32 LF CPRC CLASS IV 24-INCH REQ'D
 2-AEW FOR CPRC 24-INCH REQ'D
 MATCH EXISTING PIPE ELEVATIONS

248+48
 REMOVE & REPLACE
 24-INCH CULVERT

STA. 239+46
 REMOVING STRUCTURE (72"H X 48"W X 61 LF) REQ'D
 47 LF CPRC CLASS III 60-INCH REQ'D
 2-AEW FOR CPRC 60-INCH REQ'D
 MATCH EXISTING FLOWLINE ELEVATIONS

239+46
 REMOVE 72"HX48"W CM
 CULVERT & REPLACE
 WITH 60-INCH CPRC



9

9

PROJECT NO: 7505-00-76

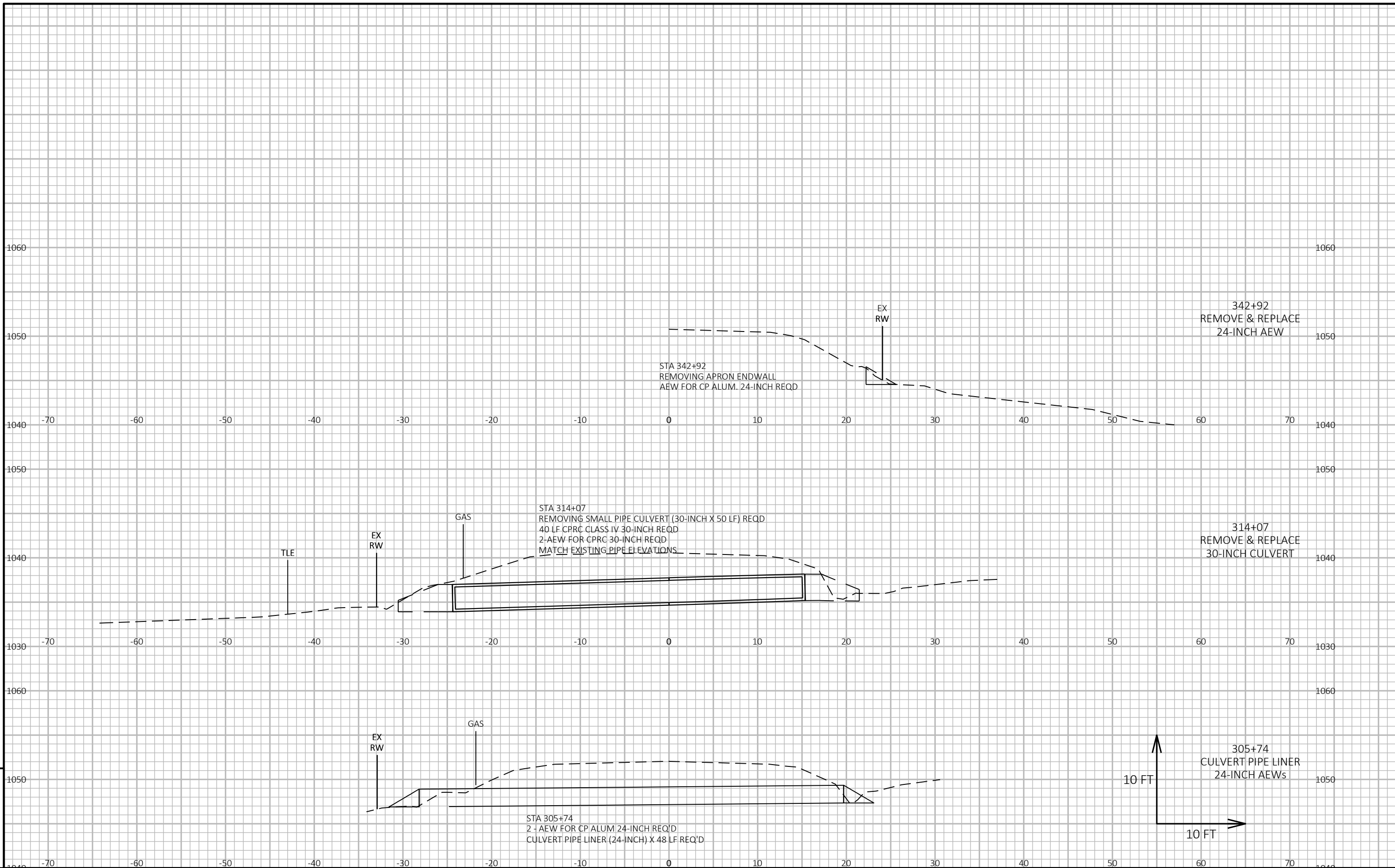
HWY: STH 121

COUNTY: JACKSON

CROSS SECTIONS: CROSS SECTIONS

SHEET

E



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PROJECT NO: 7505-00-76

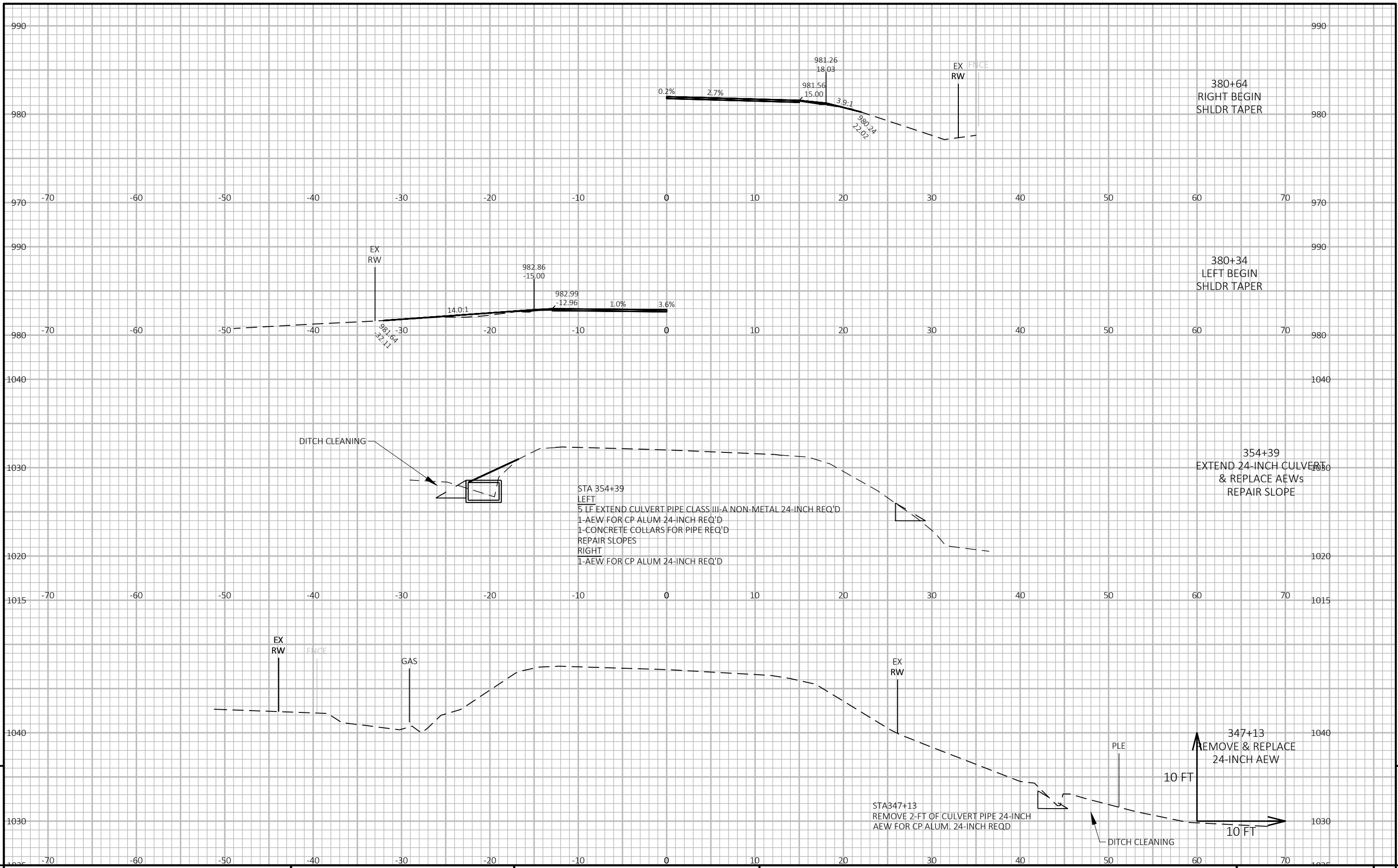
HWY: STH 121

COUNTY: JACKSON

CROSS SECTIONS: CROSS SECTIONS

SHEET

E



380+64
RIGHT BEGIN
SHLDR TAPER

380+34
LEFT BEGIN
SHLDR TAPER

354+39
EXTEND 24-INCH CULVERT
& REPLACE AEWs
REPAIR SLOPE

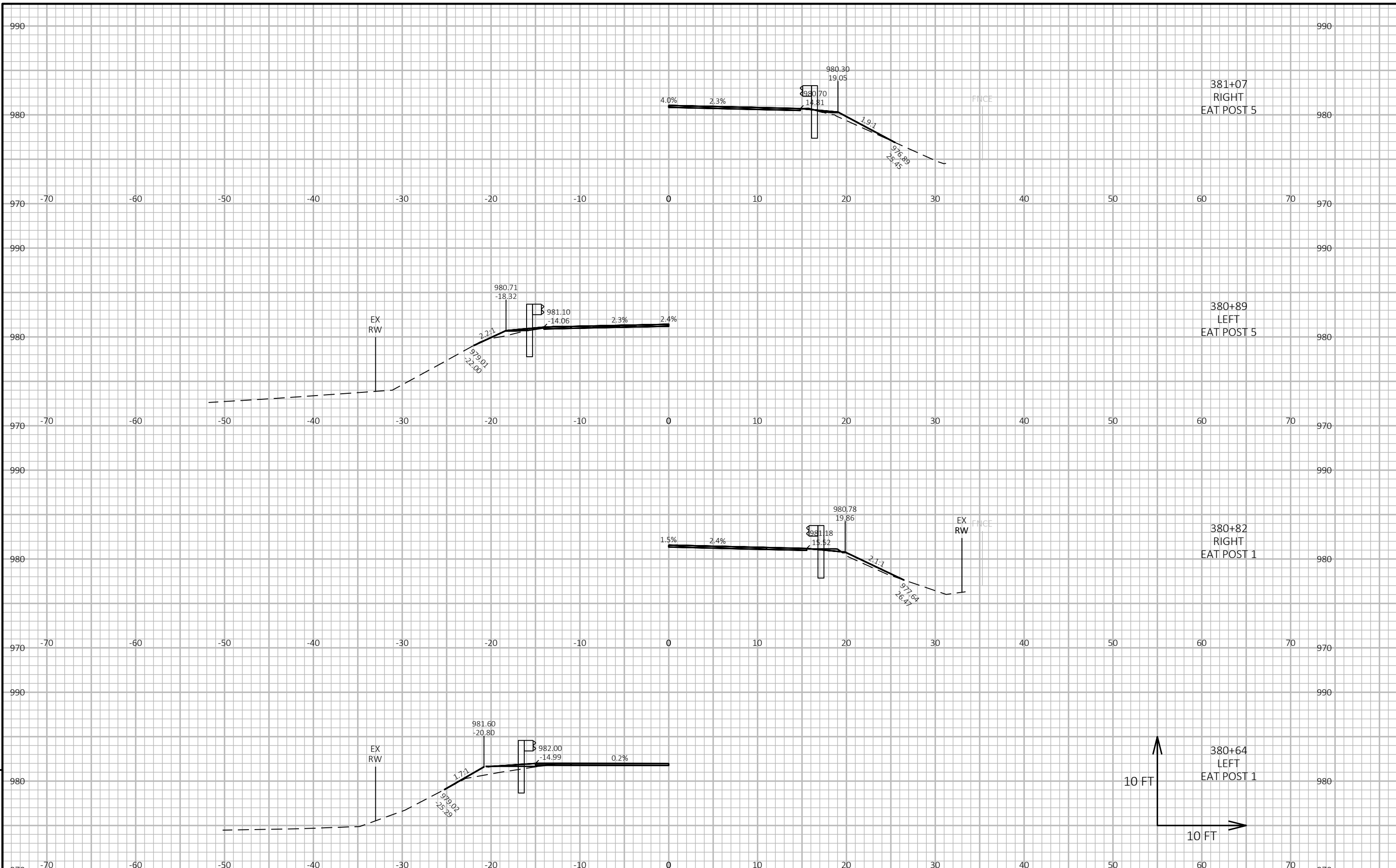
347+13
REMOVE & REPLACE
24-INCH AEW

STA 354+39
LEFT
5 LF EXTEND CULVERT PIPE CLASS III-A NON-METAL 24-INCH REQ'D
1-AEW FOR CP ALUM 24-INCH REQ'D
1-CONCRETE COLLARS FOR PIPE REQ'D
REPAIR SLOPES
RIGHT
1-AEW FOR CP ALUM 24-INCH REQ'D

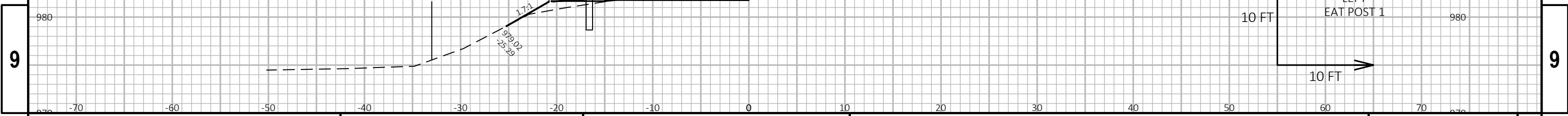
STA347+13
REMOVE 2-FT OF CULVERT PIPE 24-INCH
AEW FOR CP ALUM, 24-INCH REQ'D

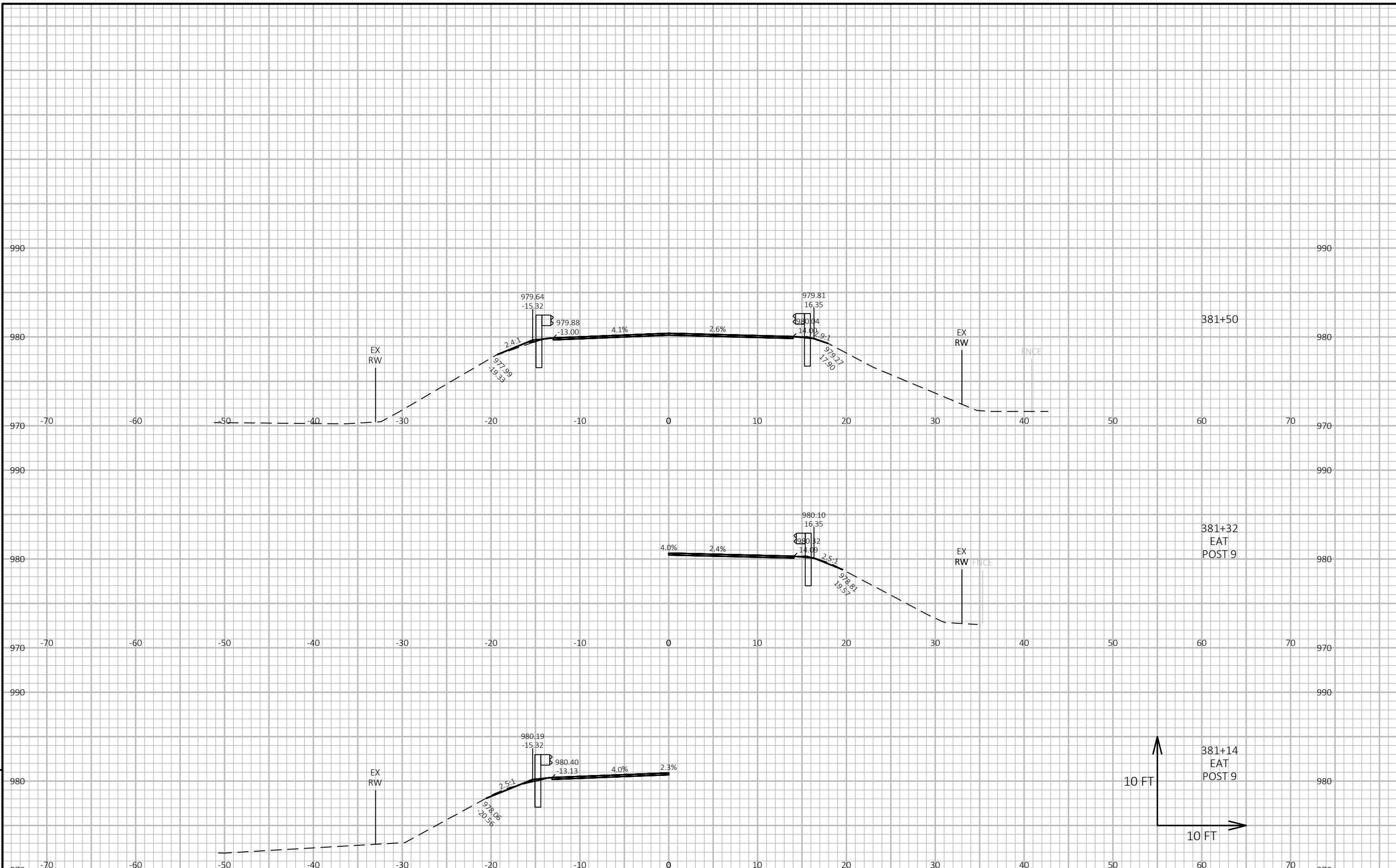
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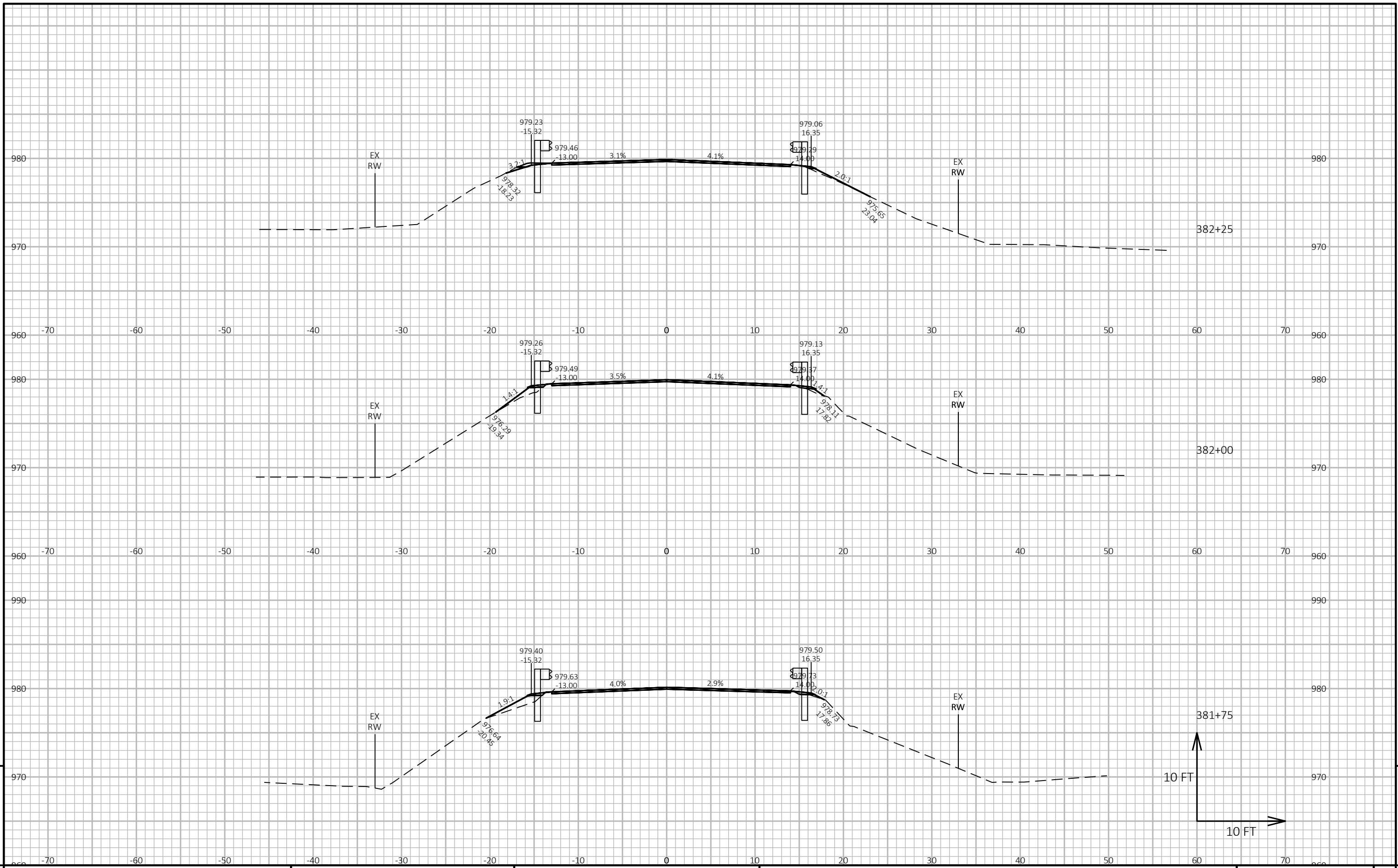


PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	CROSS SECTIONS: CROSS SECTIONS	SHEET
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PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON CROSS SECTIONS: CROSS SECTIONS SHEET E



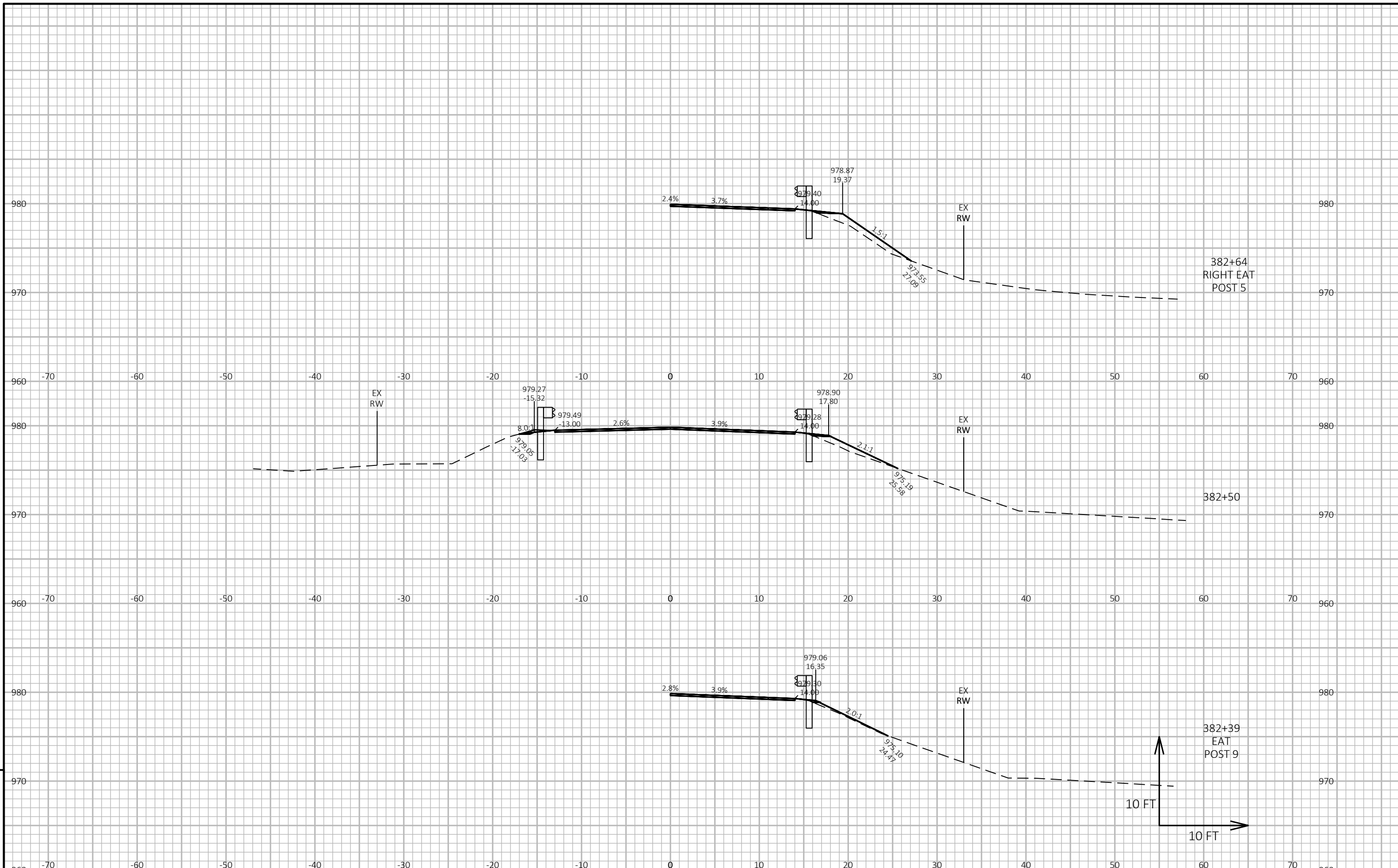
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PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON CROSS SECTIONS: CROSS SECTIONS SHEET E

FILE NAME: G:\WDOTNW\19004-004 (STH 121)\CIVIL 3D\SHEETSPLAN\090201-XS_NEW.DWG PLOT DATE: 1/31/2022 5:47 PM PLOT BY: KL ENGINEERING PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 7



PROJECT NO: 7505-00-76

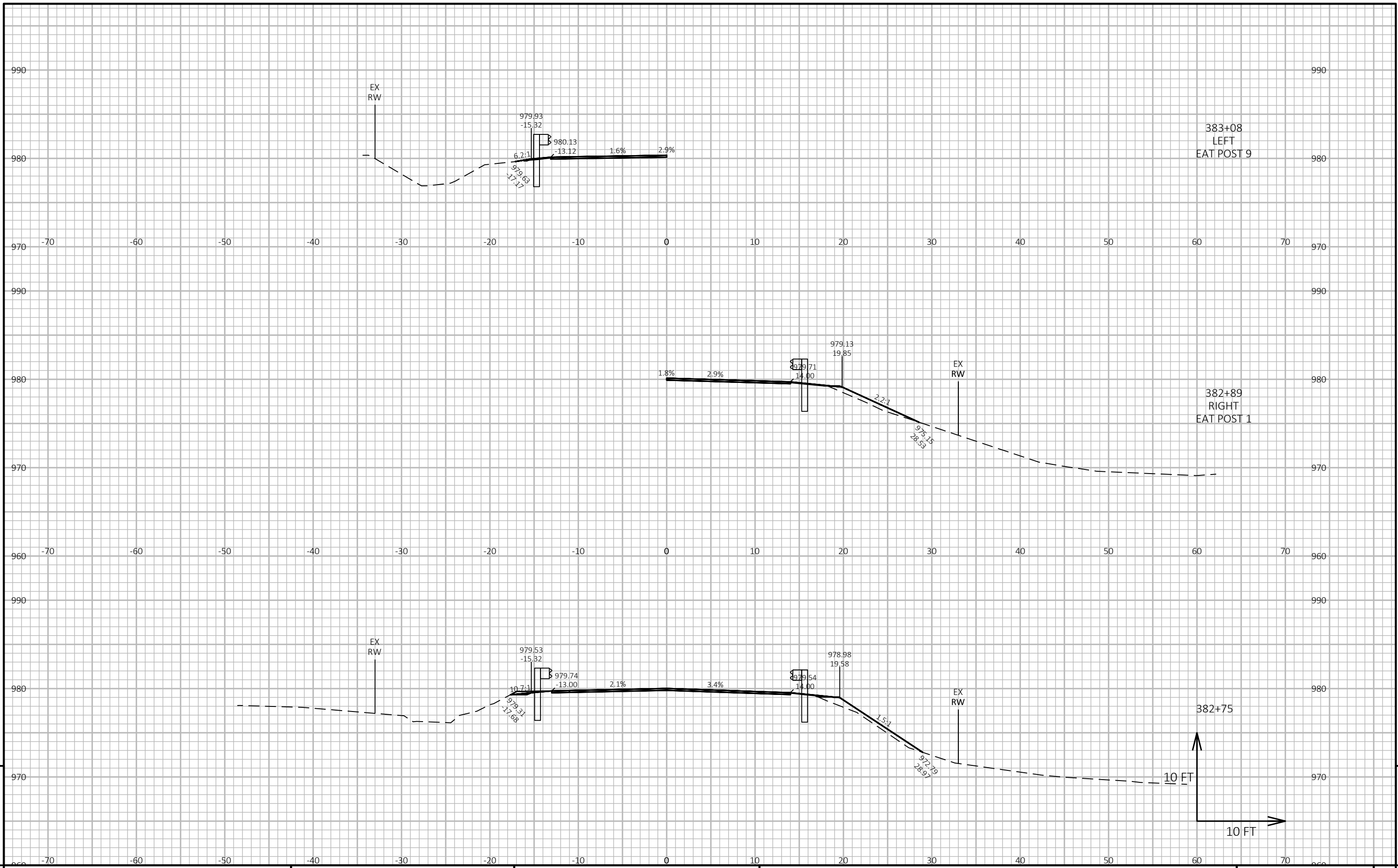
HWY: STH 121

COUNTY: JACKSON

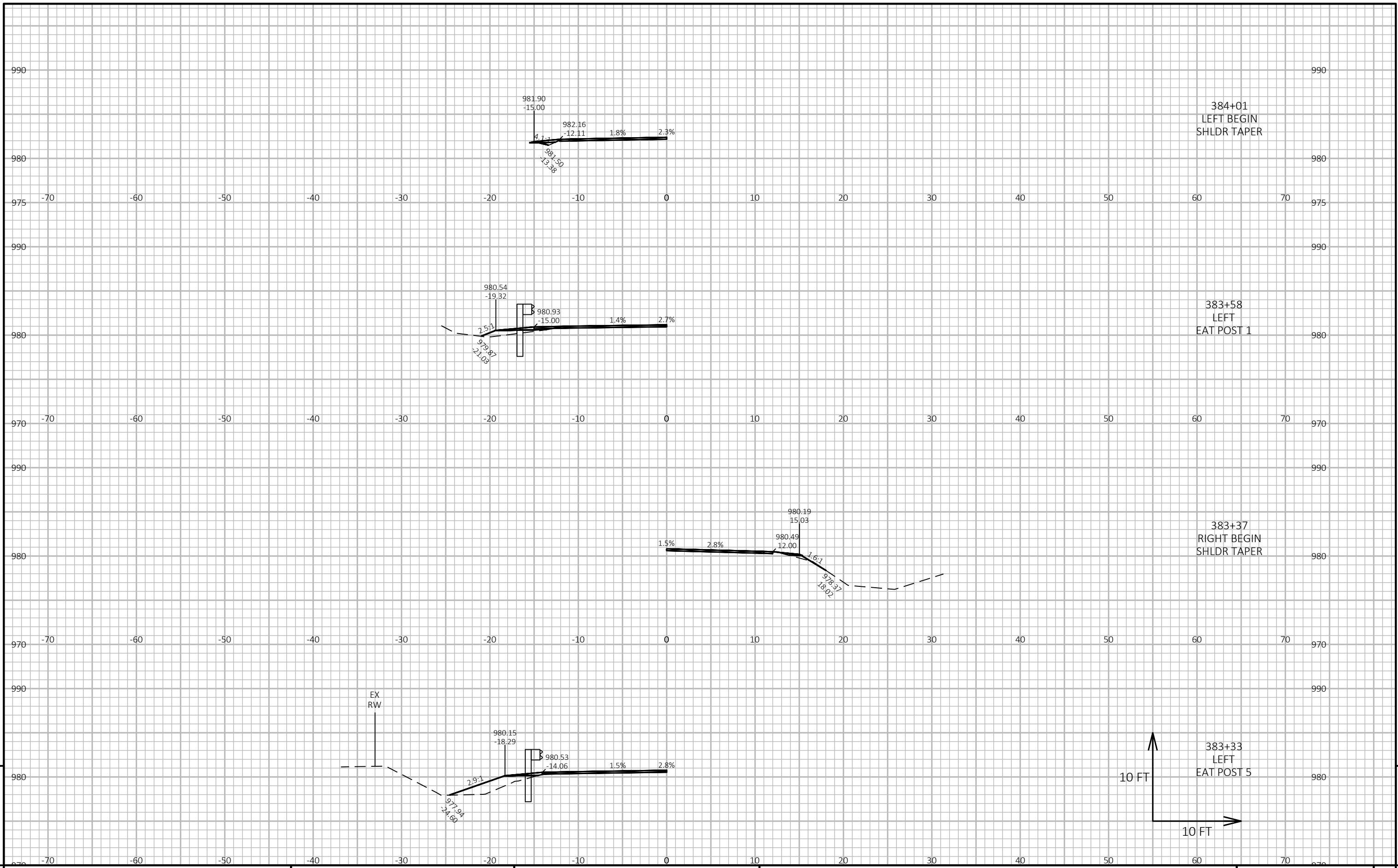
CROSS SECTIONS: CROSS SECTIONS

SHEET

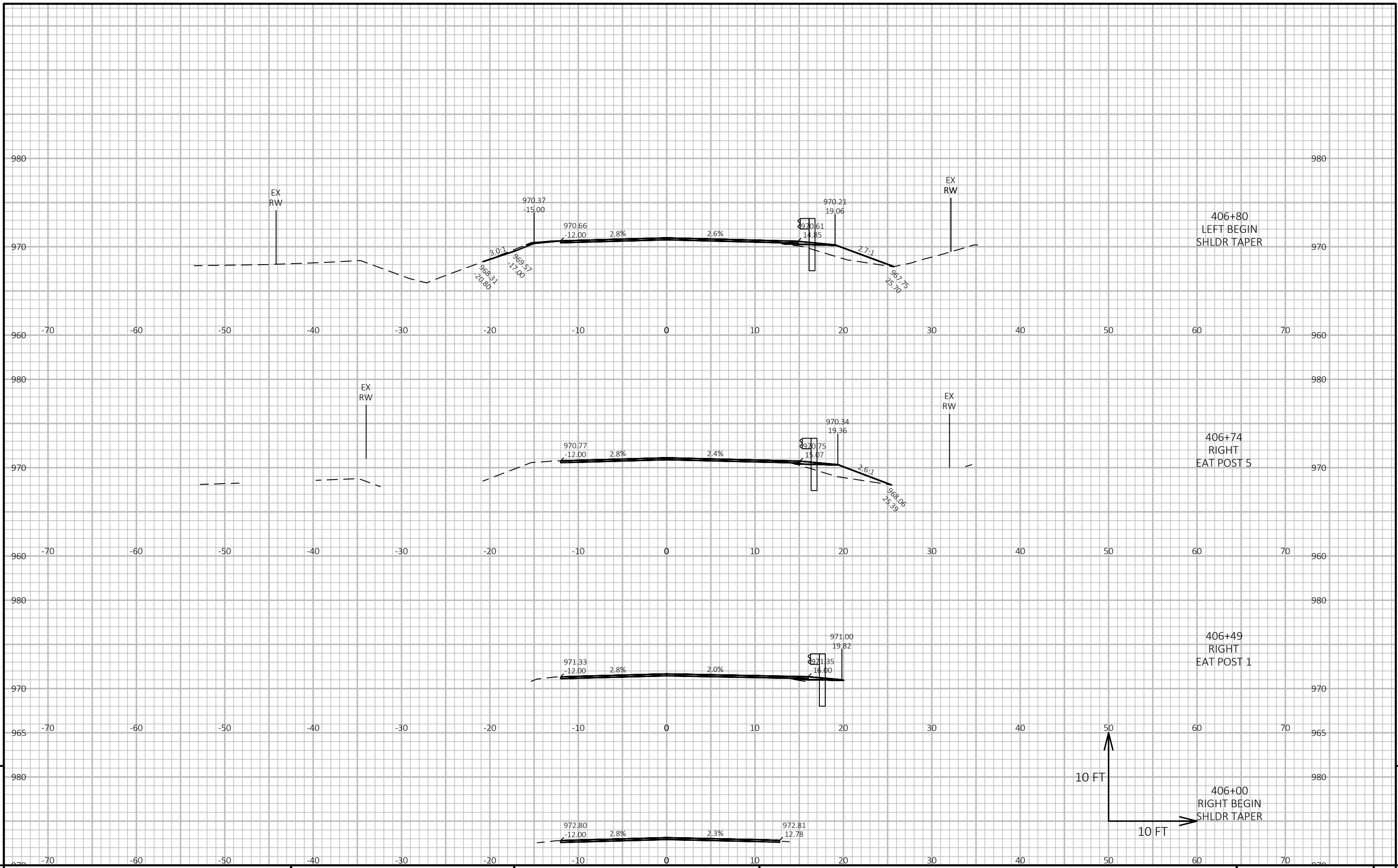
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PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON CROSS SECTIONS: CROSS SECTIONS SHEET E



PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON CROSS SECTIONS: CROSS SECTIONS SHEET E

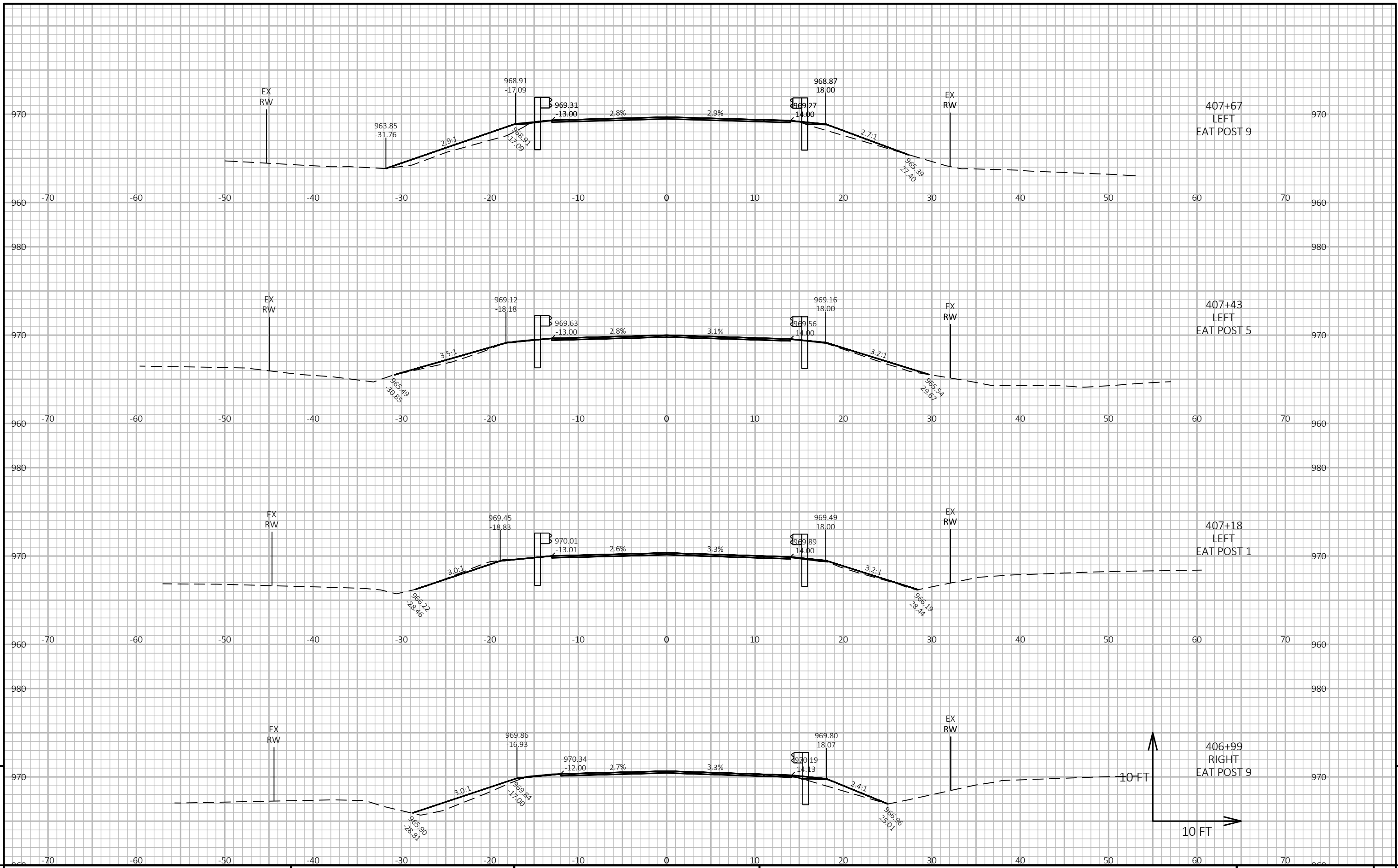


PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	CROSS SECTIONS: CROSS SECTIONS	SHEET
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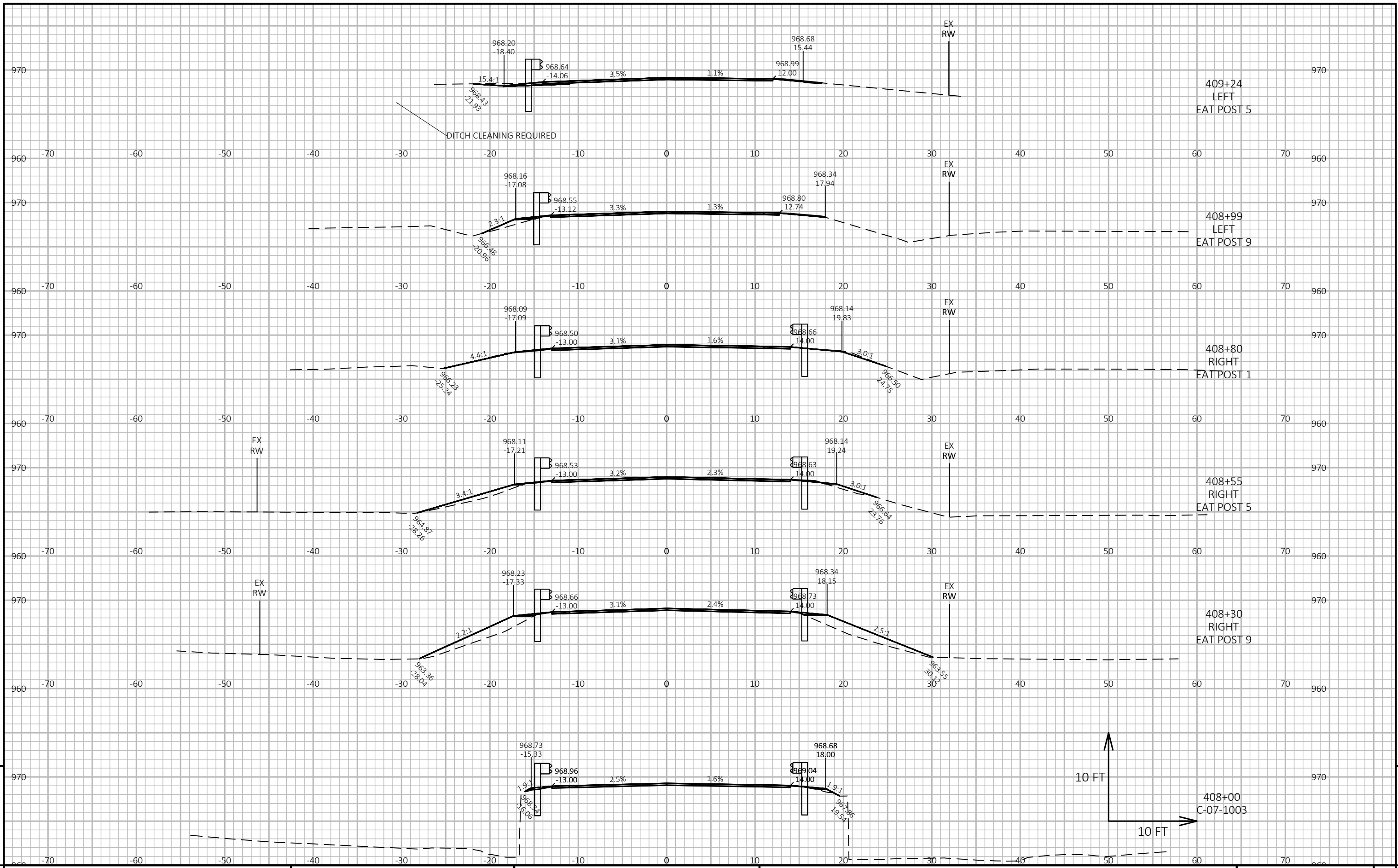
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PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON CROSS SECTIONS: CROSS SECTIONS SHEET E

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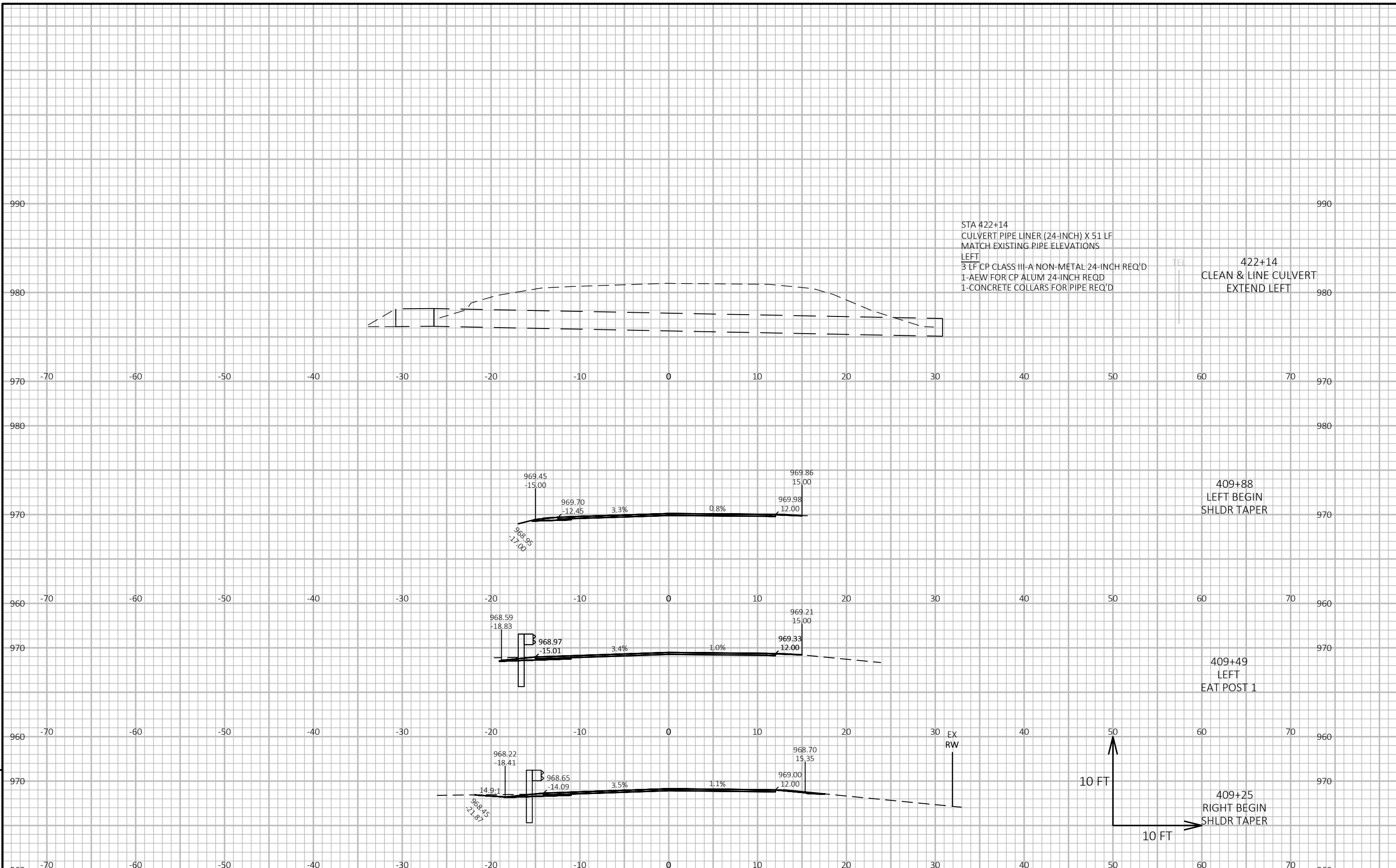
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PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	CROSS SECTIONS: CROSS SECTIONS	SHEET	E
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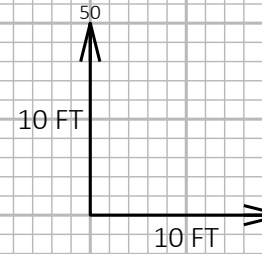
STA 422+14
 CULVERT PIPE LINER (24-INCH) X 51 LF
 MATCH EXISTING PIPE ELEVATIONS
 LEFT
 3 LF CP CLASS III-A NON-METAL 24-INCH REQ'D
 1-AEW FOR CP ALUM 24-INCH REQ'D
 1-CONCRETE COLLARS FOR PIPE REQ'D

422+14
 CLEAN & LINE CULVERT
 EXTEND LEFT

409+88
 LEFT BEGIN
 SHLDR TAPER

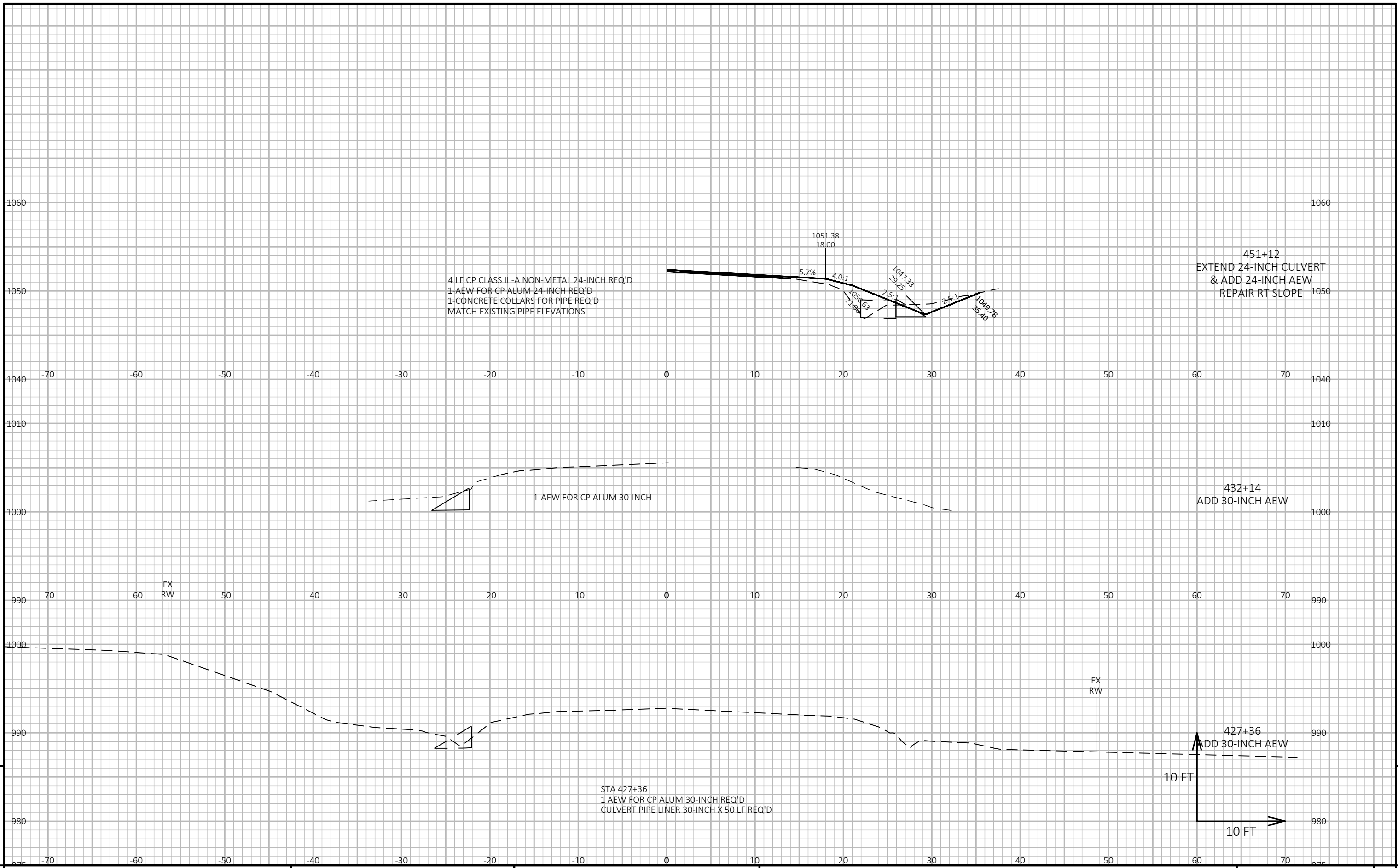
409+49
 LEFT
 EAT POST 1

409+25
 RIGHT BEGIN
 SHLDR TAPER



9

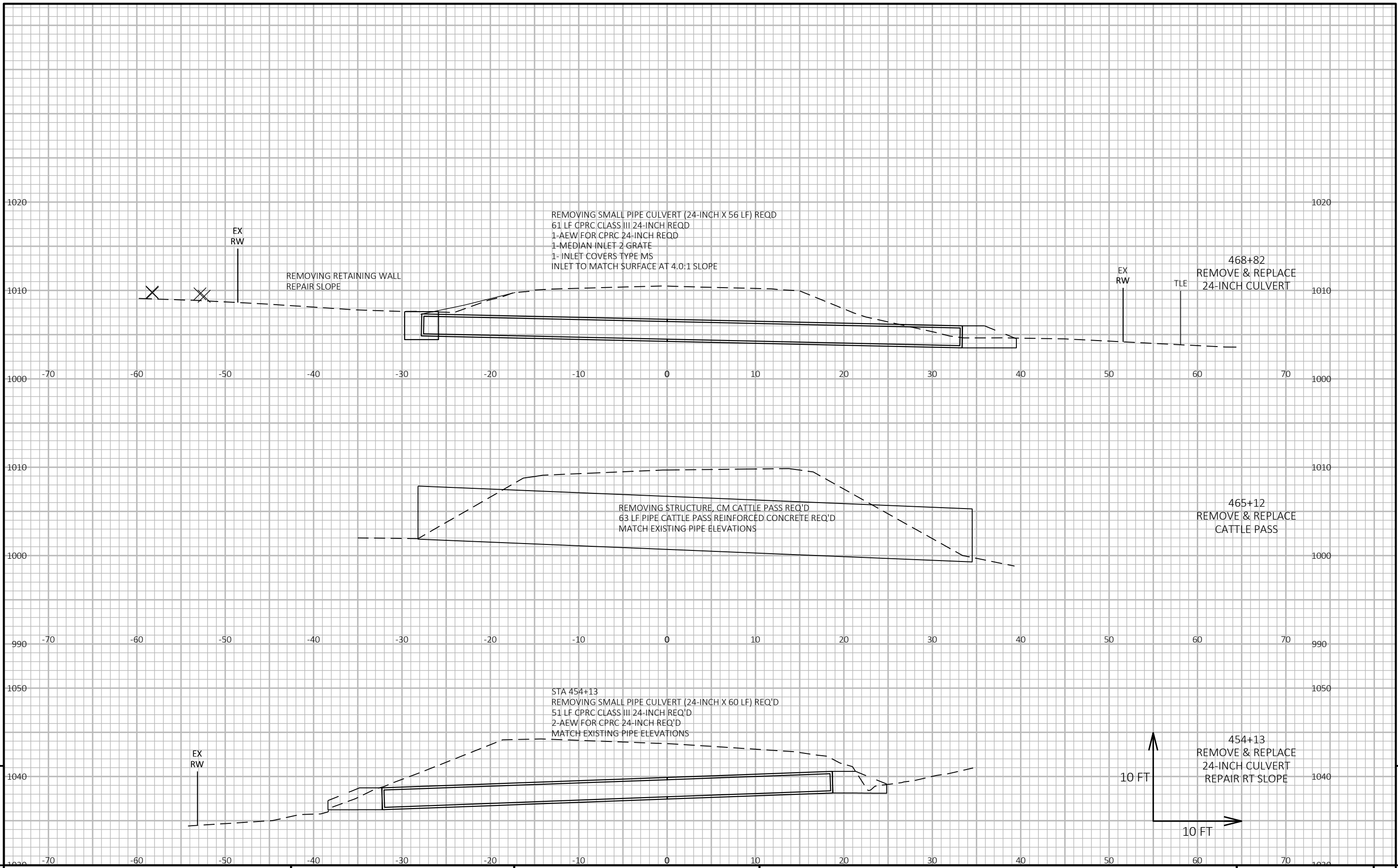
9



PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	CROSS SECTIONS: CROSS SECTIONS	SHEET	E
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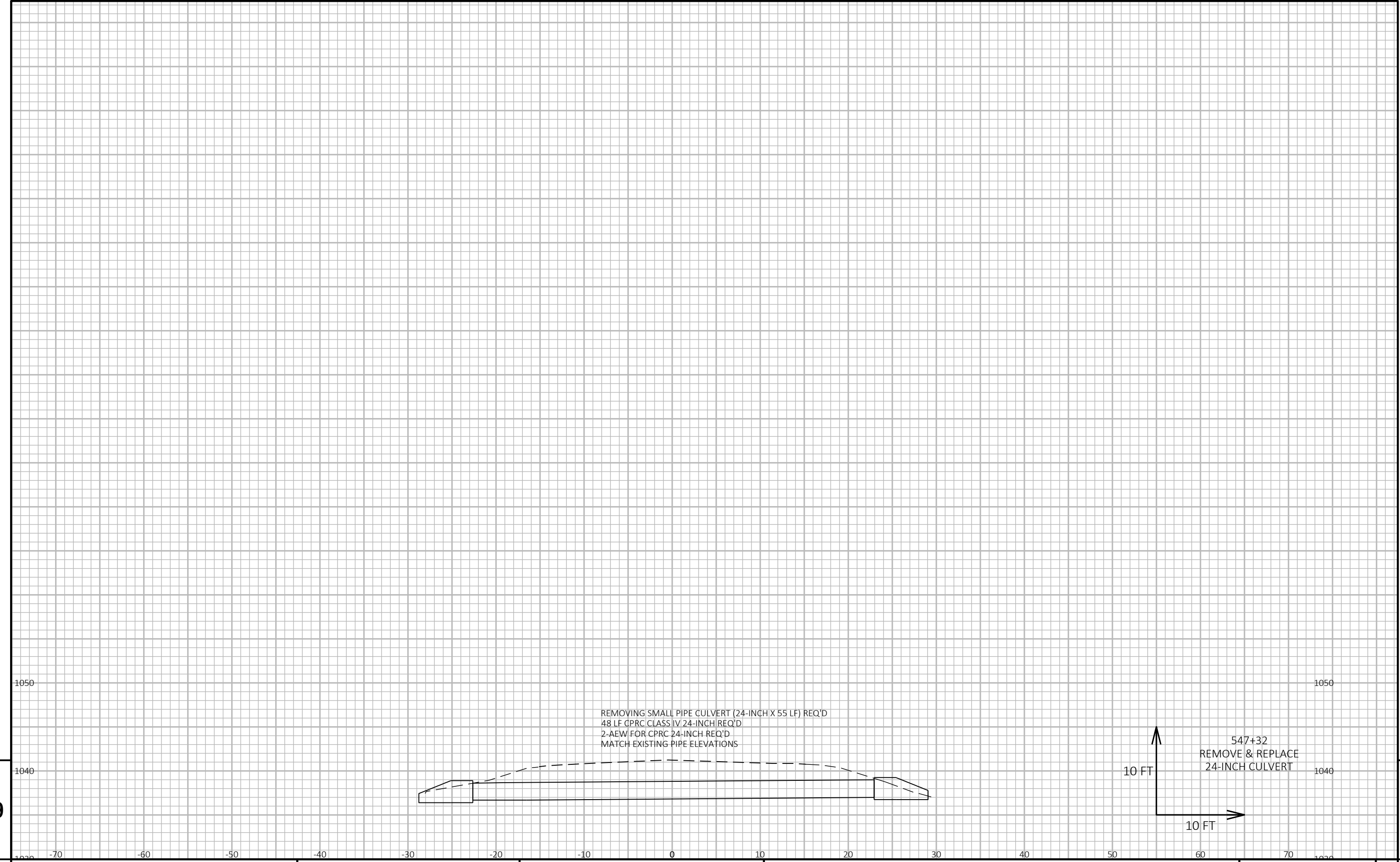
9



PROJECT NO: 7505-00-76 HWY: STH 121 COUNTY: JACKSON CROSS SECTIONS: CROSS SECTIONS SHEET E

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PROJECT NO: 7505-00-76	HWY: STH 121	COUNTY: JACKSON	CROSS SECTIONS: CROSS SECTIONS	SHEET	E
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Notes



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