

MAD
PROJECT ID:
WITH: N/A

3575-02-73

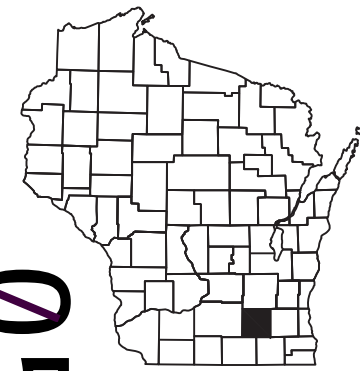
COUNTY:
JEFFERSON

FEBRUARY 2023

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Gross Sections

TOTAL SHEETS = 158



STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

C FORT ATKINSON, WHITEWATER AVENUE

MADISON AVENUE TO CTH M

USH 12

JEFFERSON COUNTY

STATE PROJECT NUMBER
3575-02-73

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
3575-02-73	_____	_____
N/A		

DESIGN DESIGNATION 3575-02-04

A.A.D.T.	2022	=	13,550
A.A.D.T.	2042	=	15,780
D.H.V.		=	N/A
D.D.		=	N/A
T.		=	17.3%
DESIGN SPEED		=	30 MPH - 50 MPH
ESALS		=	3,500,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	



LAYOUT
SCALE 0 0.5 MI
TOTAL NET LENGTH OF CENTERLINE = 1.918 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), JEFFERSON COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
PREPARED BY	MSA PROFESSIONAL SERVICES, INC.
Designer	JEREMY WHITE
Project Manager	JAMIE GRAINGER
Regional Examiner	SW REGION
Regional Supervisor	JUSTIN KUTSCHENREUTER

APPROVED FOR THE DEPARTMENT
DATE: 11/3/2022 *Jamie Grainger*
(Signature)

E

NO TREES OR SHRUBS THAT ARE NOT MARKED ON THE PLAN TO BE REMOVED SHALL BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER IN THE FIELD.

THE LOCATIONS OF EXISTING UTILITY LOCATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

CONTRACTOR TO VERIFY ELEVATIONS OF THE EXISTING PAVEMENT TO REMAIN IN PLACE, PRIOR TO STAKING.

HMA PAVEMENT AND WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, BIKE, OR PARKING LANE.

APPLY TACK COAT BETWEEN LAYERS OF ASPHALT PAVEMENT AND TO MILLED SURFACES. THE APPLICATION RATE IS 0.05 GAL/SY BETWEEN LAYERS OF NEW HMA PAVEMENT AND 0.07 GAL/SY PLACED ON EXISTING ASPHALT, MILLED SURFACES AND CONCRETE PAVEMENT OR AS DIRECTED BY THE ENGINEER.

SAWCUTS, AS SHOWN ON THE PLANS, ARE SUGGETED LOCATIONS AND MAY BE ADJUSTED AT THE DISCRETION OF THE ENGINEER TO BETTER SUIT FIELD CONDITIONS.

EXACT SIGN LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE AND THE LOCATIONS OF SIGNS ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE EROSION CONTROL DEVICES AS SHOWN ON THE EROSION CONTROL SHEETS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER. EROSION CONTROL DEVICES SHALL BE PLACED IN SEQUENCE WITH CONSTRUCTION OPERATIONS OR AS DETERMINED BY THE ENGINEER.

ALL PROPERTY LINES ON PLANS ARE APPROXIMATE.

CURB AND GUTTER PLAN GRADES AND RADII ARE AT THE FLANGE LINE UNLESS OTHERWISE NOTED.

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES EXCEPT WHEN PAVING OR PIPE LAYING OPERATIONS REQUIRE THE DRIVEWAY TO BE CLOSED. ACCESS TO DRIVEWAY SHALL BE RE-ESTABLISHED IMMEDIATELY AFTER PAVEMENT OR PIPE IN DRIVEWAY AREA IS INSTALLED. ACCESS SHALL BE PROVIDED DURING ALL NON-WORKING HOURS.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, SHALL BE FERTILIZED, SEEDED, AND EROSION MAT INSTALLED AS SHOWN ON THE EROSION CONTROL PLAN OR AS DIRECTED BY THE FIELD ENGINEER.

THE LOCATION OF DRIVEWAYS WILL BE DETERMINED BY THE ENGINEER.

UTILITIES

COMMUNICATIONS

AT&T WISCONSIN
MATT VACHALIK
411 7TH ST.
RACINE, WI 53403
PHONE: 262-707-6216
EMAIL: mv5616@att.com

COMMUNICATIONS

SPECTRUM
TIM WRIGHT
1348 PLAINFIELD AVE.
JANESVILLE, WI 53545
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CELL PHONE: 608-422-1883
EMAIL: tim.wright@charter.com

SEWER

CITY OF FORT ATKINSON
ANDY SELLE
101 N. MAIN ST
FORT ATKINSON, WI 53538
PHONE: 920-563-7760
EMAIL: aselle@fortatkinsonwi.net

WATER

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ELECTRIC

WE ENERGIES
WE ENERGIES UTILITY COORDINATOR
500 S. 116TH ST
WEST ALLIS, WI 53214
PHONE: 414-221-2738
EMAIL: We-Utility-relocations@we-energies.com

GAS

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500 S. 116TH ST
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EMAIL: We-Utility-relocations@we-energies.com



Dial  or (800)242-8511
www.DiggersHotline.com

DESIGN CONTACT

WISDOT SW REGION
JEREMY WHITE
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MADISON, WI 53704
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EMAIL: Jeremy.White@dot.wi.gov

DNR CONTACT

DNR LIAISON
SHELLEY NELSON
3911 FISH HATCHERY RD
FITCHBURG, WI 53711
PHONE: 608-444-2835
EMAIL: shelley.nelson@wisconsin.gov

STANDARD ABBREVIATIONS

SECTION 2 ORDER OF SHEETS
 GENERAL NOTES
 PROJECT OVERVIEW
 TYPICAL SECTIONS
 CONSTRUCTION DETAILS
 CURB RAMP DETAILS
 EROSION CONTROL
 PAVEMENT MARKING
 TEMPORARY PEDESTRIAN ACCESS
 TRAFFIC CONTROL
 DETOUR - STH 106

2

2

AC.	ACRE	MAX.	MAXIMUM
AGG.	AGGREGATE	MGAL	1000 GALLONS
AH	AHEAD	MIN.	MINIMUM
<	ANGLE	N.C.	NORMAL CROWN OR NO CHANGE
AE, AEW	APRON ENDWALL	N	NORTH
ASPH.	ASPHALTIC	NO.	NUMBER
A.D.T.	AVERAGE DAILY TRAFFIC	PAV'T	PAVEMENT
B.F.	BACK FACE	P.L.E.	PERMANENT LIMITED EASEMENT
BK.	BACK	P.C.	POINT OF CURVATURE
BEG.	BEGIN	P.I.	POINT OF INTERSECTION
B.M.	BENCH MARK	P.T.	POINT OF TANGENCY
C/L	CENTER LINE	V.P.C.	VERTICAL POINT OF CURVATURE
D	CENTRAL ANGLE OR DELTA	V.P.I.	VERTICAL POINT OF INTERSECTION
C.M.C.P.	CORRUGATED METAL CULVERT PIPE	V.P.T.	VERTICAL POINT OF TANGENCY
C.M.P.	CORRUGATED METAL PIPE	PCC	PORTLAND CEMENT CONCRETE
CO.	COUNTY	P.E.	PRIVATE ENTRANCE
CTH	COUNTY TRUNK HIGHWAY	P.L.	PROPERTY LINE
CR.	CREEK	R	RADIUS OR RANGE
C.A.B.C.	CRUSHED AGGREGATE BASE COURSE	R/L	REFERENCE LINE
C.Y.	CUBIC YARD	R.C.C.P.	REINFORCED CONCRETE CULVERT PIPE
C.P.	CULVERT PIPE	RT	RIGHT
C. & G.	CURB AND GUTTER	REQ'D	REQUIRED
D	DEGREE OF CURVE	R.H.F.	RIGHT HAND FORWARD
D.H.V.	DESIGN HOUR VOLUME	R/W	RIGHT OF WAY
DIA.	DIAMETER	R.	RIVER
DISCH.	DISCHARGE	RD.	ROAD
EA	EACH	SHLD.	SHOULDER(S)
E	EAST	SHR.	SHRINKAGE
ELEC.	ELECTRIC(AL), ELEC. CABLE	S	SOUTH
EL., ELEV.	ELEVATION	S.F.	SQUARE FOOT (FEET)
EXC.	EXCAVATION	SDD	STANDARD DETAIL DRAWING(S)
F.F.	FACE TO FACE	STH	STATE TRUNK HIGHWAY
FERT.	FERTILIZER	STA.	STATION
F.E.	FIELD ENTRANCE	S.E.	SUPERELEVATION
F/L, F.L.	FLOW LINE	S/L	SURVEY LINE
CWT.	HUNDRED WEIGHT	T	TANGENT
INL	INLET	TEL.	TELEPHONE
INTER.	INTERSECTION	TEMP.	TEMPORARY
JT.	JOINT	T.L.E.	TEMPORARY LIMITED EASEMENT
LT	LEFT	T.O.C.	TOP OF CURB
L.H.F.	LEFT HAND FORWARD	T.	(TRUCKS) PERCENT OF
L.	LENGTH OF CURVE	TYP.	TYPICAL
L.F.	LINEAR FOOT(FEET)	UNCL.	UNCLASSIFIED
LC.	LONG CHORD	U.G.	UNDERGROUND (CABLE)
LS	LUMP SUM	V.C.	VERTICAL CURVE
M.P.	MARKER POST	W	WEST

PROJECT NO: 3575-02-73

HWY: USH 12

COUNTY: JEFFERSON

GENERAL NOTES

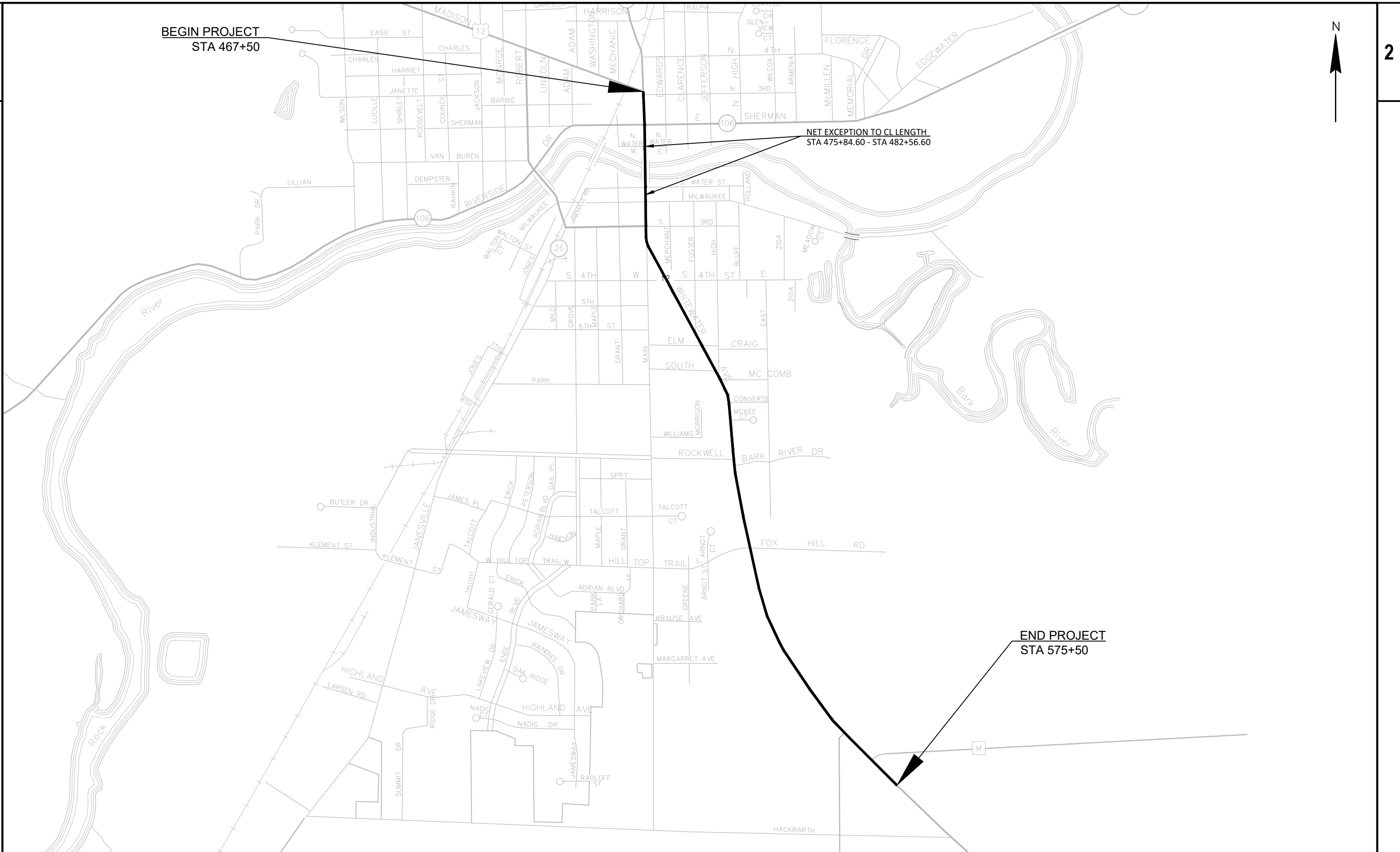
SHEET

E

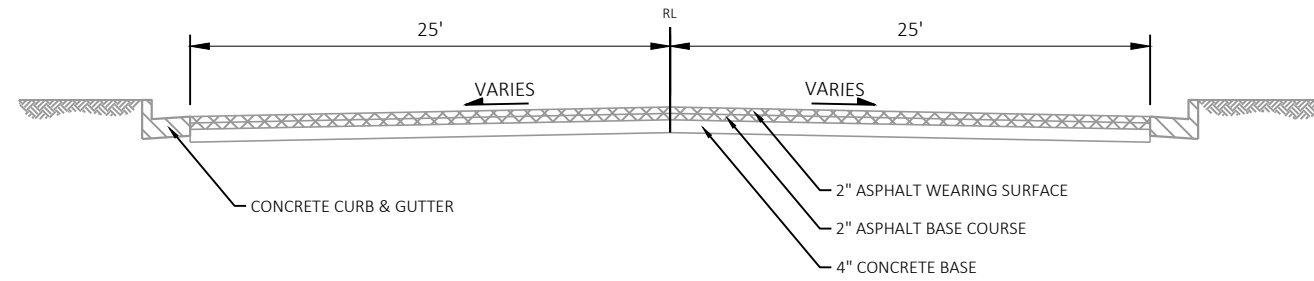
BEGIN PROJECT
STA 467+50

NET EXCEPTION TO CL LENGTH
STA 475+84.60 - STA 482+56.60

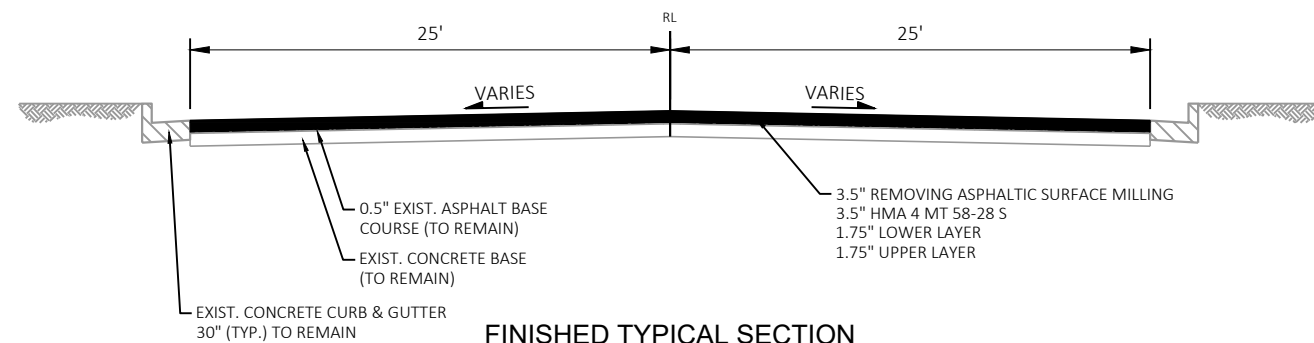
END PROJECT
STA 575+50



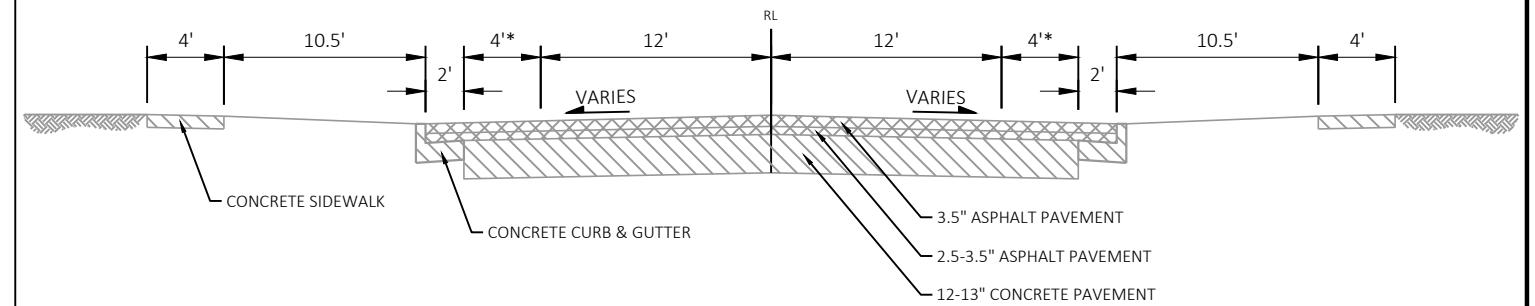
PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	PROJECT OVERVIEW	SHEET	E
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EXISTING TYPICAL SECTION
 STA 467+50 - 491+32 (MADISON AVE - WHITEWATER AVE)

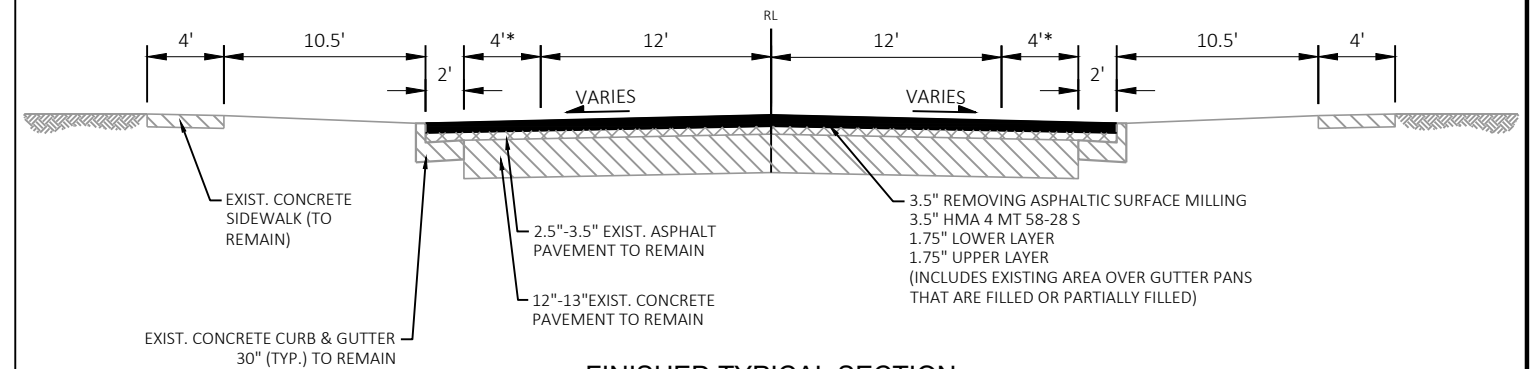


FINISHED TYPICAL SECTION
 STA 467+50 - 491+32 (MADISON AVE - WHITEWATER AVE)



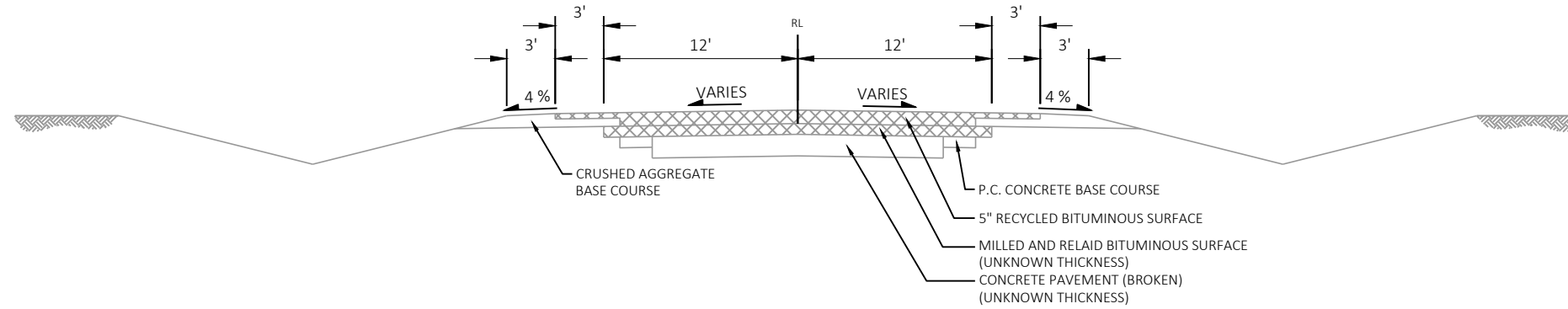
EXISTING TYPICAL SECTION
 STA 491+32 - 524+00 (WHITEWATER AVE - ROCKWELL AVE)

* SOME EXCEPTIONS TO SHOULDER WIDTH FOR PARKING LANES

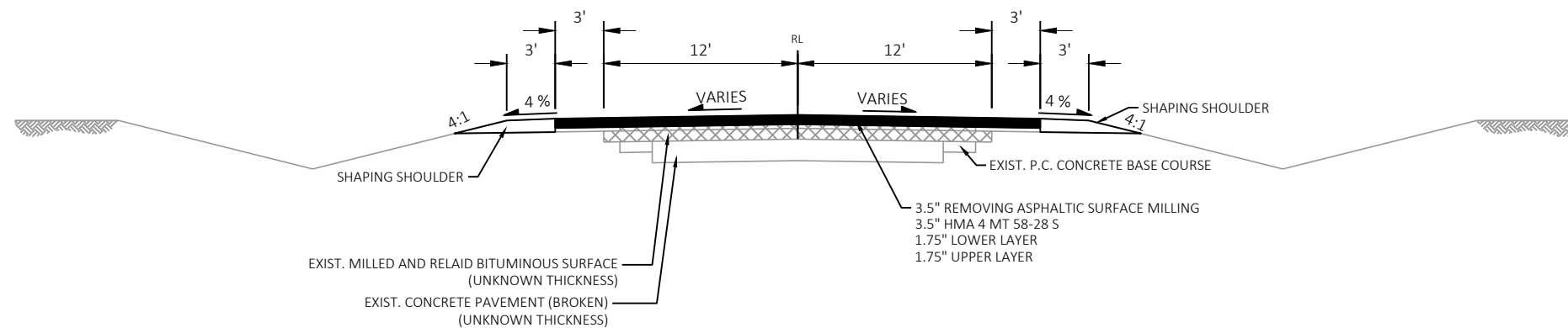


FINISHED TYPICAL SECTION
 STA 491+32 - 524+00 (WHITEWATER AVE - ROCKWELL AVE)

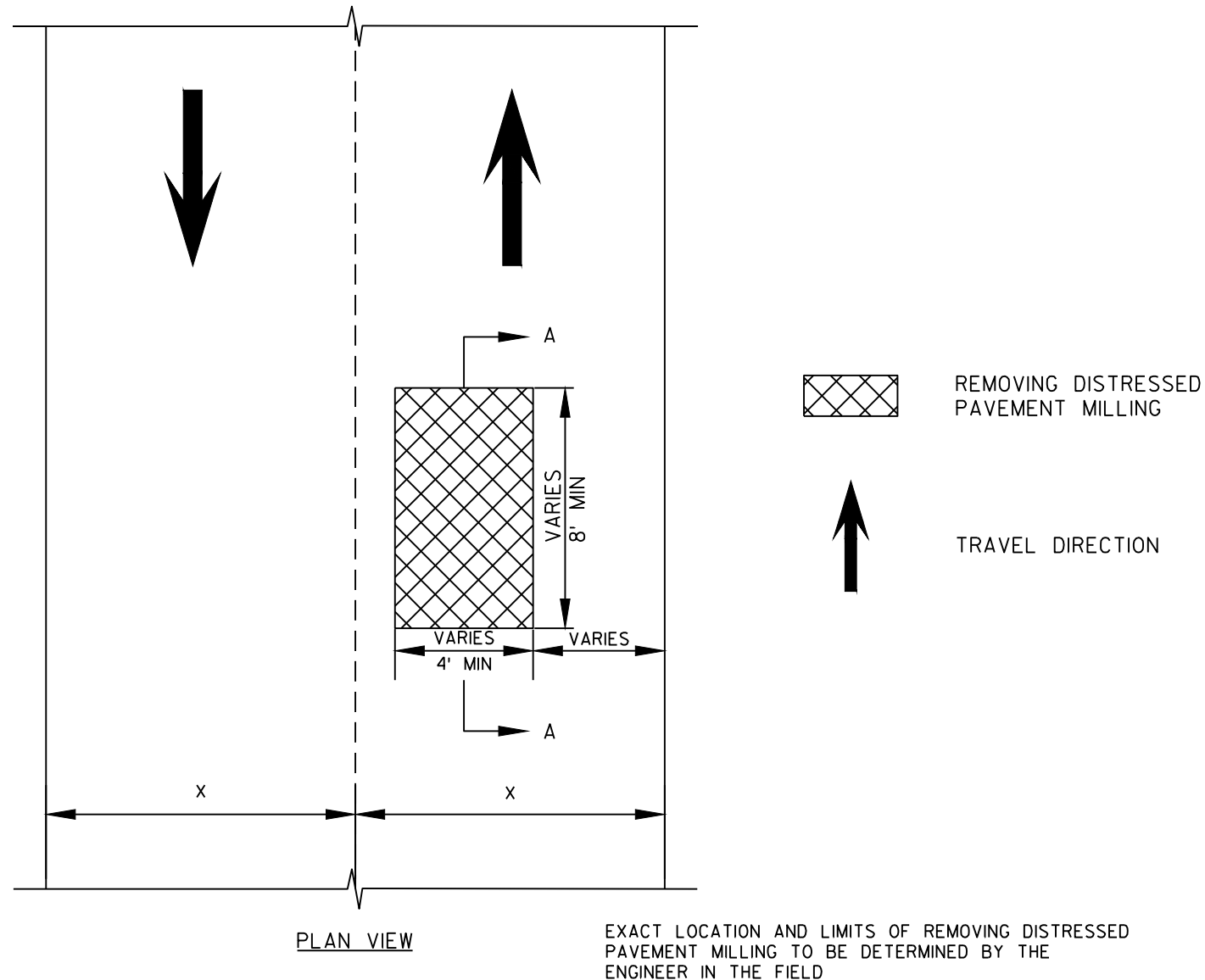
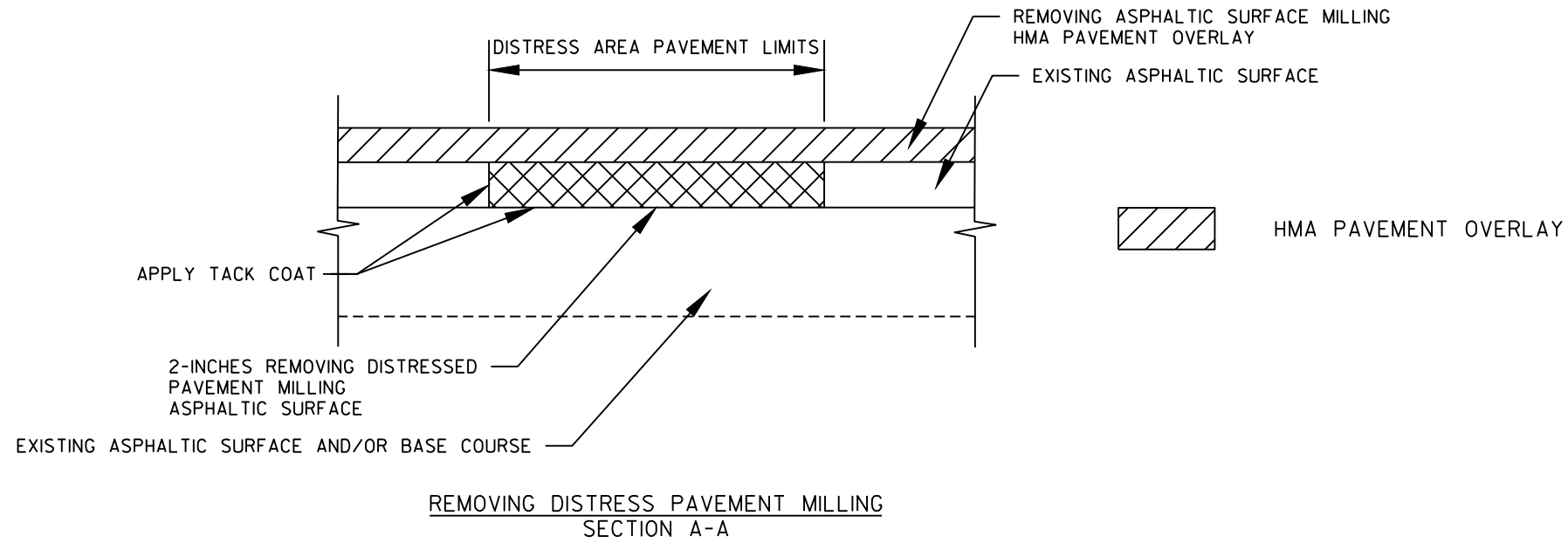
* SOME EXCEPTIONS TO SHOULDER WIDTH FOR PARKING LANES

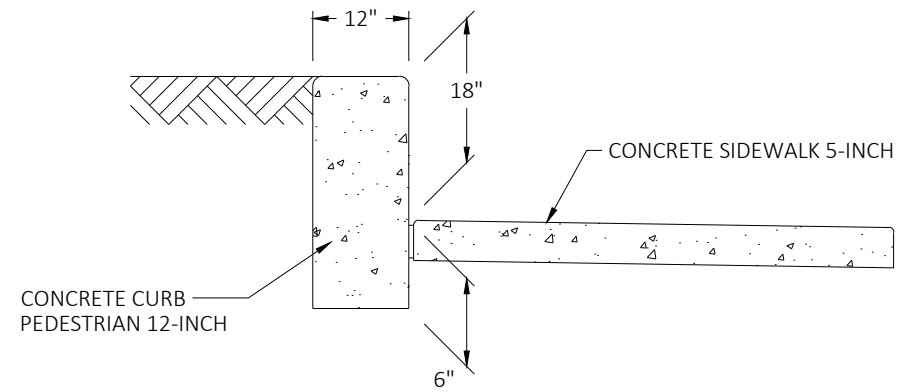


EXISTING TYPICAL SECTION
 STA 524+00-575+50 (ROCKWELL AVE - HACKBARTH RD)

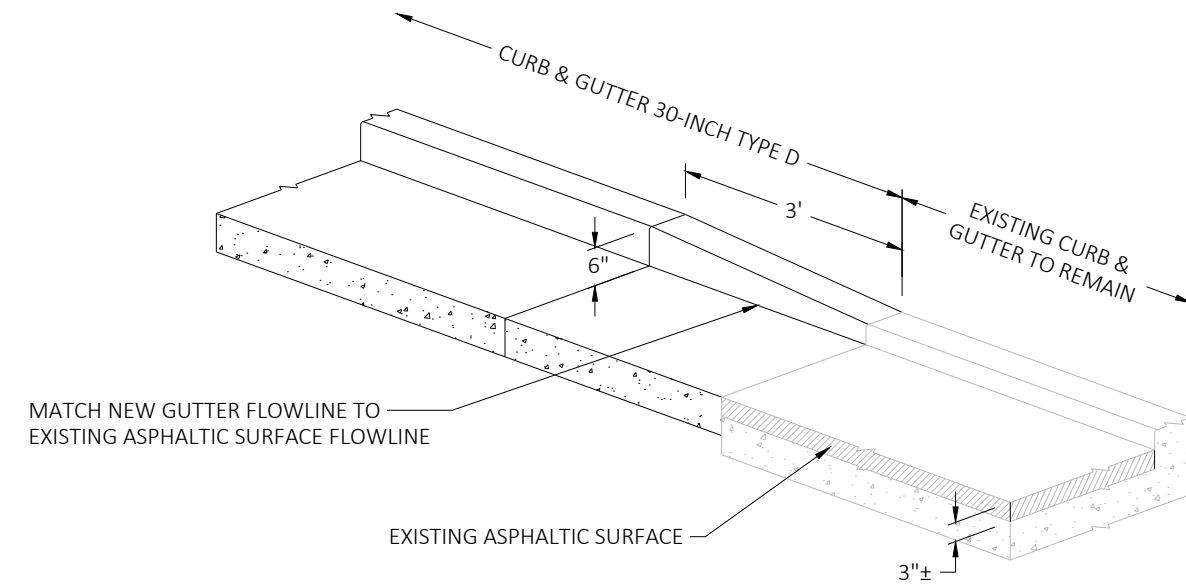


FINISHED TYPICAL SECTION
 STA 524+00 - 575+50 (ROCKWELL AVE - HACKBARTH RD)





CONCRETE CURB PEDESTRIAN 12-INCH
SECTION VIEW







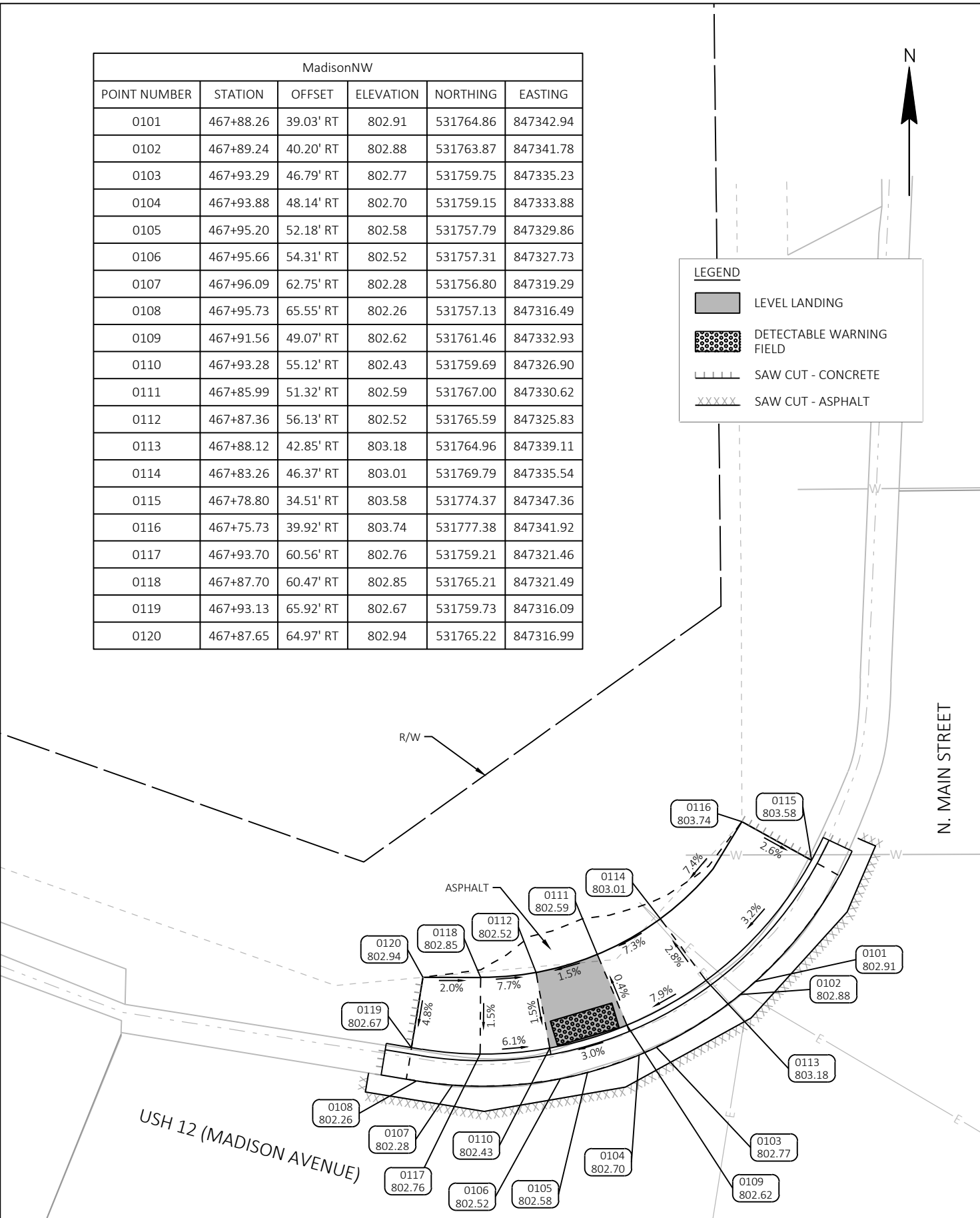
DETAIL FOR MATCHING INTO EXISTING CURB & GUTTER

*USED BETWEEN WHITEWATER AVE AND ROCKWELL AVE. REFER TO TYPICAL SECTIONS

MadisonNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0101	467+88.26	39.03' RT	802.91	531764.86	847342.94
0102	467+89.24	40.20' RT	802.88	531763.87	847341.78
0103	467+93.29	46.79' RT	802.77	531759.75	847335.23
0104	467+93.88	48.14' RT	802.70	531759.15	847333.88
0105	467+95.20	52.18' RT	802.58	531757.79	847329.86
0106	467+95.66	54.31' RT	802.52	531757.31	847327.73
0107	467+96.09	62.75' RT	802.28	531756.80	847319.29
0108	467+95.73	65.55' RT	802.26	531757.13	847316.49
0109	467+91.56	49.07' RT	802.62	531761.46	847332.93
0110	467+93.28	55.12' RT	802.43	531759.69	847326.90
0111	467+85.99	51.32' RT	802.59	531767.00	847330.62
0112	467+87.36	56.13' RT	802.52	531765.59	847325.83
0113	467+88.12	42.85' RT	803.18	531764.96	847339.11
0114	467+83.26	46.37' RT	803.01	531769.79	847335.54
0115	467+78.80	34.51' RT	803.58	531774.37	847347.36
0116	467+75.73	39.92' RT	803.74	531777.38	847341.92
0117	467+93.70	60.56' RT	802.76	531759.21	847321.46
0118	467+87.70	60.47' RT	802.85	531765.21	847321.49
0119	467+93.13	65.92' RT	802.67	531759.73	847316.09
0120	467+87.65	64.97' RT	802.94	531765.22	847316.99





LEGEND

-  LEVEL LANDING
-  DETECTABLE WARNING FIELD
-  SAW CUT - CONCRETE
-  SAW CUT - ASPHALT



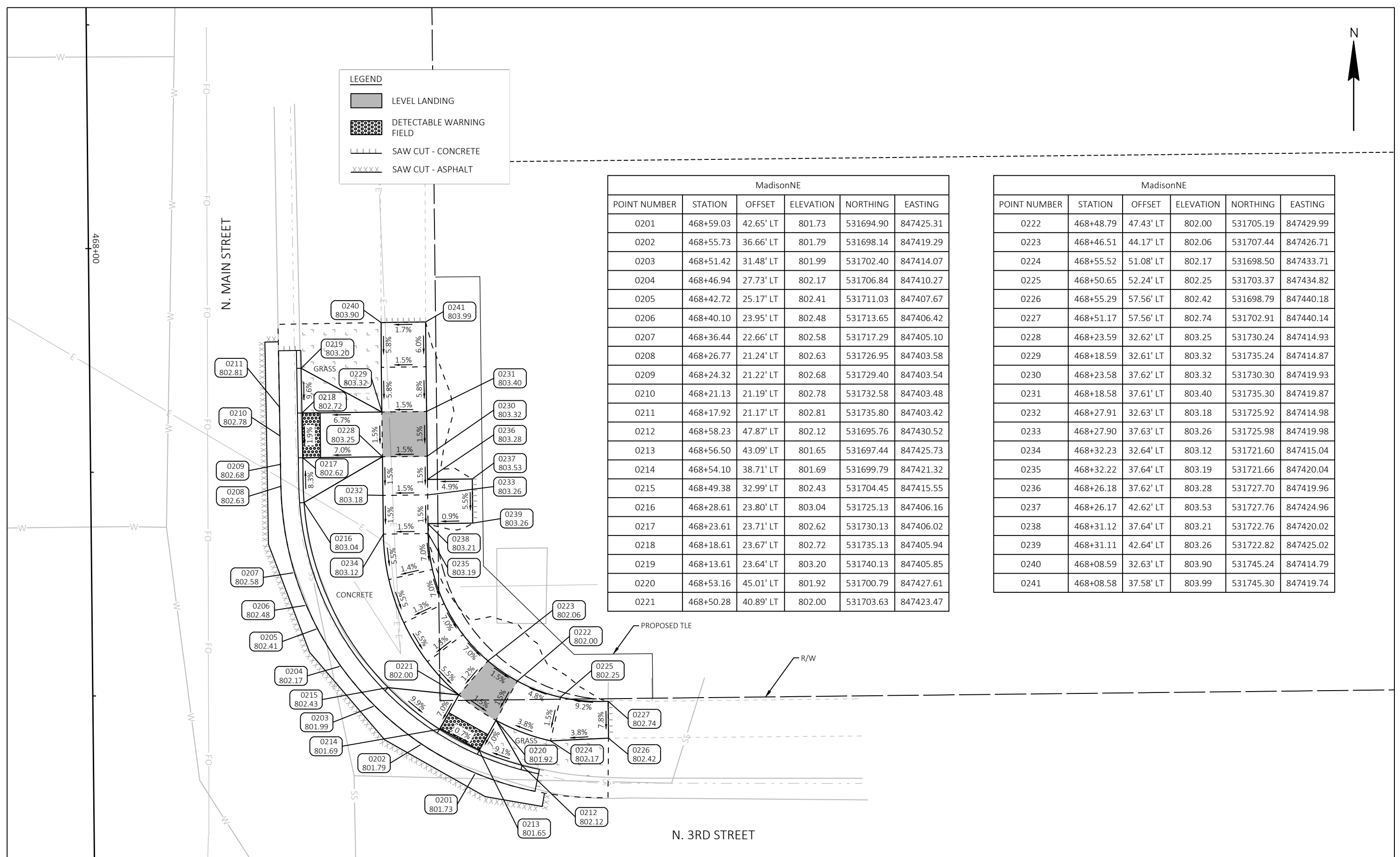


LEGEND

-  LEVEL LANDING
-  DETECTABLE WARNING FIELD
-  SAW CUT - CONCRETE
-  SAW CUT - ASPHALT

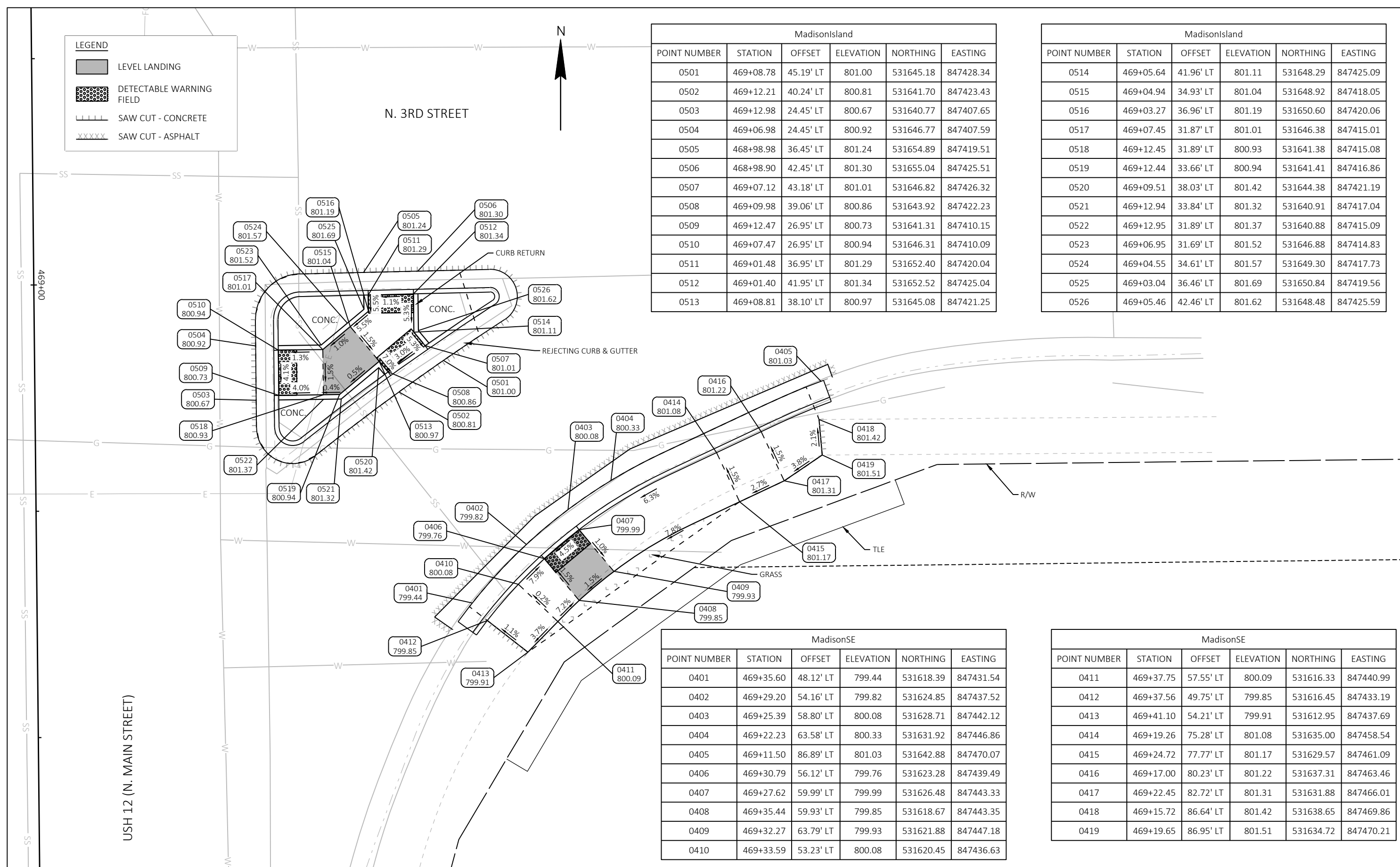
MadisonNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0201	468+59.03	42.65' LT	801.73	531694.90	847425.31
0202	468+55.73	36.66' LT	801.79	531698.14	847419.29
0203	468+51.42	31.48' LT	801.99	531702.40	847414.07
0204	468+46.94	27.73' LT	802.17	531706.84	847410.27
0205	468+42.72	25.17' LT	802.41	531711.03	847407.67
0206	468+40.10	23.95' LT	802.48	531713.65	847406.42
0207	468+36.44	22.66' LT	802.58	531717.29	847405.10
0208	468+26.77	21.24' LT	802.63	531726.95	847403.58
0209	468+24.32	21.22' LT	802.68	531729.40	847403.54
0210	468+21.13	21.19' LT	802.78	531732.58	847403.48
0211	468+17.92	21.17' LT	802.81	531735.80	847403.42
0212	468+58.23	47.87' LT	802.12	531695.76	847430.52
0213	468+56.50	43.09' LT	801.65	531697.44	847425.73
0214	468+54.10	38.71' LT	801.69	531699.79	847421.32
0215	468+49.38	32.99' LT	802.43	531704.45	847415.55
0216	468+28.61	23.80' LT	803.04	531725.13	847406.16
0217	468+23.61	23.71' LT	802.62	531730.13	847406.02
0218	468+18.61	23.67' LT	802.72	531735.13	847405.94
0219	468+13.61	23.64' LT	803.20	531740.13	847405.85
0220	468+53.16	45.01' LT	801.92	531700.79	847427.61
0221	468+50.28	40.89' LT	802.00	531703.63	847423.47

MadisonNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0222	468+48.79	47.43' LT	802.00	531705.19	847429.99
0223	468+46.51	44.17' LT	802.06	531707.44	847426.71
0224	468+55.52	51.08' LT	802.17	531698.50	847433.71
0225	468+50.65	52.24' LT	802.25	531703.37	847434.82
0226	468+55.29	57.56' LT	802.42	531698.79	847440.18
0227	468+51.17	57.56' LT	802.74	531702.91	847440.14
0228	468+23.59	32.62' LT	803.25	531730.24	847414.93
0229	468+18.59	32.61' LT	803.32	531735.24	847414.87
0230	468+23.58	37.62' LT	803.32	531730.30	847419.93
0231	468+18.58	37.61' LT	803.40	531735.30	847419.87
0232	468+27.91	32.63' LT	803.18	531725.92	847414.98
0233	468+27.90	37.63' LT	803.26	531725.98	847419.98
0234	468+32.23	32.64' LT	803.12	531721.60	847415.04
0235	468+32.22	37.64' LT	803.19	531721.66	847420.04
0236	468+26.18	37.62' LT	803.28	531727.70	847419.96
0237	468+26.17	42.62' LT	803.53	531727.76	847424.96
0238	468+31.12	37.64' LT	803.21	531722.76	847420.02
0239	468+31.11	42.64' LT	803.26	531722.82	847425.02
0240	468+08.59	32.63' LT	803.90	531745.24	847414.79
0241	468+08.58	37.58' LT	803.99	531745.30	847419.74



LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT



MadisonIsland					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0501	469+08.78	45.19' LT	801.00	531645.18	847428.34
0502	469+12.21	40.24' LT	800.81	531641.70	847423.43
0503	469+12.98	24.45' LT	800.67	531640.77	847407.65
0504	469+06.98	24.45' LT	800.92	531646.77	847407.59
0505	468+98.98	36.45' LT	801.24	531654.89	847419.51
0506	468+98.90	42.45' LT	801.30	531655.04	847425.51
0507	469+07.12	43.18' LT	801.01	531646.82	847426.32
0508	469+09.98	39.06' LT	800.86	531643.92	847422.23
0509	469+12.47	26.95' LT	800.73	531641.31	847410.15
0510	469+07.47	26.95' LT	800.94	531646.31	847410.09
0511	469+01.48	36.95' LT	801.29	531652.40	847420.04
0512	469+01.40	41.95' LT	801.34	531652.52	847425.04
0513	469+08.81	38.10' LT	800.97	531645.08	847421.25

MadisonIsland					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0514	469+05.64	41.96' LT	801.11	531648.29	847425.09
0515	469+04.94	34.93' LT	801.04	531648.92	847418.05
0516	469+03.27	36.96' LT	801.19	531650.60	847420.06
0517	469+07.45	31.87' LT	801.01	531646.38	847415.01
0518	469+12.45	31.89' LT	800.93	531641.38	847415.08
0519	469+12.44	33.66' LT	800.94	531641.41	847416.86
0520	469+09.51	38.03' LT	801.42	531644.38	847421.19
0521	469+12.94	33.84' LT	801.32	531640.91	847417.04
0522	469+12.95	31.89' LT	801.37	531640.88	847415.09
0523	469+06.95	31.69' LT	801.52	531646.88	847414.83
0524	469+04.55	34.61' LT	801.57	531649.30	847417.73
0525	469+03.04	36.46' LT	801.69	531650.84	847419.56
0526	469+05.46	42.46' LT	801.62	531648.48	847425.59

MadisonSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0401	469+35.60	48.12' LT	799.44	531618.39	847431.54
0402	469+29.20	54.16' LT	799.82	531624.85	847437.52
0403	469+25.39	58.80' LT	800.08	531628.71	847442.12
0404	469+22.23	63.58' LT	800.33	531631.92	847446.86
0405	469+11.50	86.89' LT	801.03	531642.88	847470.07
0406	469+30.79	56.12' LT	799.76	531623.28	847439.49
0407	469+27.62	59.99' LT	799.99	531626.48	847443.33
0408	469+35.44	59.93' LT	799.85	531618.67	847443.35
0409	469+32.27	63.79' LT	799.93	531621.88	847447.18
0410	469+33.59	53.23' LT	800.08	531620.45	847436.63

MadisonSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0411	469+37.75	57.55' LT	800.09	531616.33	847440.99
0412	469+37.56	49.75' LT	799.85	531616.45	847433.19
0413	469+41.10	54.21' LT	799.91	531612.95	847437.69
0414	469+19.26	75.28' LT	801.08	531635.00	847458.54
0415	469+24.72	77.77' LT	801.17	531629.57	847461.09
0416	469+17.00	80.23' LT	801.22	531637.31	847463.46
0417	469+22.45	82.72' LT	801.31	531631.88	847466.01
0418	469+15.72	86.64' LT	801.42	531638.65	847469.86
0419	469+19.65	86.95' LT	801.51	531634.72	847470.21

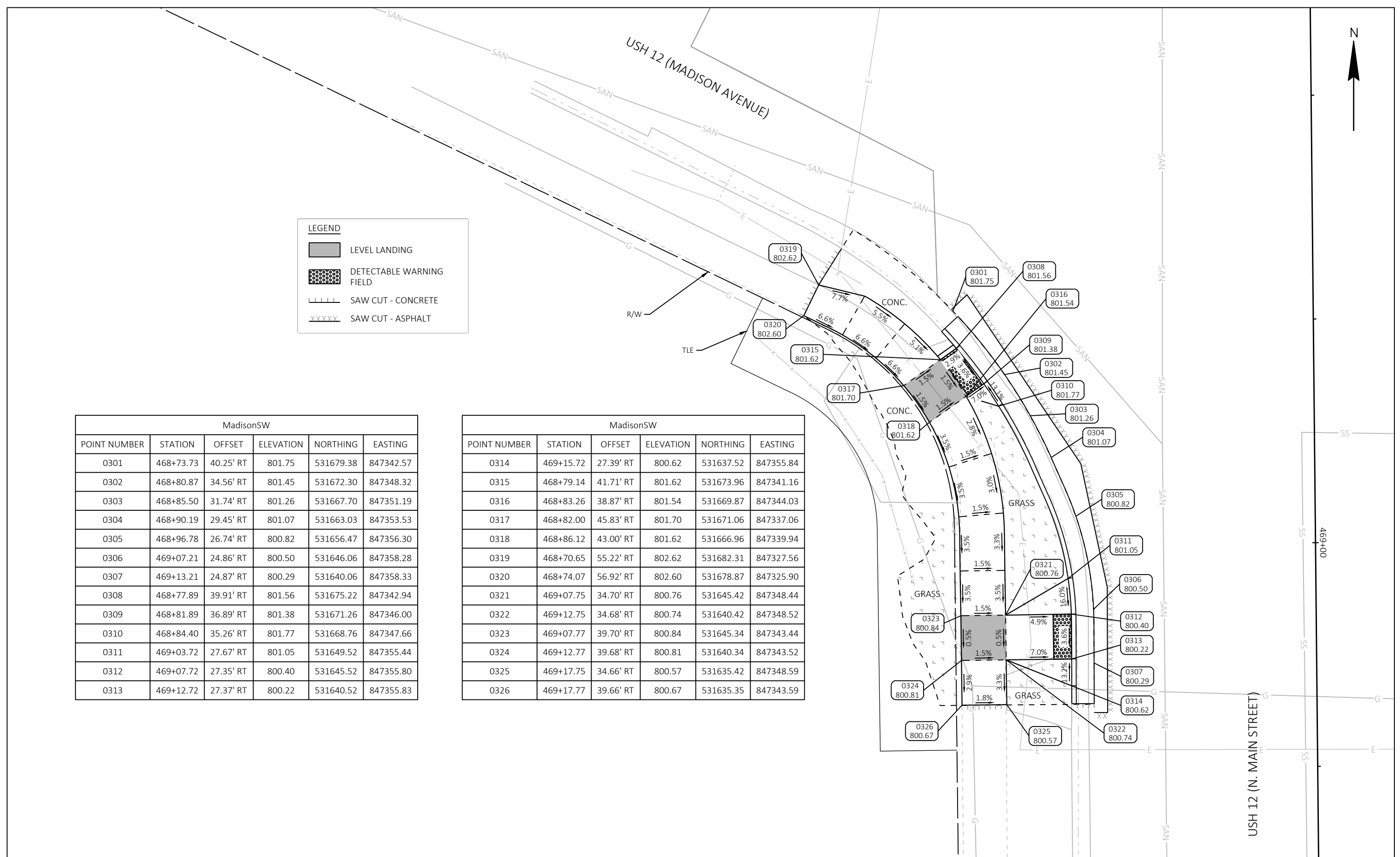


LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT

MadisonSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0301	468+73.73	40.25' RT	801.75	531679.38	847342.57
0302	468+80.87	34.56' RT	801.45	531672.30	847348.32
0303	468+85.50	31.74' RT	801.26	531667.70	847351.19
0304	468+90.19	29.45' RT	801.07	531663.03	847353.53
0305	468+96.78	26.74' RT	800.82	531656.47	847356.30
0306	469+07.21	24.86' RT	800.50	531646.06	847358.28
0307	469+13.21	24.87' RT	800.29	531640.06	847358.33
0308	468+77.89	39.91' RT	801.56	531675.22	847342.94
0309	468+81.89	36.89' RT	801.38	531671.26	847346.00
0310	468+84.40	35.26' RT	801.77	531668.76	847347.66
0311	469+03.72	27.67' RT	801.05	531649.52	847355.44
0312	469+07.72	27.35' RT	800.40	531645.52	847355.80
0313	469+12.72	27.37' RT	800.22	531640.52	847355.83





MadisonSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0314	469+15.72	27.39' RT	800.62	531637.52	847355.84
0315	468+79.14	41.71' RT	801.62	531673.96	847341.16
0316	468+83.26	38.87' RT	801.54	531669.87	847344.03
0317	468+82.00	45.83' RT	801.70	531671.06	847337.06
0318	468+86.12	43.00' RT	801.62	531666.96	847339.94
0319	468+70.65	55.22' RT	802.62	531682.31	847327.56
0320	468+74.07	56.92' RT	802.60	531678.87	847325.90
0321	469+07.75	34.70' RT	800.76	531645.42	847348.44
0322	469+12.75	34.68' RT	800.74	531640.42	847348.52
0323	469+07.77	39.70' RT	800.84	531645.34	847343.44
0324	469+12.77	39.68' RT	800.81	531640.34	847343.52
0325	469+17.75	34.66' RT	800.57	531635.42	847348.59
0326	469+17.77	39.66' RT	800.67	531635.35	847343.59

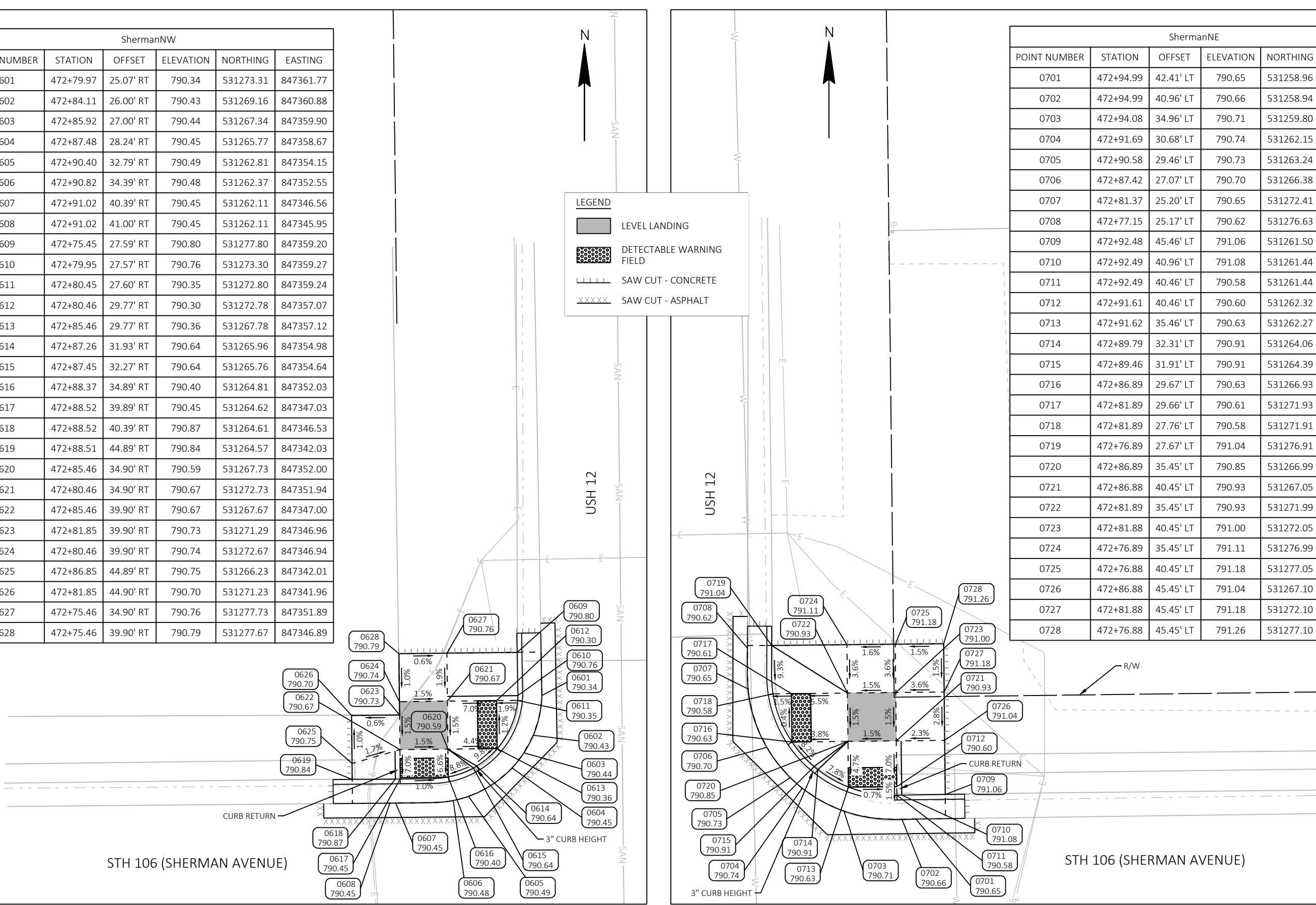


ShermanNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0601	472+79.97	25.07' RT	790.34	531273.31	847361.77
0602	472+84.11	26.00' RT	790.43	531269.16	847360.88
0603	472+85.92	27.00' RT	790.44	531267.34	847359.90
0604	472+87.48	28.24' RT	790.45	531265.77	847358.67
0605	472+90.40	32.79' RT	790.49	531262.81	847354.15
0606	472+90.82	34.39' RT	790.48	531262.37	847352.55
0607	472+91.02	40.39' RT	790.45	531262.11	847346.56
0608	472+91.02	41.00' RT	790.45	531262.11	847345.95
0609	472+75.45	27.59' RT	790.80	531277.80	847359.20
0610	472+79.95	27.57' RT	790.76	531273.30	847359.27
0611	472+80.45	27.60' RT	790.35	531272.80	847359.24
0612	472+80.46	29.77' RT	790.30	531272.78	847357.07
0613	472+85.46	29.77' RT	790.36	531267.78	847357.12
0614	472+87.26	31.93' RT	790.64	531265.96	847354.98
0615	472+87.45	32.27' RT	790.64	531265.76	847354.64
0616	472+88.37	34.89' RT	790.40	531264.81	847352.03
0617	472+88.52	39.89' RT	790.45	531264.62	847347.03
0618	472+88.52	40.39' RT	790.87	531264.61	847346.53
0619	472+88.51	44.89' RT	790.84	531264.57	847342.03
0620	472+85.46	34.90' RT	790.59	531267.73	847352.00
0621	472+80.46	34.90' RT	790.67	531272.73	847351.94
0622	472+85.46	39.90' RT	790.67	531267.67	847347.00
0623	472+81.85	39.90' RT	790.73	531271.29	847346.96
0624	472+80.46	39.90' RT	790.74	531272.67	847346.94
0625	472+86.85	44.89' RT	790.75	531266.23	847342.01
0626	472+81.85	44.90' RT	790.70	531271.23	847341.96
0627	472+75.46	34.90' RT	790.76	531277.73	847351.89
0628	472+75.46	39.90' RT	790.79	531277.67	847346.89

ShermanNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0701	472+94.99	42.41' LT	790.65	531258.96	847429.39
0702	472+94.99	40.96' LT	790.66	531258.94	847427.94
0703	472+94.08	34.96' LT	790.71	531259.80	847421.93
0704	472+91.69	30.68' LT	790.74	531262.15	847417.63
0705	472+90.58	29.46' LT	790.73	531263.24	847416.40
0706	472+87.42	27.07' LT	790.70	531266.38	847413.98
0707	472+81.37	25.20' LT	790.65	531272.41	847412.05
0708	472+77.15	25.17' LT	790.62	531276.63	847411.97
0709	472+92.48	45.46' LT	791.06	531261.50	847432.42
0710	472+92.49	40.96' LT	791.08	531261.44	847427.92
0711	472+92.49	40.46' LT	790.58	531261.44	847427.42
0712	472+91.61	40.46' LT	790.60	531262.32	847427.41
0713	472+91.62	35.46' LT	790.63	531262.27	847422.41
0714	472+89.79	32.31' LT	790.91	531264.06	847419.24
0715	472+89.46	31.91' LT	790.91	531264.39	847418.84
0716	472+86.89	29.67' LT	790.63	531266.93	847416.57
0717	472+81.89	29.66' LT	790.61	531271.93	847416.52
0718	472+81.89	27.76' LT	790.58	531271.91	847414.62
0719	472+76.89	27.67' LT	791.04	531276.91	847414.47
0720	472+86.89	35.45' LT	790.85	531266.99	847422.36
0721	472+86.88	40.45' LT	790.93	531267.05	847427.36
0722	472+81.89	35.45' LT	790.93	531271.99	847422.30
0723	472+81.88	40.45' LT	791.00	531272.05	847427.30
0724	472+76.89	35.45' LT	791.11	531276.99	847422.25
0725	472+76.88	40.45' LT	791.18	531277.05	847427.25
0726	472+86.88	45.45' LT	791.04	531267.10	847432.36
0727	472+81.88	45.45' LT	791.18	531272.10	847432.30
0728	472+76.88	45.45' LT	791.26	531277.10	847432.25

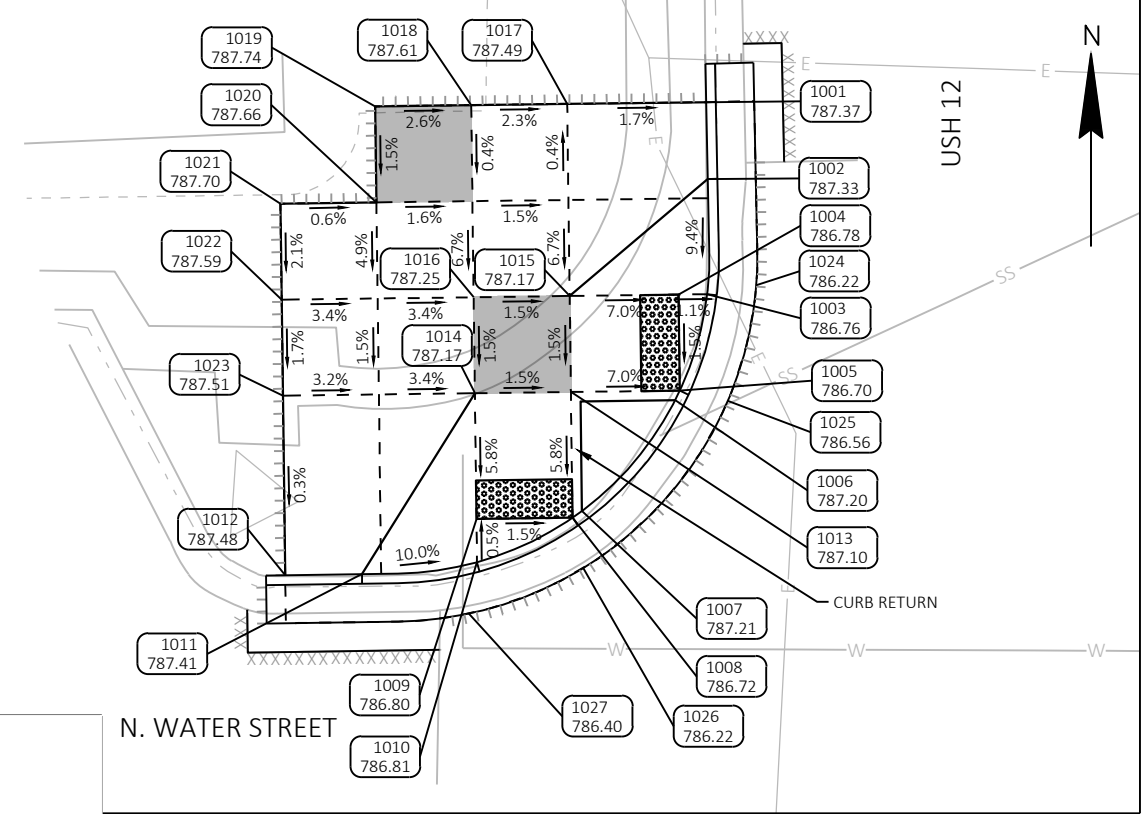
LEGEND

-  LEVEL LANDING
-  DETECTABLE WARNING FIELD
-  SAW CUT - CONCRETE
-  SAW CUT - ASPHALT



NWaterNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1001	475+81.80	27.26' RT	787.37	530971.48	847362.57
1002	475+85.80	27.21' RT	787.33	530967.48	847362.66
1003	475+91.80	27.30' RT	786.76	530961.48	847362.63
1004	475+91.80	28.79' RT	786.78	530961.46	847361.14
1005	475+96.80	28.77' RT	786.70	530956.46	847361.21
1006	475+97.30	29.02' RT	787.20	530955.96	847360.96
1007	476+03.02	33.93' RT	787.21	530950.19	847356.11
1008	476+03.37	34.43' RT	786.72	530949.84	847355.61
1009	476+03.38	39.43' RT	786.80	530949.78	847350.61
1010	476+05.63	39.43' RT	786.81	530947.52	847350.64
1011	476+06.19	45.42' RT	787.41	530946.91	847344.65
1012	476+06.21	49.42' RT	787.48	530946.85	847340.65
1013	475+96.82	34.45' RT	787.10	530956.39	847355.53
1014	475+96.83	39.45' RT	787.17	530956.32	847350.53

NWaterNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1015	475+91.82	34.46' RT	787.17	530961.39	847355.47
1016	475+91.83	39.46' RT	787.25	530961.32	847350.47
1017	475+81.82	34.49' RT	787.49	530971.39	847355.34
1018	475+81.83	39.49' RT	787.61	530971.32	847350.34
1019	475+81.85	44.49' RT	787.74	530971.26	847345.34
1020	475+86.85	44.48' RT	787.66	530966.26	847345.40
1021	475+86.86	49.48' RT	787.70	530966.20	847340.40
1022	475+91.86	49.46' RT	787.59	530961.20	847340.47
1023	475+96.86	49.45' RT	787.51	530956.20	847340.53
1024	475+91.35	24.75' RT	786.22	530961.95	847365.18
1025	475+97.35	26.27' RT	786.56	530955.94	847363.71
1026	476+05.98	33.89' RT	786.22	530947.23	847356.18
1027	476+08.29	39.88' RT	786.40	530944.86	847350.21

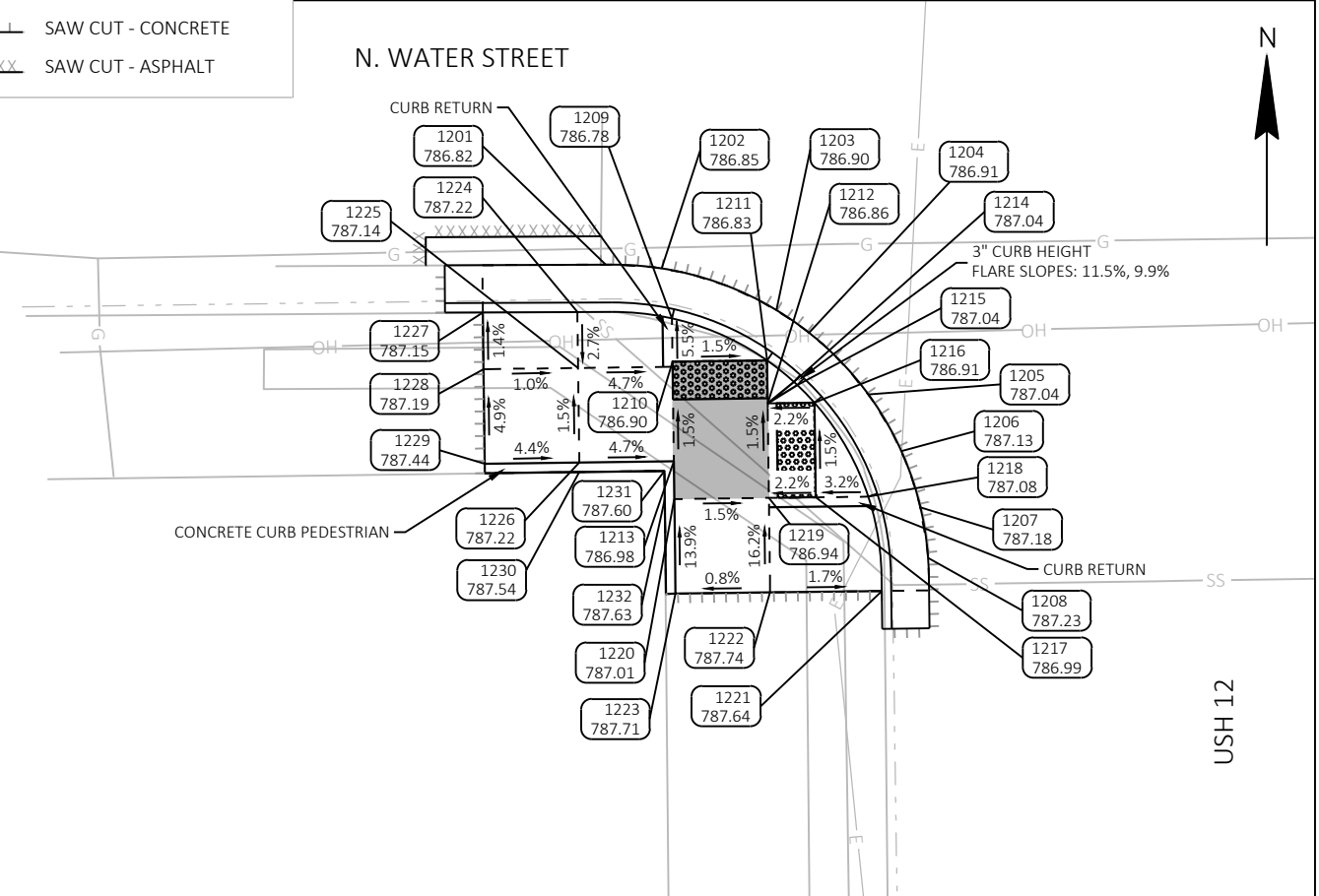


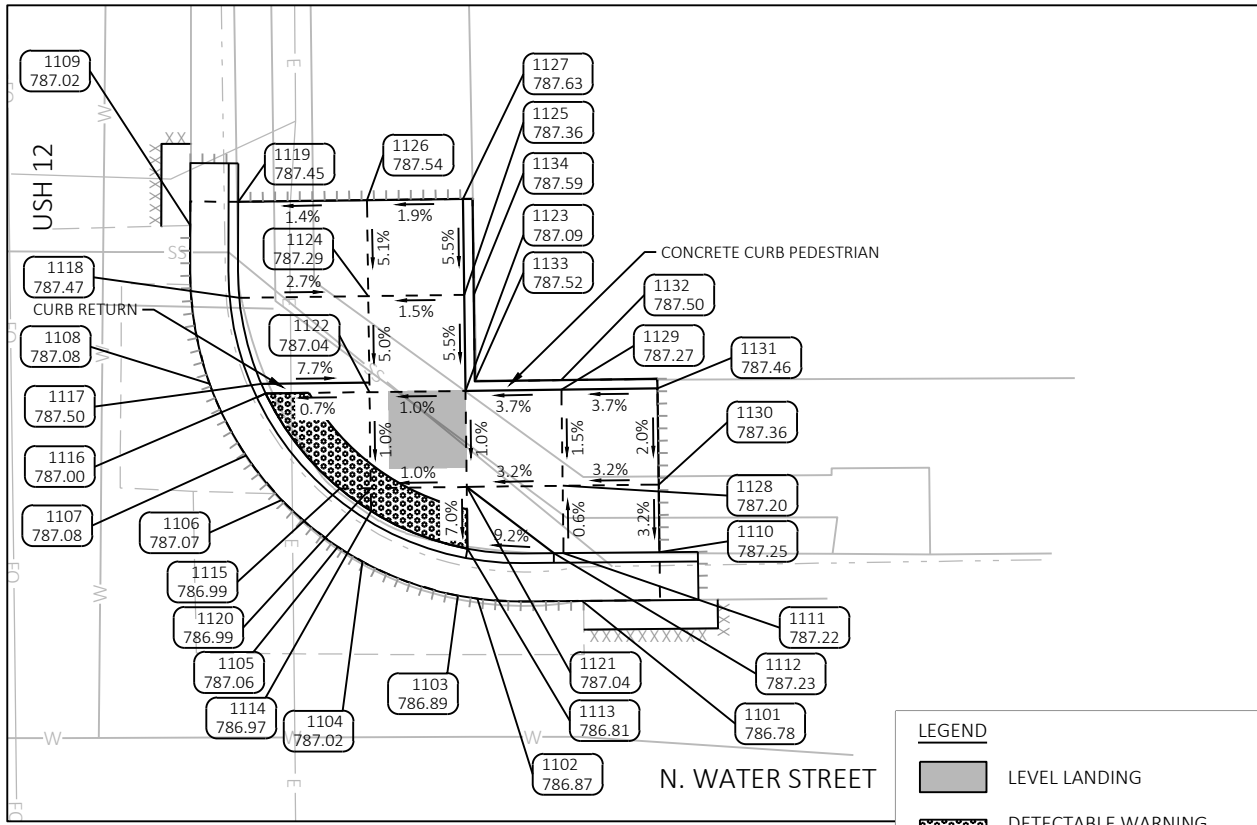
LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT

NWaterSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1201	476+50.64	42.15' RT	786.82	530902.50	847348.36
1202	476+50.88	39.21' RT	786.85	530902.28	847351.31
1203	476+53.16	33.17' RT	786.90	530900.06	847357.37
1204	476+54.35	31.45' RT	786.91	530898.89	847359.10
1205	476+57.64	28.37' RT	787.04	530895.63	847362.21
1206	476+60.65	26.64' RT	787.13	530892.64	847363.97
1207	476+63.64	25.27' RT	787.18	530889.66	847364.97
1208	476+66.32	25.27' RT	787.23	530886.98	847365.39
1209	476+53.51	38.69' RT	786.78	530899.65	847351.85
1210	476+55.80	38.68' RT	786.90	530897.37	847351.88
1211	476+55.79	33.68' RT	786.83	530897.43	847356.88
1212	476+58.06	33.68' RT	786.86	530895.16	847356.91
1213	476+61.07	38.67' RT	786.98	530892.09	847351.95
1214	476+56.94	32.27' RT	787.04	530896.29	847358.30
1215	476+57.11	32.09' RT	787.04	530896.12	847358.48
1216	476+58.05	31.20' RT	786.91	530895.19	847359.39

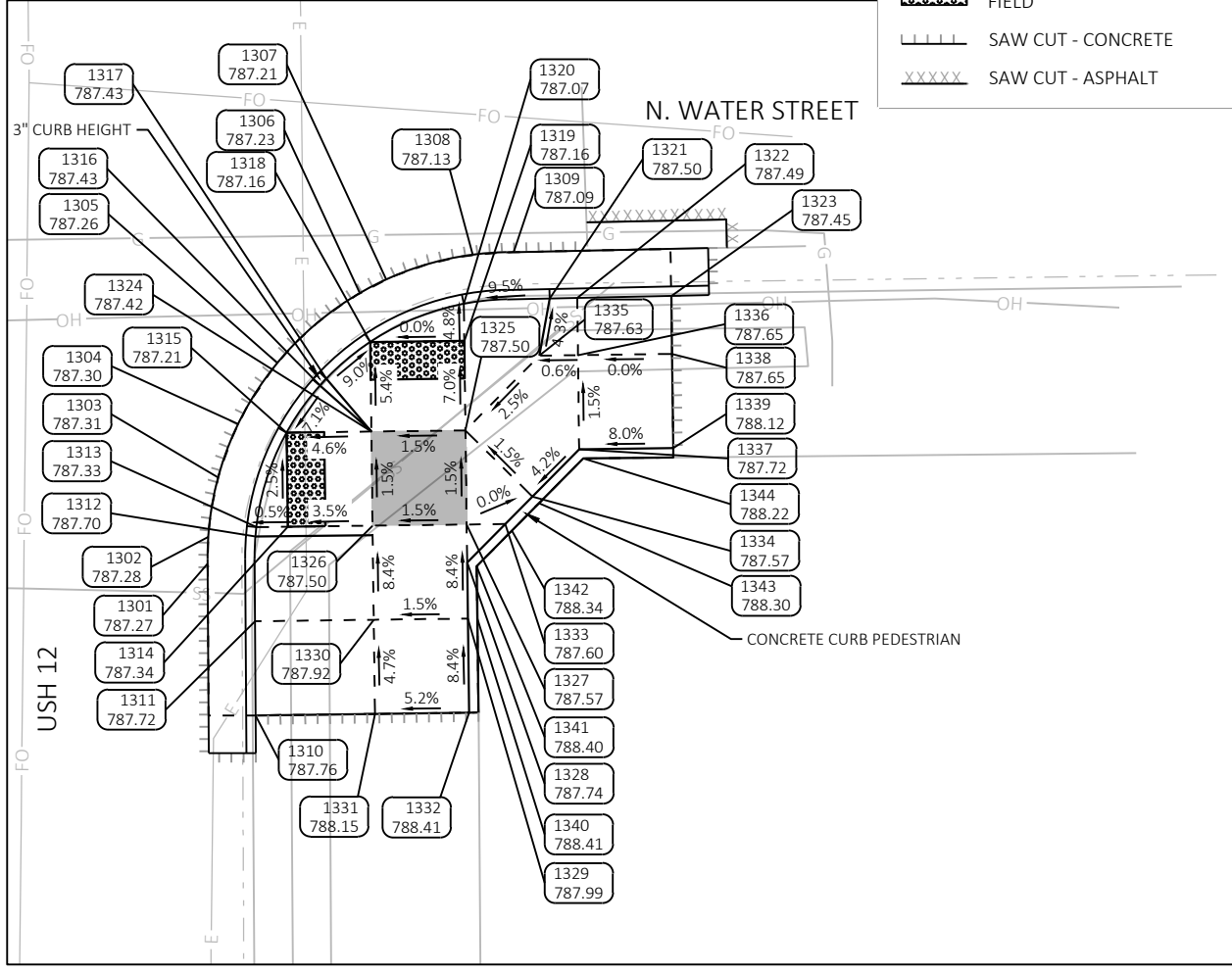
NWaterSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1217	476+63.05	31.18' RT	786.99	530890.19	847359.45
1218	476+63.04	28.41' RT	787.08	530890.22	847362.23
1219	476+63.06	33.66' RT	786.94	530890.16	847356.97
1220	476+63.07	38.66' RT	787.01	530890.09	847351.97
1221	476+68.04	27.74' RT	787.64	530885.23	847362.94
1222	476+68.06	33.65' RT	787.74	530885.16	847357.04
1223	476+68.07	38.65' RT	787.71	530885.09	847352.04
1224	476+53.13	43.69' RT	787.22	530899.99	847346.85
1225	476+56.09	43.68' RT	787.14	530897.03	847346.89
1226	476+61.09	43.67' RT	787.22	530892.03	847346.95
1227	476+53.10	48.69' RT	787.15	530899.96	847341.85
1228	476+56.10	48.68' RT	787.19	530896.97	847341.89
1229	476+61.10	48.67' RT	787.44	530891.97	847341.95
1230	476+61.59	43.67' RT	787.54	530891.53	847346.96
1231	476+61.57	39.17' RT	787.60	530891.59	847351.46
1232	476+63.07	39.16' RT	787.63	530890.09	847351.47





NWWaterNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1101	476+03.97	45.42' LT	786.78	530950.03	847435.46
1102	476+03.80	39.97' LT	786.87	530950.15	847430.02
1103	476+03.63	38.96' LT	786.89	530950.30	847429.00
1104	476+01.79	34.00' LT	787.02	530952.10	847424.02
1105	476+01.03	32.73' LT	787.06	530952.84	847422.74
1106	475+98.55	29.82' LT	787.07	530955.29	847419.82
1107	475+96.22	27.99' LT	787.08	530957.60	847417.96
1108	475+92.47	26.14' LT	787.08	530961.34	847416.07
1109	475+84.22	25.20' LT	787.02	530969.57	847415.05
1110	476+01.47	49.48' LT	787.25	530952.57	847439.50
1111	476+01.47	44.48' LT	787.22	530952.52	847434.50
1112	476+01.47	43.97' LT	787.23	530952.52	847433.99
1113	476+01.19	39.48' LT	786.81	530952.75	847429.50
1114	475+99.18	34.47' LT	786.97	530954.71	847424.47
1115	475+97.99	32.86' LT	786.99	530955.88	847422.84
1116	475+93.00	29.05' LT	787.00	530960.83	847418.98
1117	475+92.50	28.82' LT	787.50	530961.33	847418.76

NWWaterNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1118	475+88.01	27.72' LT	787.47	530965.81	847417.61
1119	475+83.01	27.72' LT	787.45	530970.81	847417.56
1120	475+97.99	34.47' LT	786.99	530955.90	847424.46
1121	475+97.97	39.47' LT	787.04	530955.96	847429.46
1122	475+92.99	34.46' LT	787.04	530960.90	847424.39
1123	475+92.97	39.46' LT	787.09	530960.96	847429.39
1124	475+87.99	34.44' LT	787.29	530965.90	847424.33
1125	475+87.97	39.44' LT	787.36	530965.96	847429.33
1126	475+82.99	34.43' LT	787.54	530970.90	847424.27
1127	475+82.97	39.43' LT	787.63	530970.96	847429.27
1128	475+97.96	44.47' LT	787.20	530956.03	847434.46
1129	475+92.96	44.46' LT	787.27	530961.03	847434.39
1130	475+97.95	49.47' LT	787.36	530956.09	847439.46
1131	475+92.95	49.46' LT	787.46	530961.09	847439.39
1132	475+92.46	44.45' LT	787.50	530961.53	847434.39
1133	475+92.47	39.95' LT	787.52	530961.47	847429.89
1134	475+87.97	39.94' LT	787.59	530965.97	847429.83

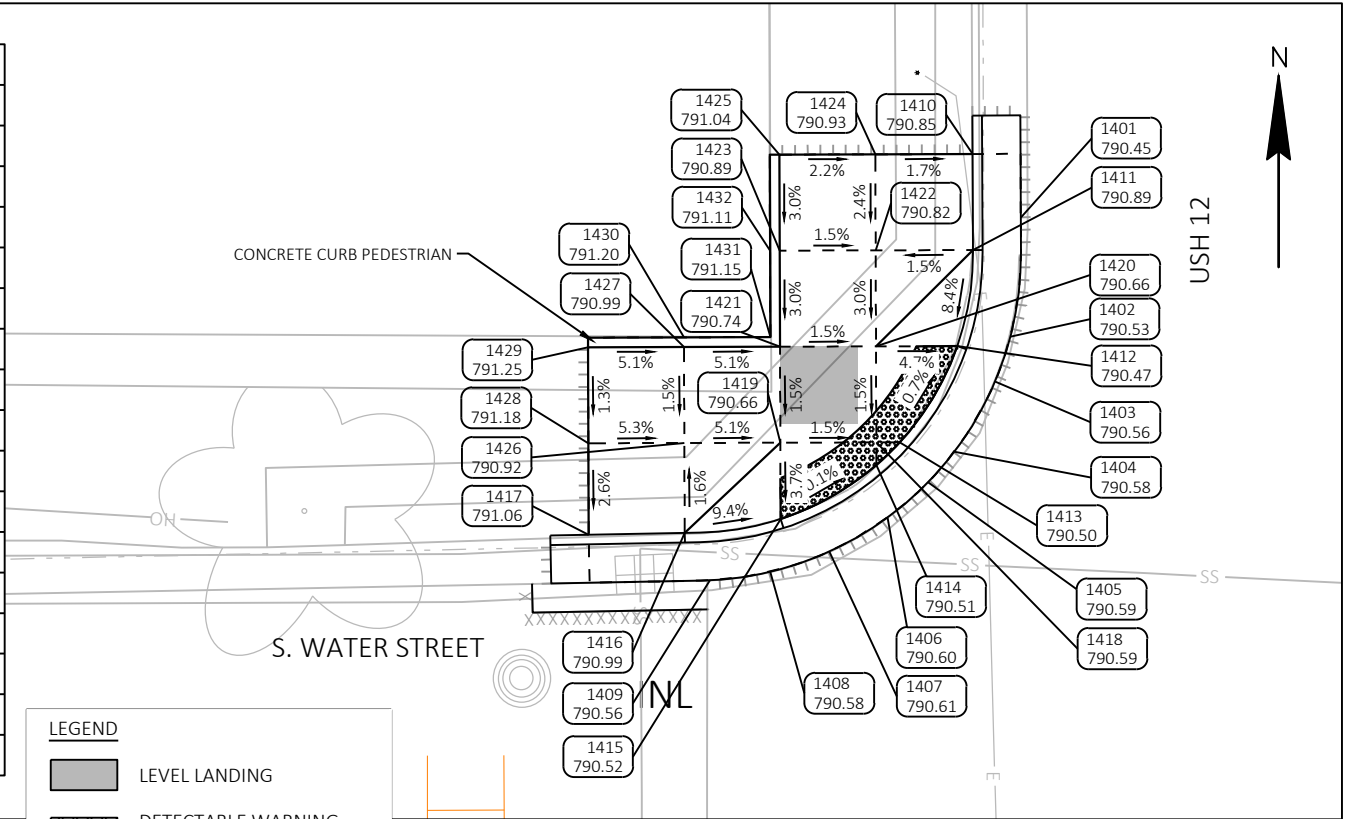


NWWaterSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1301	476+66.85	25.17' LT	787.27	530886.95	847415.84
1302	476+65.43	25.22' LT	787.28	530888.37	847415.88
1303	476+62.27	25.78' LT	787.31	530891.53	847416.41
1304	476+59.46	26.88' LT	787.30	530894.36	847417.48
1305	476+56.23	28.98' LT	787.26	530897.61	847419.55
1306	476+52.57	33.40' LT	787.23	530901.31	847423.93
1307	476+51.85	34.82' LT	787.21	530902.05	847425.34
1308	476+50.60	39.43' LT	787.13	530903.34	847429.93
1309	476+50.46	41.59' LT	787.09	530903.50	847432.10
1310	476+74.94	27.64' LT	787.76	530878.89	847418.39
1311	476+69.94	27.66' LT	787.72	530883.89	847418.36
1312	476+65.44	27.73' LT	787.70	530888.39	847418.38
1313	476+64.94	27.78' LT	787.33	530888.89	847418.43
1314	476+64.93	29.46' LT	787.34	530888.91	847420.11
1315	476+59.93	29.44' LT	787.21	530893.91	847420.04
1316	476+57.44	31.24' LT	787.43	530896.42	847421.82
1317	476+57.08	31.58' LT	787.43	530896.78	847422.16
1318	476+55.16	33.93' LT	787.16	530898.72	847424.48
1319	476+55.15	38.93' LT	787.16	530898.78	847429.48
1320	476+53.18	38.92' LT	787.07	530900.75	847429.45
1321	476+52.94	43.49' LT	787.50	530901.04	847434.02
1322	476+52.92	44.95' LT	787.49	530901.07	847435.48

NWWaterSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1323	476+52.87	49.95' LT	787.45	530901.17	847440.48
1324	476+59.92	33.94' LT	787.42	530893.97	847424.54
1325	476+59.90	38.94' LT	787.50	530894.03	847429.54
1326	476+64.92	33.95' LT	787.50	530888.97	847424.60
1327	476+64.90	38.95' LT	787.57	530889.03	847429.60
1328	476+66.92	38.96' LT	787.74	530887.01	847429.63
1329	476+69.90	38.97' LT	787.99	530884.03	847429.67
1330	476+69.92	33.97' LT	787.92	530883.97	847424.67
1331	476+74.92	33.98' LT	788.15	530878.97	847424.73
1332	476+74.90	38.98' LT	788.41	530879.03	847429.73
1333	476+64.90	41.00' LT	787.60	530889.06	847431.65
1334	476+63.45	42.46' LT	787.57	530890.52	847433.09
1335	476+55.97	42.91' LT	787.63	530898.01	847433.47
1336	476+55.96	44.96' LT	787.65	530898.03	847435.52
1337	476+60.96	44.97' LT	787.72	530893.03	847435.58
1338	476+55.95	49.96' LT	787.65	530898.10	847440.52
1339	476+60.95	49.97' LT	788.12	530893.10	847440.58
1340	476+69.90	39.47' LT	788.41	530884.04	847430.17
1341	476+67.13	39.46' LT	788.40	530886.81	847430.13
1342	476+65.25	41.36' LT	788.34	530888.71	847432.01
1343	476+63.81	42.81' LT	788.30	530890.16	847433.45
1344	476+61.46	45.18' LT	788.22	530892.53	847435.79

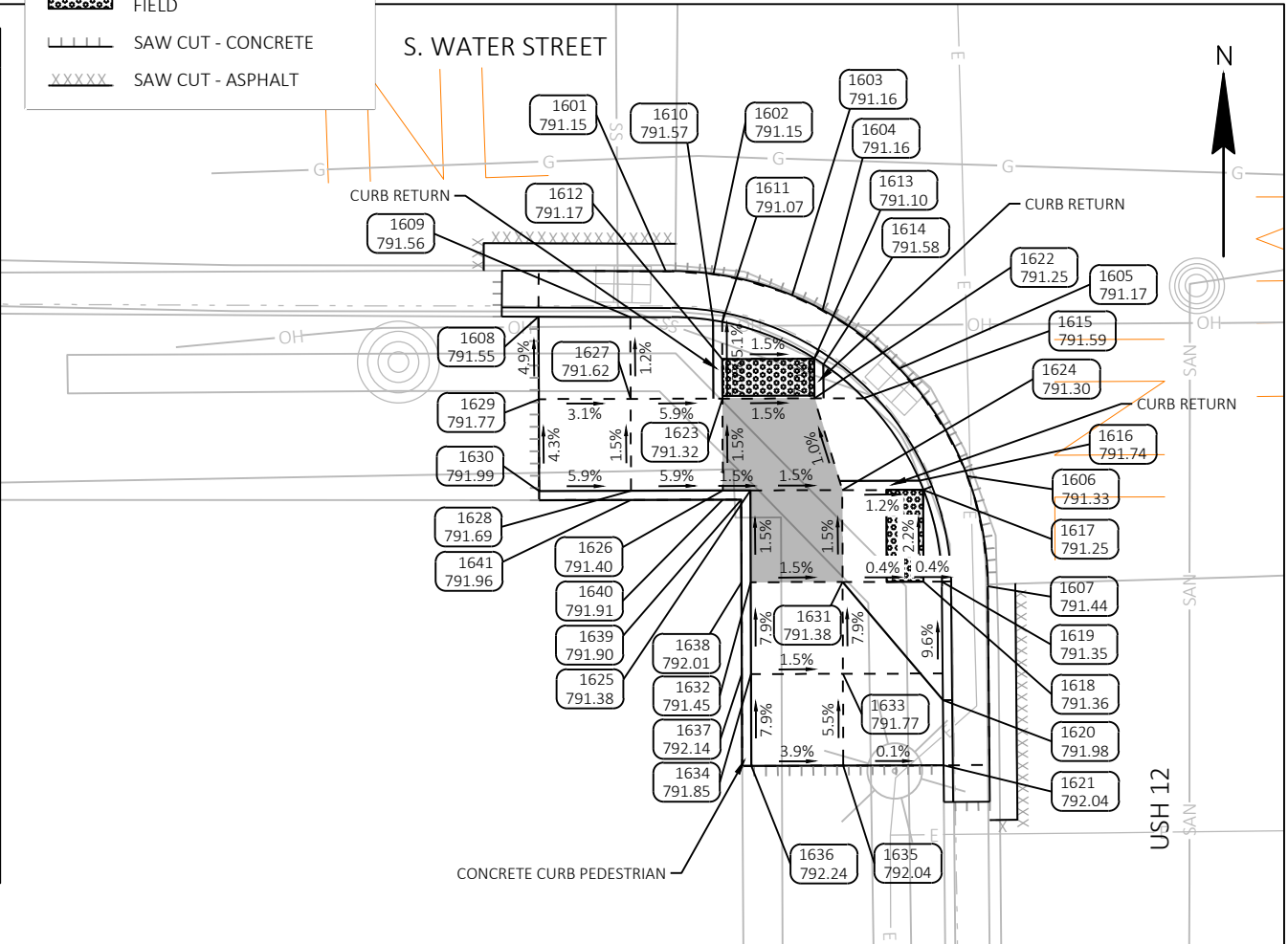
SWaterNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1401	481+86.76	25.85' RT	790.45	530366.56	847369.98
1402	481+92.97	26.47' RT	790.53	530360.35	847369.42
1403	481+95.30	27.27' RT	790.56	530358.01	847368.64
1404	481+98.92	29.46' RT	790.58	530354.36	847366.49
1405	482+00.50	30.81' RT	790.59	530352.77	847365.15
1406	482+02.31	32.94' RT	790.60	530350.93	847363.04
1407	482+04.06	36.00' RT	790.61	530349.16	847360.00
1408	482+05.06	39.08' RT	790.58	530348.13	847356.92
1409	482+05.50	42.22' RT	790.56	530347.66	847353.79
1410	481+83.44	28.33' RT	790.85	530369.85	847367.46
1411	481+88.44	28.35' RT	790.89	530364.85	847367.49
1412	481+93.44	29.22' RT	790.47	530359.85	847366.67
1413	481+98.41	32.27' RT	790.50	530354.84	847363.67
1414	481+99.58	33.49' RT	790.51	530353.66	847362.46
1415	482+02.34	38.51' RT	790.52	530350.85	847357.47
1416	482+03.02	43.52' RT	790.99	530350.13	847352.47

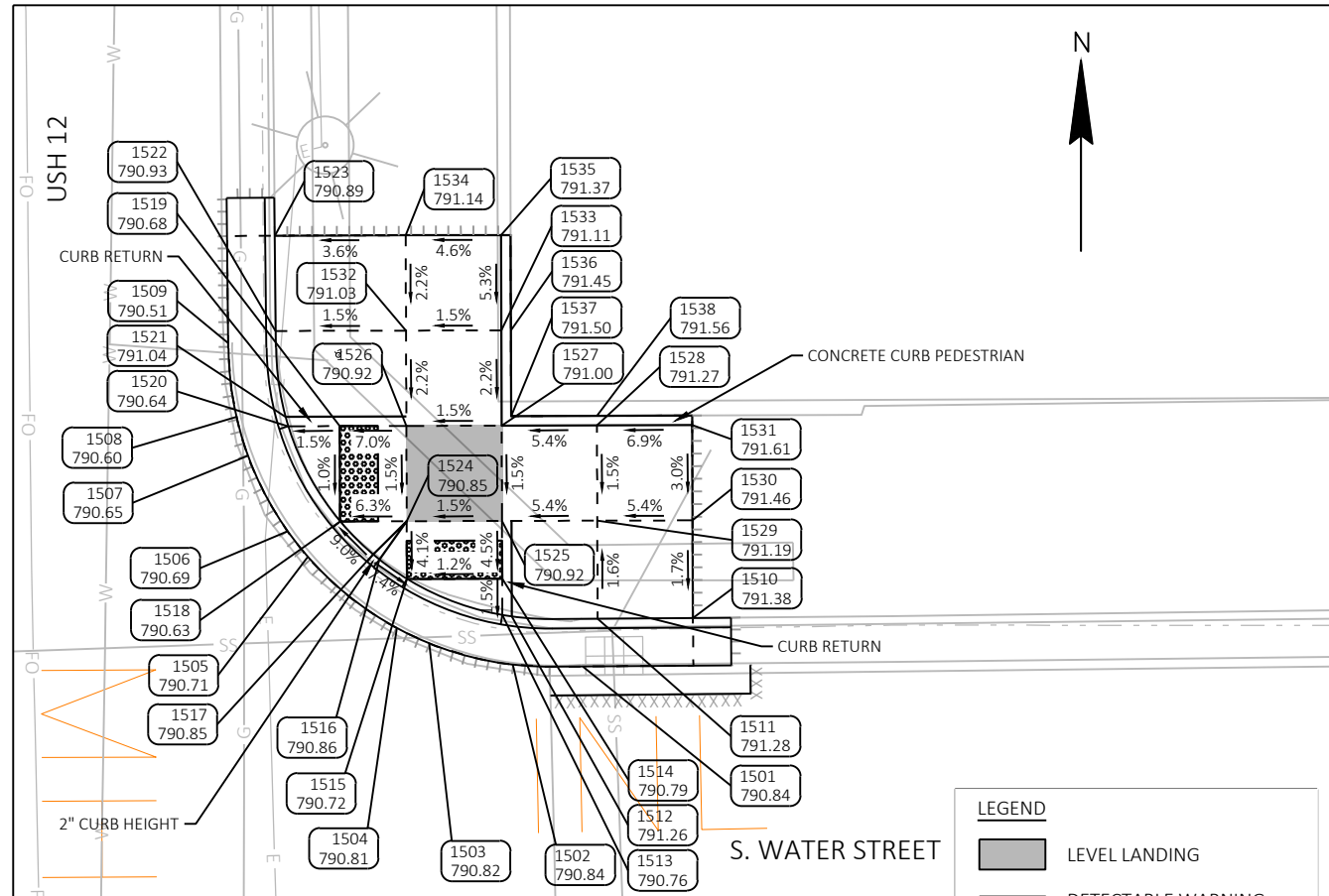
SWaterNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1417	482+03.06	48.52' RT	791.06	530350.03	847347.47
1418	481+98.41	33.49' RT	790.59	530354.84	847362.45
1419	481+98.37	38.49' RT	790.66	530354.82	847357.45
1420	481+93.41	33.46' RT	790.66	530359.84	847362.44
1421	481+93.37	38.46' RT	790.74	530359.82	847357.44
1422	481+88.41	33.42' RT	790.82	530364.84	847362.42
1423	481+88.37	38.42' RT	790.89	530364.82	847357.42
1424	481+83.41	33.39' RT	790.93	530369.84	847362.40
1425	481+83.37	38.39' RT	791.04	530369.82	847357.40
1426	481+98.33	43.49' RT	790.92	530354.81	847352.45
1427	481+93.33	43.46' RT	790.99	530359.81	847352.44
1428	481+98.30	48.49' RT	791.18	530354.80	847347.45
1429	481+93.30	48.46' RT	791.25	530359.80	847347.44
1430	481+92.83	43.45' RT	791.20	530360.31	847352.43
1431	481+92.87	38.95' RT	791.15	530360.32	847356.93
1432	481+88.37	38.92' RT	791.11	530364.82	847356.92



SWaterSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1601	482+39.94	43.36' RT	791.15	530313.21	847352.99
1602	482+40.13	40.77' RT	791.15	530313.04	847355.59
1603	482+41.33	36.51' RT	791.16	530311.89	847359.86
1604	482+42.16	34.86' RT	791.16	530311.07	847361.52
1605	482+45.36	30.73' RT	791.17	530307.91	847365.68
1606	482+51.24	27.09' RT	791.33	530302.07	847369.38
1607	482+57.24	26.01' RT	791.44	530296.08	847370.52
1608	482+42.34	50.32' RT	791.55	530310.74	847346.06
1609	482+42.41	45.32' RT	791.56	530310.72	847351.06
1610	482+42.65	40.82' RT	791.57	530310.52	847355.56
1611	482+42.74	40.32' RT	791.07	530310.43	847356.06
1612	482+44.74	40.33' RT	791.17	530308.44	847356.07
1613	482+44.77	35.33' RT	791.10	530308.46	847361.07
1614	482+45.10	34.83' RT	791.58	530308.13	847361.57
1615	482+46.95	32.67' RT	791.59	530306.31	847363.75
1616	482+51.47	29.68' RT	791.74	530301.82	847366.79
1617	482+51.97	29.47' RT	791.25	530301.32	847367.00
1618	482+56.97	29.51' RT	791.36	530296.32	847367.01
1619	482+56.98	28.51' RT	791.35	530296.32	847368.01
1620	482+63.42	28.51' RT	791.98	530289.87	847368.08
1621	482+66.98	28.51' RT	792.04	530286.32	847368.11

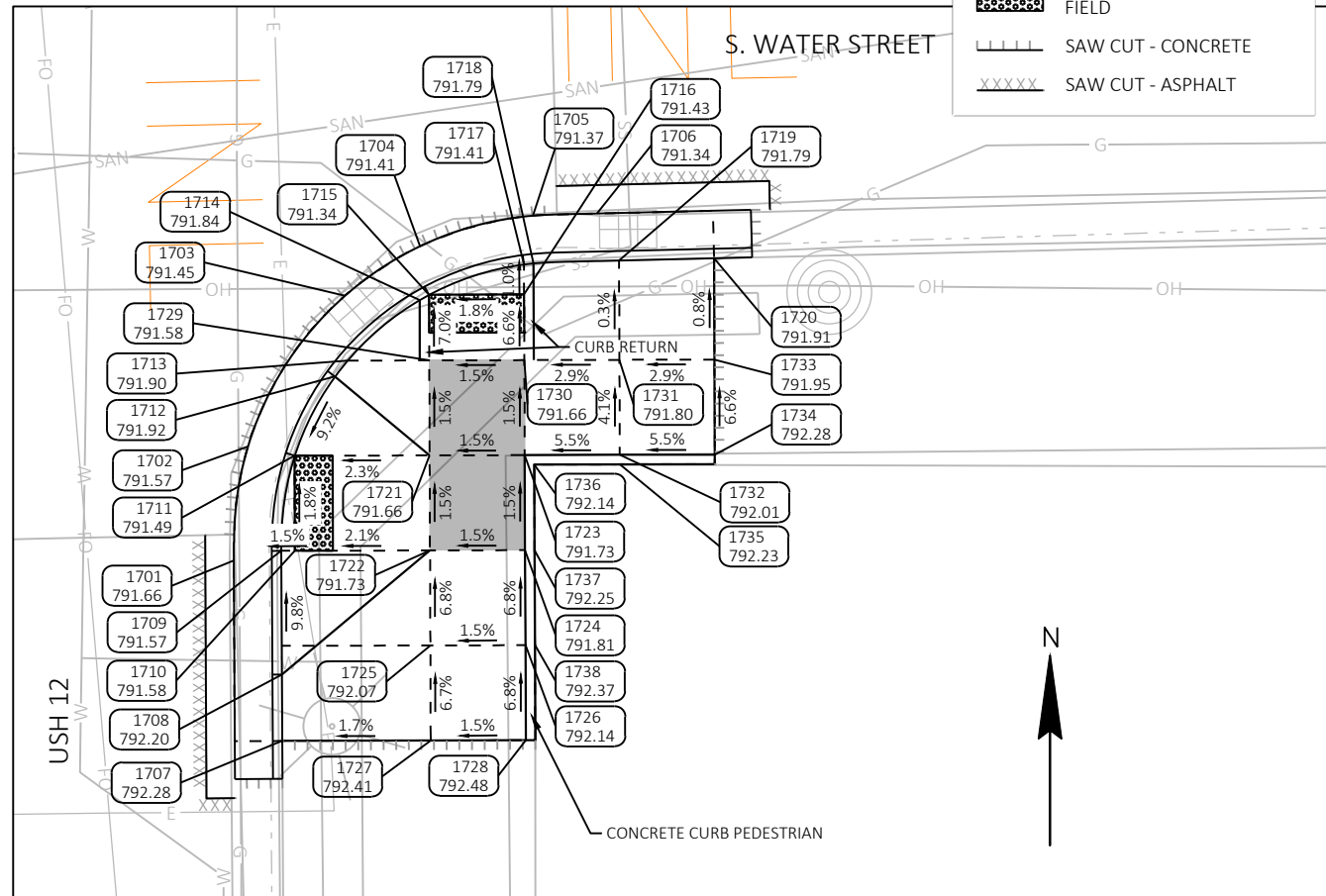
SWaterSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1622	482+46.93	35.34' RT	791.25	530306.30	847361.08
1623	482+46.89	40.34' RT	791.32	530306.29	847356.08
1624	482+51.94	33.88' RT	791.30	530301.31	847362.60
1625	482+51.90	38.88' RT	791.38	530301.29	847357.60
1626	482+51.89	40.38' RT	791.40	530301.29	847356.10
1627	482+46.86	45.34' RT	791.62	530306.27	847351.08
1628	482+51.86	45.38' RT	791.69	530301.27	847351.10
1629	482+46.82	50.34' RT	791.77	530306.26	847346.08
1630	482+51.82	50.38' RT	791.99	530301.26	847346.10
1631	482+56.94	33.91' RT	791.38	530296.31	847362.61
1632	482+56.90	38.91' RT	791.45	530296.29	847357.61
1633	482+61.94	33.94' RT	791.77	530291.31	847362.63
1634	482+61.90	38.94' RT	791.85	530291.29	847357.63
1635	482+66.94	33.97' RT	792.04	530286.31	847362.65
1636	482+66.90	38.97' RT	792.24	530286.29	847357.65
1637	482+61.90	39.44' RT	792.14	530291.29	847357.13
1638	482+56.90	39.41' RT	792.01	530296.29	847357.11
1639	482+52.40	39.38' RT	791.90	530300.79	847357.10
1640	482+52.39	40.38' RT	791.91	530300.79	847356.10
1641	482+52.36	45.38' RT	791.96	530300.77	847351.10





SWaterNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1501	482+07.04	42.84' LT	790.84	530346.96	847438.87
1502	482+06.90	39.15' LT	790.84	530347.06	847435.18
1503	482+05.75	34.78' LT	790.82	530348.17	847430.79
1504	482+04.92	33.08' LT	790.81	530348.98	847429.08
1505	482+01.15	28.45' LT	790.71	530352.70	847424.41
1506	481+99.79	27.40' LT	790.69	530354.05	847423.35
1507	481+95.81	25.35' LT	790.65	530358.02	847421.27
1508	481+93.75	24.79' LT	790.60	530360.07	847420.69
1509	481+89.89	24.38' LT	790.51	530363.92	847420.24
1510	482+04.55	48.63' LT	791.38	530349.51	847444.63
1511	482+04.54	43.63' LT	791.28	530349.47	847439.63
1512	482+04.38	39.13' LT	791.26	530349.58	847435.13
1513	482+04.30	38.63' LT	790.76	530349.66	847434.63
1514	482+02.39	38.64' LT	790.79	530351.57	847434.62
1515	482+02.36	33.64' LT	790.72	530351.55	847429.62
1516	482+01.33	32.20' LT	790.86	530352.56	847428.17
1517	482+01.06	31.88' LT	790.85	530352.83	847427.84
1518	481+99.30	30.16' LT	790.63	530354.57	847426.10
1519	481+94.30	30.19' LT	790.68	530359.57	847426.09

SWaterNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1520	481+94.28	27.48' LT	790.64	530359.57	847423.38
1521	481+93.78	27.34' LT	791.04	530360.07	847423.24
1522	481+89.28	26.88' LT	790.93	530364.56	847422.73
1523	481+84.28	26.89' LT	790.89	530369.56	847422.68
1524	481+99.33	33.66' LT	790.85	530354.58	847429.61
1525	481+99.36	38.66' LT	790.92	530354.60	847434.61
1526	481+94.33	33.69' LT	790.92	530359.58	847429.59
1527	481+94.36	38.69' LT	791.00	530359.60	847434.59
1528	481+94.40	43.69' LT	791.27	530359.61	847439.59
1529	481+99.40	43.66' LT	791.19	530354.61	847439.61
1530	481+99.43	48.66' LT	791.46	530354.63	847444.61
1531	481+94.43	48.69' LT	791.61	530359.63	847444.59
1532	481+89.33	33.73' LT	791.03	530364.58	847429.57
1533	481+89.36	38.73' LT	791.11	530364.60	847434.57
1534	481+84.33	33.76' LT	791.14	530369.58	847429.56
1535	481+84.36	38.76' LT	791.37	530369.60	847434.56
1536	481+89.36	39.23' LT	791.45	530364.60	847435.07
1537	481+93.87	39.20' LT	791.50	530360.10	847435.09
1538	481+93.90	43.70' LT	791.56	530360.11	847439.59



SWaterSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1701	482+57.48	24.36' LT	791.66	530296.34	847420.88
1702	482+51.48	25.16' LT	791.57	530302.34	847421.63
1703	482+43.61	30.33' LT	791.45	530310.27	847426.72
1704	482+41.05	34.32' LT	791.41	530312.87	847430.68
1705	482+39.49	40.27' LT	791.37	530314.48	847436.62
1706	482+39.41	43.64' LT	791.34	530314.60	847439.99
1707	482+67.00	26.82' LT	792.28	530286.85	847423.44
1708	482+63.50	26.84' LT	792.20	530290.35	847423.42
1709	482+57.00	26.86' LT	791.57	530296.85	847423.38
1710	482+57.00	27.58' LT	791.58	530296.85	847424.10
1711	482+52.00	27.61' LT	791.49	530301.85	847424.08
1712	482+47.85	29.77' LT	791.92	530306.02	847426.20
1713	482+47.02	30.44' LT	791.90	530306.86	847426.86
1714	482+43.87	34.27' LT	791.84	530310.04	847430.66
1715	482+43.60	34.77' LT	791.34	530310.32	847431.16
1716	482+43.63	39.77' LT	791.43	530310.34	847436.16
1717	482+42.05	39.78' LT	791.41	530311.92	847436.15
1718	482+42.00	40.28' LT	791.79	530311.98	847436.65
1719	482+41.90	44.78' LT	791.79	530312.12	847441.15

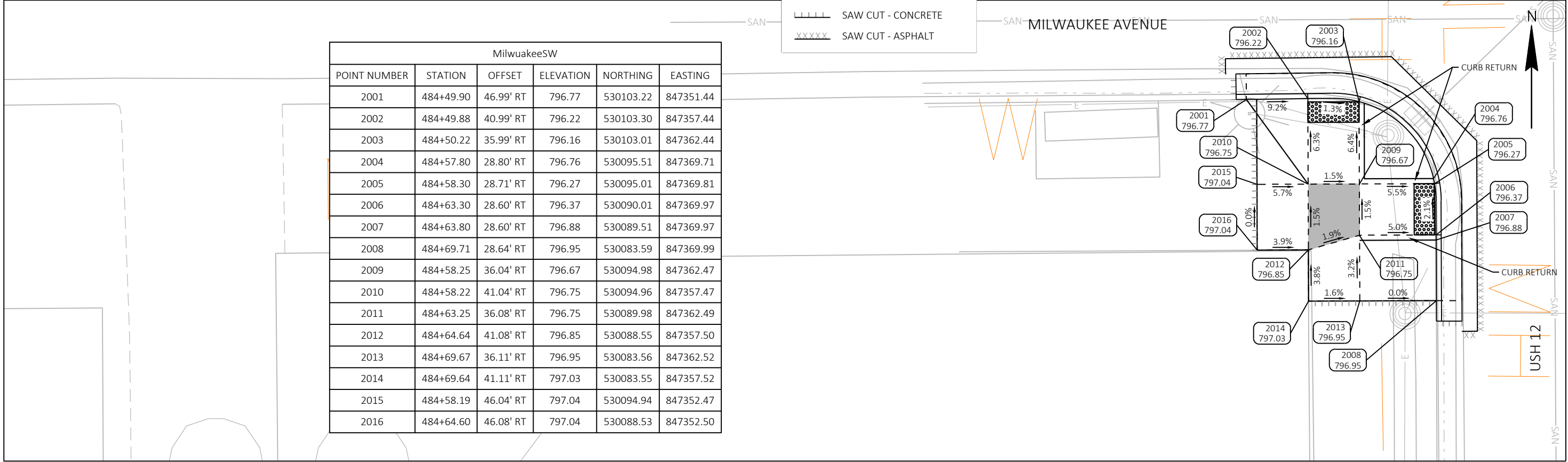
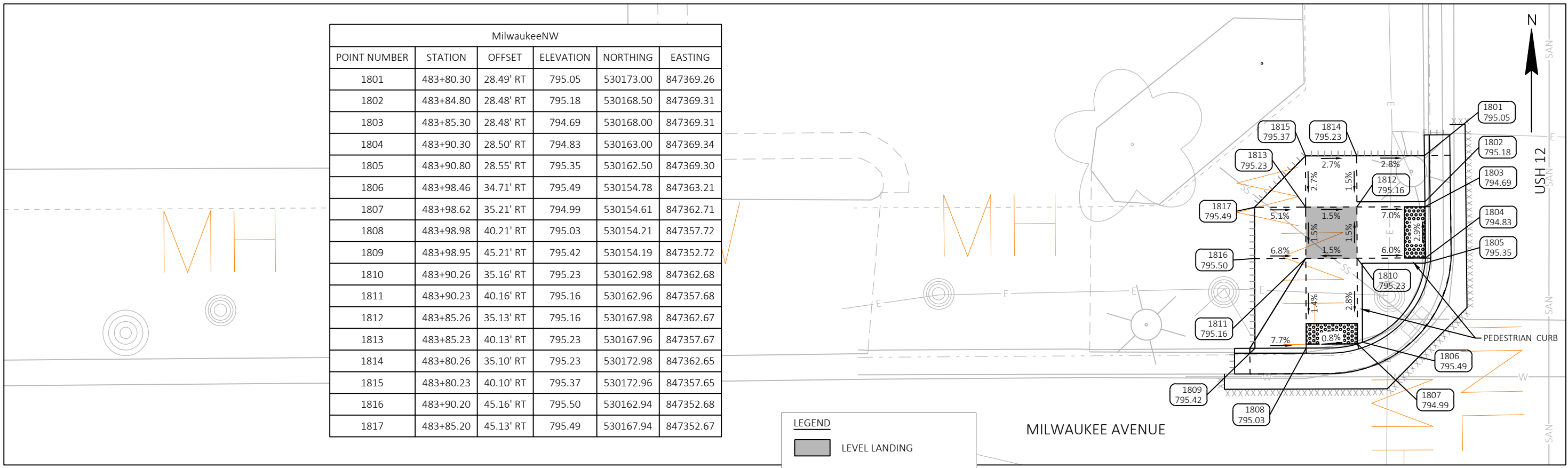
SWaterSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1720	482+41.83	49.78' LT	791.91	530312.24	847446.15
1721	482+52.05	34.72' LT	791.66	530301.87	847431.19
1722	482+57.05	34.69' LT	791.73	530296.87	847431.21
1723	482+52.09	39.72' LT	791.73	530301.89	847436.19
1724	482+57.09	39.69' LT	791.81	530296.89	847436.21
1725	482+62.05	34.66' LT	792.07	530291.87	847431.23
1726	482+62.09	39.66' LT	792.14	530291.89	847436.23
1727	482+67.05	34.63' LT	792.41	530286.87	847431.25
1728	482+67.09	39.63' LT	792.48	530286.89	847436.25
1729	482+47.05	34.75' LT	791.58	530306.87	847431.17
1730	482+47.09	39.75' LT	791.66	530306.89	847436.17
1731	482+47.12	44.75' LT	791.80	530306.90	847441.17
1732	482+52.12	44.72' LT	792.01	530301.90	847441.19
1733	482+47.16	49.75' LT	791.95	530306.91	847446.17
1734	482+52.16	49.72' LT	792.28	530301.91	847446.19
1735	482+52.62	44.72' LT	792.23	530301.40	847441.19
1736	482+52.59	40.22' LT	792.14	530301.39	847436.69
1737	482+57.09	40.19' LT	792.25	530296.89	847436.71
1738	482+62.09	40.16' LT	792.37	530291.89	847436.73

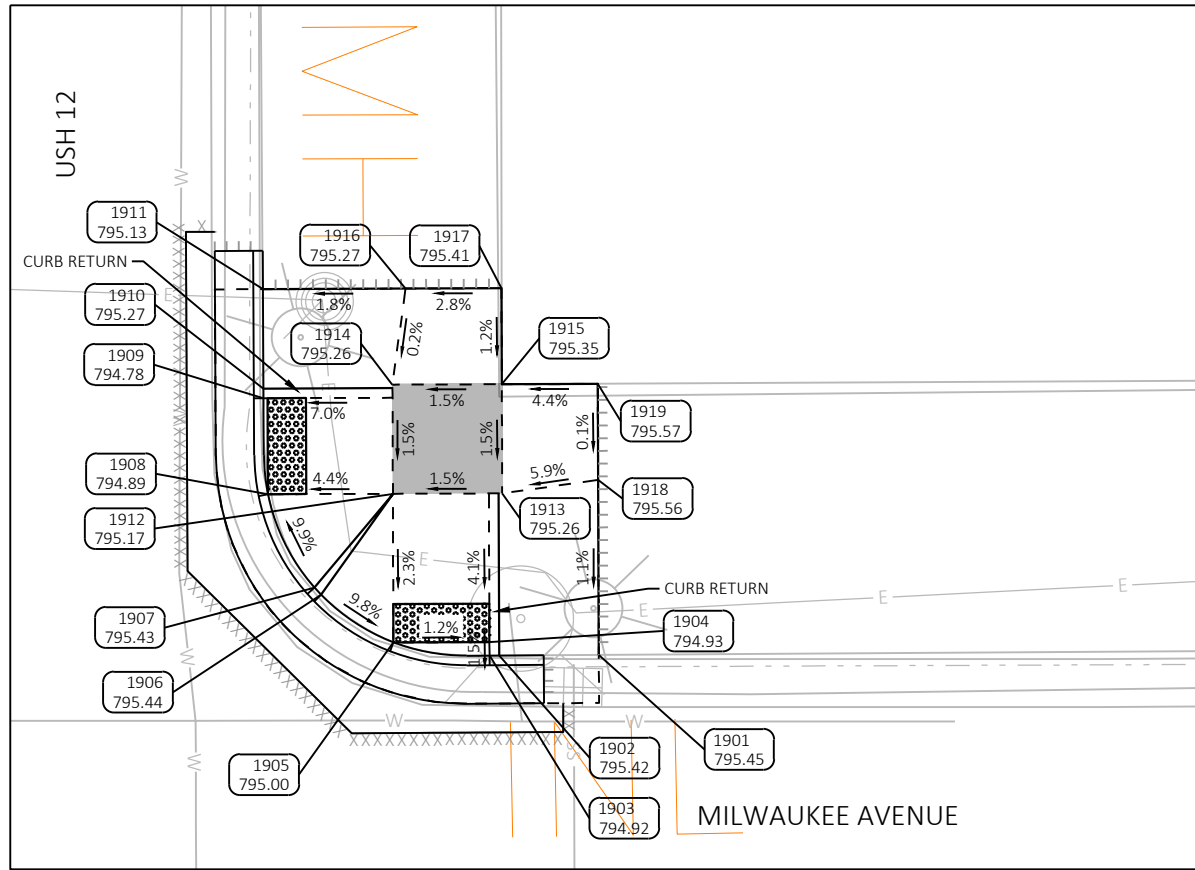
MilwaukeeNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1801	483+80.30	28.49' RT	795.05	530173.00	847369.26
1802	483+84.80	28.48' RT	795.18	530168.50	847369.31
1803	483+85.30	28.48' RT	794.69	530168.00	847369.31
1804	483+90.30	28.50' RT	794.83	530163.00	847369.34
1805	483+90.80	28.55' RT	795.35	530162.50	847369.30
1806	483+98.46	34.71' RT	795.49	530154.78	847363.21
1807	483+98.62	35.21' RT	794.99	530154.61	847362.71
1808	483+98.98	40.21' RT	795.03	530154.21	847357.72
1809	483+98.95	45.21' RT	795.42	530154.19	847352.72
1810	483+90.26	35.16' RT	795.23	530162.98	847362.68
1811	483+90.23	40.16' RT	795.16	530162.96	847357.68
1812	483+85.26	35.13' RT	795.16	530167.98	847362.67
1813	483+85.23	40.13' RT	795.23	530167.96	847357.67
1814	483+80.26	35.10' RT	795.23	530172.98	847362.65
1815	483+80.23	40.10' RT	795.37	530172.96	847357.65
1816	483+90.20	45.16' RT	795.50	530162.94	847352.68
1817	483+85.20	45.13' RT	795.49	530167.94	847352.67

MilwaukeeSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2001	484+49.90	46.99' RT	796.77	530103.22	847351.44
2002	484+49.88	40.99' RT	796.22	530103.30	847357.44
2003	484+50.22	35.99' RT	796.16	530103.01	847362.44
2004	484+57.80	28.80' RT	796.76	530095.51	847369.71
2005	484+58.30	28.71' RT	796.27	530095.01	847369.81
2006	484+63.30	28.60' RT	796.37	530090.01	847369.97
2007	484+63.80	28.60' RT	796.88	530089.51	847369.97
2008	484+69.71	28.64' RT	796.95	530083.59	847369.99
2009	484+58.25	36.04' RT	796.67	530094.98	847362.47
2010	484+58.22	41.04' RT	796.75	530094.96	847357.47
2011	484+63.25	36.08' RT	796.75	530089.98	847362.49
2012	484+64.64	41.08' RT	796.85	530088.55	847357.50
2013	484+69.67	36.11' RT	796.95	530083.56	847362.52
2014	484+69.64	41.11' RT	797.03	530083.55	847357.52
2015	484+58.19	46.04' RT	797.04	530094.94	847352.47
2016	484+64.60	46.08' RT	797.04	530088.53	847352.50

LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT



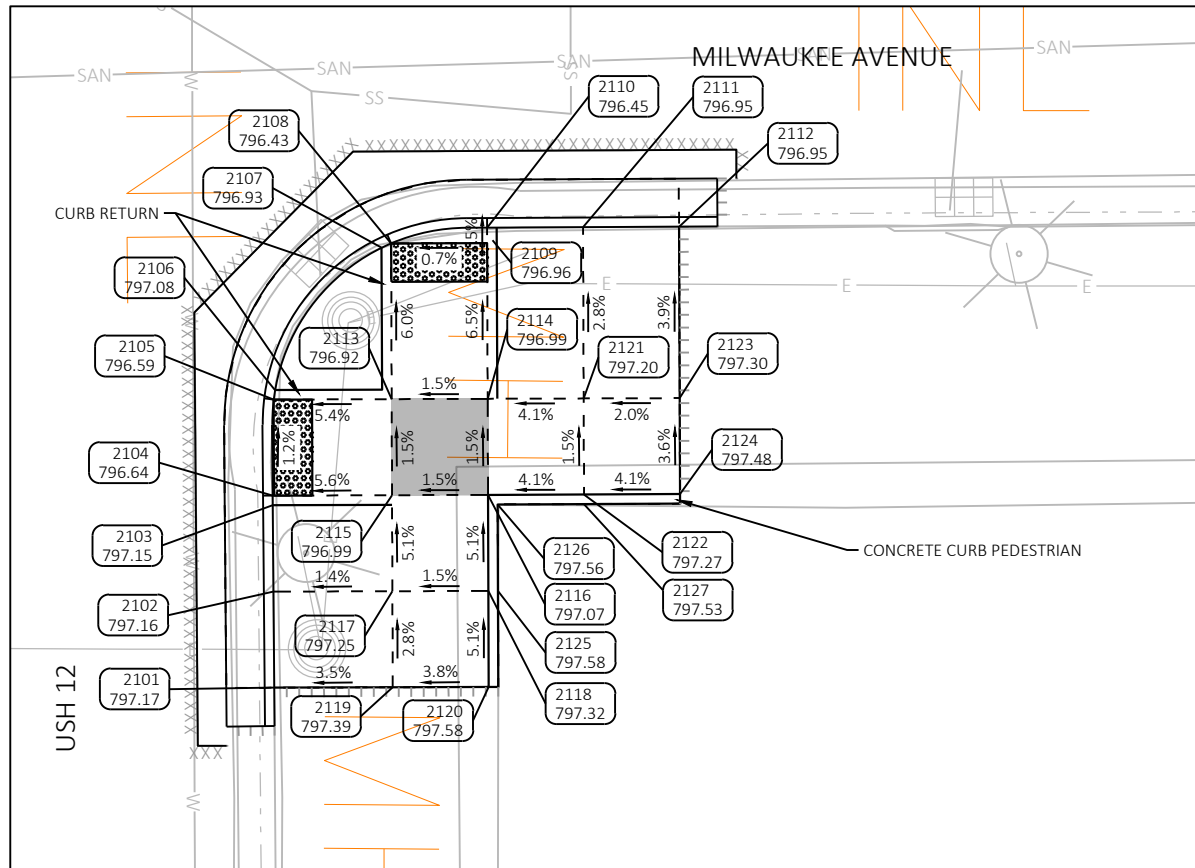


MilwaukeeNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1901	483+99.18	43.72' LT	795.45	530154.84	847441.65
1902	483+99.16	38.52' LT	795.42	530154.81	847436.45
1903	483+99.16	38.02' LT	794.92	530154.80	847435.95
1904	483+98.46	38.03' LT	794.93	530155.50	847435.95
1905	483+98.43	33.03' LT	795.00	530155.49	847430.95
1906	483+95.91	29.28' LT	795.44	530157.96	847427.18
1907	483+95.54	28.94' LT	795.43	530158.33	847426.83
1908	483+90.65	26.56' LT	794.89	530163.20	847424.41
1909	483+85.64	26.39' LT	794.78	530168.20	847424.18
1910	483+85.14	26.39' LT	795.27	530168.70	847424.18

MilwaukeeNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1911	483+79.96	26.41' LT	795.13	530173.88	847424.15
1912	483+90.69	33.08' LT	795.17	530163.23	847430.92
1913	483+90.72	38.77' LT	795.26	530163.25	847436.62
1914	483+85.01	33.11' LT	795.26	530168.91	847430.90
1915	483+85.04	38.81' LT	795.35	530168.93	847436.60
1916	483+80.01	33.84' LT	795.27	530173.91	847431.58
1917	483+80.04	38.84' LT	795.41	530173.93	847436.58
1918	483+90.07	43.78' LT	795.56	530163.95	847441.61
1919	483+85.07	43.81' LT	795.57	530168.95	847441.59

LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT



MilwaukeeSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2101	484+73.64	26.39' LT	797.17	530080.21	847425.05
2102	484+68.64	26.40' LT	797.16	530085.21	847425.01
2103	484+64.14	26.41' LT	797.15	530089.71	847424.98
2104	484+63.64	26.41' LT	796.64	530090.21	847424.97
2105	484+58.64	26.50' LT	796.59	530095.21	847425.02
2106	484+58.14	26.58' LT	797.08	530095.71	847425.09
2107	484+50.79	32.20' LT	796.93	530103.12	847430.64
2108	484+50.57	32.70' LT	796.43	530103.34	847431.14
2109	484+50.46	37.98' LT	796.96	530103.50	847436.42
2110	484+49.80	37.71' LT	796.45	530104.16	847436.14
2111	484+49.83	42.71' LT	796.95	530104.18	847441.14
2112	484+49.85	47.71' LT	796.95	530104.21	847446.14
2113	484+58.68	32.65' LT	796.92	530095.23	847431.17
2114	484+58.71	37.65' LT	796.99	530095.25	847436.17

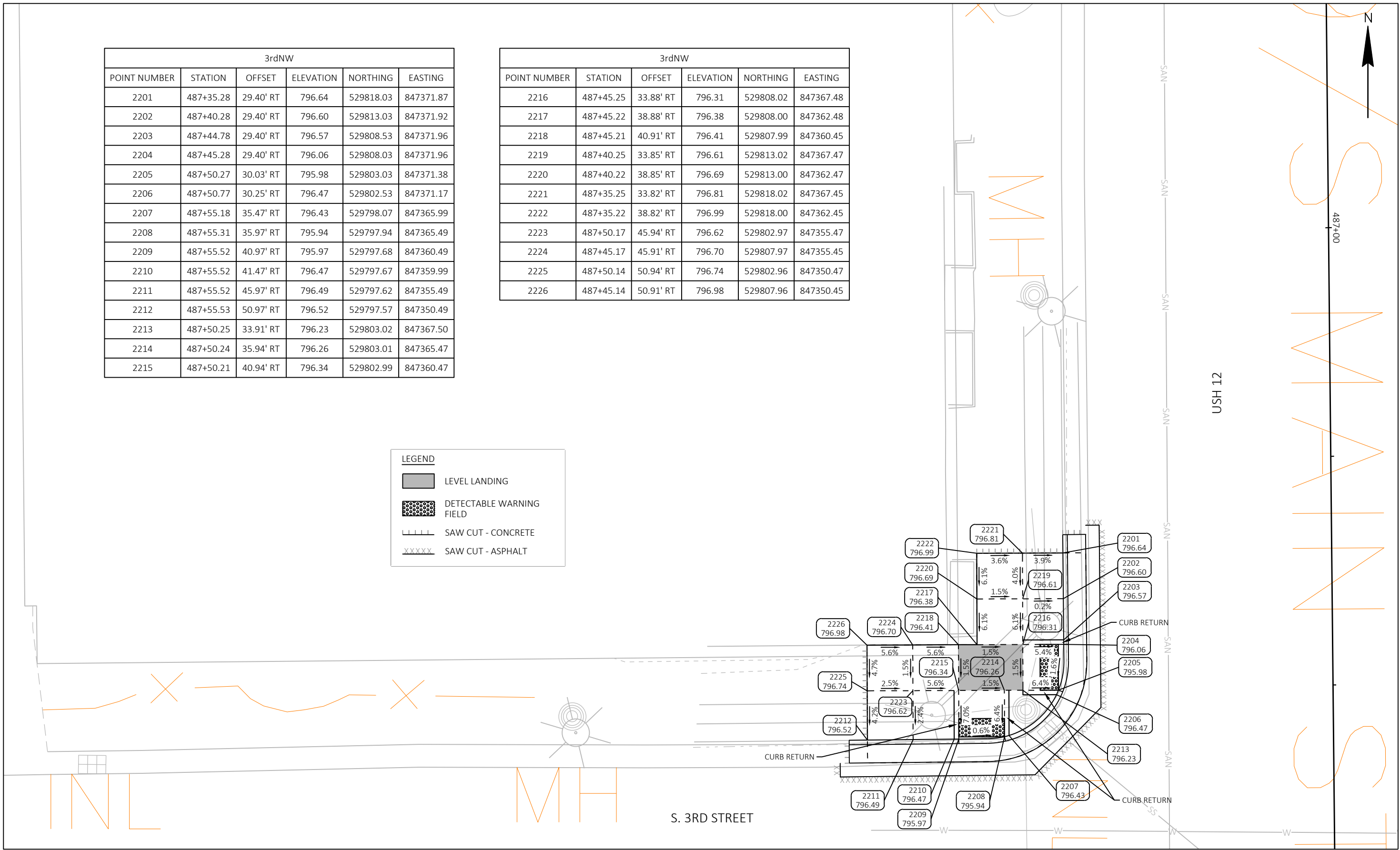
MilwaukeeSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2115	484+63.68	32.62' LT	796.99	530090.23	847431.19
2116	484+63.71	37.62' LT	797.07	530090.25	847436.19
2117	484+68.68	32.59' LT	797.25	530085.23	847431.21
2118	484+68.71	37.59' LT	797.32	530085.25	847436.21
2119	484+73.68	32.56' LT	797.39	530080.23	847431.23
2120	484+73.71	37.56' LT	797.58	530080.25	847436.23
2121	484+58.74	42.65' LT	797.20	530095.27	847441.17
2122	484+63.74	42.62' LT	797.27	530090.27	847441.19
2123	484+58.77	47.65' LT	797.30	530095.29	847446.17
2124	484+63.77	47.62' LT	797.48	530090.29	847446.19
2125	484+68.71	38.09' LT	797.58	530085.25	847436.71
2126	484+64.21	38.12' LT	797.56	530089.75	847436.69
2127	484+64.24	42.62' LT	797.53	530089.77	847441.19

3rdNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2201	487+35.28	29.40' RT	796.64	529818.03	847371.87
2202	487+40.28	29.40' RT	796.60	529813.03	847371.92
2203	487+44.78	29.40' RT	796.57	529808.53	847371.96
2204	487+45.28	29.40' RT	796.06	529808.03	847371.96
2205	487+50.27	30.03' RT	795.98	529803.03	847371.38
2206	487+50.77	30.25' RT	796.47	529802.53	847371.17
2207	487+55.18	35.47' RT	796.43	529798.07	847365.99
2208	487+55.31	35.97' RT	795.94	529797.94	847365.49
2209	487+55.52	40.97' RT	795.97	529797.68	847360.49
2210	487+55.52	41.47' RT	796.47	529797.67	847359.99
2211	487+55.52	45.97' RT	796.49	529797.62	847355.49
2212	487+55.53	50.97' RT	796.52	529797.57	847350.49
2213	487+50.25	33.91' RT	796.23	529803.02	847367.50
2214	487+50.24	35.94' RT	796.26	529803.01	847365.47
2215	487+50.21	40.94' RT	796.34	529802.99	847360.47

3rdNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2216	487+45.25	33.88' RT	796.31	529808.02	847367.48
2217	487+45.22	38.88' RT	796.38	529808.00	847362.48
2218	487+45.21	40.91' RT	796.41	529807.99	847360.45
2219	487+40.25	33.85' RT	796.61	529813.02	847367.47
2220	487+40.22	38.85' RT	796.69	529813.00	847362.47
2221	487+35.25	33.82' RT	796.81	529818.02	847367.45
2222	487+35.22	38.82' RT	796.99	529818.00	847362.45
2223	487+50.17	45.94' RT	796.62	529802.97	847355.47
2224	487+45.17	45.91' RT	796.70	529807.97	847355.45
2225	487+50.14	50.94' RT	796.74	529802.96	847350.47
2226	487+45.14	50.91' RT	796.98	529807.96	847350.45

LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT







S. 3RD STREET

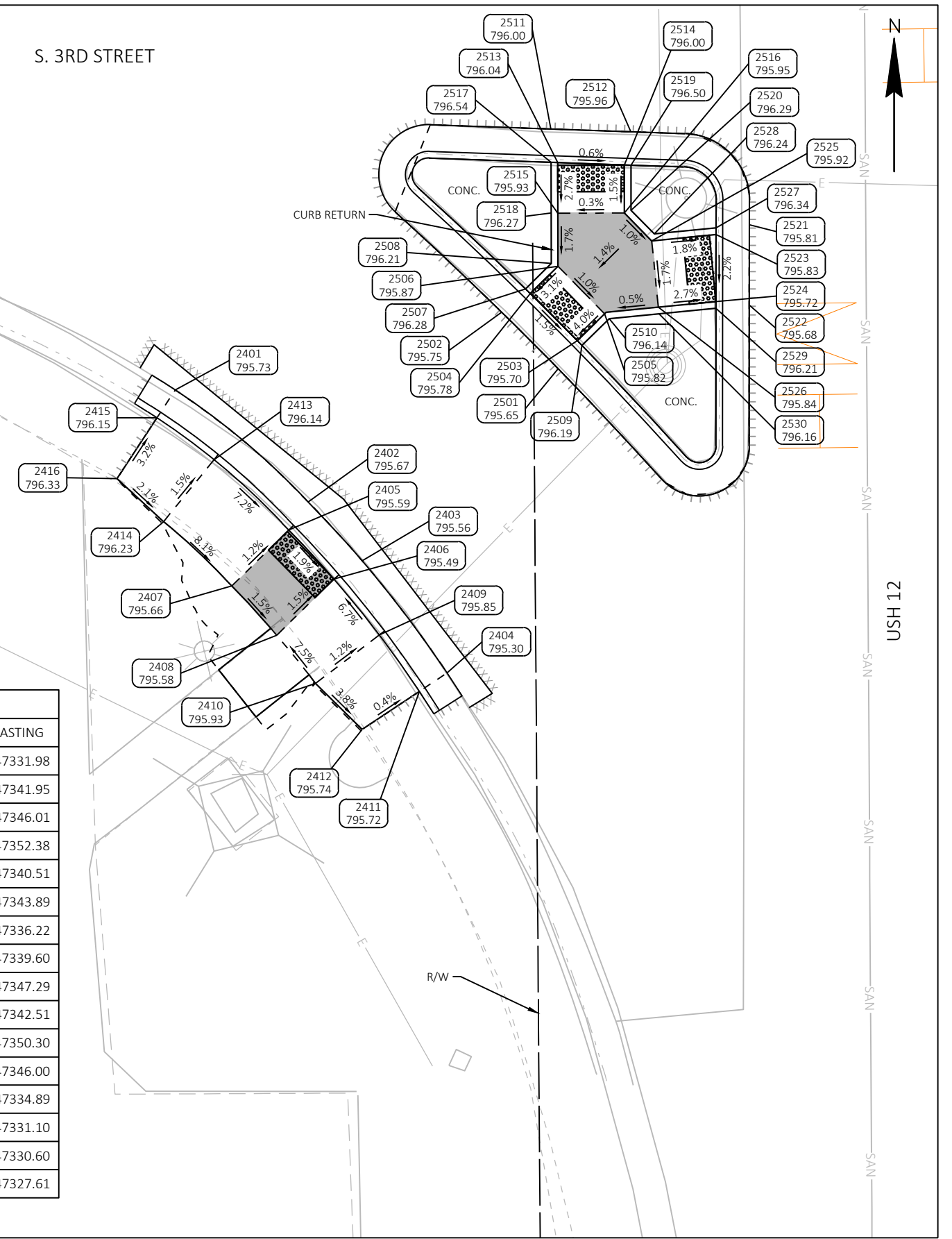


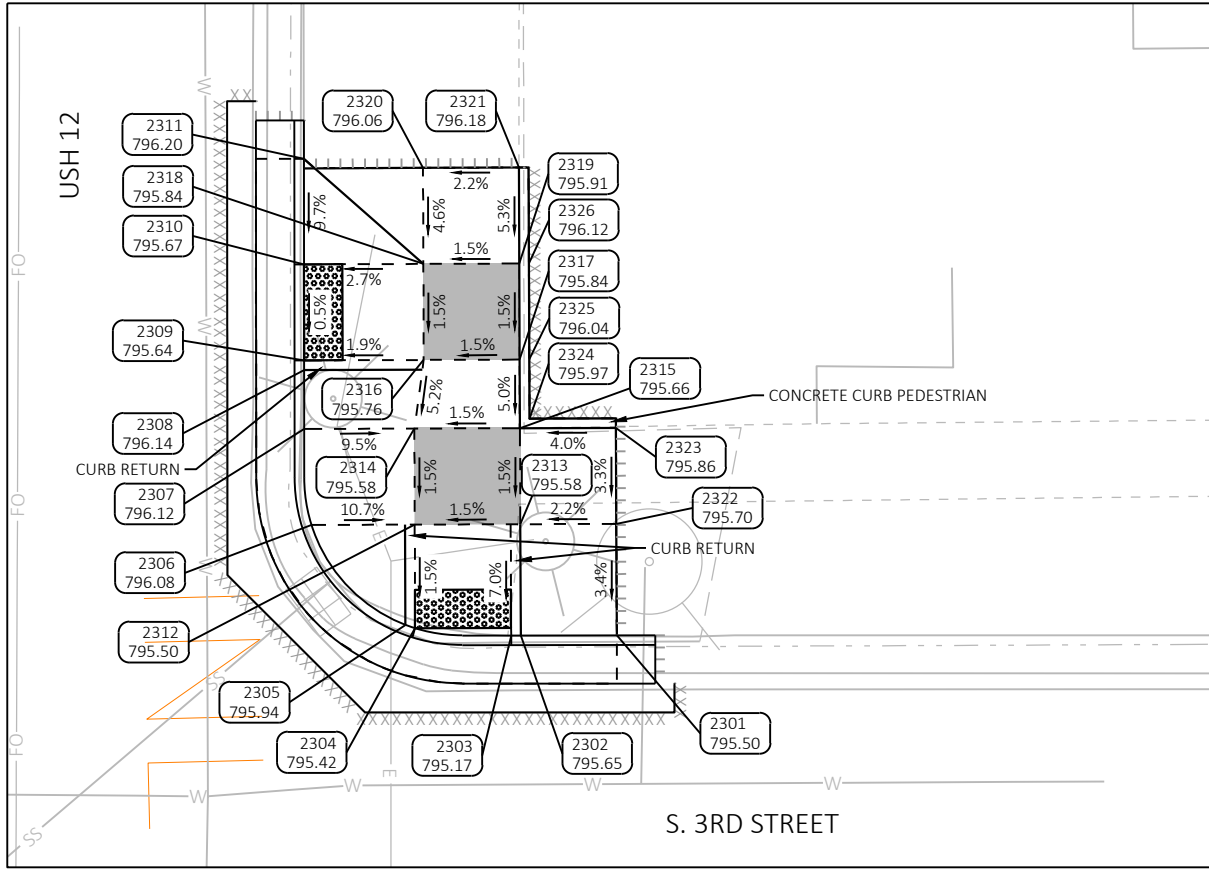
3rdIsland					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2501	488+21.93	41.40' RT	795.65	529731.26	847360.72
2502	488+17.65	45.60' RT	795.75	529735.51	847356.48
2503	488+19.84	39.95' RT	795.70	529733.37	847362.15
2504	488+16.27	43.45' RT	795.78	529736.91	847358.62
2505	488+17.85	37.88' RT	795.82	529735.38	847364.21
2506	488+14.25	41.34' RT	795.87	529738.95	847360.71
2507	488+15.92	43.80' RT	796.28	529737.26	847358.26
2508	488+14.03	41.84' RT	796.21	529739.16	847360.21
2509	488+20.20	39.60' RT	796.19	529733.01	847362.50
2510	488+18.22	37.54' RT	796.14	529735.02	847364.55
2511	488+03.94	41.77' RT	796.00	529749.25	847360.17
2512	488+04.21	35.78' RT	795.96	529749.04	847366.17
2513	488+06.47	41.29' RT	796.04	529746.73	847360.68
2514	488+06.69	36.29' RT	796.00	529746.56	847365.68
2515	488+10.27	41.31' RT	795.93	529742.93	847360.69
2516	488+10.30	36.31' RT	795.95	529742.95	847365.69
2517	488+06.45	41.79' RT	796.54	529746.75	847360.18
2518	488+10.24	41.81' RT	796.27	529742.95	847360.19
2519	488+06.71	35.79' RT	796.50	529746.54	847366.18
2520	488+10.10	35.81' RT	796.29	529743.15	847366.19
2521	488+11.27	26.95' RT	795.81	529742.07	847375.06
2522	488+17.29	27.02' RT	795.68	529736.05	847375.06
2523	488+11.99	29.46' RT	795.83	529741.32	847372.56
2524	488+17.01	29.52' RT	795.72	529736.30	847372.56
2525	488+12.41	34.28' RT	795.92	529740.85	847367.74
2526	488+17.39	33.85' RT	795.84	529735.88	847368.23
2527	488+11.49	29.46' RT	796.34	529741.83	847372.56
2528	488+11.89	34.09' RT	796.24	529741.37	847367.93
2529	488+17.52	29.52' RT	796.21	529735.80	847372.56
2530	488+17.89	33.80' RT	796.16	529735.38	847368.28

LEGEND

-  LEVEL LANDING
-  DETECTABLE WARNING FIELD
-  SAW CUT - CONCRETE
-  SAW CUT - ASPHALT

3rdSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2401	488+23.24	70.16' RT	795.73	529729.67	847331.98
2402	488+31.73	60.27' RT	795.67	529721.28	847341.95
2403	488+36.19	56.26' RT	795.56	529716.86	847346.01
2404	488+44.67	49.97' RT	795.30	529708.44	847352.38
2405	488+33.83	61.73' RT	795.59	529719.17	847340.51
2406	488+37.55	58.39' RT	795.49	529715.48	847343.89
2407	488+37.98	66.06' RT	795.66	529714.97	847336.22
2408	488+41.71	62.72' RT	795.58	529711.28	847339.60
2409	488+41.78	55.03' RT	795.85	529711.29	847347.29
2410	488+45.35	59.85' RT	795.93	529707.66	847342.51
2411	488+46.03	52.07' RT	795.72	529707.06	847350.30
2412	488+48.85	56.39' RT	795.74	529704.20	847346.00
2413	488+28.50	67.30' RT	796.14	529724.44	847334.89
2414	488+33.11	71.14' RT	796.23	529719.79	847331.10
2415	488+25.32	71.55' RT	796.15	529727.58	847330.60
2416	488+29.84	74.60' RT	796.33	529723.03	847327.61



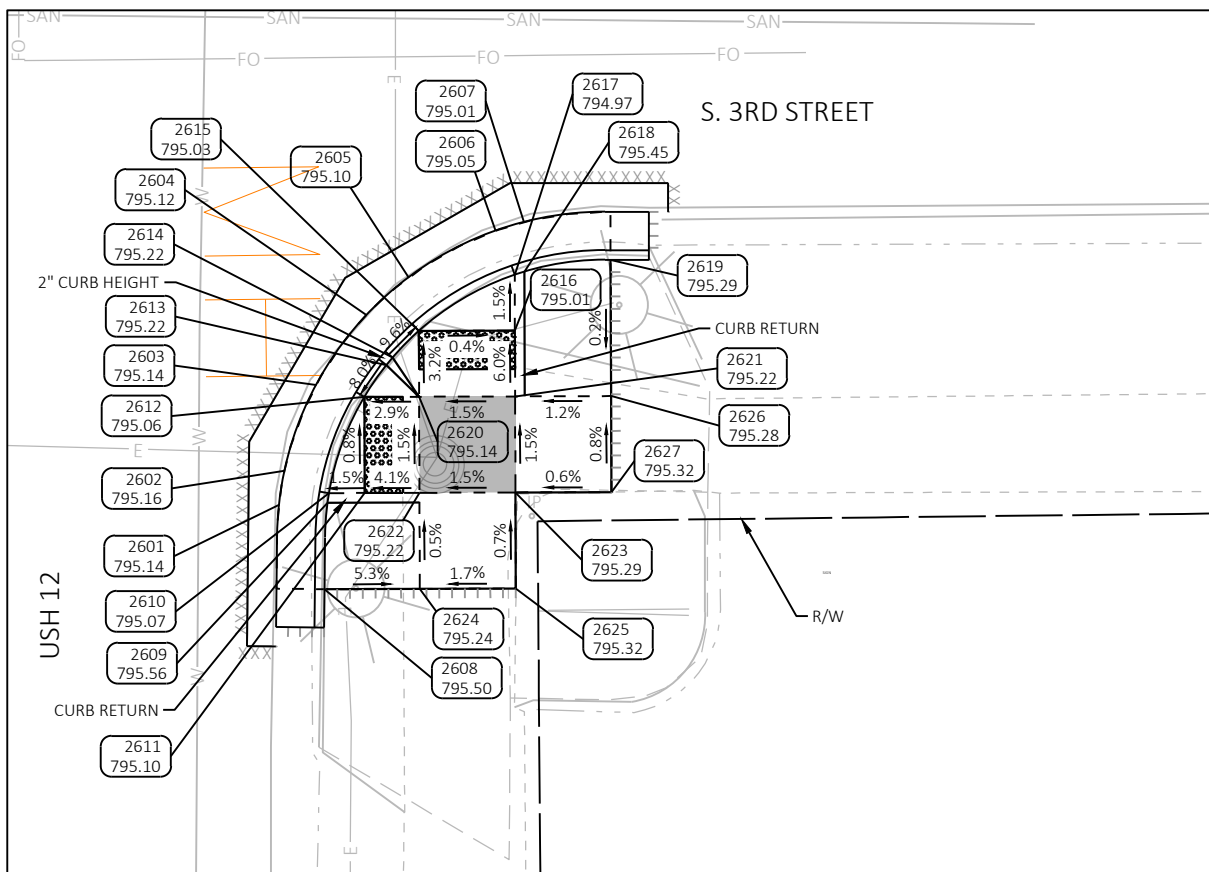


3rdNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2301	487+58.36	41.67' LT	795.50	529795.66	847443.16
2302	487+58.31	36.68' LT	795.65	529795.66	847438.16
2303	487+58.31	36.18' LT	795.17	529795.66	847437.66
2304	487+57.88	31.18' LT	795.42	529796.03	847432.66
2305	487+57.71	30.68' LT	795.94	529796.20	847432.16
2306	487+52.45	25.84' LT	796.08	529801.41	847427.27
2307	487+47.45	25.50' LT	796.12	529806.41	847426.88
2308	487+44.38	25.53' LT	796.14	529809.48	847426.88
2309	487+43.88	25.55' LT	795.64	529809.98	847426.89
2310	487+38.88	25.58' LT	795.67	529814.98	847426.87
2311	487+33.38	25.63' LT	796.20	529820.48	847426.87
2312	487+52.48	31.21' LT	795.50	529801.43	847432.64
2313	487+52.52	36.71' LT	795.58	529801.45	847438.14

3rdNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2314	487+47.48	31.24' LT	795.58	529806.43	847432.62
2315	487+47.52	36.74' LT	795.66	529806.45	847438.12
2316	487+43.92	31.76' LT	795.76	529810.00	847433.11
2317	487+43.95	36.76' LT	795.84	529810.02	847438.11
2318	487+38.92	31.80' LT	795.84	529815.00	847433.09
2319	487+38.95	36.80' LT	795.91	529815.02	847438.09
2320	487+33.92	31.83' LT	796.06	529820.00	847433.07
2321	487+33.95	36.83' LT	796.18	529820.02	847438.07
2322	487+52.55	41.71' LT	795.70	529801.47	847443.14
2323	487+47.55	41.74' LT	795.86	529806.47	847443.12
2324	487+47.02	37.25' LT	795.97	529806.95	847438.62
2325	487+43.95	37.26' LT	796.04	529810.02	847438.61
2326	487+38.95	37.30' LT	796.12	529815.02	847438.59

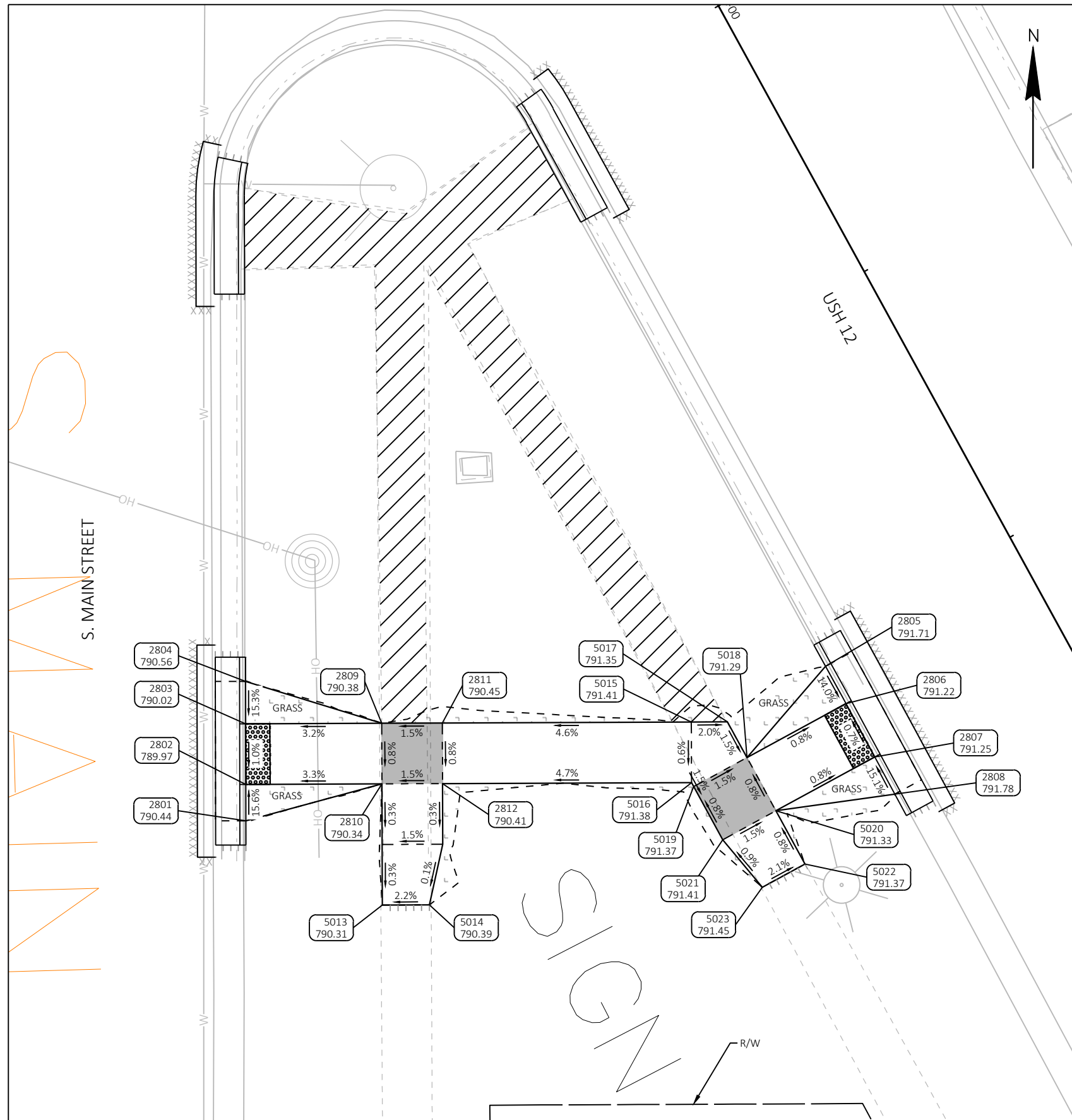
LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT



3rdSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2601	488+12.53	23.34' LT	795.14	529741.30	847425.36
2602	488+10.77	23.62' LT	795.16	529743.07	847425.62
2603	488+06.32	25.33' LT	795.14	529747.54	847427.30
2604	488+02.70	27.97' LT	795.12	529751.18	847429.89
2605	488+00.78	30.20' LT	795.10	529753.12	847432.11
2606	487+98.37	34.75' LT	795.05	529755.57	847436.63
2607	487+97.96	36.23' LT	795.01	529756.01	847438.11
2608	488+16.95	25.64' LT	795.50	529736.91	847427.71
2609	488+12.45	25.84' LT	795.56	529741.41	847427.87
2610	488+11.95	25.93' LT	795.07	529741.91	847427.95
2611	488+11.96	27.79' LT	795.10	529741.92	847429.81
2612	488+06.96	27.82' LT	795.06	529746.92	847429.79
2613	488+05.32	28.98' LT	795.22	529748.57	847430.94
2614	488+04.93	29.31' LT	795.22	529748.97	847431.26

3rdSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2615	488+03.55	30.69' LT	795.03	529750.36	847432.63
2616	488+03.58	35.69' LT	795.01	529750.38	847437.63
2617	488+00.69	35.71' LT	794.97	529753.27	847437.62
2618	488+00.53	36.21' LT	795.45	529753.43	847438.12
2619	487+99.95	40.71' LT	795.29	529754.06	847442.61
2620	488+06.98	30.67' LT	795.14	529746.93	847432.64
2621	488+07.01	35.67' LT	795.22	529746.95	847437.64
2622	488+11.98	30.64' LT	795.22	529741.93	847432.66
2623	488+12.01	35.64' LT	795.29	529741.95	847437.66
2624	488+16.98	30.61' LT	795.24	529736.93	847432.68
2625	488+17.01	35.61' LT	795.32	529736.95	847437.68
2626	488+07.04	40.67' LT	795.28	529746.97	847442.64
2627	488+12.04	40.64' LT	795.32	529741.97	847442.66

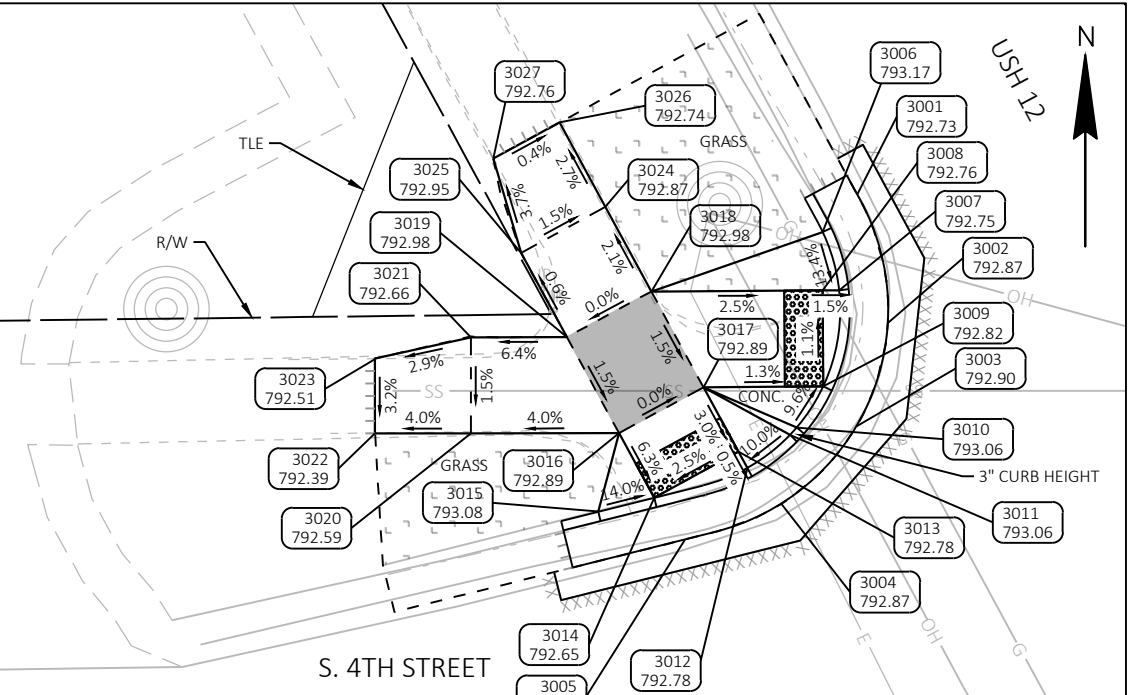


MainMid					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2801	492+40.19	66.51' RT	790.44	529298.82	847422.15
2802	492+37.56	65.08' RT	789.97	529301.82	847422.14
2803	492+33.17	62.68' RT	790.02	529306.82	847422.13
2804	492+30.09	61.01' RT	790.56	529310.32	847422.12
2805	492+52.01	18.47' RT	791.71	529311.56	847469.97
2806	492+55.52	18.46' RT	791.22	529308.48	847471.66
2807	492+60.52	18.50' RT	791.25	529304.08	847474.03
2808	492+64.01	18.52' RT	791.78	529301.02	847475.68
2809	492+38.52	52.82' RT	790.38	529306.87	847433.36
2810	492+42.92	55.21' RT	790.34	529301.87	847433.38
2811	492+40.91	48.43' RT	790.45	529306.89	847438.36
2812	492+45.30	50.82' RT	790.41	529301.89	847438.38
5013	492+51.70	59.98' RT	790.31	529291.87	847433.42
5014	492+53.55	56.58' RT	790.39	529291.88	847437.29
5015	492+50.72	30.38' RT	791.41	529306.97	847458.90
5016	492+55.11	32.77' RT	791.38	529301.97	847458.92
5017	492+52.14	27.76' RT	791.35	529306.98	847461.88
5018	492+55.50	27.77' RT	791.29	529304.03	847463.49
5019	492+55.48	32.77' RT	791.37	529301.64	847459.10
5020	492+60.50	27.78' RT	791.33	529299.64	847465.88
5021	492+60.48	32.78' RT	791.41	529297.25	847461.49
5022	492+65.50	27.80' RT	791.37	529295.25	847468.27
5023	492+65.49	31.80' RT	791.45	529293.33	847464.75

LEGEND	
	LEVEL LANDING
	DETECTABLE WARNING FIELD
	REMOVE CONCRETE SIDEWALK
	SAW CUT - CONCRETE
	SAW CUT - ASPHALT

4thNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3001	495+44.51	17.02' RT	792.73	529055.80	847611.90
3002	495+51.14	18.83' RT	792.87	529049.11	847613.49
3003	495+55.07	22.85' RT	792.90	529043.74	847611.86
3004	495+56.71	28.22' RT	792.87	529039.71	847607.94
3005	495+55.94	33.51' RT	792.76	529037.85	847602.93
3006	495+45.36	19.53' RT	793.17	529053.85	847610.10
3007	495+48.42	20.23' RT	792.75	529050.83	847610.96
3008	495+48.01	20.99' RT	792.76	529050.82	847610.10
3009	495+52.41	23.37' RT	792.82	529045.82	847610.13
3010	495+53.63	25.60' RT	793.06	529043.68	847608.75
3011	495+53.80	26.09' RT	793.06	529043.30	847608.40
3012	495+54.23	28.88' RT	792.78	529041.57	847606.17
3013	495+53.27	28.87' RT	792.78	529042.42	847605.71
3014	495+53.25	33.87' RT	792.65	529040.03	847601.32

4thNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3015	495+52.49	36.77' RT	793.08	529039.30	847598.41
3016	495+49.43	33.86' RT	792.89	529043.39	847599.49
3017	495+49.45	28.86' RT	792.89	529045.78	847603.88
3018	495+43.78	28.84' RT	792.98	529050.76	847601.17
3019	495+43.74	33.84' RT	792.98	529048.38	847596.77
3020	495+45.73	40.63' RT	792.59	529043.38	847591.78
3021	495+41.34	38.23' RT	792.66	529048.38	847591.77
3022	495+43.33	45.01' RT	792.39	529043.37	847586.78
3023	495+39.94	43.16' RT	792.51	529047.24	847586.78
3024	495+38.76	28.83' RT	792.87	529055.17	847598.78
3025	495+38.74	33.83' RT	792.95	529052.78	847594.38
3026	495+33.76	28.81' RT	792.74	529059.56	847596.39
3027	495+33.75	32.72' RT	792.76	529057.69	847592.95

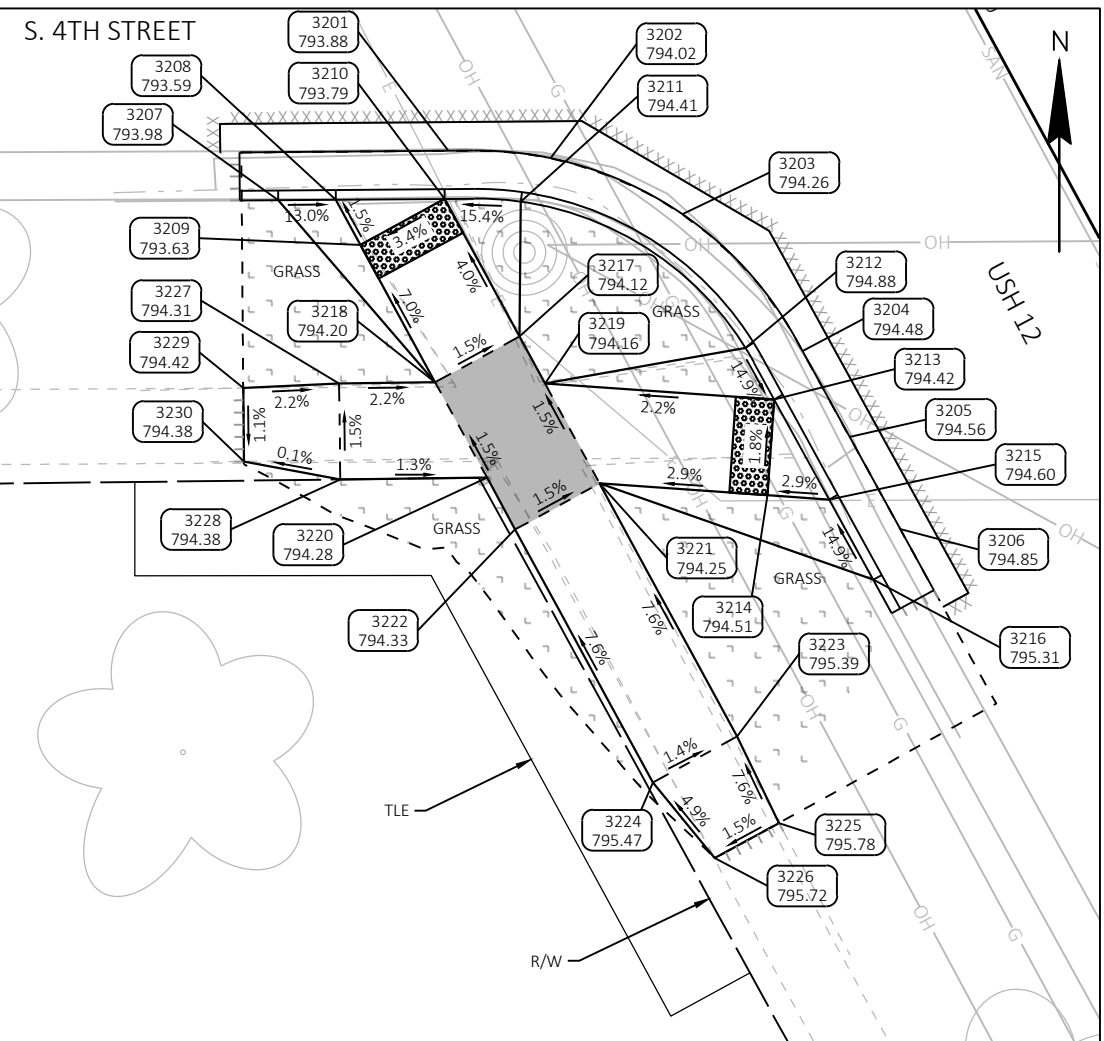


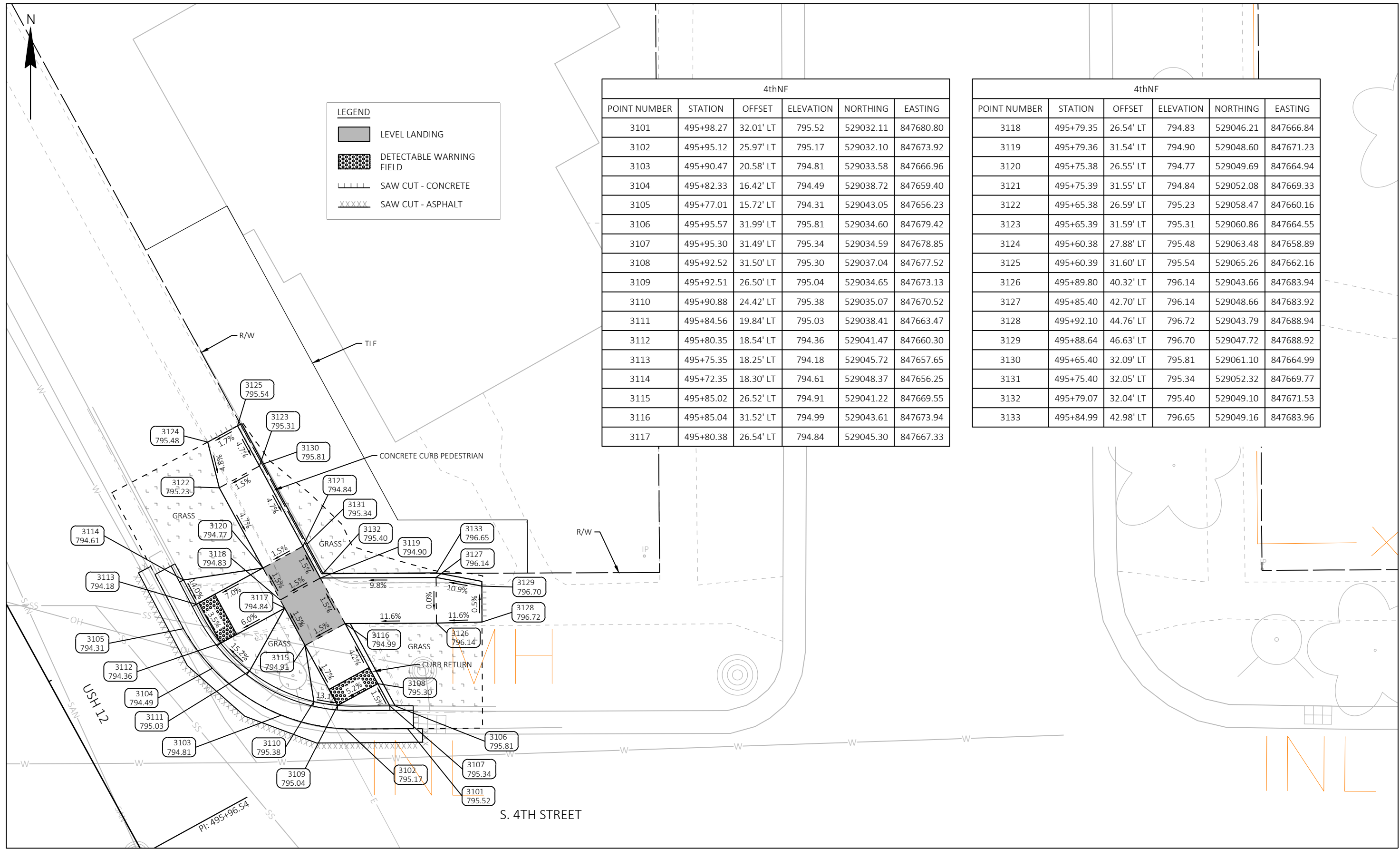
LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT

4thSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3201	495+94.35	27.63' RT	793.88	529007.00	847626.56
3202	495+97.36	23.16' RT	794.02	529006.61	847631.87
3203	496+03.19	18.52' RT	794.26	529003.70	847638.73
3204	496+12.44	16.41' RT	794.48	528996.57	847644.99
3205	496+17.54	16.38' RT	794.56	528992.09	847647.45
3206	496+23.02	16.35' RT	794.85	528987.30	847650.08
3207	495+92.34	36.65' RT	793.98	529004.43	847617.68
3208	495+93.76	34.01' RT	793.59	529004.45	847620.68
3209	495+96.43	34.01' RT	793.63	529002.10	847621.96
3210	495+96.45	29.01' RT	793.79	529004.49	847626.35
3211	495+98.61	25.60' RT	794.41	529004.34	847630.32
3212	496+10.87	18.94' RT	794.88	528996.74	847642.01
3213	496+13.95	18.90' RT	794.42	528994.05	847643.52
3214	496+18.17	21.58' RT	794.51	528989.06	847643.17
3215	496+19.90	18.87' RT	794.60	528988.84	847646.38

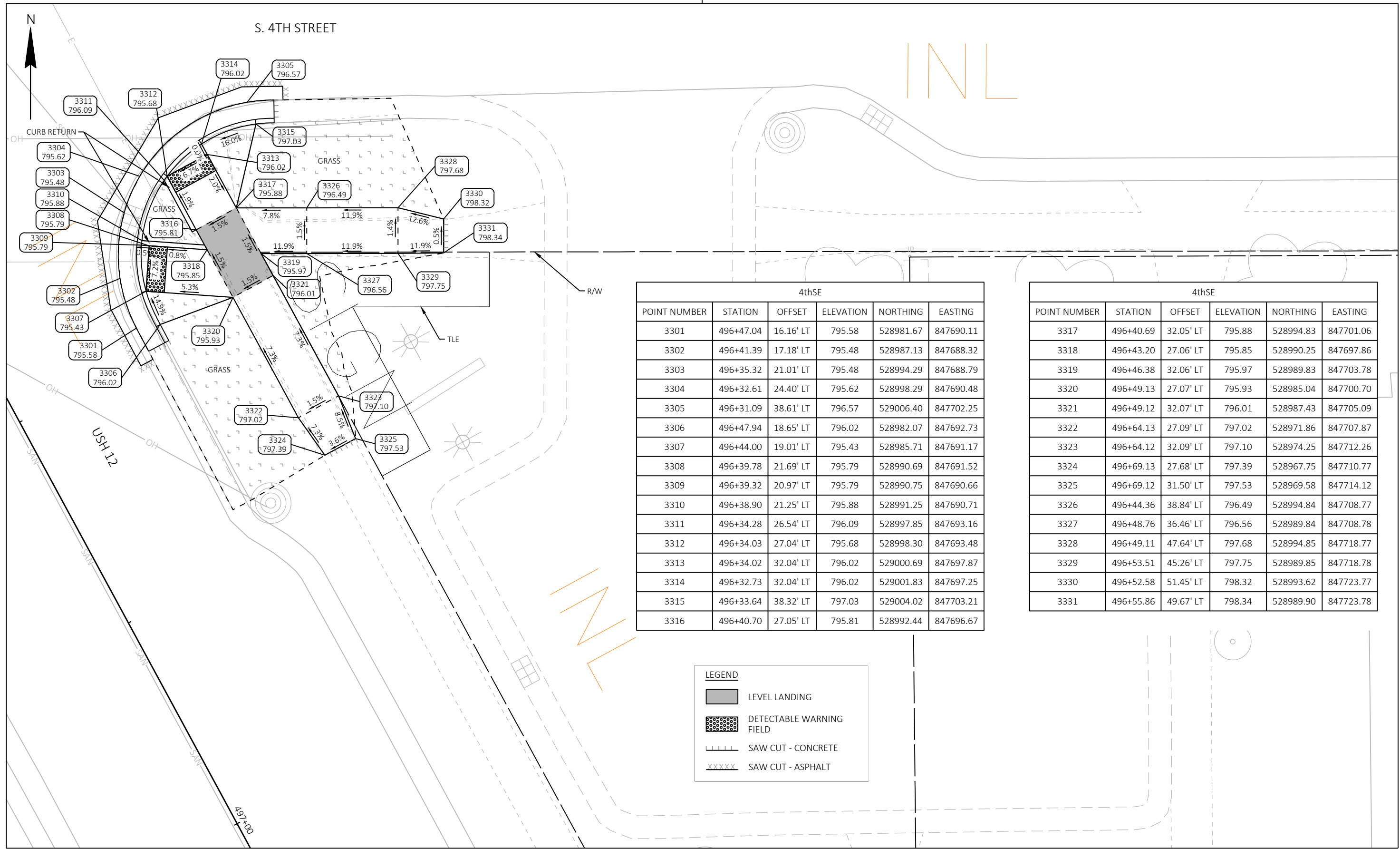
4thSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3216	496+24.66	18.84' RT	795.31	528984.67	847648.68
3217	496+04.74	29.00' RT	794.12	528997.33	847630.25
3218	496+04.75	34.00' RT	794.20	528994.94	847625.86
3219	496+07.53	29.00' RT	794.16	528994.89	847631.58
3220	496+10.40	33.99' RT	794.28	528989.98	847628.56
3221	496+13.46	28.99' RT	794.25	528989.68	847634.42
3222	496+13.47	33.99' RT	794.33	528987.29	847630.02
3223	496+28.46	28.97' RT	795.39	528976.50	847641.59
3224	496+28.47	33.97' RT	795.47	528974.11	847637.19
3225	496+33.46	29.20' RT	795.78	528971.99	847643.76
3226	496+33.46	33.01' RT	795.72	528970.17	847640.42
3227	496+02.42	38.43' RT	794.31	528994.88	847620.86
3228	496+06.84	40.75' RT	794.38	528989.88	847620.92
3229	496+00.24	42.93' RT	794.42	528994.65	847615.86
3230	496+03.63	44.71' RT	794.38	528990.82	847615.91





4thNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3101	495+98.27	32.01' LT	795.52	529032.11	847680.80
3102	495+95.12	25.97' LT	795.17	529032.10	847673.92
3103	495+90.47	20.58' LT	794.81	529033.58	847666.96
3104	495+82.33	16.42' LT	794.49	529038.72	847659.40
3105	495+77.01	15.72' LT	794.31	529043.05	847656.23
3106	495+95.57	31.99' LT	795.81	529034.60	847679.42
3107	495+95.30	31.49' LT	795.34	529034.59	847678.85
3108	495+92.52	31.50' LT	795.30	529037.04	847677.52
3109	495+92.51	26.50' LT	795.04	529034.65	847673.13
3110	495+90.88	24.42' LT	795.38	529035.07	847670.52
3111	495+84.56	19.84' LT	795.03	529038.41	847663.47
3112	495+80.35	18.54' LT	794.36	529041.47	847660.30
3113	495+75.35	18.25' LT	794.18	529045.72	847657.65
3114	495+72.35	18.30' LT	794.61	529048.37	847656.25
3115	495+85.02	26.52' LT	794.91	529041.22	847669.55
3116	495+85.04	31.52' LT	794.99	529043.61	847673.94
3117	495+80.38	26.54' LT	794.84	529045.30	847667.33

4thNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3118	495+79.35	26.54' LT	794.83	529046.21	847666.84
3119	495+79.36	31.54' LT	794.90	529048.60	847671.23
3120	495+75.38	26.55' LT	794.77	529049.69	847664.94
3121	495+75.39	31.55' LT	794.84	529052.08	847669.33
3122	495+65.38	26.59' LT	795.23	529058.47	847660.16
3123	495+65.39	31.59' LT	795.31	529060.86	847664.55
3124	495+60.38	27.88' LT	795.48	529063.48	847658.89
3125	495+60.39	31.60' LT	795.54	529065.26	847662.16
3126	495+89.80	40.32' LT	796.14	529043.66	847683.94
3127	495+85.40	42.70' LT	796.14	529048.66	847683.92
3128	495+92.10	44.76' LT	796.72	529043.79	847688.94
3129	495+88.64	46.63' LT	796.70	529047.72	847688.92
3130	495+65.40	32.09' LT	795.81	529061.10	847664.99
3131	495+75.40	32.05' LT	795.34	529052.32	847669.77
3132	495+79.07	32.04' LT	795.40	529049.10	847671.53
3133	495+84.99	42.98' LT	796.65	529049.16	847683.96



4thSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3301	496+47.04	16.16' LT	795.58	528981.67	847690.11
3302	496+41.39	17.18' LT	795.48	528987.13	847688.32
3303	496+35.32	21.01' LT	795.48	528994.29	847688.79
3304	496+32.61	24.40' LT	795.62	528998.29	847690.48
3305	496+31.09	38.61' LT	796.57	529006.40	847702.25
3306	496+47.94	18.65' LT	796.02	528982.07	847692.73
3307	496+44.00	19.01' LT	795.43	528985.71	847691.17
3308	496+39.78	21.69' LT	795.79	528990.69	847691.52
3309	496+39.32	20.97' LT	795.79	528990.75	847690.66
3310	496+38.90	21.25' LT	795.88	528991.25	847690.71
3311	496+34.28	26.54' LT	796.09	528997.85	847693.16
3312	496+34.03	27.04' LT	795.68	528998.30	847693.48
3313	496+34.02	32.04' LT	796.02	529000.69	847697.87
3314	496+32.73	32.04' LT	796.02	529001.83	847697.25
3315	496+33.64	38.32' LT	797.03	529004.02	847703.21
3316	496+40.70	27.05' LT	795.81	528992.44	847696.67





4thSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3317	496+40.69	32.05' LT	795.88	528994.83	847701.06
3318	496+43.20	27.06' LT	795.85	528990.25	847697.86
3319	496+46.38	32.06' LT	795.97	528989.83	847703.78
3320	496+49.13	27.07' LT	795.93	528985.04	847700.70
3321	496+49.12	32.07' LT	796.01	528987.43	847705.09
3322	496+64.13	27.09' LT	797.02	528971.86	847707.87
3323	496+64.12	32.09' LT	797.10	528974.25	847712.26
3324	496+69.13	27.68' LT	797.39	528967.75	847710.77
3325	496+69.12	31.50' LT	797.53	528969.58	847714.12
3326	496+44.36	38.84' LT	796.49	528994.84	847708.77
3327	496+48.76	36.46' LT	796.56	528989.84	847708.78
3328	496+49.11	47.64' LT	797.68	528994.85	847718.77
3329	496+53.51	45.26' LT	797.75	528989.85	847718.78
3330	496+52.58	51.45' LT	798.32	528993.62	847723.77
3331	496+55.86	49.67' LT	798.34	528989.90	847723.78

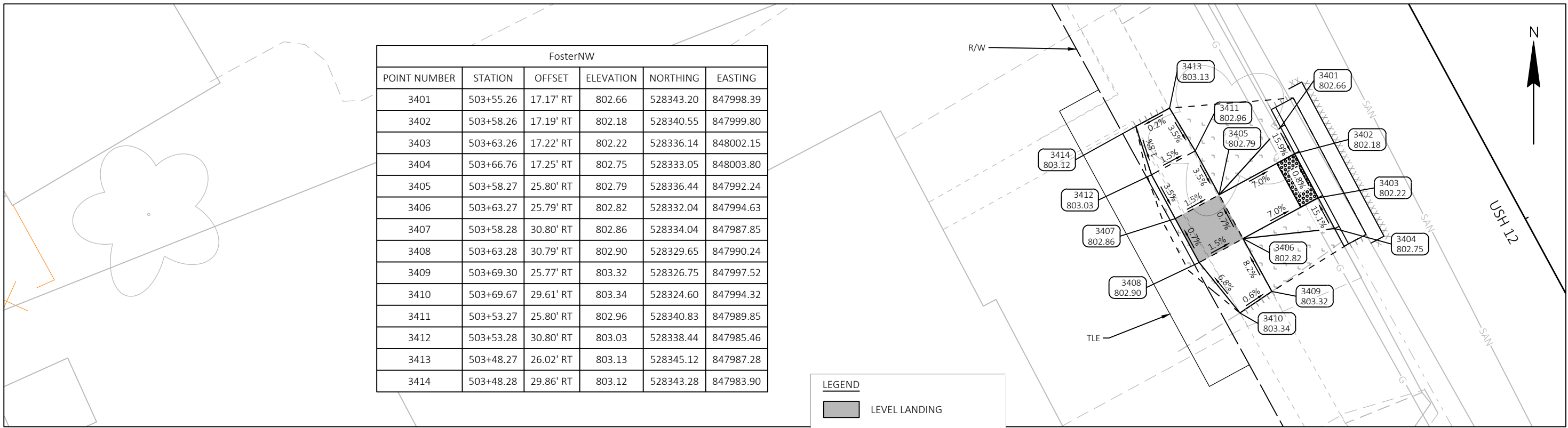
LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT

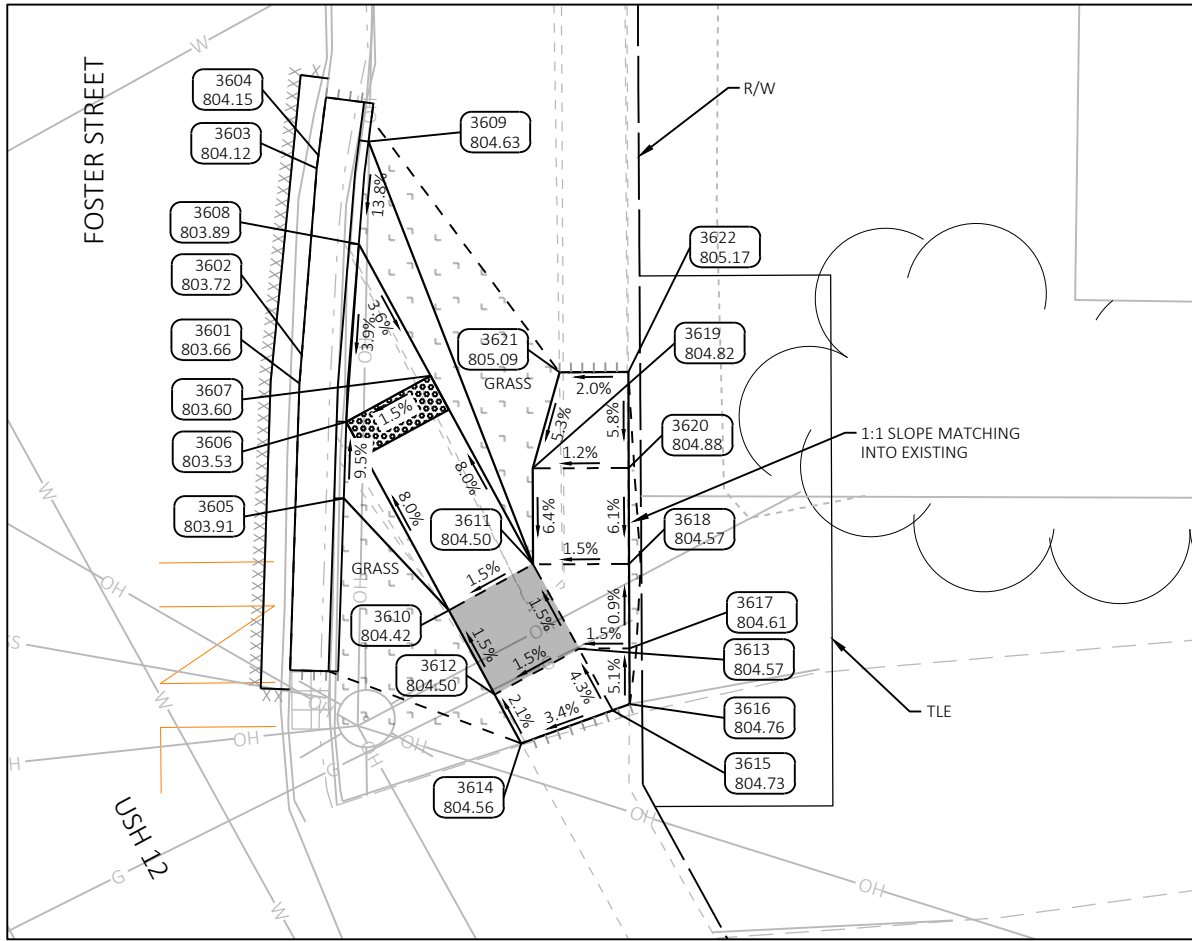
FosterNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3401	503+55.26	17.17' RT	802.66	528343.20	847998.39
3402	503+58.26	17.19' RT	802.18	528340.55	847999.80
3403	503+63.26	17.22' RT	802.22	528336.14	848002.15
3404	503+66.76	17.25' RT	802.75	528333.05	848003.80
3405	503+58.27	25.80' RT	802.79	528336.44	847992.24
3406	503+63.27	25.79' RT	802.82	528332.04	847994.63
3407	503+58.28	30.80' RT	802.86	528334.04	847987.85
3408	503+63.28	30.79' RT	802.90	528329.65	847990.24
3409	503+69.30	25.77' RT	803.32	528326.75	847997.52
3410	503+69.67	29.61' RT	803.34	528324.60	847994.32
3411	503+53.27	25.80' RT	802.96	528340.83	847989.85
3412	503+53.28	30.80' RT	803.03	528338.44	847985.46
3413	503+48.27	26.02' RT	803.13	528345.12	847987.28
3414	503+48.28	29.86' RT	803.12	528343.28	847983.90

LEGEND

-  LEVEL LANDING
-  DETECTABLE WARNING FIELD
-  SAW CUT - CONCRETE
-  SAW CUT - ASPHALT



FOSTER STREET



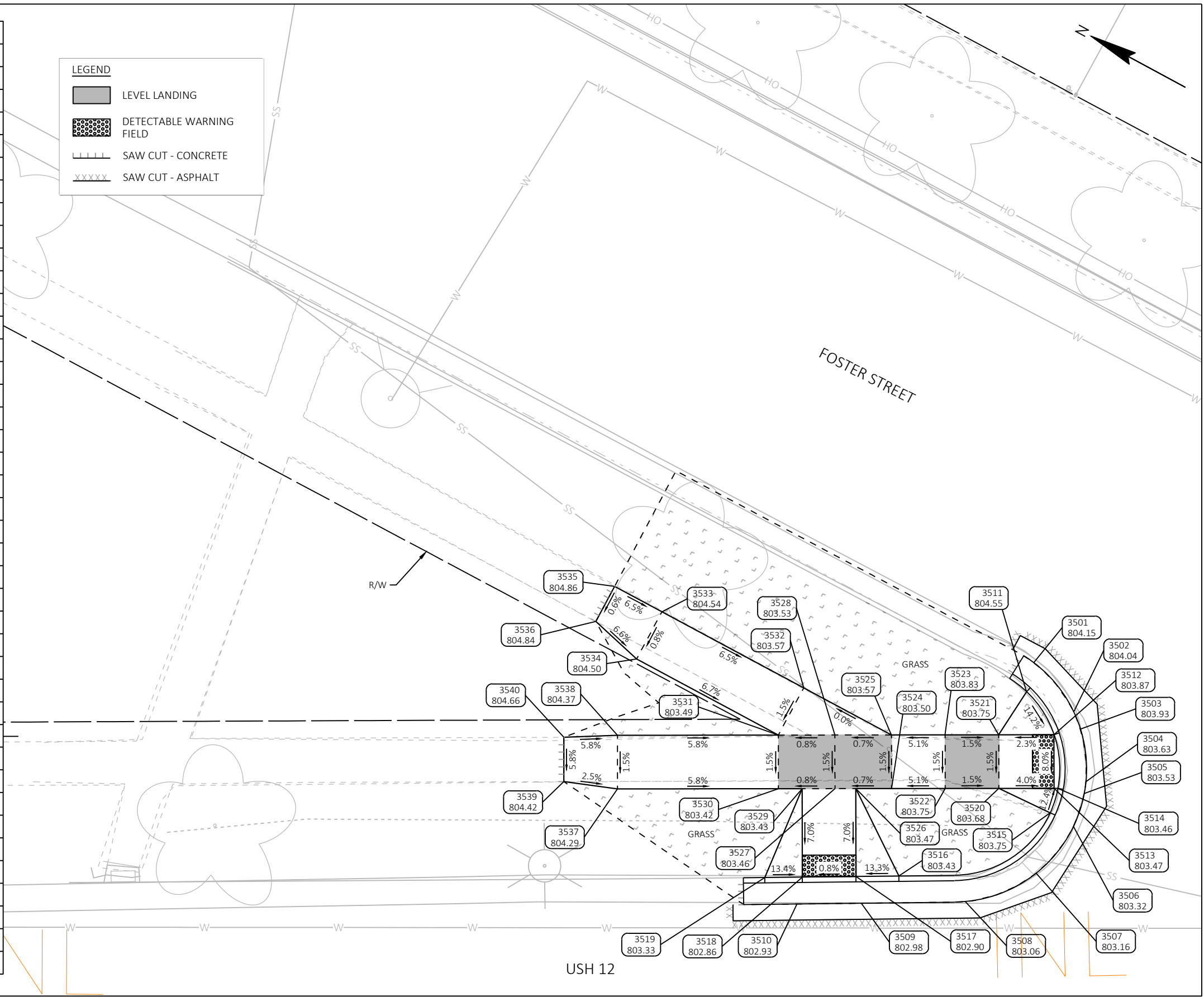
FosterSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3601	504+64.56	29.45' LT	803.66	528269.33	848091.47
3602	504+63.30	30.27' LT	803.72	528270.83	848091.59
3603	504+55.14	35.59' LT	804.12	528280.54	848092.37
3604	504+54.58	35.99' LT	804.15	528281.23	848092.46
3605	504+70.93	28.62' LT	803.91	528263.34	848093.78
3606	504+67.47	30.64' LT	803.53	528267.34	848093.91
3607	504+67.47	35.64' LT	803.60	528269.73	848098.30
3608	504+59.65	35.63' LT	803.89	528276.59	848094.56
3609	504+55.20	38.62' LT	804.63	528281.93	848095.07
3610	504+78.65	30.66' LT	804.42	528257.52	848099.25
3611	504+78.64	35.66' LT	804.50	528259.91	848103.64

FosterSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3612	504+83.65	30.67' LT	804.50	528253.13	848101.64
3613	504+83.64	35.67' LT	804.57	528255.52	848106.03
3614	504+86.55	30.67' LT	804.56	528250.58	848103.02
3615	504+87.30	35.67' LT	804.73	528252.31	848107.78
3616	504+87.44	36.61' LT	804.76	528252.63	848108.67
3617	504+84.88	37.98' LT	804.61	528255.53	848108.66
3618	504+81.01	40.06' LT	804.57	528259.94	848108.64
3619	504+74.24	38.02' LT	804.82	528264.91	848103.62
3620	504+76.60	42.43' LT	804.88	528264.94	848108.62
3621	504+70.50	41.64' LT	805.09	528269.92	848105.01
3622	504+72.19	44.79' LT	805.17	528269.94	848108.60

FosterNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3501	503+79.77	39.91' LT	804.15	528348.86	848060.25
3502	503+83.50	35.32' LT	804.04	528343.39	848057.99
3503	503+84.11	33.71' LT	803.93	528342.08	848056.87
3504	503+84.64	28.82' LT	803.63	528339.29	848052.82
3505	503+84.48	27.71' LT	803.53	528338.90	848051.77
3506	503+83.48	24.66' LT	803.32	528338.33	848048.61
3507	503+80.01	20.28' LT	803.16	528339.30	848043.11
3508	503+73.41	17.64' LT	803.06	528343.83	848037.63
3509	503+63.71	17.48' LT	802.98	528352.29	848032.88
3510	503+57.71	17.42' LT	802.93	528357.53	848029.96
3511	503+79.10	37.20' LT	804.55	528348.15	848057.55
3512	503+81.67	33.20' LT	803.87	528343.99	848055.26
3513	503+81.67	28.20' LT	803.47	528341.60	848050.86
3514	503+82.03	28.20' LT	803.46	528341.29	848051.03
3515	503+81.31	25.92' LT	803.75	528340.83	848048.69
3516	503+67.20	20.02' LT	803.43	528350.43	848036.78
3517	503+63.20	19.98' LT	802.90	528353.92	848034.83
3518	503+58.20	19.92' LT	802.86	528358.29	848032.40
3519	503+54.71	19.88' LT	803.33	528361.34	848030.69
3520	503+76.52	28.20' LT	803.68	528346.13	848048.40
3521	503+76.52	33.20' LT	803.75	528348.52	848052.79
3522	503+71.52	28.19' LT	803.75	528350.52	848046.01
3523	503+71.52	33.19' LT	803.83	528352.91	848050.40
3524	503+66.52	28.18' LT	803.50	528354.91	848043.62
3525	503+66.52	33.18' LT	803.57	528357.30	848048.01
3526	503+63.19	28.17' LT	803.47	528357.84	848042.03
3527	503+61.25	28.17' LT	803.46	528359.54	848041.10
3528	503+61.24	33.17' LT	803.53	528361.93	848045.49
3529	503+58.19	28.17' LT	803.43	528362.23	848039.64
3530	503+55.98	28.16' LT	803.42	528364.17	848038.58
3531	503+55.97	33.16' LT	803.49	528366.56	848042.97
3532	503+58.34	37.57' LT	803.57	528366.58	848047.97
3533	503+45.12	44.66' LT	804.54	528381.58	848047.91
3534	503+42.76	40.26' LT	804.50	528381.56	848042.91
3535	503+40.71	47.02' LT	804.86	528386.58	848047.88
3536	503+38.96	43.77' LT	804.84	528386.57	848044.19
3537	503+40.98	28.14' LT	804.29	528377.35	848031.41
3538	503+40.97	33.14' LT	804.37	528379.74	848035.80
3539	503+35.98	28.82' LT	804.42	528382.07	848029.63
3540	503+35.97	32.96' LT	804.66	528384.04	848033.26

LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT



ElmNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3701	505+69.78	19.60' RT	800.63	528153.45	848098.50
3702	505+74.72	19.71' RT	800.50	528149.06	848100.76
3703	505+79.72	19.82' RT	800.37	528144.62	848103.05
3704	505+84.72	19.93' RT	800.22	528140.17	848105.34
3705	505+90.57	20.05' RT	799.93	528134.97	848108.01
3706	505+95.16	20.74' RT	799.64	528130.60	848109.60
3707	505+98.09	22.01' RT	799.57	528127.42	848109.88
3708	506+01.59	24.78' RT	799.47	528123.02	848109.11
3709	506+03.62	27.53' RT	799.53	528119.93	848107.66
3710	506+04.96	30.69' RT	799.57	528117.25	848105.52
3711	506+05.46	36.69' RT	799.61	528113.95	848100.49
3712	505+69.73	22.10' RT	801.05	528152.31	848096.28
3713	505+74.73	22.21' RT	800.92	528147.86	848098.57
3714	505+79.73	22.32' RT	800.79	528143.41	848100.85
3715	505+84.73	22.43' RT	800.64	528138.97	848103.14
3716	505+85.19	22.44' RT	800.62	528138.56	848103.35
3717	505+90.18	22.54' RT	799.88	528134.12	848105.64
3718	505+95.11	23.36' RT	799.54	528129.39	848107.27
3719	505+97.69	24.70' RT	799.70	528126.49	848107.32
3720	505+98.52	25.33' RT	799.67	528125.46	848107.17
3721	506+00.79	27.74' RT	799.42	528122.32	848106.12
3722	506+02.88	32.73' RT	799.51	528118.10	848102.73
3723	506+00.81	32.74' RT	799.52	528119.91	848101.74
3724	506+03.02	36.05' RT	800.03	528116.40	848099.89
3725	505+98.18	27.75' RT	799.62	528124.61	848104.87
3726	505+94.80	25.36' RT	799.65	528128.71	848105.36
3727	505+89.74	25.38' RT	799.73	528133.15	848102.93
3728	505+89.76	30.38' RT	799.78	528130.75	848098.55

ElmNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3729	505+91.06	30.38' RT	799.76	528129.61	848099.17
3730	505+95.46	32.76' RT	799.71	528124.61	848099.17
3731	505+93.07	37.16' RT	800.02	528124.61	848094.17
3732	505+88.68	34.77' RT	800.09	528129.61	848094.17
3733	505+87.22	45.36' RT	800.75	528125.84	848084.17
3734	505+83.90	43.56' RT	800.80	528129.62	848084.17
3735	505+84.74	25.40' RT	800.07	528137.54	848100.53
3736	505+84.76	30.40' RT	800.15	528135.14	848096.15
3737	505+79.74	25.42' RT	800.42	528141.92	848098.13
3738	505+79.76	30.42' RT	800.49	528139.52	848093.74
3739	505+74.74	25.44' RT	800.77	528146.31	848095.73
3740	505+74.76	30.44' RT	800.84	528143.91	848091.34
3741	505+69.74	25.46' RT	801.12	528150.69	848093.33
3742	505+69.76	29.34' RT	801.16	528148.83	848089.93

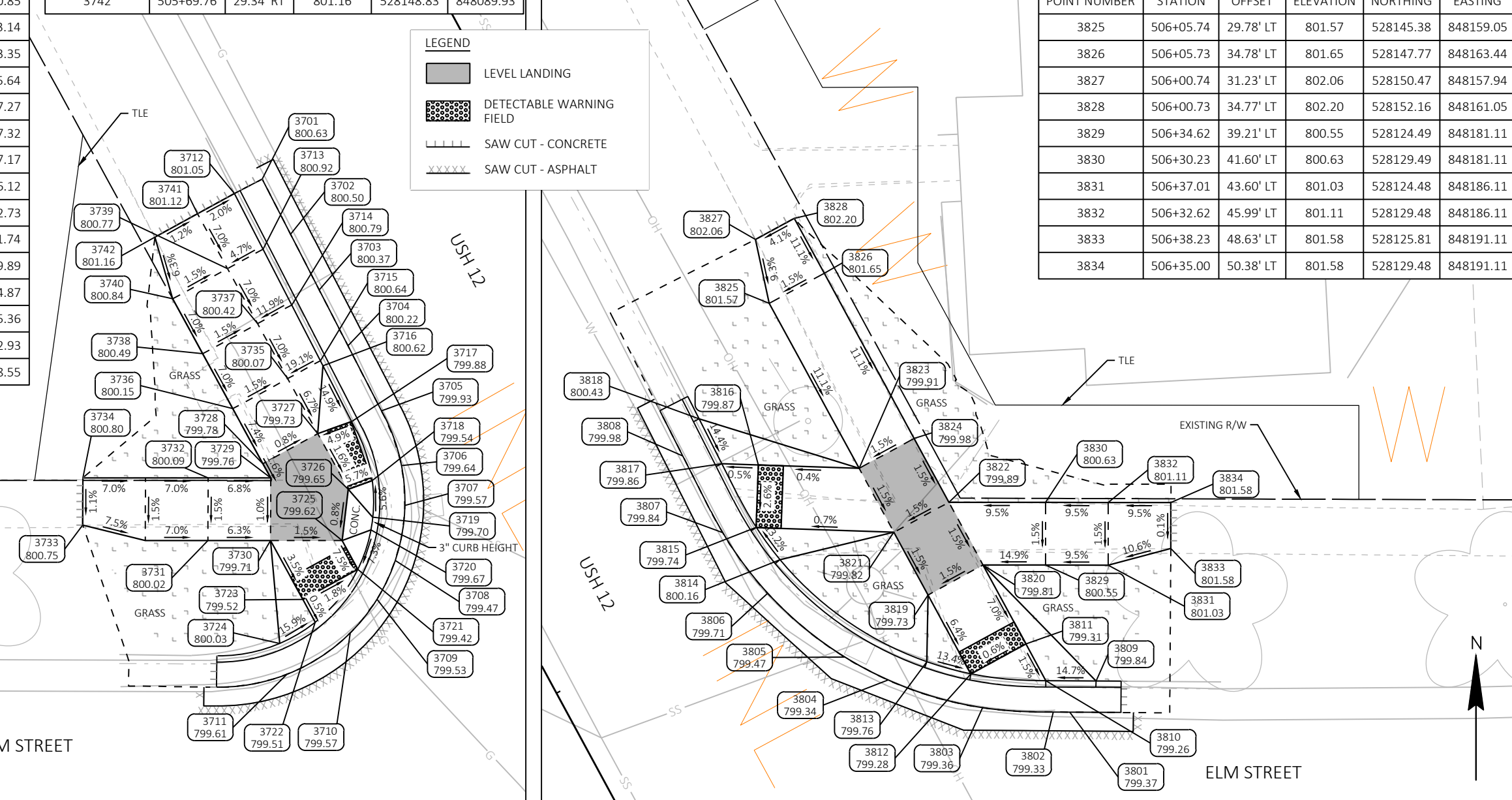
ElmNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3801	506+45.86	35.32' LT	799.37	528112.75	848183.05
3802	506+45.22	34.15' LT	799.33	528112.76	848181.72
3803	506+42.23	29.35' LT	799.36	528113.10	848176.07
3804	506+36.69	23.77' LT	799.34	528115.31	848168.53
3805	506+33.42	21.60' LT	799.47	528117.15	848165.05
3806	506+26.03	18.62' LT	799.71	528122.23	848158.91
3807	506+20.06	17.78' LT	799.84	528127.07	848155.33
3808	506+13.16	17.90' LT	799.98	528133.19	848152.14
3809	506+44.67	38.34' LT	799.84	528115.23	848185.13
3810	506+42.74	34.83' LT	799.26	528115.26	848181.13
3811	506+39.41	34.83' LT	799.31	528118.19	848179.54
3812	506+39.42	29.83' LT	799.28	528115.80	848175.15

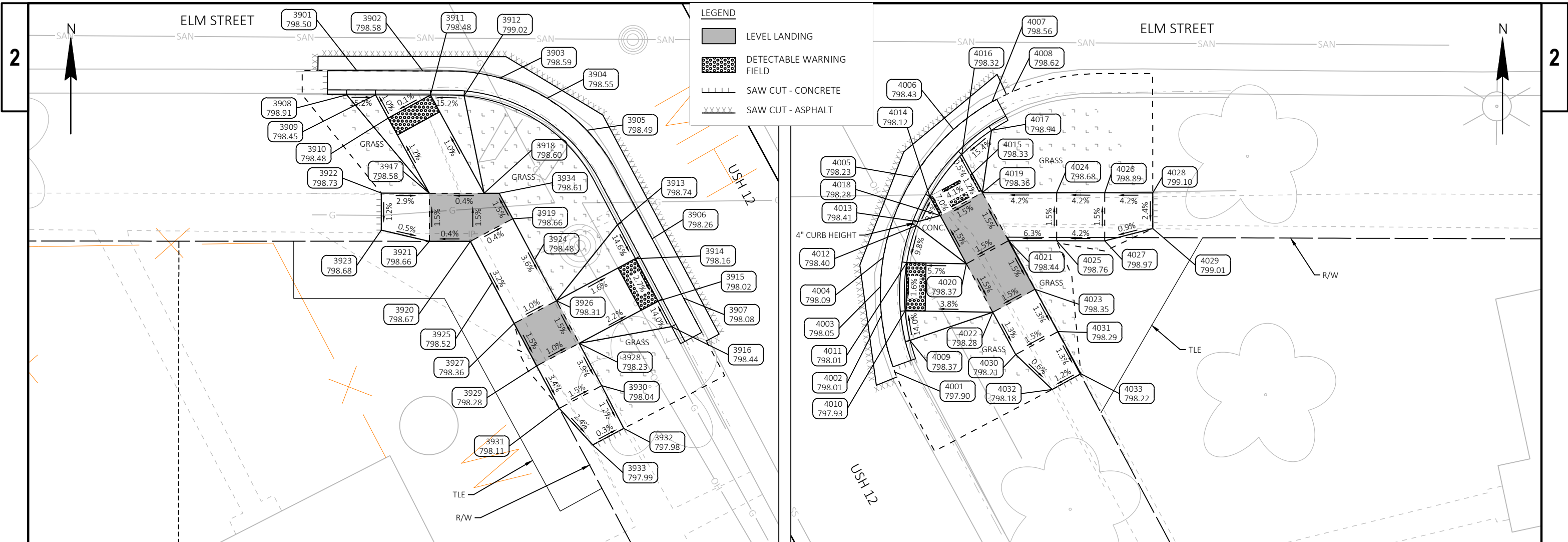
ElmNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3813	506+36.89	27.19' LT	799.76	528116.76	848171.62
3814	506+24.14	20.76' LT	800.16	528124.91	848159.89
3815	506+20.97	20.33' LT	799.74	528127.49	848158.00
3816	506+16.66	22.86' LT	799.87	528132.48	848158.18
3817	506+15.18	20.34' LT	799.86	528132.58	848155.25
3818	506+11.18	20.45' LT	800.43	528136.16	848153.44
3819	506+32.24	29.82' LT	799.73	528122.10	848171.72
3820	506+32.23	34.82' LT	799.81	528124.49	848176.11
3821	506+26.55	29.81' LT	799.82	528127.10	848168.99
3822	506+26.54	34.81' LT	799.89	528129.49	848173.39
3823	506+20.74	29.80' LT	799.91	528132.20	848166.22
3824	506+20.73	34.80' LT	799.98	528134.59	848170.61

ElmNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3825	506+05.74	29.78' LT	801.57	528145.38	848159.05
3826	506+05.73	34.78' LT	801.65	528147.77	848163.44
3827	506+00.74	31.23' LT	802.06	528150.47	848157.94
3828	506+00.73	34.77' LT	802.20	528152.16	848161.05
3829	506+34.62	39.21' LT	800.55	528124.49	848181.11
3830	506+30.23	41.60' LT	800.63	528129.49	848181.11
3831	506+37.01	43.60' LT	801.03	528124.48	848186.11
3832	506+32.62	45.99' LT	801.11	528129.48	848186.11
3833	506+38.23	48.63' LT	801.58	528125.81	848191.11
3834	506+35.00	50.38' LT	801.58	528129.48	848191.11

LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT





ElmSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3901	506+37.93	33.09' RT	798.50	528087.11	848119.13
3902	506+41.21	27.08' RT	798.58	528087.10	848125.98
3903	506+45.69	20.35' RT	798.59	528086.36	848134.02
3904	506+49.87	17.07' RT	798.55	528084.26	848138.91
3905	506+54.77	15.01' RT	798.49	528080.92	848143.05
3906	506+67.93	14.41' RT	798.26	528069.65	848149.85
3907	506+74.90	14.47' RT	798.08	528063.49	848153.12
3908	506+39.62	35.21' RT	798.91	528084.62	848118.07
3909	506+41.06	32.58' RT	798.45	528084.61	848121.07
3910	506+43.81	32.57' RT	798.48	528082.20	848122.39
3911	506+43.79	27.57' RT	798.48	528084.60	848126.78
3912	506+45.52	24.53' RT	799.02	528084.52	848130.27
3913	506+64.92	16.89' RT	798.74	528071.11	848146.24
3914	506+68.92	16.92' RT	798.16	528067.58	848148.11
3915	506+73.92	16.96' RT	798.02	528063.16	848150.46
3916	506+76.92	16.99' RT	798.44	528060.51	848151.87
3917	506+52.72	32.53' RT	798.58	528074.38	848126.67

ElmSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3918	506+55.44	27.52' RT	798.60	528074.38	848132.37
3919	506+59.18	26.16' RT	798.66	528071.73	848135.35
3920	506+59.15	31.16' RT	798.67	528069.38	848130.93
3921	506+57.11	34.92' RT	798.66	528069.38	848126.66
3922	506+50.33	36.93' RT	798.73	528074.39	848121.67
3923	506+53.71	38.76' RT	798.68	528070.54	848121.66
3924	506+64.02	26.20' RT	798.48	528067.47	848137.62
3925	506+63.98	31.20' RT	798.52	528065.11	848133.21
3926	506+68.85	26.23' RT	798.31	528063.20	848139.89
3927	506+68.82	31.23' RT	798.36	528060.85	848135.48
3928	506+73.85	26.27' RT	798.23	528058.79	848142.25
3929	506+73.82	31.27' RT	798.28	528056.43	848137.83
3930	506+78.85	26.30' RT	798.04	528054.37	848144.60
3931	506+78.82	31.30' RT	798.11	528052.02	848140.19
3932	506+83.85	26.40' RT	797.98	528049.93	848146.90
3933	506+83.83	30.07' RT	797.99	528048.21	848143.66
3934	506+56.18	26.14' RT	798.61	528074.38	848133.93

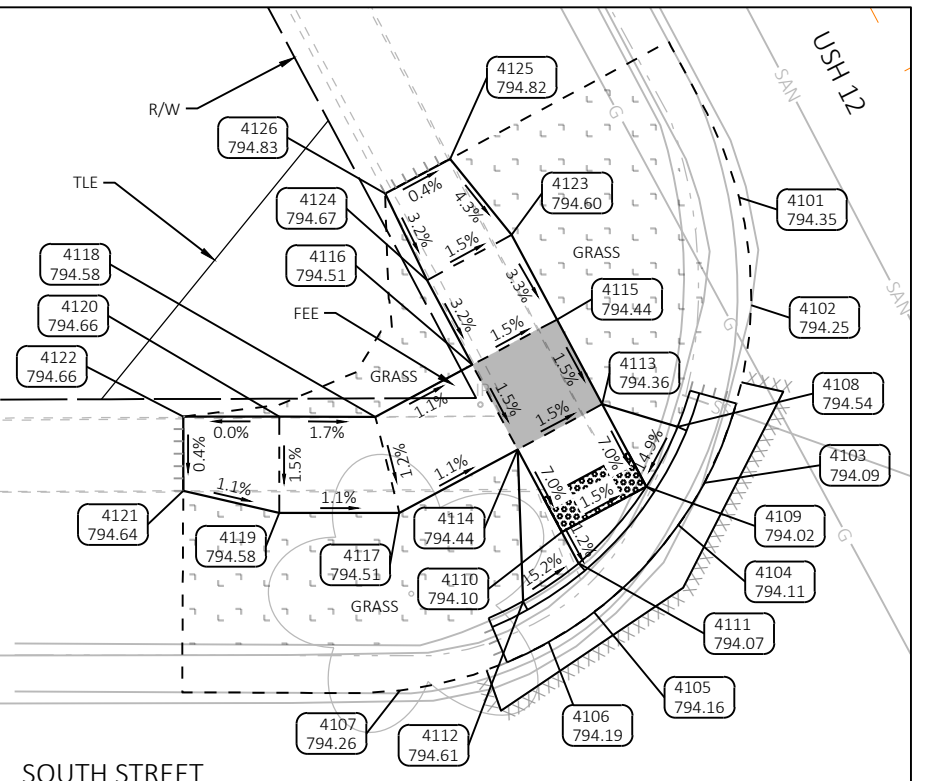
ElmSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4001	507+01.14	17.61' LT	797.90	528055.72	848193.83
4002	506+95.28	19.20' LT	798.01	528061.62	848192.43
4003	506+92.63	20.52' LT	798.05	528064.58	848192.33
4004	506+90.11	22.23' LT	798.09	528067.62	848192.63
4005	506+84.26	28.99' LT	798.23	528075.98	848195.78
4006	506+81.89	35.01' LT	798.43	528080.93	848199.95
4007	506+81.28	39.88' LT	798.56	528083.79	848203.94
4008	506+81.36	42.23' LT	798.62	528084.84	848206.04
4009	506+99.00	20.57' LT	798.37	528059.01	848195.41
4010	506+96.06	21.59' LT	797.93	528062.08	848194.90
4011	506+91.74	24.12' LT	798.01	528067.08	848195.07
4012	506+88.84	26.83' LT	798.40	528070.92	848196.07
4013	506+88.55	27.17' LT	798.41	528071.34	848196.23
4014	506+86.85	29.51' LT	798.12	528073.95	848197.48
4015	506+86.84	34.51' LT	798.33	528076.34	848201.87
4016	506+84.61	34.51' LT	798.32	528078.30	848200.80
4017	506+83.85	38.44' LT	798.94	528080.84	848203.90

ElmSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4018	506+89.10	29.51' LT	798.28	528071.97	848198.55
4019	506+89.09	34.51' LT	798.36	528074.36	848202.94
4020	506+94.79	29.52' LT	798.37	528066.97	848201.27
4021	506+94.79	34.52' LT	798.44	528069.36	848205.66
4022	507+00.54	29.53' LT	798.28	528061.92	848204.02
4023	507+00.53	34.53' LT	798.35	528064.31	848208.41
4024	506+92.78	41.30' LT	798.68	528074.36	848210.67
4025	506+97.17	38.92' LT	798.76	528069.36	848210.66
4026	506+95.17	45.70' LT	798.89	528074.35	848215.67
4027	506+99.56	43.31' LT	798.97	528069.35	848215.66
4028	506+97.56	50.08' LT	799.10	528074.34	848220.67
4029	507+00.82	48.31' LT	799.01	528070.63	848220.67
4030	507+05.54	29.54' LT	798.21	528057.53	848206.41
4031	507+05.53	34.54' LT	798.29	528059.92	848210.80
4032	507+10.54	31.15' LT	798.18	528053.91	848210.21
4033	507+10.53	34.55' LT	798.22	528055.53	848213.19

PROJECT NO: 3575-02-73 HWY: USH 12 COUNTY: JEFFERSON CURB RAMP DETAILS - ELM ST SHEET E

SouthNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4101	509+73.33	16.41' RT	794.35	527800.22	848293.66
4102	509+78.56	18.52' RT	794.25	527794.61	848294.30
4103	509+85.53	25.06' RT	794.09	527785.37	848291.87
4104	509+86.80	27.32' RT	794.11	527783.18	848290.49
4105	509+88.69	33.33' RT	794.16	527778.65	848286.11
4106	509+88.96	36.16' RT	794.19	527777.07	848283.75
4107	509+87.48	44.16' RT	794.26	527774.55	848276.01
4108	509+82.31	24.91' RT	794.54	527788.28	848290.47
4109	509+84.27	27.82' RT	794.02	527785.16	848288.84
4110	509+84.24	32.82' RT	794.10	527782.80	848284.44
4111	509+86.09	32.83' RT	794.07	527781.17	848285.31
4112	509+86.46	36.32' RT	794.61	527779.19	848282.42
4113	509+79.40	27.79' RT	794.36	527789.46	848286.54

SouthNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4114	509+79.37	32.79' RT	794.44	527787.10	848282.13
4115	509+74.40	27.77' RT	794.44	527793.87	848284.18
4116	509+74.37	32.77' RT	794.51	527791.51	848279.77
4117	509+79.33	39.78' RT	794.51	527783.80	848275.97
4118	509+74.34	38.50' RT	794.58	527788.80	848274.72
4119	509+76.35	45.28' RT	794.58	527783.81	848269.72
4120	509+71.95	42.89' RT	794.66	527788.81	848269.72
4121	509+72.96	49.13' RT	794.64	527784.95	848264.72
4122	509+69.57	47.29' RT	794.66	527788.81	848264.72
4123	509+69.40	27.74' RT	794.60	527798.28	848281.82
4124	509+69.37	32.74' RT	794.67	527795.92	848277.41
4125	509+64.39	28.67' RT	794.82	527802.23	848278.62
4126	509+64.37	32.52' RT	794.83	527800.42	848275.23

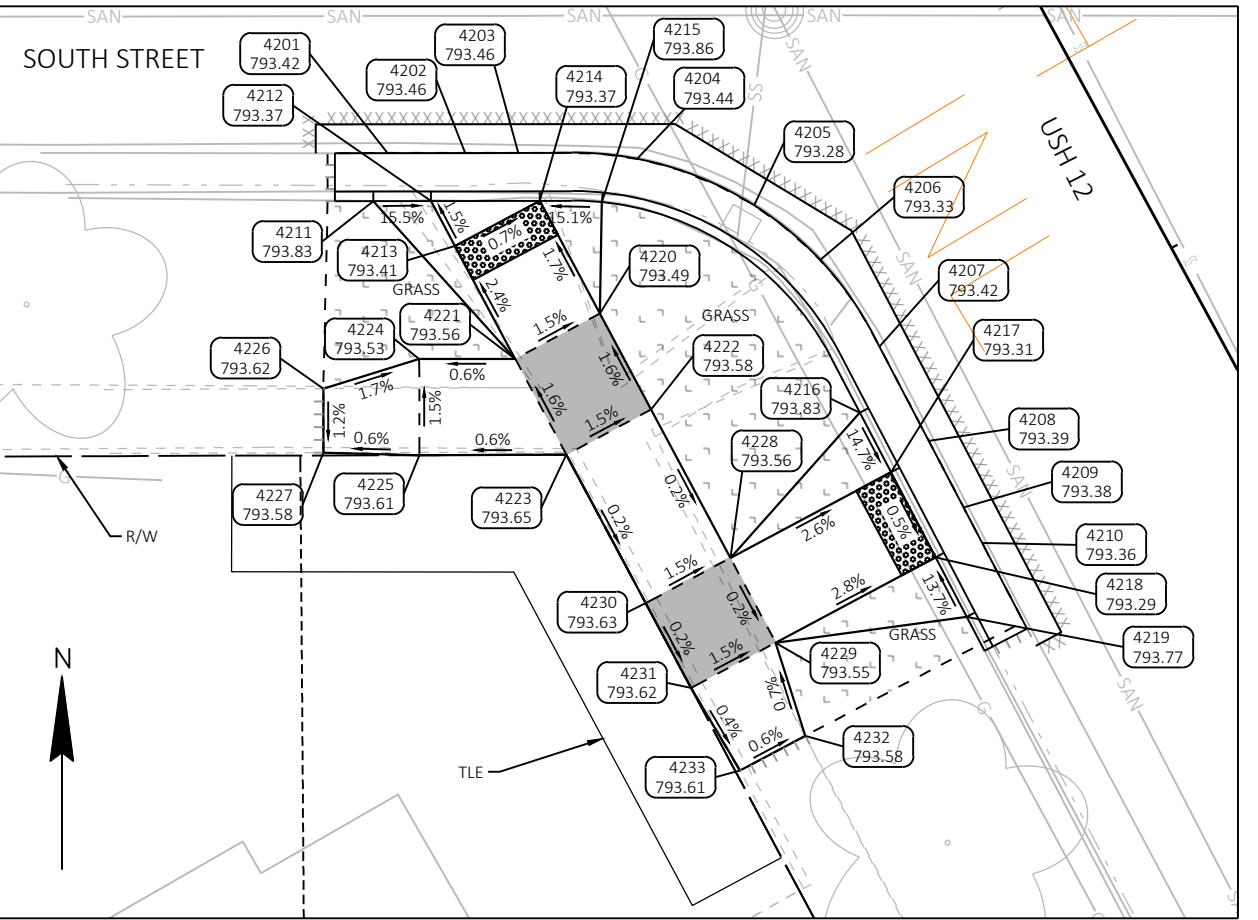


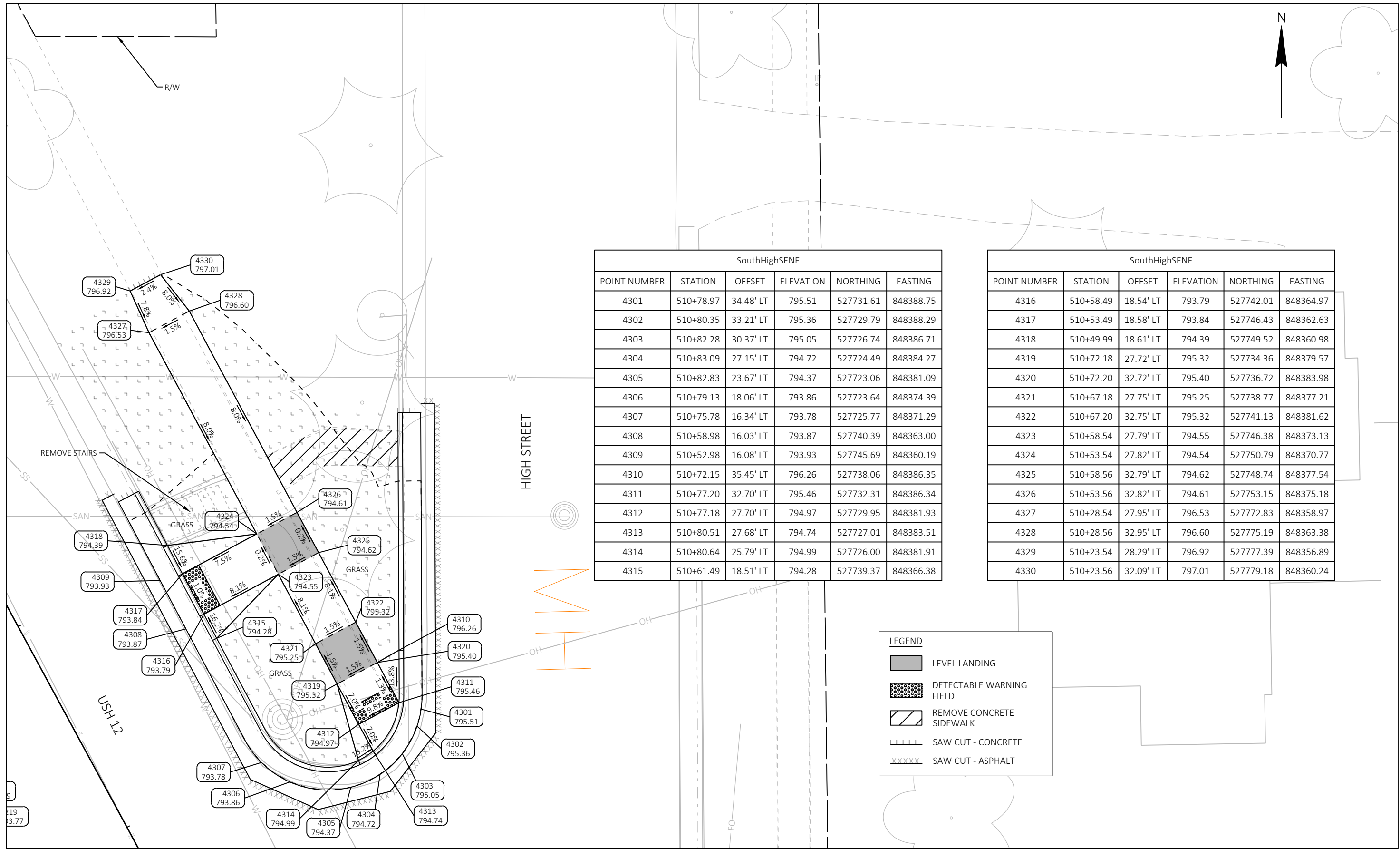
LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT

SouthSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4201	510+26.19	33.54' RT	793.42	527745.58	848303.80
4202	510+28.12	29.98' RT	793.46	527745.59	848307.84
4203	510+29.43	27.56' RT	793.46	527745.59	848310.60
4204	510+32.66	22.26' RT	793.44	527745.27	848316.79
4205	510+37.70	18.06' RT	793.28	527742.85	848322.89
4206	510+41.82	16.39' RT	793.33	527740.02	848326.33
4207	510+47.27	15.82' RT	793.42	527735.51	848329.42
4208	510+52.81	15.90' RT	793.39	527730.59	848331.99
4209	510+56.71	15.96' RT	793.38	527727.14	848333.80
4210	510+58.81	15.99' RT	793.36	527725.28	848334.78
4211	510+28.04	35.38' RT	793.83	527743.08	848303.06
4212	510+29.46	32.74' RT	793.37	527743.09	848306.06
4213	510+32.13	32.76' RT	793.41	527740.74	848307.31
4214	510+32.16	27.76' RT	793.37	527743.09	848311.72
4215	510+33.77	24.95' RT	793.86	527743.02	848314.97
4216	510+49.80	18.35' RT	793.83	527732.07	848328.40
4217	510+53.30	18.41' RT	793.31	527728.97	848330.02

SouthSW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4218	510+58.30	18.48' RT	793.29	527724.54	848332.34
4219	510+61.80	18.53' RT	793.77	527721.44	848333.96
4220	510+38.81	27.80' RT	793.49	527737.23	848314.86
4221	510+38.78	32.80' RT	793.56	527734.87	848310.45
4222	510+44.47	27.83' RT	793.58	527732.23	848317.53
4223	510+44.44	32.83' RT	793.65	527729.88	848313.12
4224	510+36.41	37.20' RT	793.53	527734.85	848305.45
4225	510+40.81	39.57' RT	793.61	527729.85	848305.46
4226	510+35.38	42.33' RT	793.62	527733.32	848300.45
4227	510+38.32	43.91' RT	793.58	527729.98	848300.46
4228	510+53.24	27.89' RT	793.56	527724.50	848321.66
4229	510+58.24	27.92' RT	793.55	527720.09	848324.01
4230	510+53.21	32.89' RT	793.63	527722.14	848317.25
4231	510+58.21	32.92' RT	793.62	527717.73	848319.60
4232	510+63.23	28.88' RT	793.58	527715.24	848325.55
4233	510+63.21	32.73' RT	793.61	527713.43	848322.15



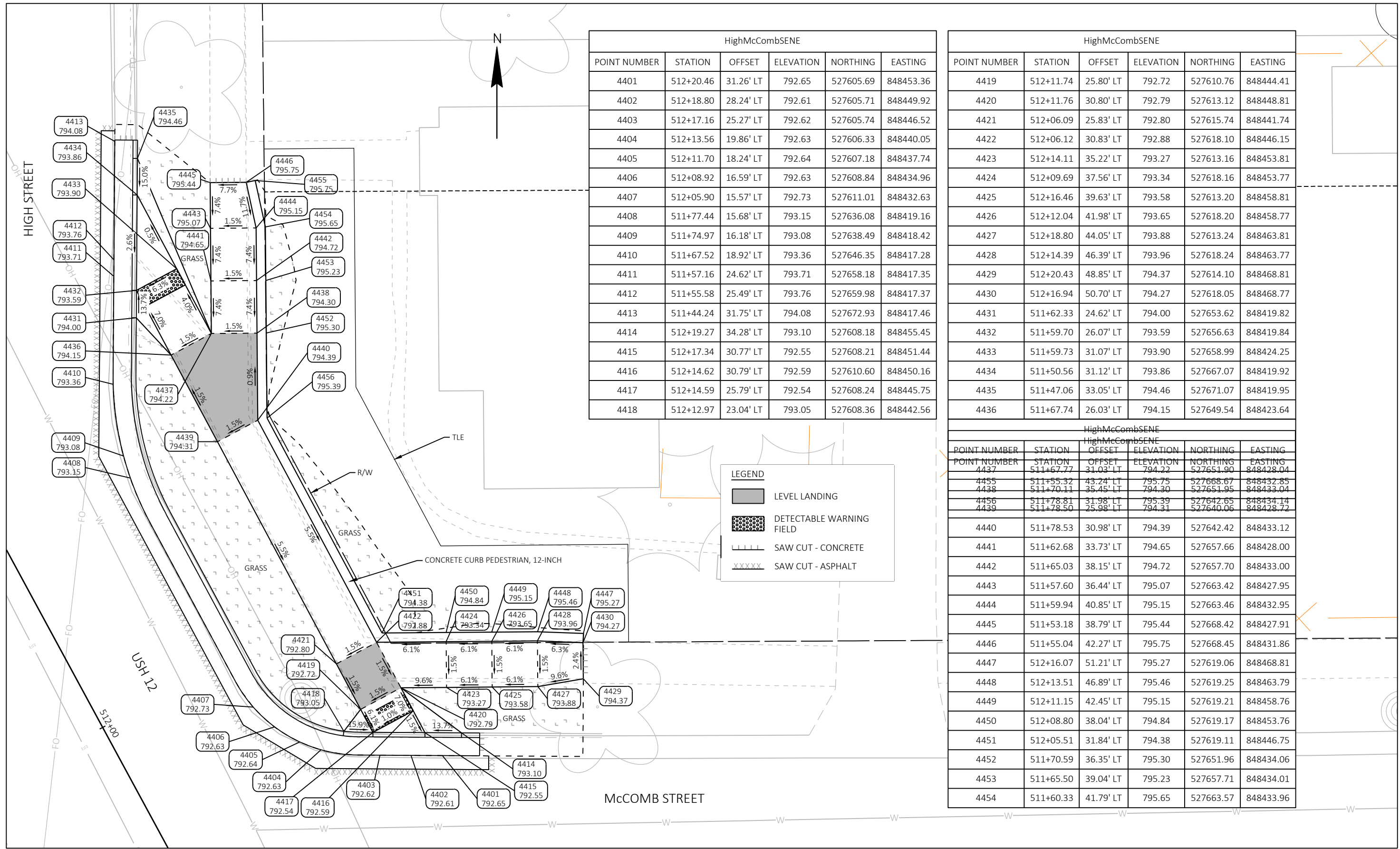


SouthHighSENE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4301	510+78.97	34.48' LT	795.51	527731.61	848388.75
4302	510+80.35	33.21' LT	795.36	527729.79	848388.29
4303	510+82.28	30.37' LT	795.05	527726.74	848386.71
4304	510+83.09	27.15' LT	794.72	527724.49	848384.27
4305	510+82.83	23.67' LT	794.37	527723.06	848381.09
4306	510+79.13	18.06' LT	793.86	527723.64	848374.39
4307	510+75.78	16.34' LT	793.78	527725.77	848371.29
4308	510+58.98	16.03' LT	793.87	527740.39	848363.00
4309	510+52.98	16.08' LT	793.93	527745.69	848360.19
4310	510+72.15	35.45' LT	796.26	527738.06	848386.35
4311	510+77.20	32.70' LT	795.46	527732.31	848386.34
4312	510+77.18	27.70' LT	794.97	527729.95	848381.93
4313	510+80.51	27.68' LT	794.74	527727.01	848383.51
4314	510+80.64	25.79' LT	794.99	527726.00	848381.91
4315	510+61.49	18.51' LT	794.28	527739.37	848366.38

SouthHighSENE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4316	510+58.49	18.54' LT	793.79	527742.01	848364.97
4317	510+53.49	18.58' LT	793.84	527746.43	848362.63
4318	510+49.99	18.61' LT	794.39	527749.52	848360.98
4319	510+72.18	27.72' LT	795.32	527734.36	848379.57
4320	510+72.20	32.72' LT	795.40	527736.72	848383.98
4321	510+67.18	27.75' LT	795.25	527738.77	848377.21
4322	510+67.20	32.75' LT	795.32	527741.13	848381.62
4323	510+58.54	27.79' LT	794.55	527746.38	848373.13
4324	510+53.54	27.82' LT	794.54	527750.79	848370.77
4325	510+58.56	32.79' LT	794.62	527748.74	848377.54
4326	510+53.56	32.82' LT	794.61	527753.15	848375.18
4327	510+28.54	27.95' LT	796.53	527772.83	848358.97
4328	510+28.56	32.95' LT	796.60	527775.19	848363.38
4329	510+23.54	28.29' LT	796.92	527777.39	848356.89
4330	510+23.56	32.09' LT	797.01	527779.18	848360.24

LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- REMOVE CONCRETE SIDEWALK
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT



HighMcCombSENE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4401	512+20.46	31.26' LT	792.65	527605.69	848453.36
4402	512+18.80	28.24' LT	792.61	527605.71	848449.92
4403	512+17.16	25.27' LT	792.62	527605.74	848446.52
4404	512+13.56	19.86' LT	792.63	527606.33	848440.05
4405	512+11.70	18.24' LT	792.64	527607.18	848437.74
4406	512+08.92	16.59' LT	792.63	527608.84	848434.96
4407	512+05.90	15.57' LT	792.73	527611.01	848432.63
4408	511+77.44	15.68' LT	793.15	527636.08	848419.16
4409	511+74.97	16.18' LT	793.08	527638.49	848418.42
4410	511+67.52	18.92' LT	793.36	527646.35	848417.28
4411	511+57.16	24.62' LT	793.71	527658.18	848417.35
4412	511+55.58	25.49' LT	793.76	527659.98	848417.37
4413	511+44.24	31.75' LT	794.08	527672.93	848417.46
4414	512+19.27	34.28' LT	793.10	527608.18	848455.45
4415	512+17.34	30.77' LT	792.55	527608.21	848451.44
4416	512+14.62	30.79' LT	792.59	527610.60	848450.16
4417	512+14.59	25.79' LT	792.54	527608.24	848445.75
4418	512+12.97	23.04' LT	793.05	527608.36	848442.56





HighMcCombSENE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4419	512+11.74	25.80' LT	792.72	527610.76	848444.41
4420	512+11.76	30.80' LT	792.79	527613.12	848448.81
4421	512+06.09	25.83' LT	792.80	527615.74	848441.74
4422	512+06.12	30.83' LT	792.88	527618.10	848446.15
4423	512+14.11	35.22' LT	793.27	527613.16	848453.81
4424	512+09.69	37.56' LT	793.34	527618.16	848453.77
4425	512+16.46	39.63' LT	793.58	527613.20	848458.81
4426	512+12.04	41.98' LT	793.65	527618.20	848458.77
4427	512+18.80	44.05' LT	793.88	527613.24	848463.81
4428	512+14.39	46.39' LT	793.96	527618.24	848463.77
4429	512+20.43	48.85' LT	794.37	527614.10	848468.81
4430	512+16.94	50.70' LT	794.27	527618.05	848468.77
4431	511+62.33	24.62' LT	794.00	527653.62	848419.82
4432	511+59.70	26.07' LT	793.59	527656.63	848419.84
4433	511+59.73	31.07' LT	793.90	527658.99	848424.25
4434	511+50.56	31.12' LT	793.86	527667.07	848419.92
4435	511+47.06	33.05' LT	794.46	527671.07	848419.95
4436	511+67.74	26.03' LT	794.15	527649.54	848423.64

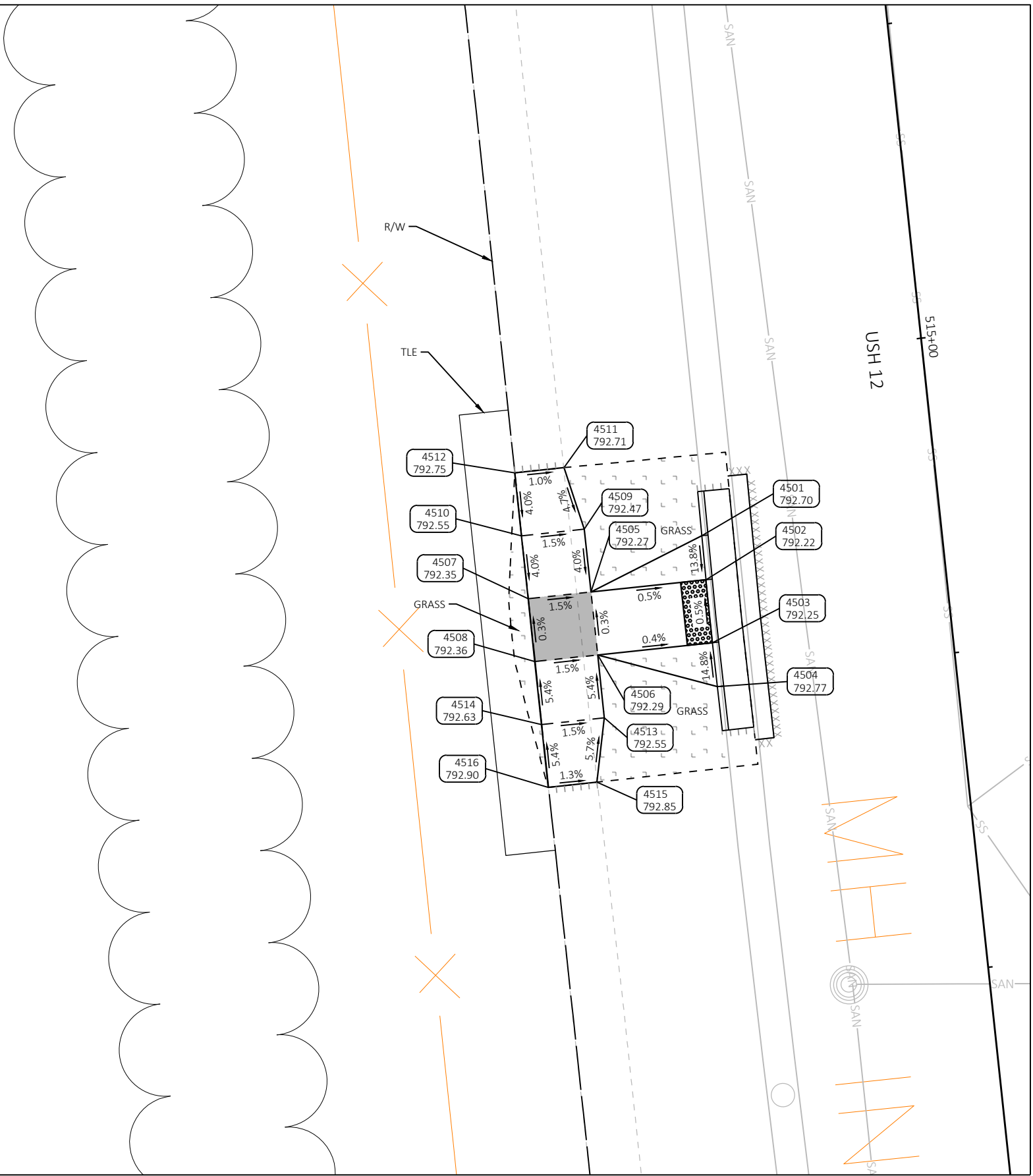
LEGEND

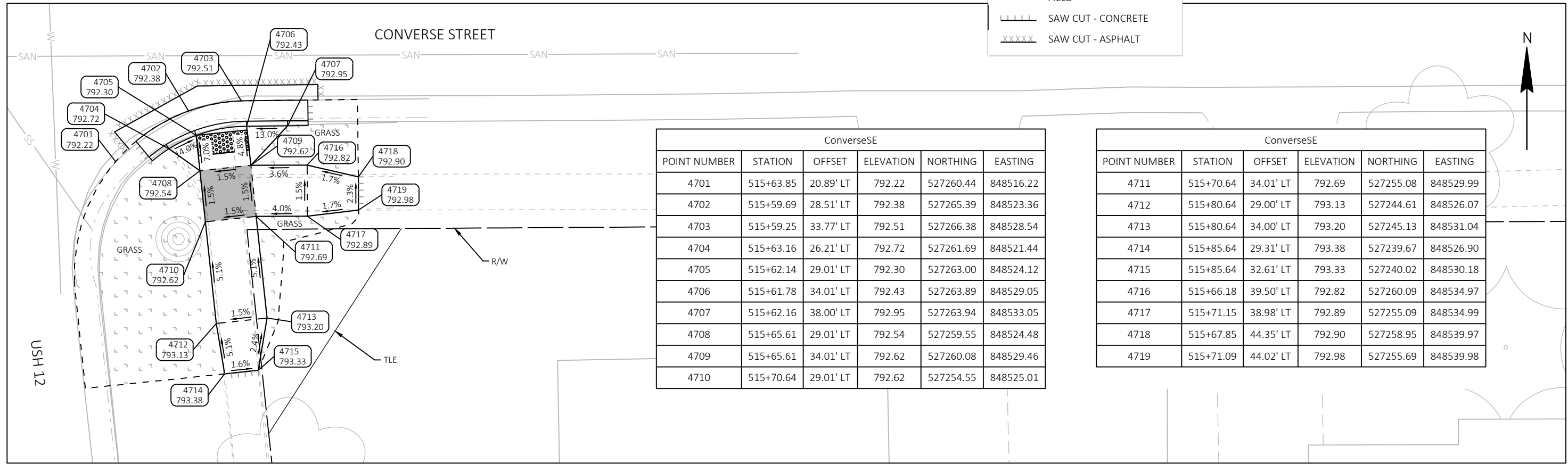
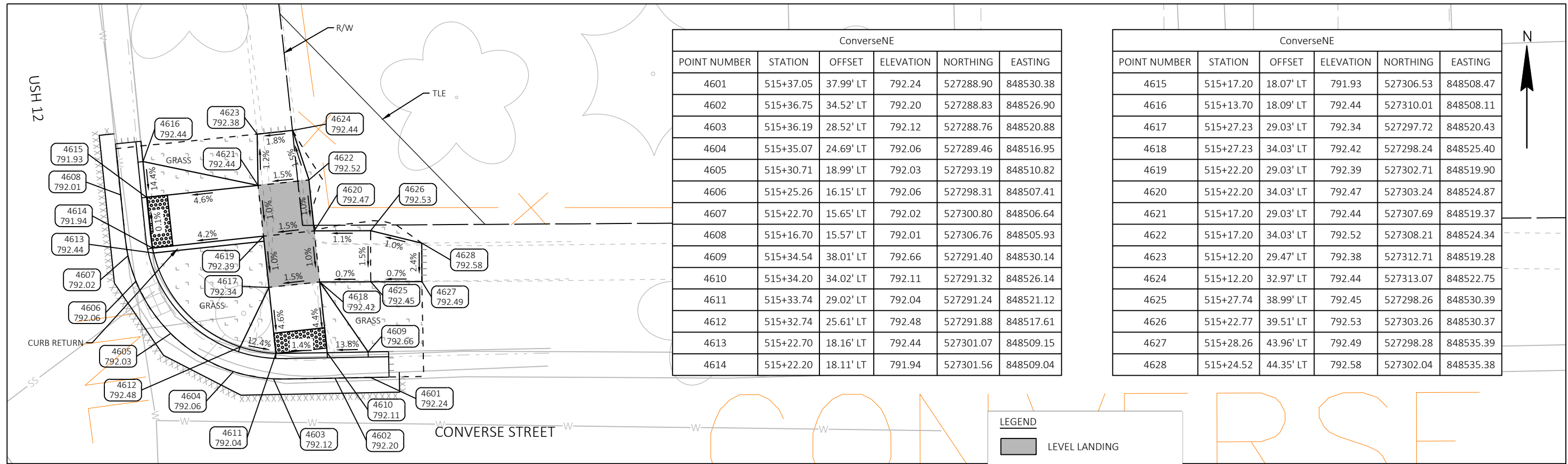
- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT

HighMcCombSENE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4437	511+67.77	31.03' LT	794.22	527651.90	848428.04
4455	511+55.32	43.24' LT	795.75	527668.67	848432.85
4438	511+70.11	35.45' LT	794.30	527651.95	848433.04
4456	511+78.81	31.98' LT	795.39	527642.65	848434.14
4439	511+78.50	25.98' LT	794.31	527640.06	848428.72
4440	511+78.53	30.98' LT	794.39	527642.42	848433.12
4441	511+62.68	33.73' LT	794.65	527657.66	848428.00
4442	511+65.03	38.15' LT	794.72	527657.70	848433.00
4443	511+57.60	36.44' LT	795.07	527663.42	848427.95
4444	511+59.94	40.85' LT	795.15	527663.46	848432.95
4445	511+53.18	38.79' LT	795.44	527668.42	848427.91
4446	511+55.04	42.27' LT	795.75	527668.45	848431.86
4447	512+16.07	51.21' LT	795.27	527619.06	848468.81
4448	512+13.51	46.89' LT	795.46	527619.25	848463.79
4449	512+11.15	42.45' LT	795.15	527619.21	848458.76
4450	512+08.80	38.04' LT	794.84	527619.17	848453.76
4451	512+05.51	31.84' LT	794.38	527619.11	848446.75
4452	511+70.59	36.35' LT	795.30	527651.96	848434.06
4453	511+65.50	39.04' LT	795.23	527657.71	848434.01
4454	511+60.33	41.79' LT	795.65	527663.57	848433.96

ConverseNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4501	515+13.68	18.90' RT	792.70	527306.10	848471.34
4502	515+17.18	18.93' RT	792.22	527302.62	848471.67
4503	515+22.18	18.96' RT	792.25	527297.65	848472.18
4504	515+25.68	18.94' RT	792.77	527294.17	848472.57
4505	515+17.18	28.08' RT	792.27	527301.65	848462.58
4506	515+22.18	28.08' RT	792.29	527296.68	848463.11
4507	515+17.18	33.08' RT	792.35	527301.13	848457.61
4508	515+22.18	33.08' RT	792.36	527296.15	848458.13
4509	515+12.18	28.08' RT	792.47	527306.63	848462.05
4510	515+12.18	33.08' RT	792.55	527306.10	848457.08
4511	515+07.18	29.16' RT	792.71	527311.48	848460.44
4512	515+07.18	33.07' RT	792.75	527311.07	848456.55
4513	515+27.18	28.08' RT	792.55	527291.71	848463.63
4514	515+27.18	33.08' RT	792.63	527291.18	848458.66
4515	515+32.18	29.21' RT	792.85	527286.62	848463.04
4516	515+32.18	33.09' RT	792.90	527286.21	848459.19

LEGEND	
	LEVEL LANDING
	DETECTABLE WARNING FIELD
	SAW CUT - CONCRETE
	SAW CUT - ASPHALT



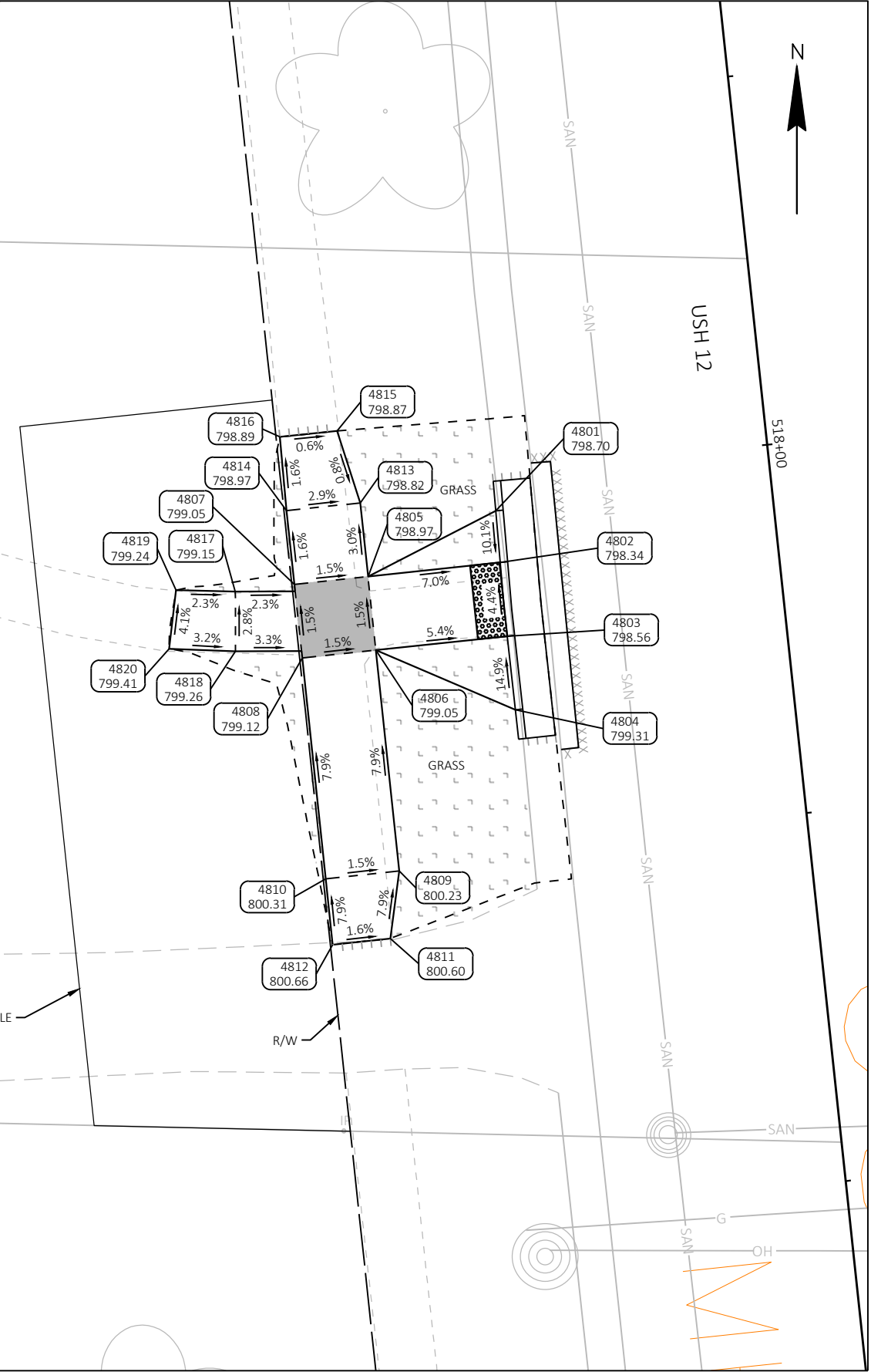


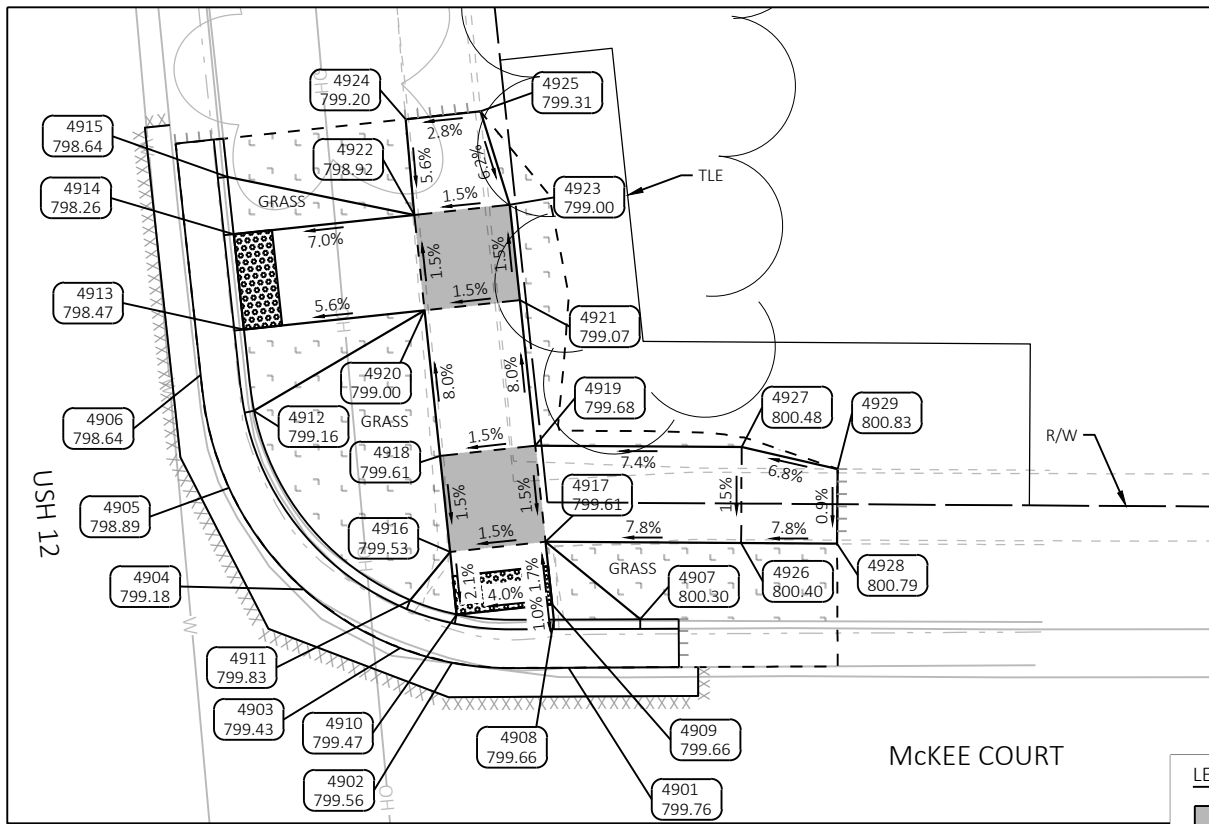
McKeeNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4801	518+02.49	18.71' RT	798.70	527018.95	848502.15
4802	518+05.99	18.78' RT	798.34	527015.46	848502.46
4803	518+10.99	18.83' RT	798.56	527010.48	848502.93
4804	518+15.99	18.81' RT	799.31	527005.51	848503.48
4805	518+05.99	27.76' RT	798.97	527014.51	848493.52
4806	518+10.99	27.77' RT	799.05	527009.54	848494.05
4807	518+05.98	32.76' RT	799.05	527013.98	848488.55
4808	518+10.98	32.77' RT	799.12	527009.01	848489.08
4809	518+25.99	27.77' RT	800.23	526994.62	848495.64
4810	518+25.98	32.77' RT	800.31	526994.09	848490.66

McKeeNW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4811	518+30.45	28.86' RT	800.60	526990.06	848495.03
4812	518+30.45	32.77' RT	800.66	526989.65	848491.14
4813	518+00.99	27.76' RT	798.82	527019.48	848493.00
4814	518+00.98	32.76' RT	798.97	527018.95	848488.02
4815	517+95.99	28.80' RT	798.87	527024.34	848491.43
4816	517+95.98	32.68' RT	798.89	527023.93	848487.57
4817	518+06.04	36.79' RT	799.15	527013.49	848484.56
4818	518+10.06	37.21' RT	799.26	527009.46	848484.57
4819	518+05.53	40.76' RT	799.24	527013.59	848480.56
4820	518+09.41	41.65' RT	799.41	527009.63	848480.08

LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT



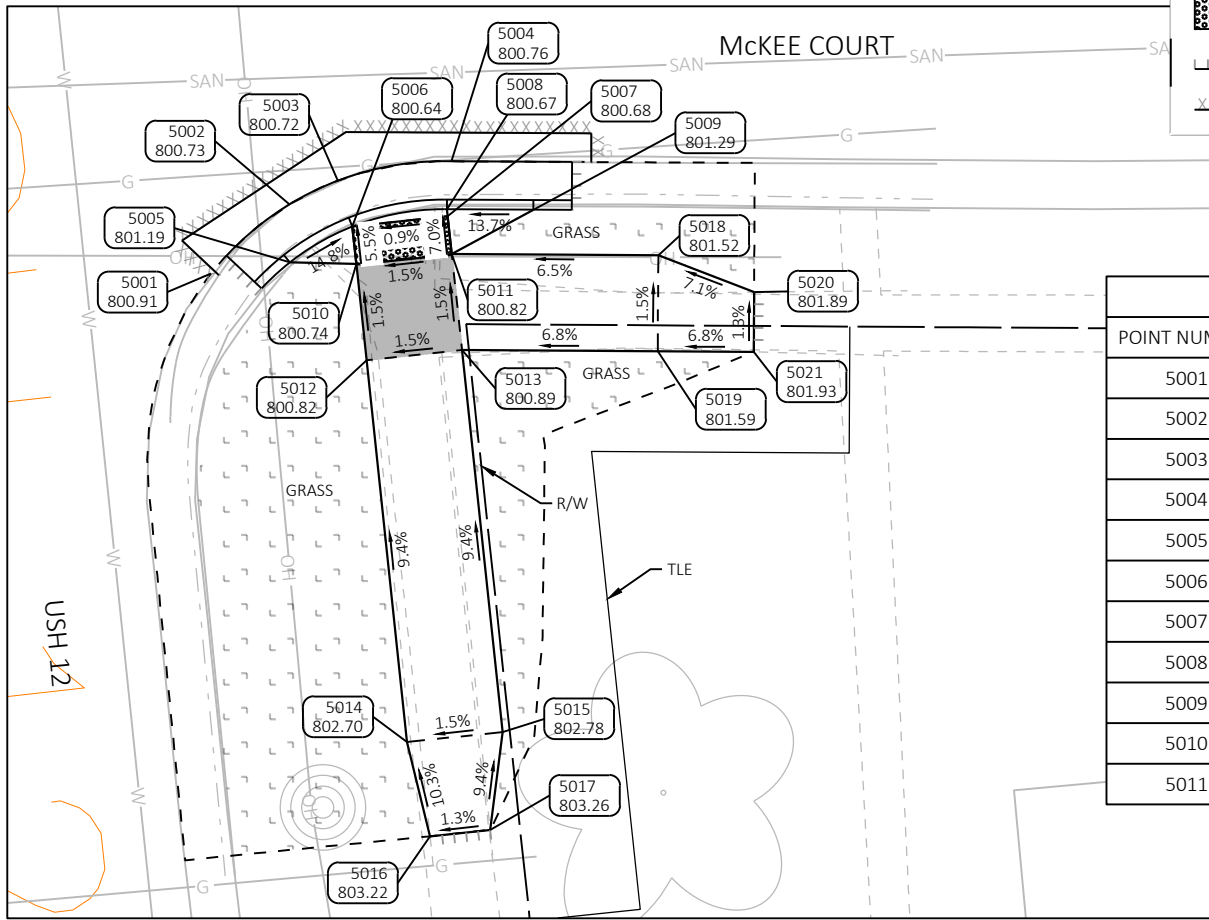


McKeeNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4901	518+30.30	33.24' LT	799.76	526996.80	848556.76
4902	518+29.39	27.24' LT	799.56	526997.07	848550.70
4903	518+28.35	24.62' LT	799.43	526997.83	848547.98
4904	518+24.78	19.95' LT	799.18	527000.88	848542.96
4905	518+19.12	16.64' LT	798.89	527006.16	848539.06
4906	518+13.12	15.79' LT	798.64	527012.04	848537.59
4907	518+28.19	37.22' LT	800.30	526999.32	848560.49
4908	518+27.74	32.74' LT	799.66	526999.30	848555.99
4909	518+26.94	32.74' LT	799.66	527000.09	848555.91
4910	518+26.93	27.74' LT	799.47	526999.57	848550.93
4911	518+25.94	25.41' LT	799.83	527000.31	848548.52
4912	518+15.27	18.35' LT	799.16	527010.17	848540.37
4913	518+11.01	18.29' LT	798.47	527014.40	848539.85
4914	518+06.01	18.29' LT	798.26	527019.37	848539.32
4915	518+03.00	18.28' LT	798.64	527022.36	848539.00

McKeeNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4916	518+23.65	27.74' LT	799.53	527002.83	848550.59
4917	518+23.66	32.74' LT	799.61	527003.35	848555.56
4918	518+18.62	27.74' LT	799.61	527007.83	848550.06
4919	518+18.63	32.74' LT	799.68	527008.36	848555.03
4920	518+11.01	27.75' LT	799.00	527015.40	848549.25
4921	518+11.01	32.75' LT	799.07	527015.93	848554.22
4922	518+06.01	27.75' LT	798.92	527020.37	848548.72
4923	518+06.01	32.75' LT	799.00	527020.90	848553.70
4924	518+01.01	27.83' LT	799.20	527025.35	848548.27
4925	518+01.01	31.74' LT	799.31	527025.77	848552.17
4926	518+24.80	42.86' LT	800.40	527003.29	848565.75
4927	518+19.83	43.42' LT	800.48	527008.29	848565.78
4928	518+25.36	47.83' LT	800.79	527003.26	848570.75
4929	518+21.50	48.27' LT	800.83	527007.14	848570.77

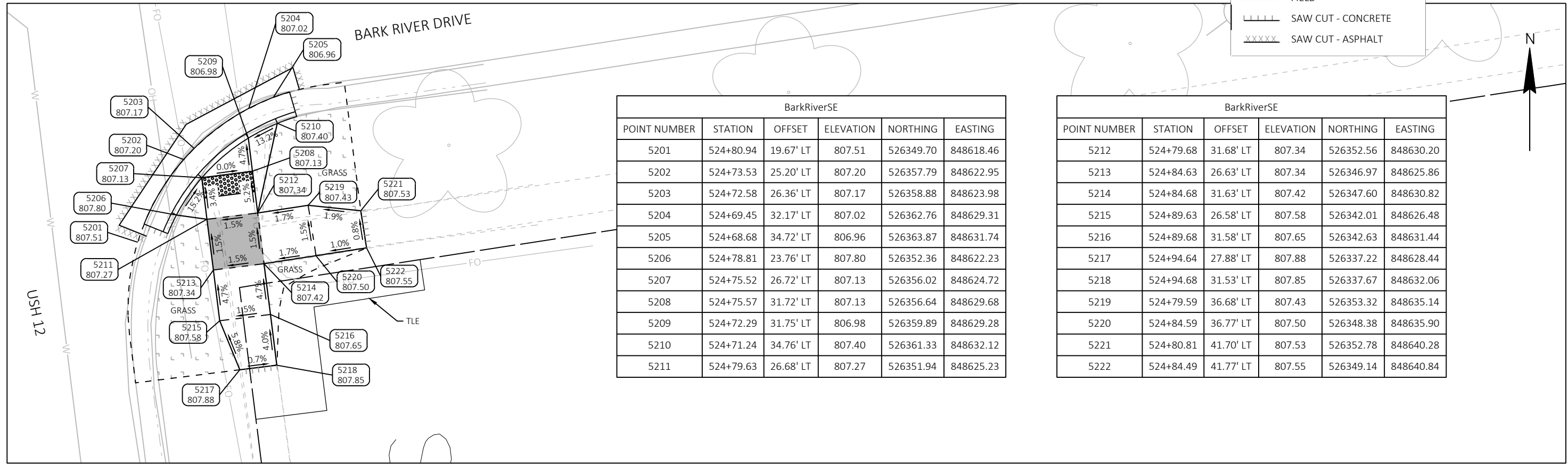
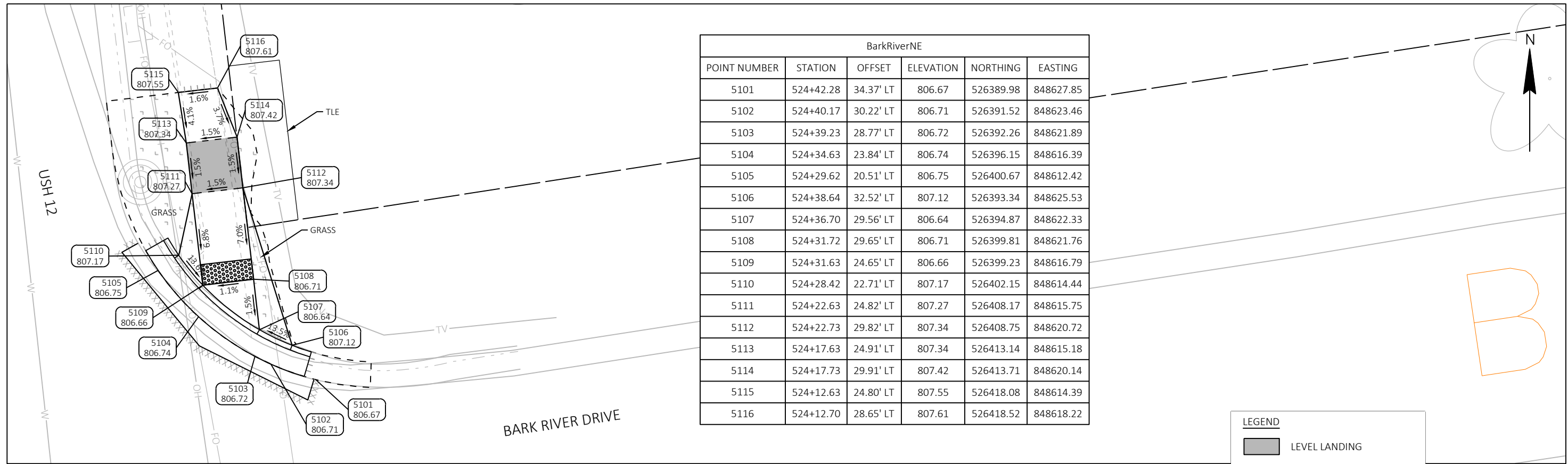
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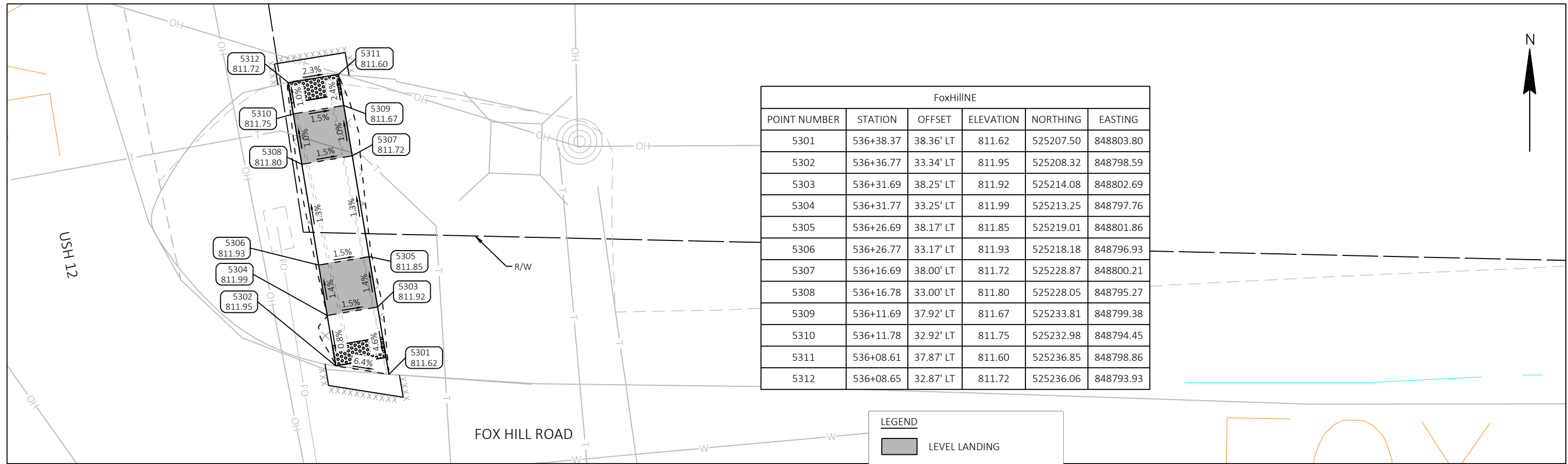
- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- SAW CUT - ASPHALT



McKeeSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
5001	518+57.86	20.15' LT	800.91	526968.01	848546.67
5002	518+54.71	24.59' LT	800.73	526971.62	848550.75
5003	518+53.76	27.23' LT	800.72	526972.84	848553.27
5004	518+53.37	33.23' LT	800.76	526973.86	848559.20
5005	518+57.71	24.29' LT	801.19	526968.60	848550.77
5006	518+56.20	27.73' LT	800.64	526970.47	848554.03
5007	518+56.20	32.73' LT	800.68	526970.99	848559.00
5008	518+55.83	32.73' LT	800.67	526971.36	848558.96
5009	518+56.33	37.19' LT	801.29	526971.34	848563.45
5010	518+58.16	27.72' LT	800.74	526968.51	848554.23
5011	518+58.17	32.72' LT	800.82	526969.04	848559.20

McKeeSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
5012	518+63.20	27.72' LT	800.82	526963.51	848554.76
5013	518+63.20	32.72' LT	800.89	526964.04	848559.74
5014	518+83.20	27.71' LT	802.70	526943.62	848556.88
5015	518+83.20	32.71' LT	802.78	526944.15	848561.85
5016	518+88.20	28.39' LT	803.22	526938.72	848558.07
5017	518+88.20	31.53' LT	803.26	526939.05	848561.21
5018	518+59.37	43.42' LT	801.52	526968.97	848569.96
5019	518+64.34	42.86' LT	801.59	526963.97	848569.93
5020	518+61.82	48.17' LT	801.89	526967.05	848574.95
5021	518+64.90	47.82' LT	801.93	526963.94	848574.93

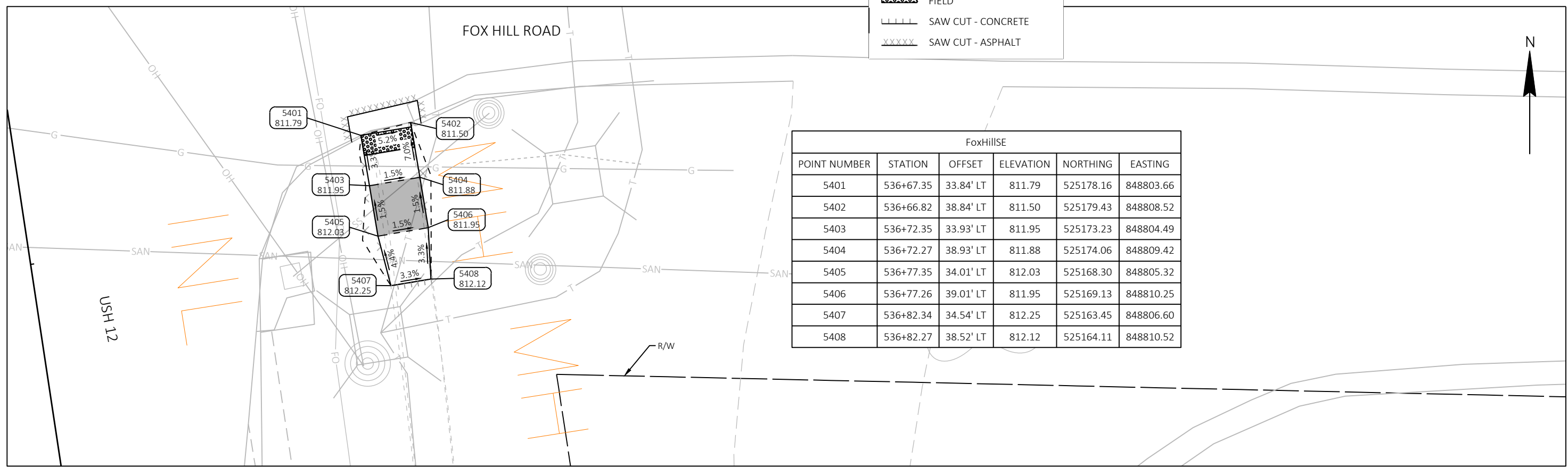




FoxHillNE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
5301	536+38.37	38.36' LT	811.62	525207.50	848803.80
5302	536+36.77	33.34' LT	811.95	525208.32	848798.59
5303	536+31.69	38.25' LT	811.92	525214.08	848802.69
5304	536+31.77	33.25' LT	811.99	525213.25	848797.76
5305	536+26.69	38.17' LT	811.85	525219.01	848801.86
5306	536+26.77	33.17' LT	811.93	525218.18	848796.93
5307	536+16.69	38.00' LT	811.72	525228.87	848800.21
5308	536+16.78	33.00' LT	811.80	525228.05	848795.27
5309	536+11.69	37.92' LT	811.67	525233.81	848799.38
5310	536+11.78	32.92' LT	811.75	525232.98	848794.45
5311	536+08.61	37.87' LT	811.60	525236.85	848798.86
5312	536+08.65	32.87' LT	811.72	525236.06	848793.93

LEGEND

- LEVEL LANDING
- DETECTABLE WARNING FIELD
- SAW CUT - CONCRETE
- XXXXXX SAW CUT - ASPHALT



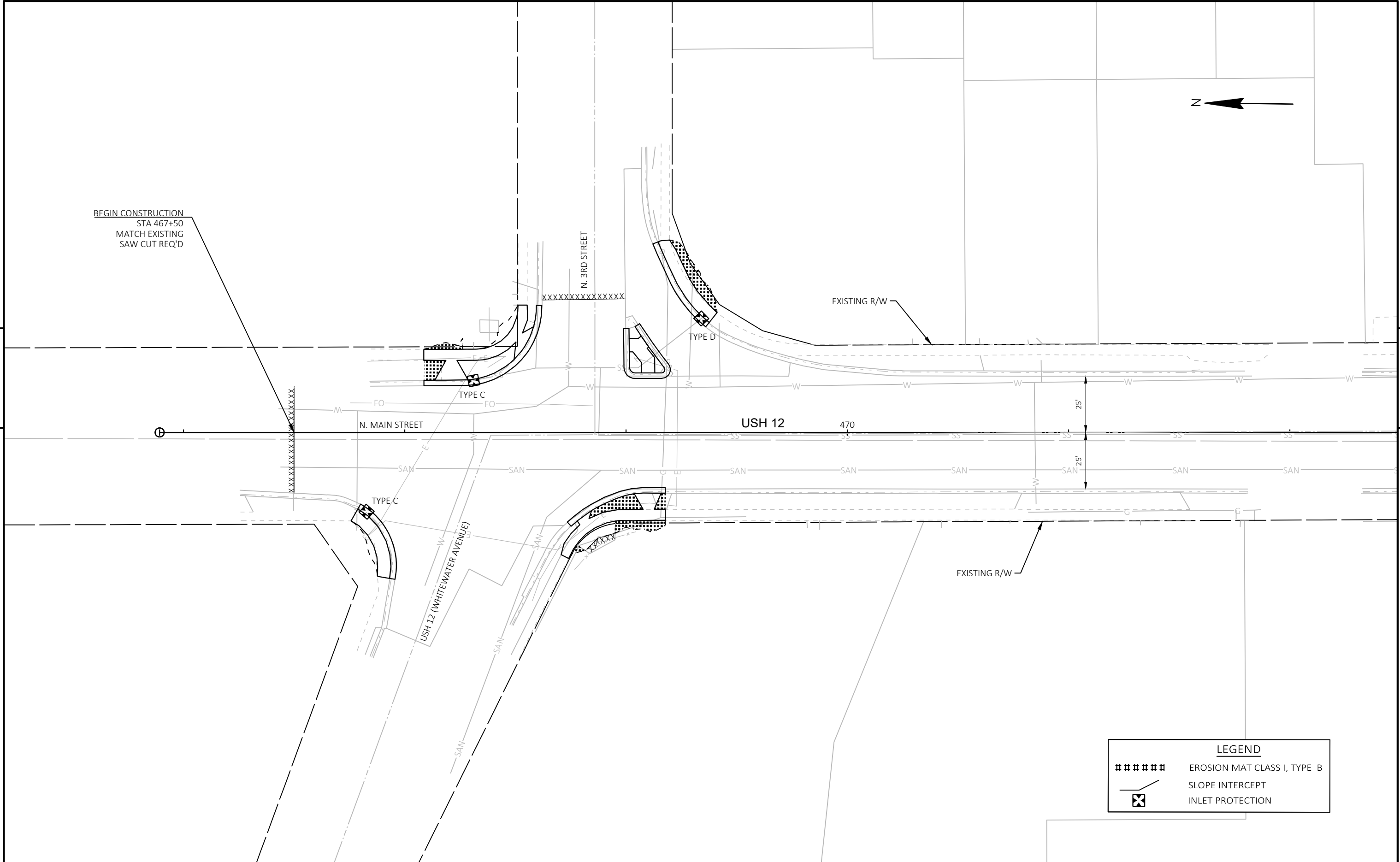
FoxHillSE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
5401	536+67.35	33.84' LT	811.79	525178.16	848803.66
5402	536+66.82	38.84' LT	811.50	525179.43	848808.52
5403	536+72.35	33.93' LT	811.95	525173.23	848804.49
5404	536+72.27	38.93' LT	811.88	525174.06	848809.42
5405	536+77.35	34.01' LT	812.03	525168.30	848805.32
5406	536+77.26	39.01' LT	811.95	525169.13	848810.25
5407	536+82.34	34.54' LT	812.25	525163.45	848806.60
5408	536+82.27	38.52' LT	812.12	525164.11	848810.52





BEGIN CONSTRUCTION
STA 467+50
MATCH EXISTING
SAW CUT REQ'D

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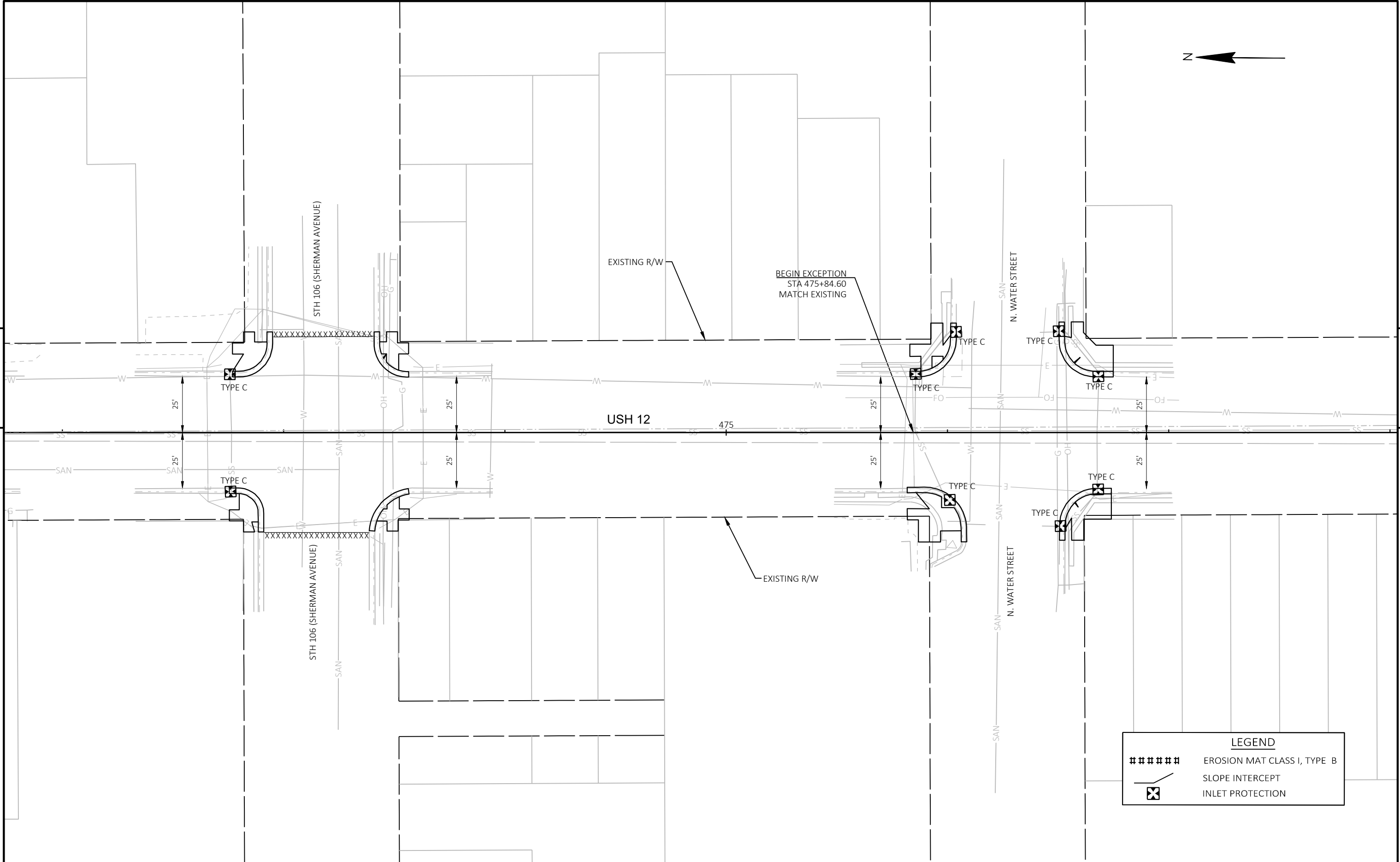
LEGEND

- ##### EROSION MAT CLASS I, TYPE B
-  SLOPE INTERCEPT
-  INLET PROTECTION



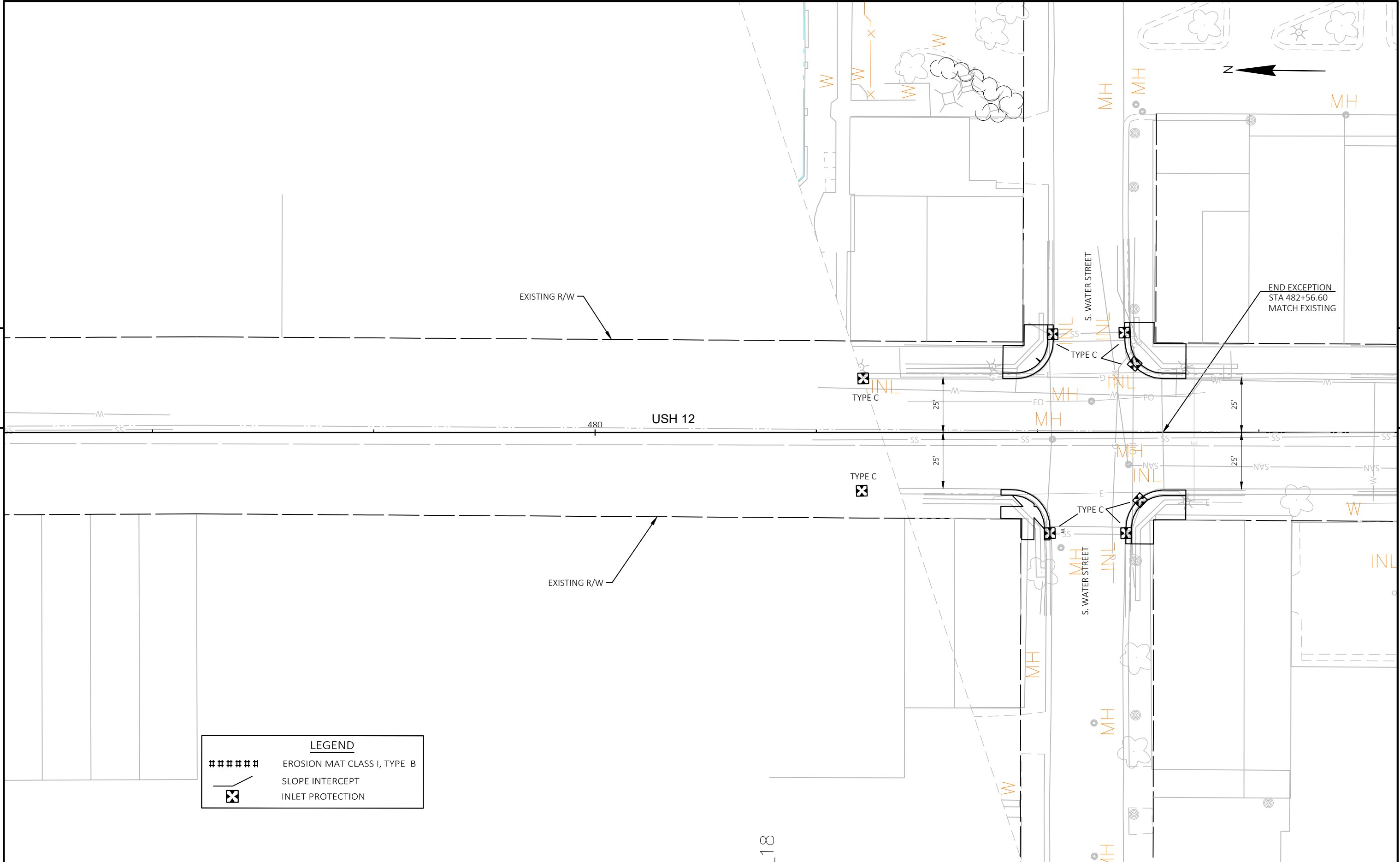
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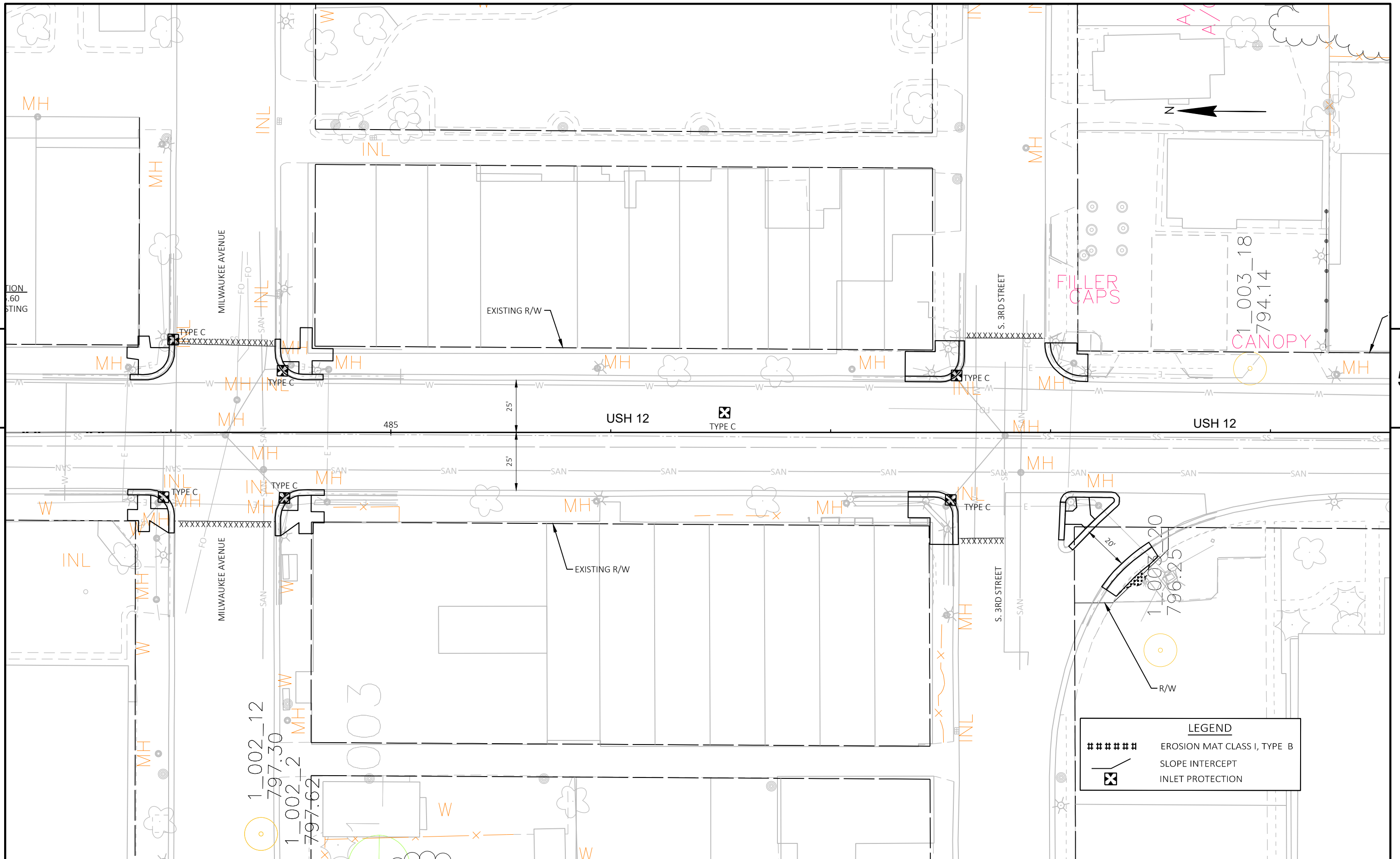


LEGEND

- ##### EROSION MAT CLASS I, TYPE B
- /— SLOPE INTERCEPT
- ⊗ INLET PROTECTION



LEGEND	
#####	EROSION MAT CLASS I, TYPE B
—/—	SLOPE INTERCEPT
⊗	INLET PROTECTION



LEGEND	
#####	EROSION MAT CLASS I, TYPE B
—/—	SLOPE INTERCEPT
⊗	INLET PROTECTION

PROJECT NO: 3575-02-73

HWY: USH 12

COUNTY: JEFFERSON

EROSION CONTROL

SHEET

E

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LAYOUT NAME - 04

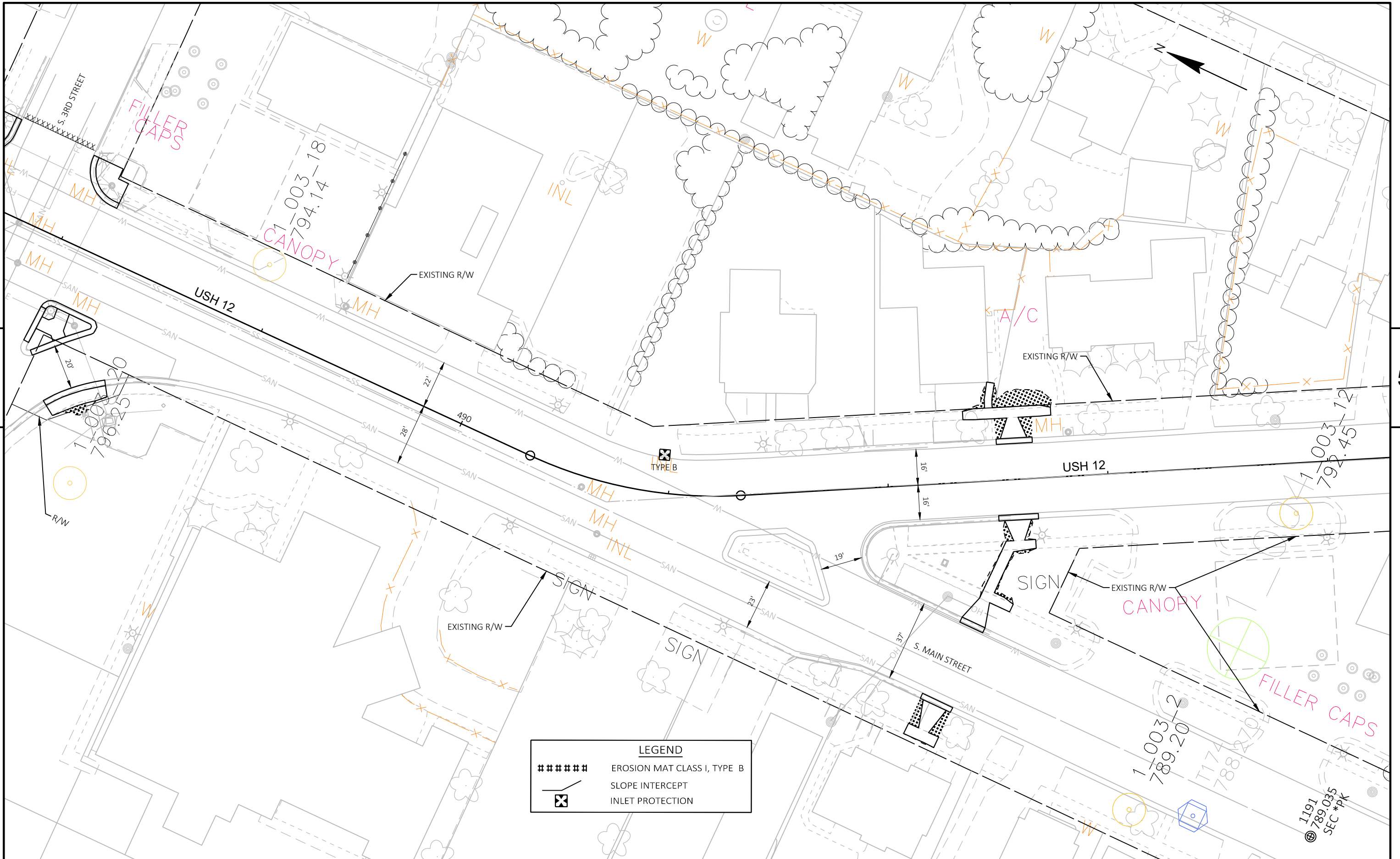
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PLOT BY : WHITE, JEREMY J

PLOT NAME :

PLOT SCALE : 1 IN=40 FT

WISDOT/CADD SHEET 44



LEGEND	
#####	EROSION MAT CLASS I, TYPE B
—/—	SLOPE INTERCEPT
⊗	INLET PROTECTION

PROJECT NO: 3575-02-73

HWY: USH 12

COUNTY: JEFFERSON

EROSION CONTROL

SHEET

E

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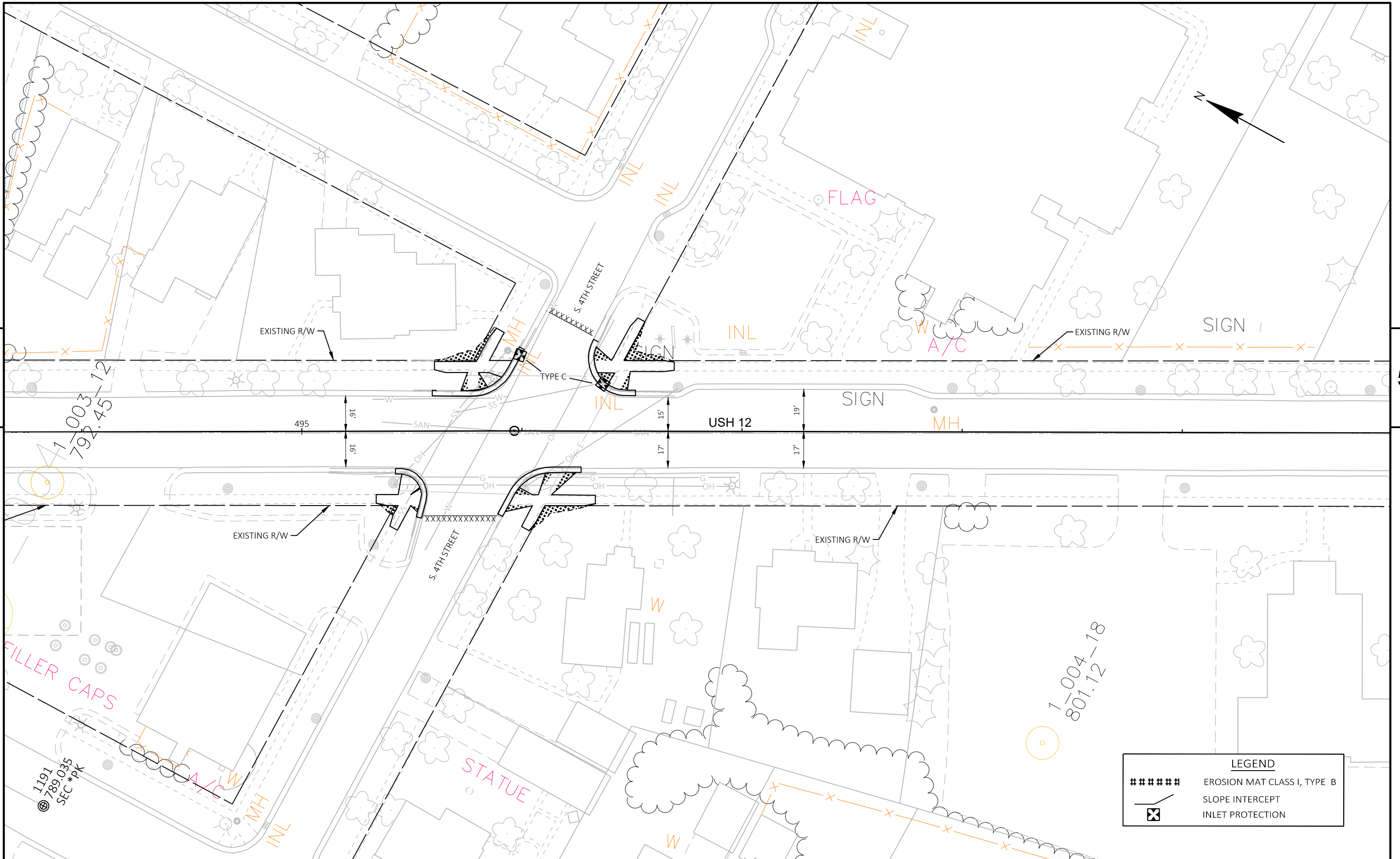
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PLOT NAME :

PLOT SCALE : 1 IN=40 FT

WISDOT/CADD SHEET 44



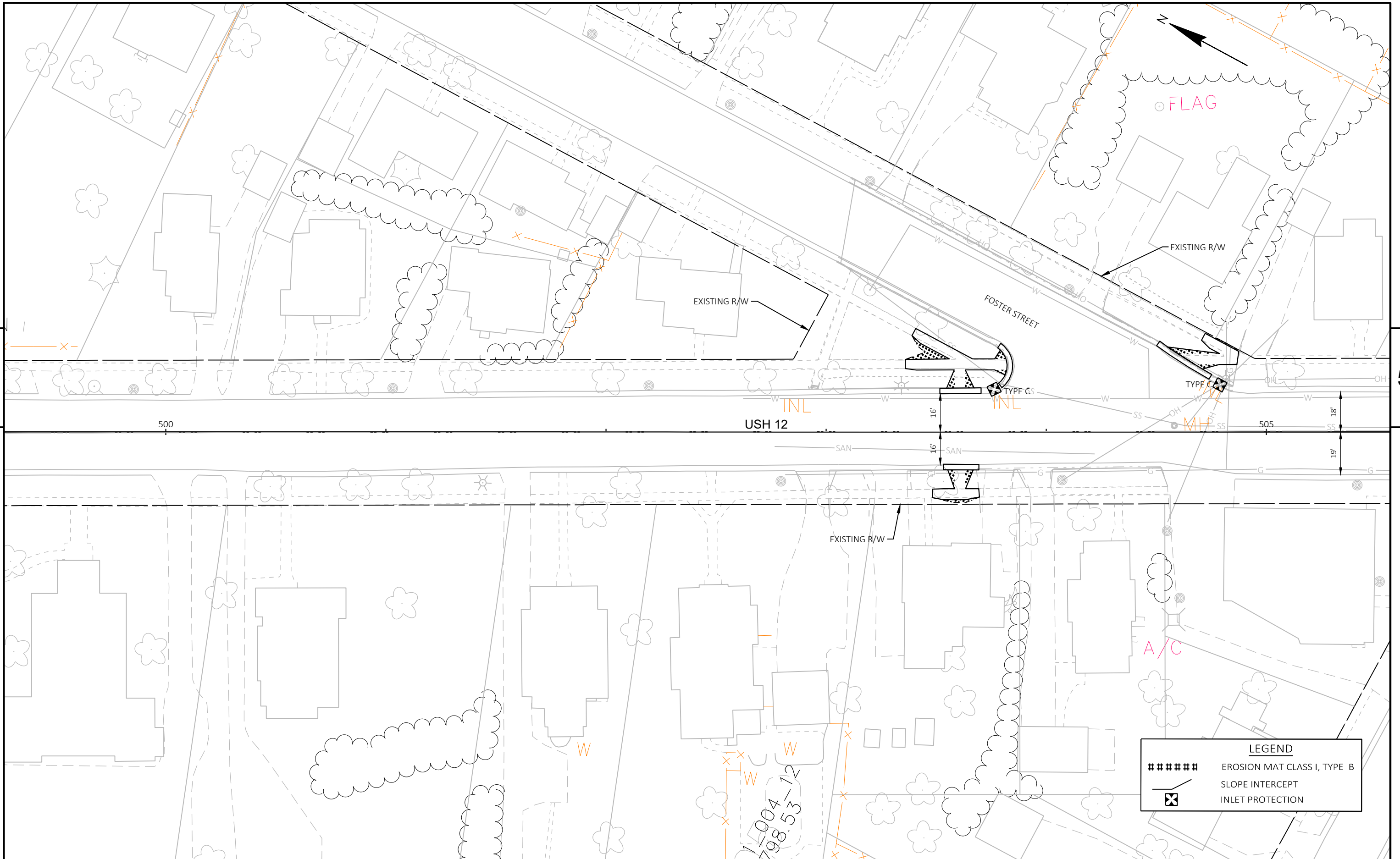
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	EROSION CONTROL	SHEET	E
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LEGEND

- ##### EROSION MAT CLASS I, TYPE B
- SLOPE INTERCEPT
- X INLET PROTECTION



5

5

PROJECT NO: 3575-02-73

HWY: USH 12

COUNTY: JEFFERSON

EROSION CONTROL

SHEET

E

FILE NAME : N:\PDS\C3D\35750204\SHEETSP\022001-EROSION CONTROL\022001-EC.DWG
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PLOT DATE : 10/30/2022 9:36 PM

PLOT BY : WHITE, JEREMY J

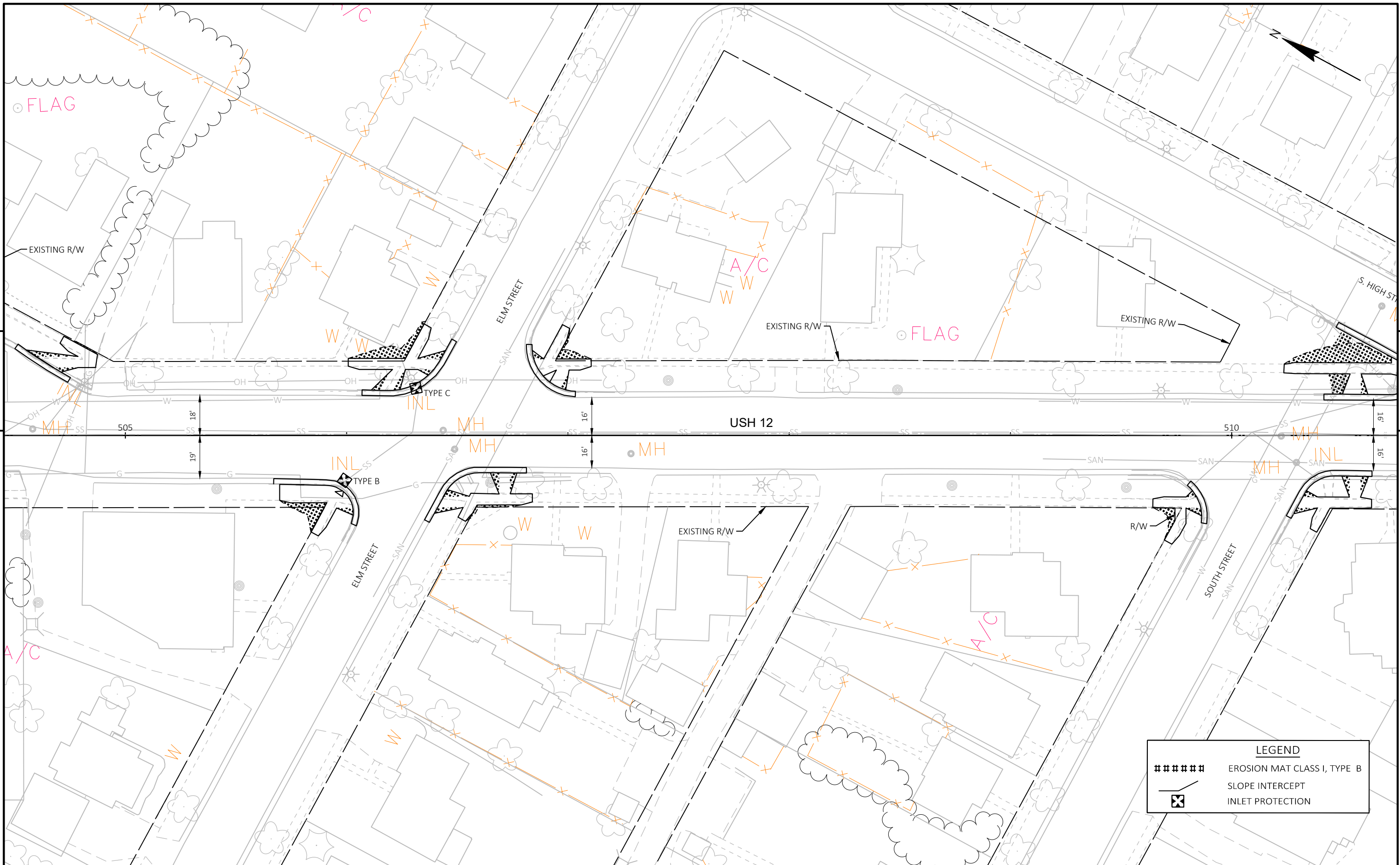
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WISDOT/CADDs SHEET 44

LEGEND

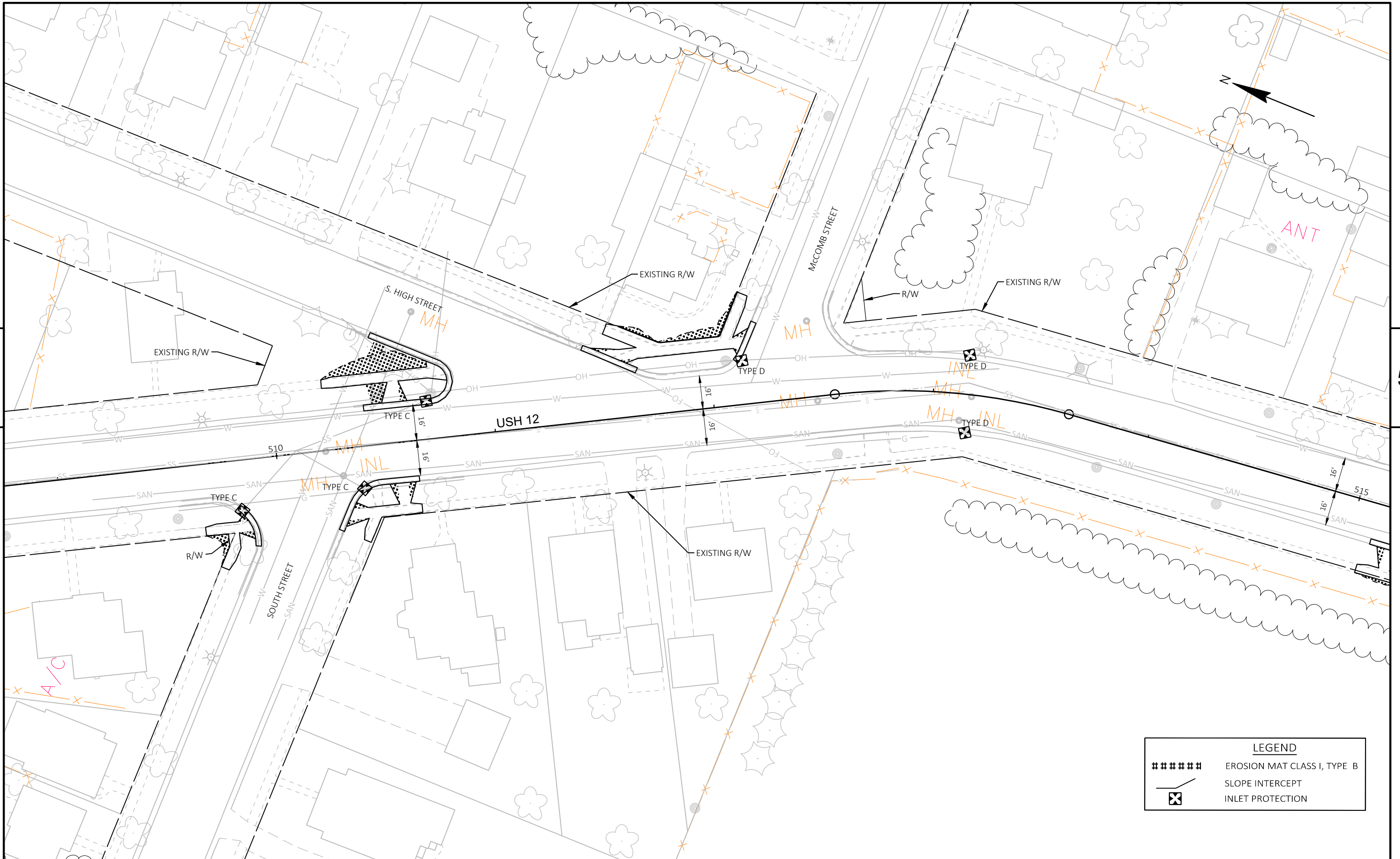
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- /— SLOPE INTERCEPT
- ⊗ INLET PROTECTION



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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	EROSION CONTROL	SHEET	E
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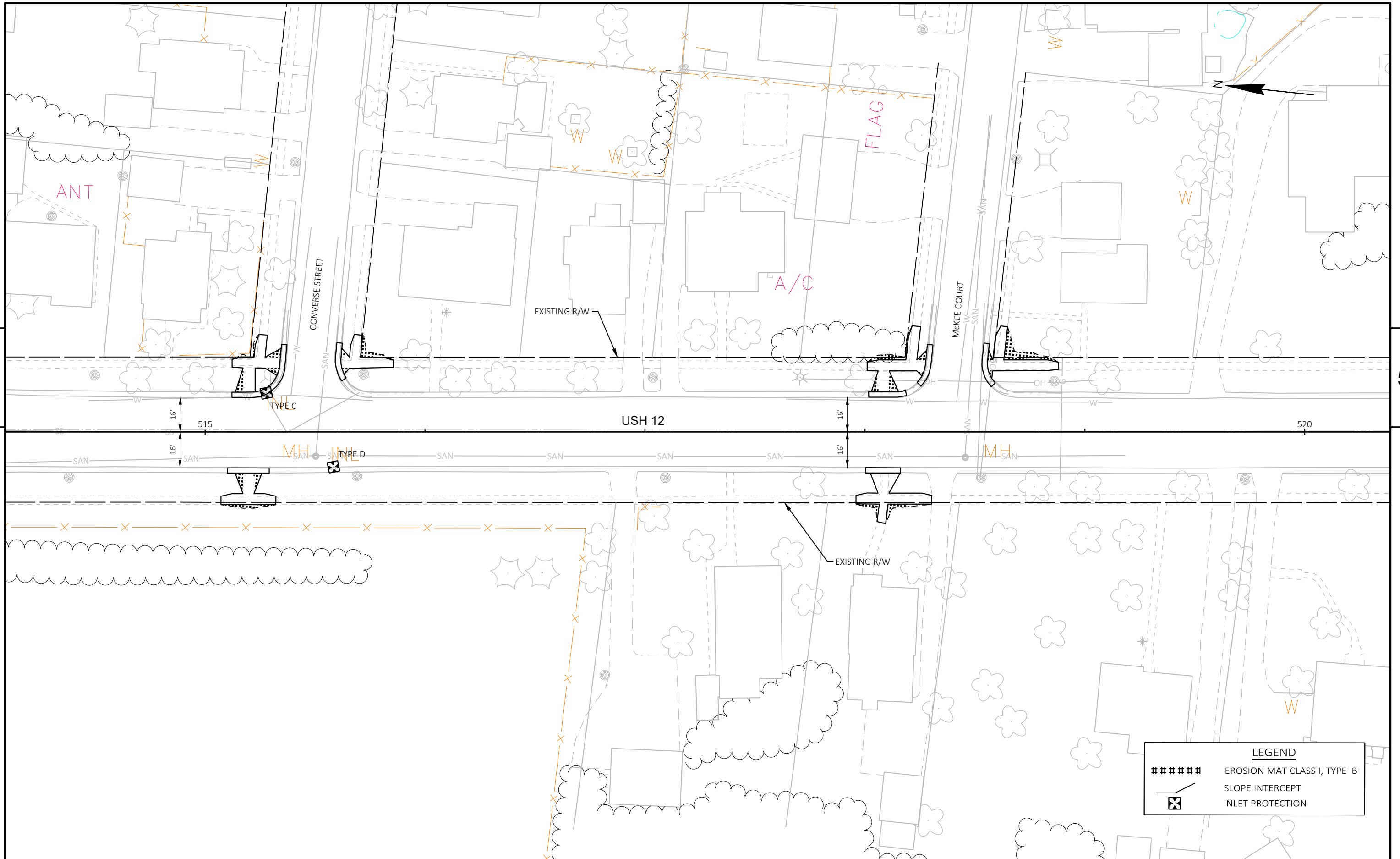


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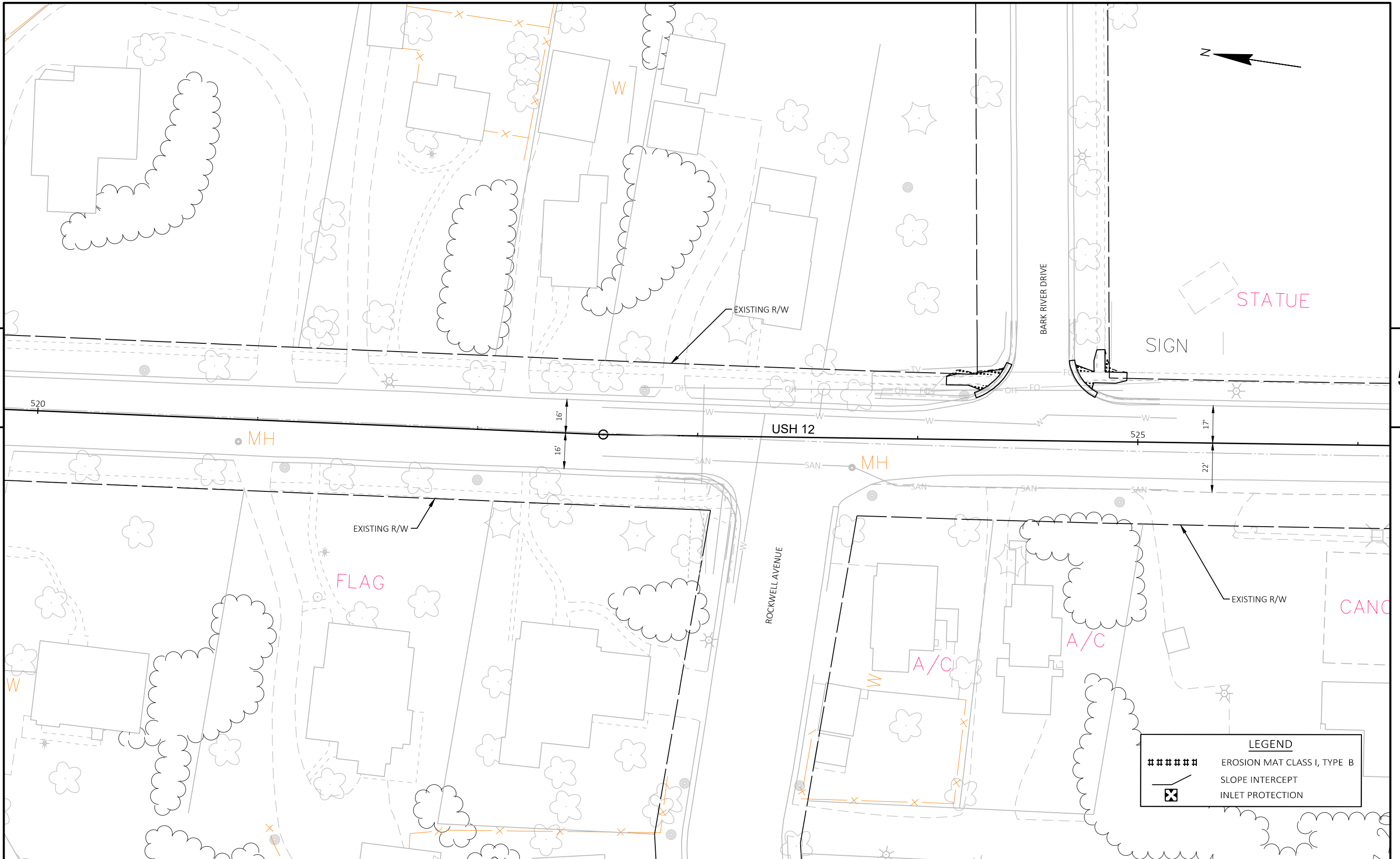
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LEGEND

- ##### EROSION MAT CLASS I, TYPE B
- /— SLOPE INTERCEPT
- ☒ INLET PROTECTION



LEGEND	
#####	EROSION MAT CLASS I, TYPE B
—	SLOPE INTERCEPT
⊗	INLET PROTECTION



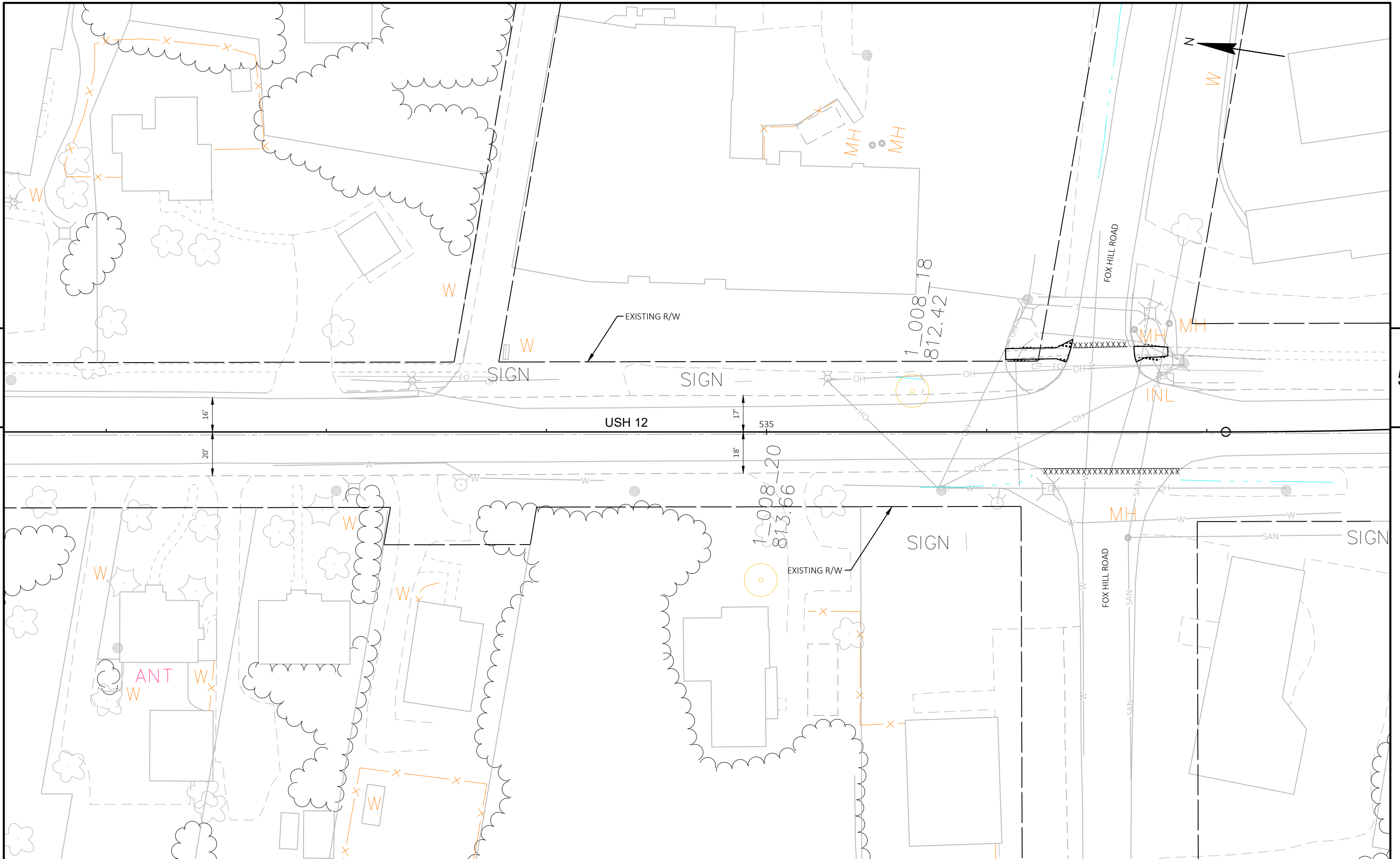
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	EROSION CONTROL	SHEET	E
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LEGEND

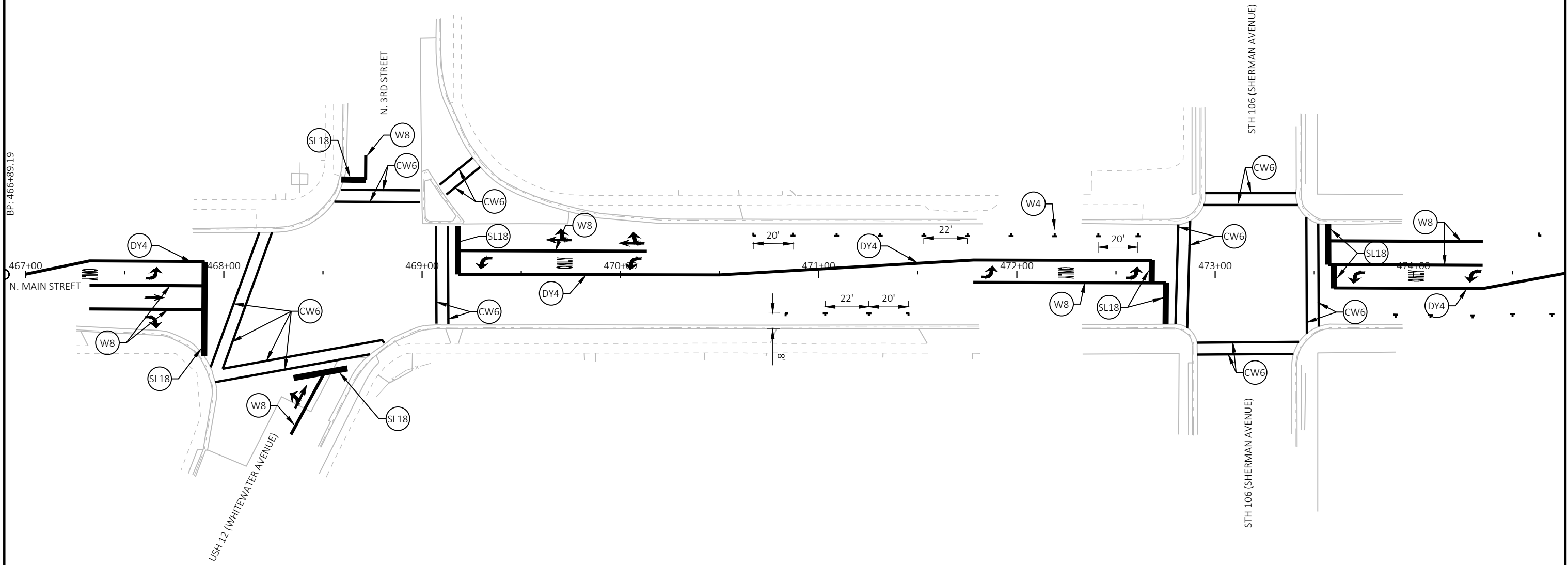
- ##### EROSION MAT CLASS I, TYPE B
- Slope Intercept
- ⊗ Inlet Protection



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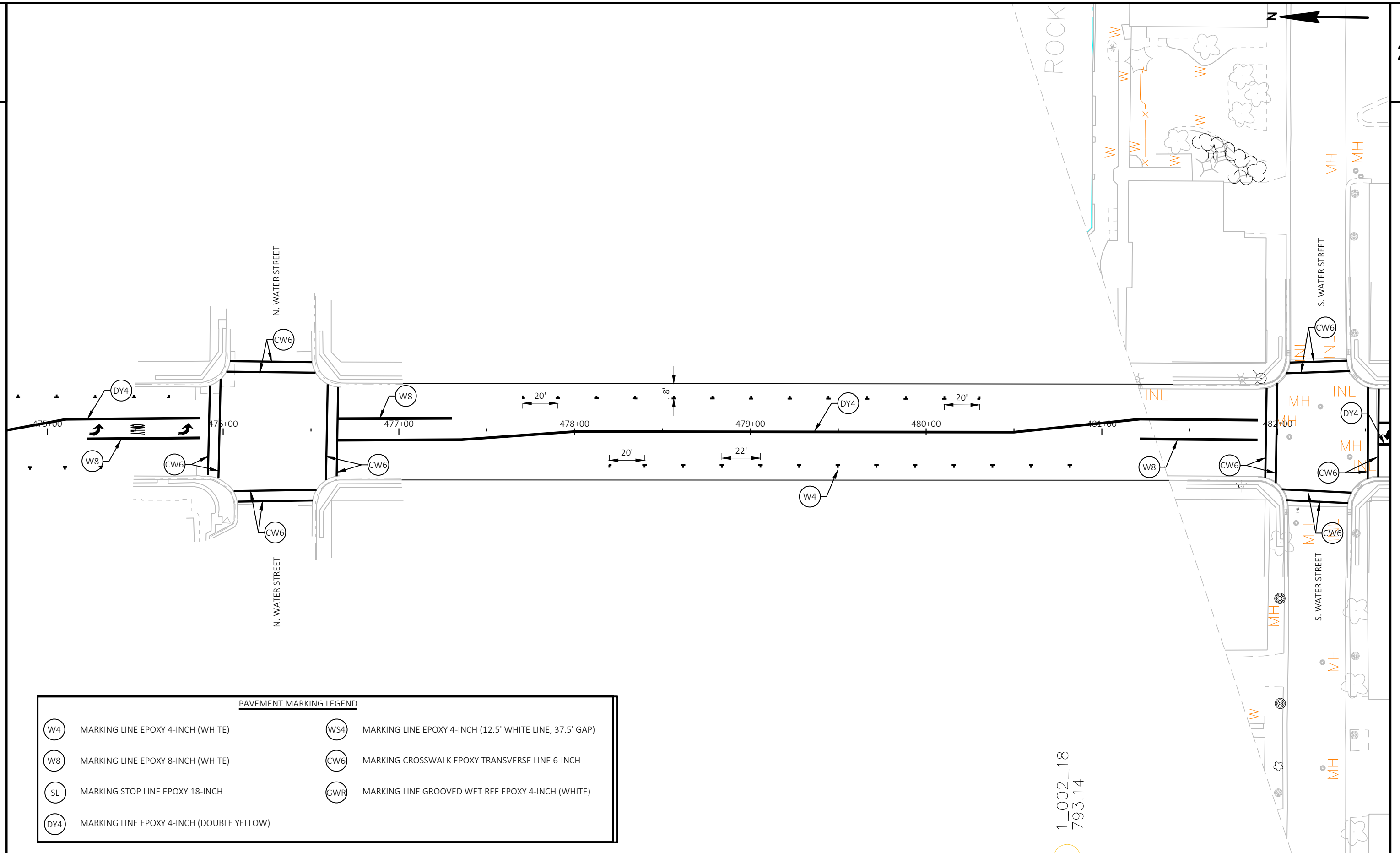
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	EROSION CONTROL	SHEET	E
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PAVEMENT MARKING LEGEND

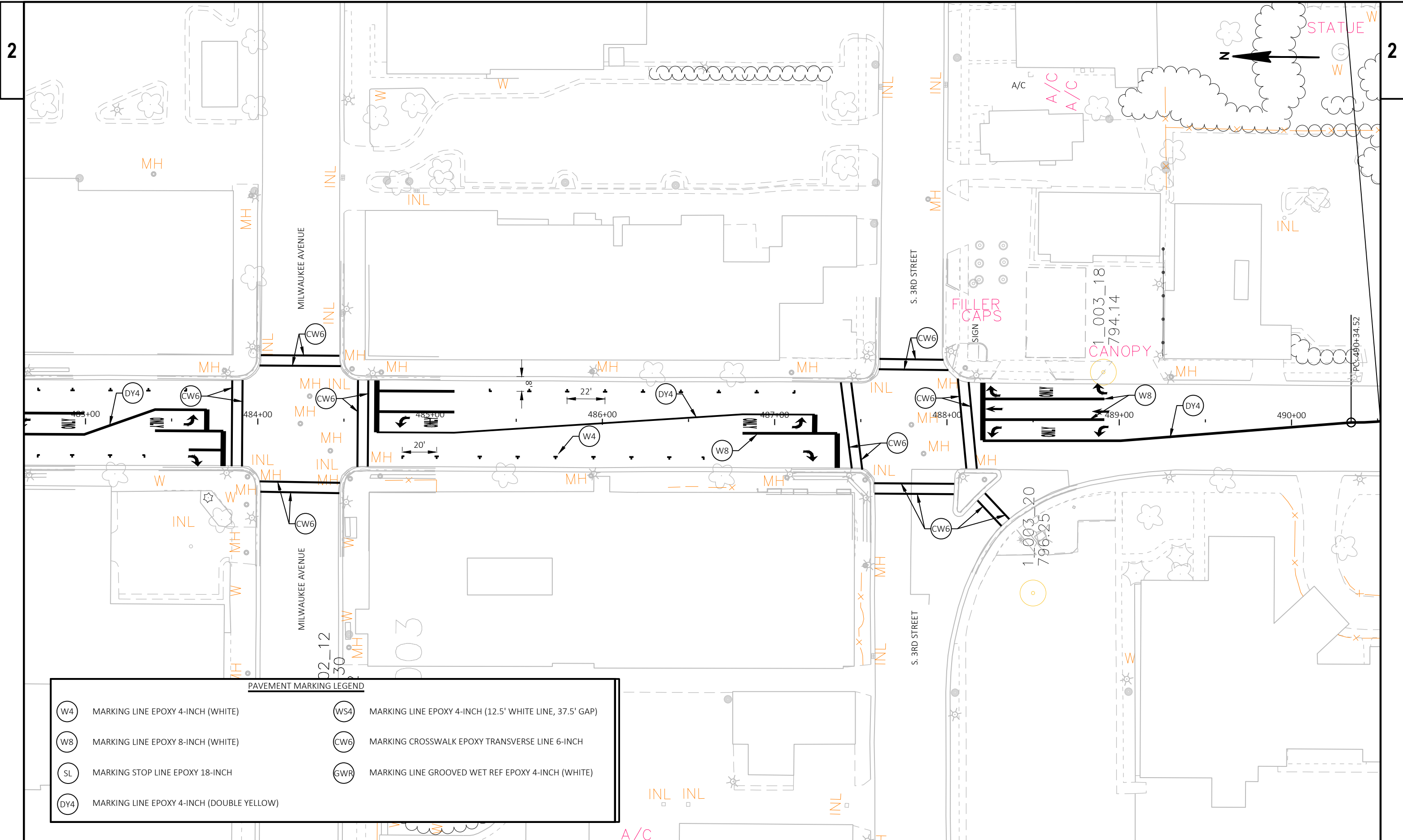
(W4)	MARKING LINE EPOXY 4-INCH (WHITE)	(WS4)	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
(W8)	MARKING LINE EPOXY 8-INCH (WHITE)	(CW6)	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
(SL)	MARKING STOP LINE EPOXY 18-INCH	(GWR)	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
(DY4)	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		



PAVEMENT MARKING LEGEND

- | | |
|---|---|
| (W4) MARKING LINE EPOXY 4-INCH (WHITE) | (WS4) MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP) |
| (W8) MARKING LINE EPOXY 8-INCH (WHITE) | (CW6) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH |
| (SL) MARKING STOP LINE EPOXY 18-INCH | (GWR) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE) |
| (DY4) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW) | |

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793.14



PAVEMENT MARKING LEGEND

- | | | | |
|-------|---|-------|---|
| (W4) | MARKING LINE EPOXY 4-INCH (WHITE) | (WS4) | MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP) |
| (W8) | MARKING LINE EPOXY 8-INCH (WHITE) | (CW6) | MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH |
| (SL) | MARKING STOP LINE EPOXY 18-INCH | (GWR) | MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE) |
| (DY4) | MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW) | | |

PROJECT NO: 3575-02-73

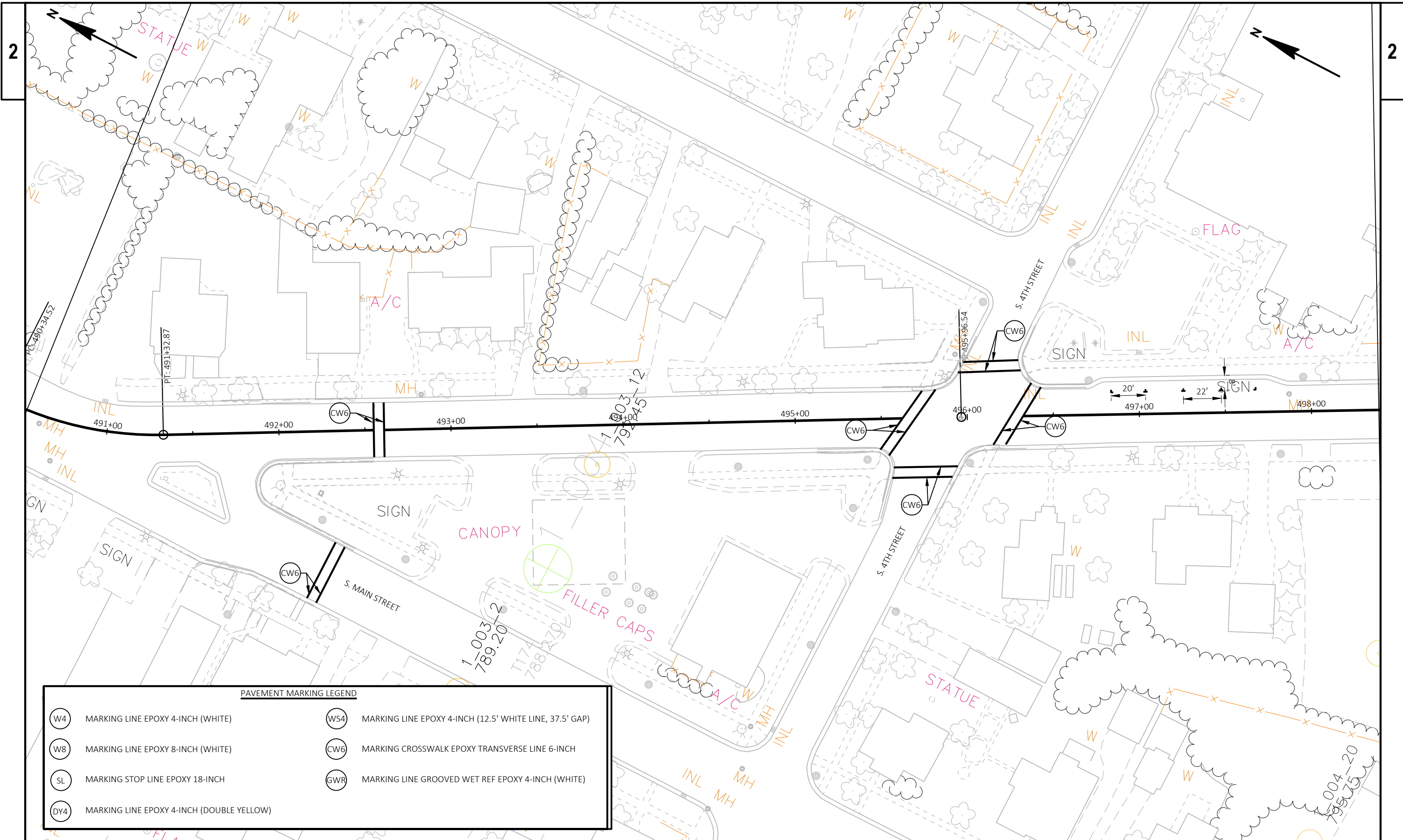
HWY: USH 12

COUNTY: JEFFERSON

PAVEMENT MARKING

SHEET

E



PAVEMENT MARKING LEGEND

(W4)	MARKING LINE EPOXY 4-INCH (WHITE)	(WS4)	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
(W8)	MARKING LINE EPOXY 8-INCH (WHITE)	(CW6)	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
(SL)	MARKING STOP LINE EPOXY 18-INCH	(GWR)	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
(DY4)	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		

PROJECT NO: 3575-02-73

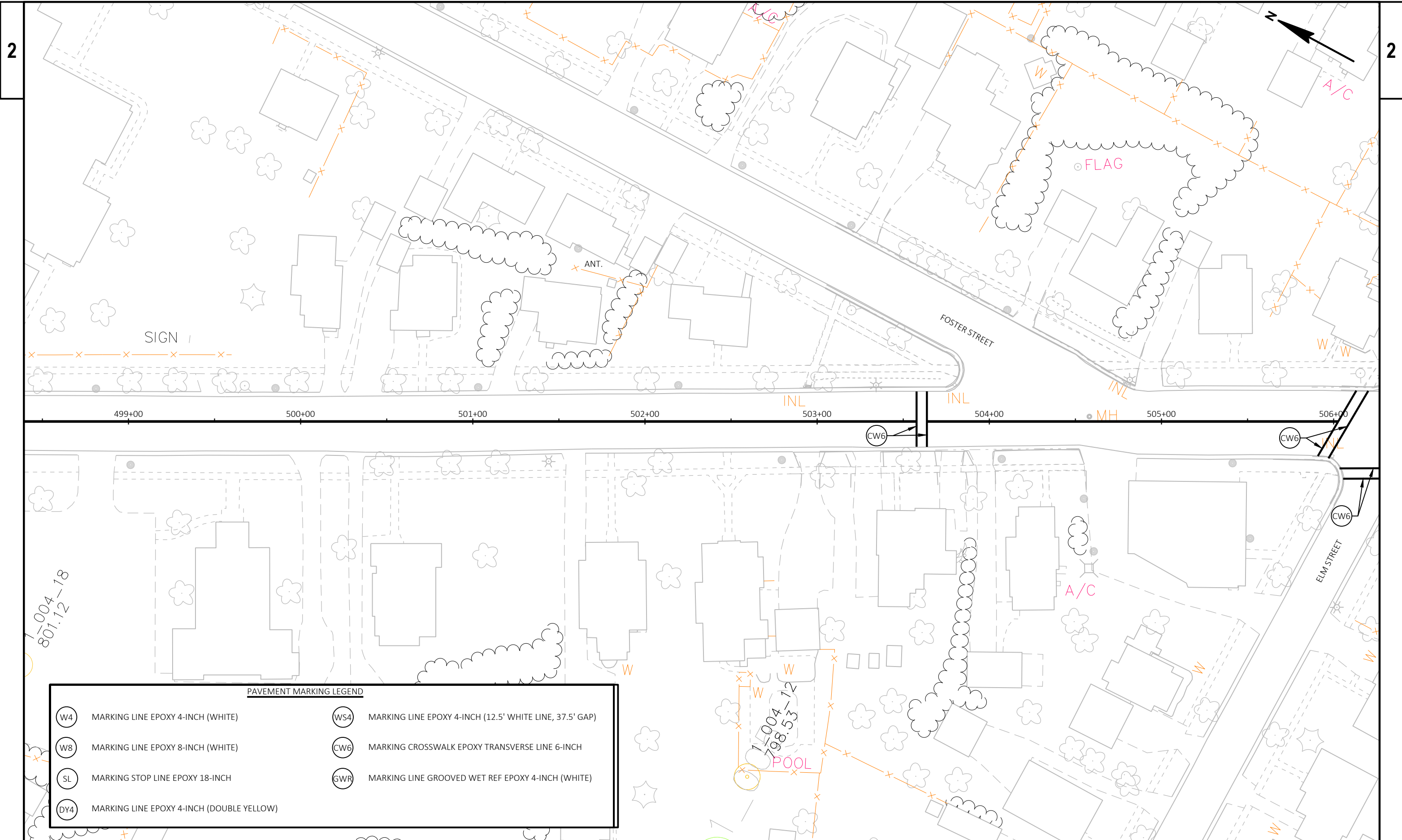
HWY: USH 12

COUNTY: JEFFERSON

PAVEMENT MARKING

SHEET

E



PAVEMENT MARKING LEGEND

(W4)	MARKING LINE EPOXY 4-INCH (WHITE)	(WS4)	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
(W8)	MARKING LINE EPOXY 8-INCH (WHITE)	(CW6)	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
(SL)	MARKING STOP LINE EPOXY 18-INCH	(GWR)	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
(DY4)	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		

PROJECT NO: 3575-02-73

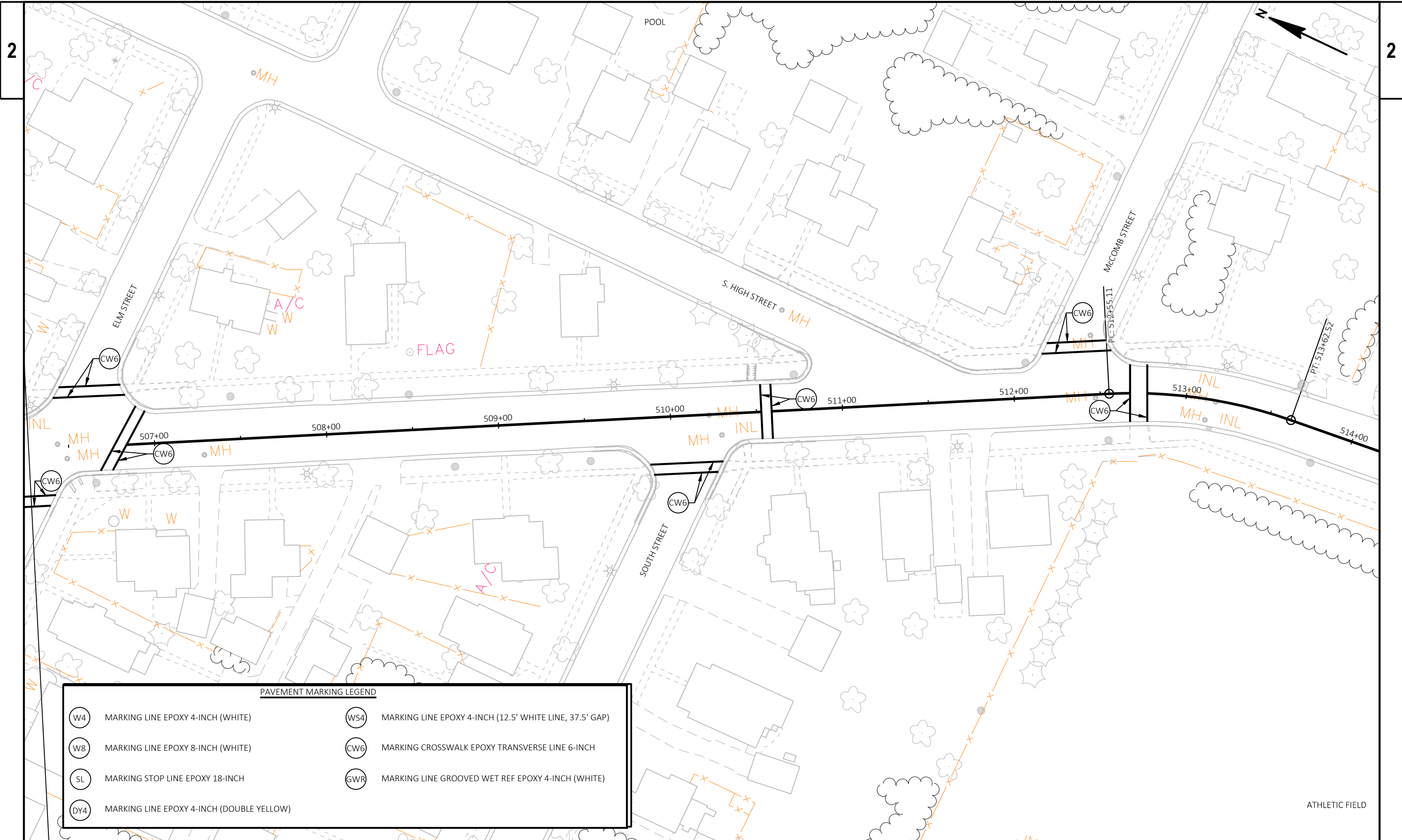
HWY: USH 12

COUNTY: JEFFERSON

PAVEMENT MARKING

SHEET

E



PAVEMENT MARKING LEGEND

(W4)	MARKING LINE EPOXY 4-INCH (WHITE)	(WS4)	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
(W8)	MARKING LINE EPOXY 8-INCH (WHITE)	(CW6)	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
(SL)	MARKING STOP LINE EPOXY 18-INCH	(GWR)	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
(DY4)	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		

PROJECT NO: 3575-02-73

HWY: USH 12

COUNTY: JEFFERSON

PAVEMENT MARKING

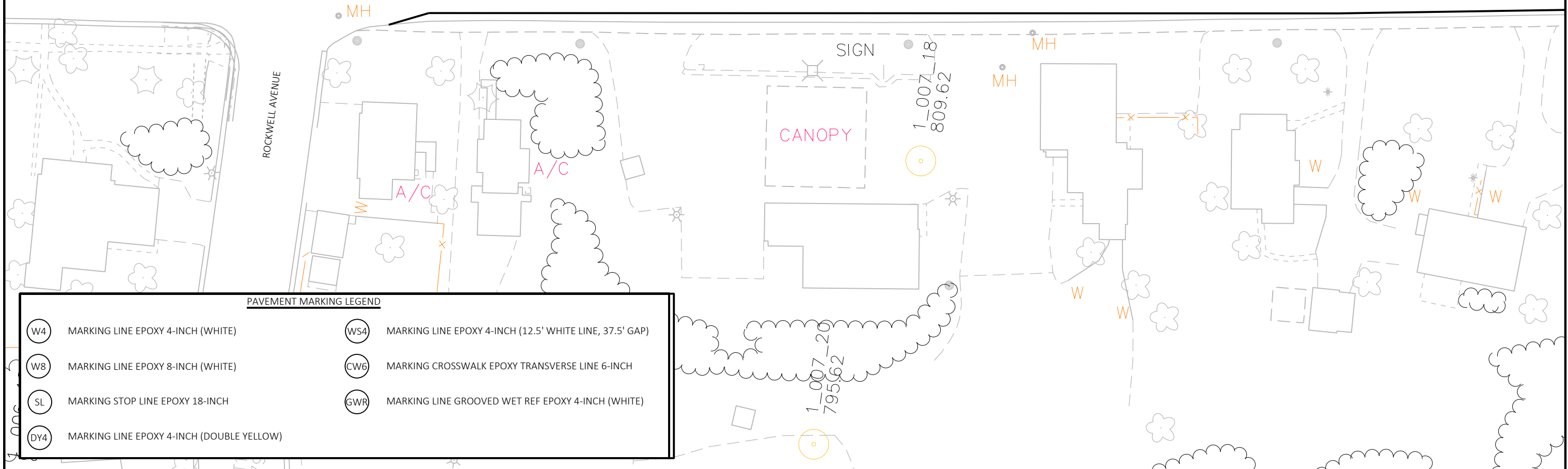
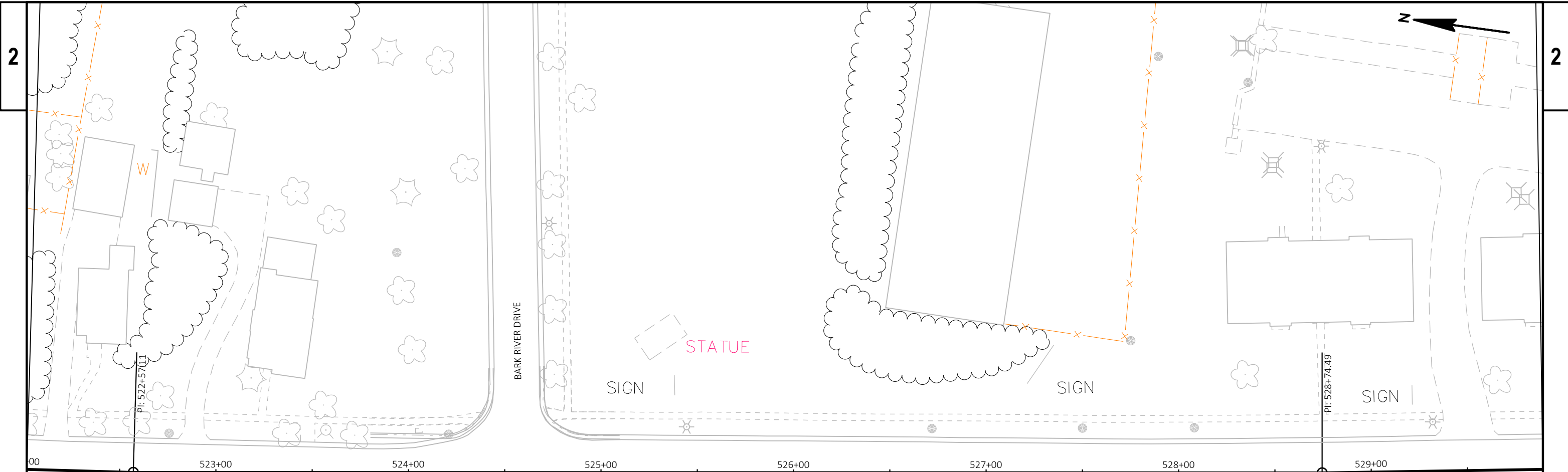
SHEET

E



PAVEMENT MARKING LEGEND

(W4)	MARKING LINE EPOXY 4-INCH (WHITE)	(WS4)	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
(W8)	MARKING LINE EPOXY 8-INCH (WHITE)	(CW6)	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
(SL)	MARKING STOP LINE EPOXY 18-INCH	(GWR)	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
(DY4)	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		



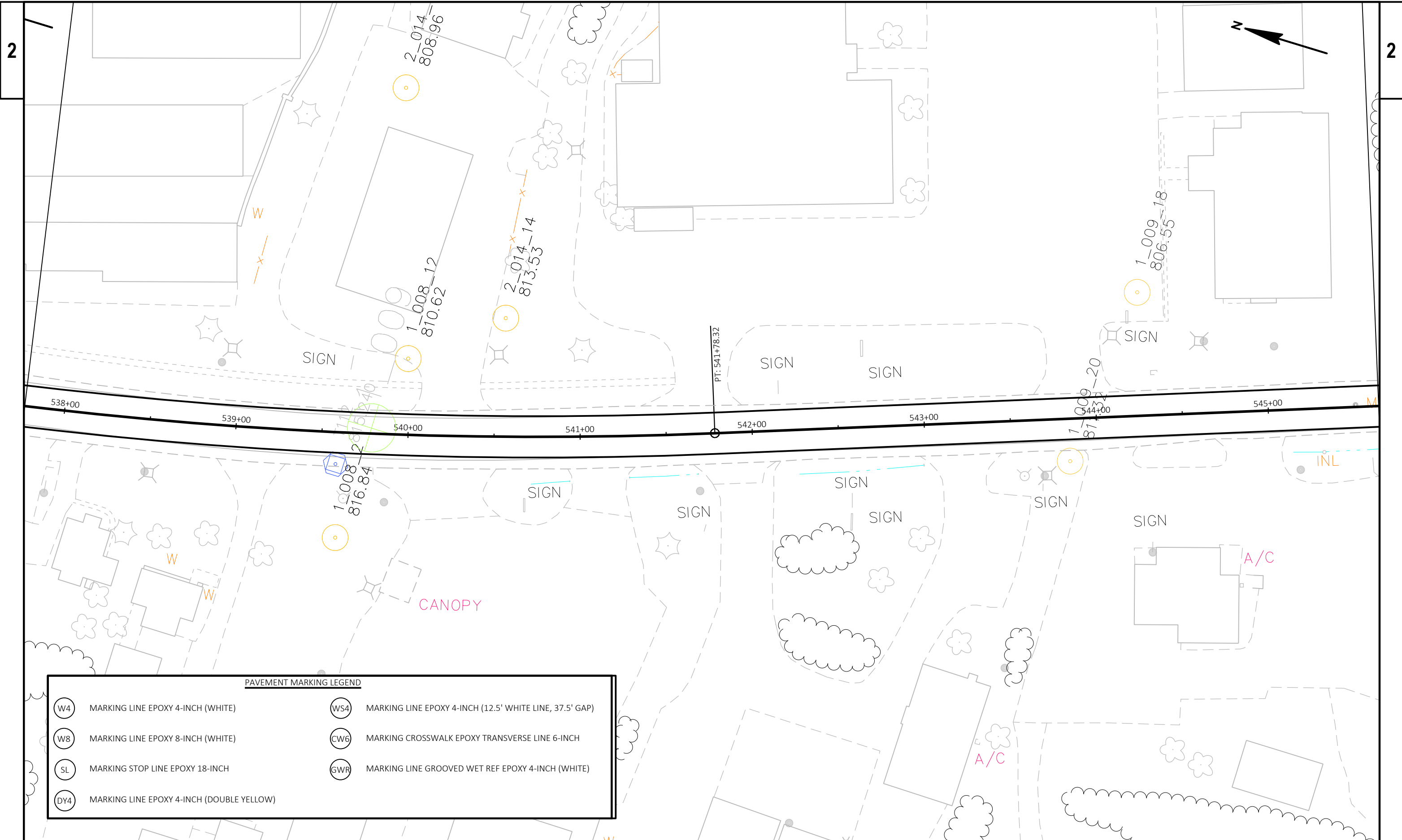
PAVEMENT MARKING LEGEND

(W4)	MARKING LINE EPOXY 4-INCH (WHITE)	(WS4)	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
(W8)	MARKING LINE EPOXY 8-INCH (WHITE)	(CW6)	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
(SL)	MARKING STOP LINE EPOXY 18-INCH	(GWR)	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
(DY4)	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		



PAVEMENT MARKING LEGEND

(W4)	MARKING LINE EPOXY 4-INCH (WHITE)	(WS4)	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
(W8)	MARKING LINE EPOXY 8-INCH (WHITE)	(CW6)	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
(SL)	MARKING STOP LINE EPOXY 18-INCH	(GWR)	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
(DY4)	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		



PAVEMENT MARKING LEGEND

(W4)	MARKING LINE EPOXY 4-INCH (WHITE)	(WS4)	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
(W8)	MARKING LINE EPOXY 8-INCH (WHITE)	(CW6)	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
(SL)	MARKING STOP LINE EPOXY 18-INCH	(GWR)	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
(DY4)	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		

PROJECT NO: 3575-02-73

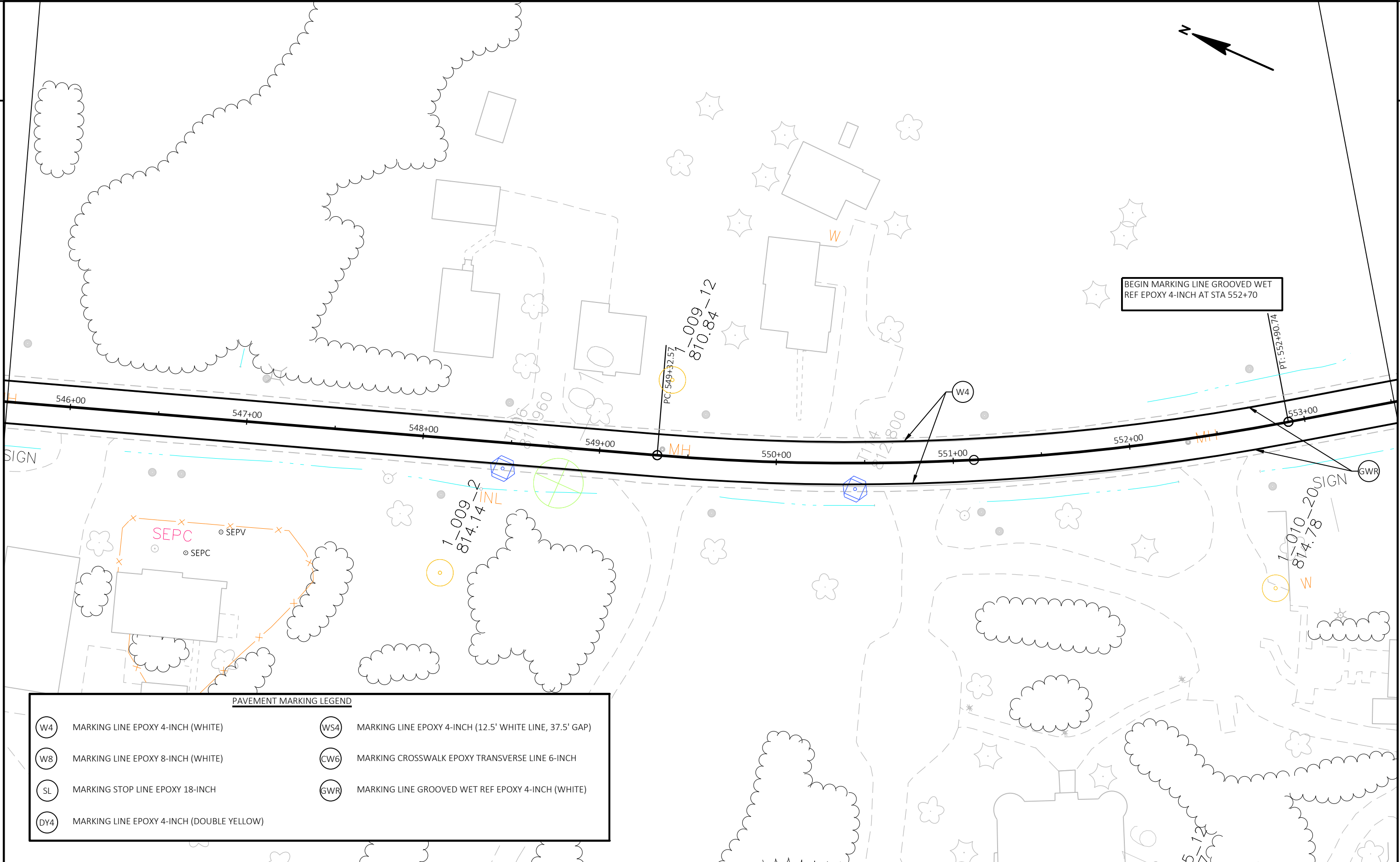
HWY: USH 12

COUNTY: JEFFERSON

PAVEMENT MARKING

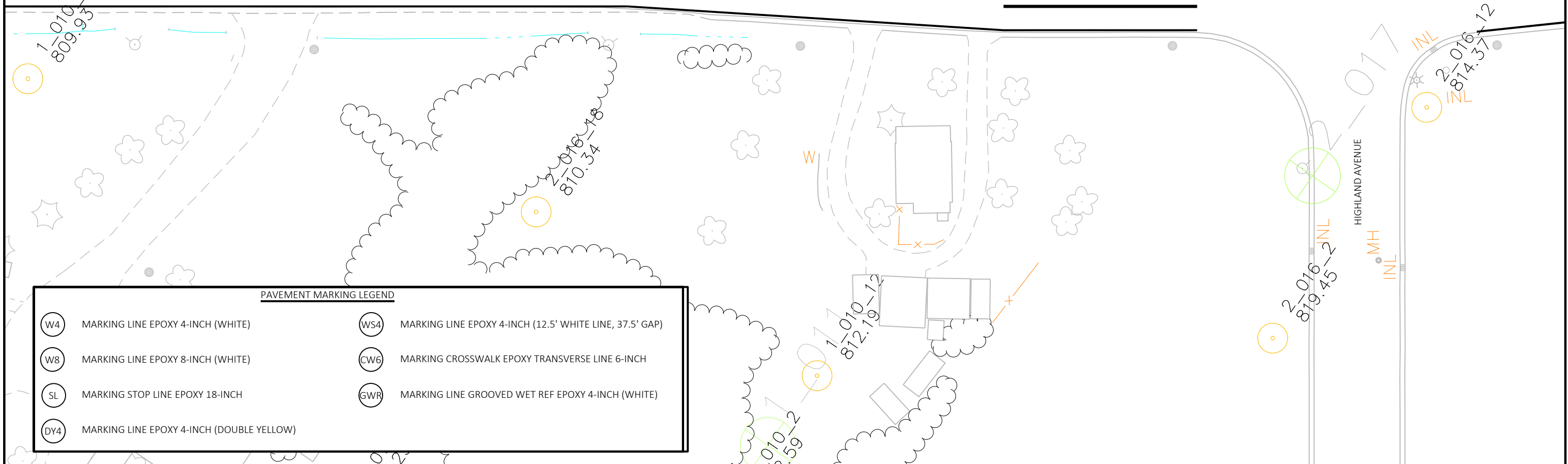
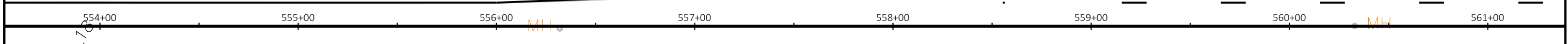
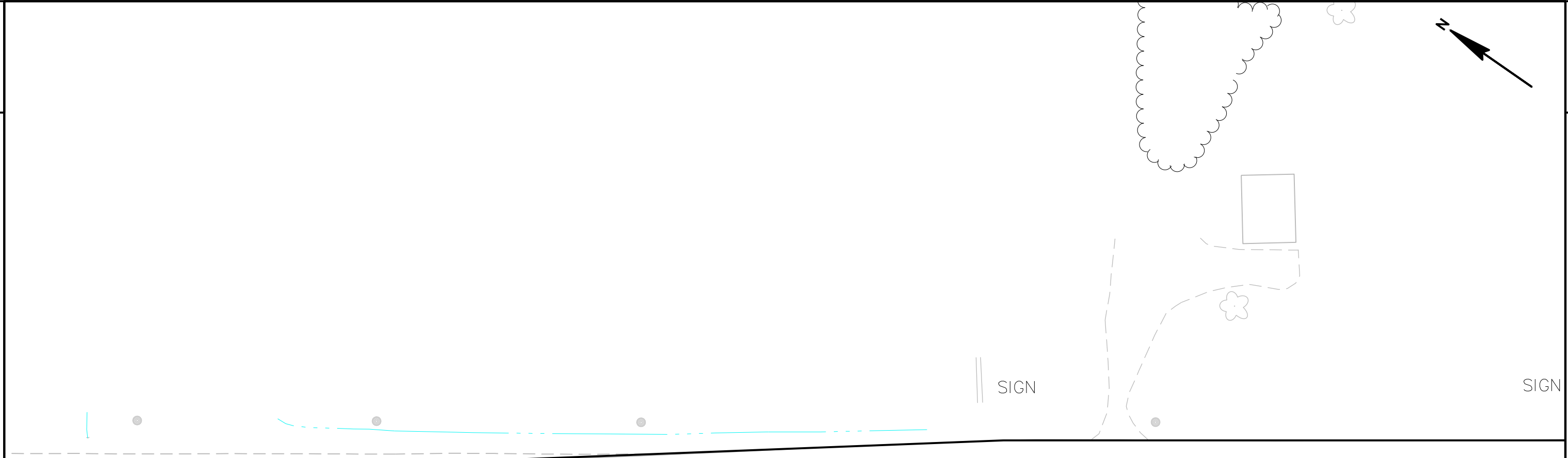
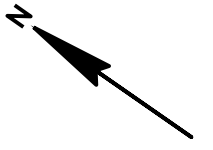
SHEET

E



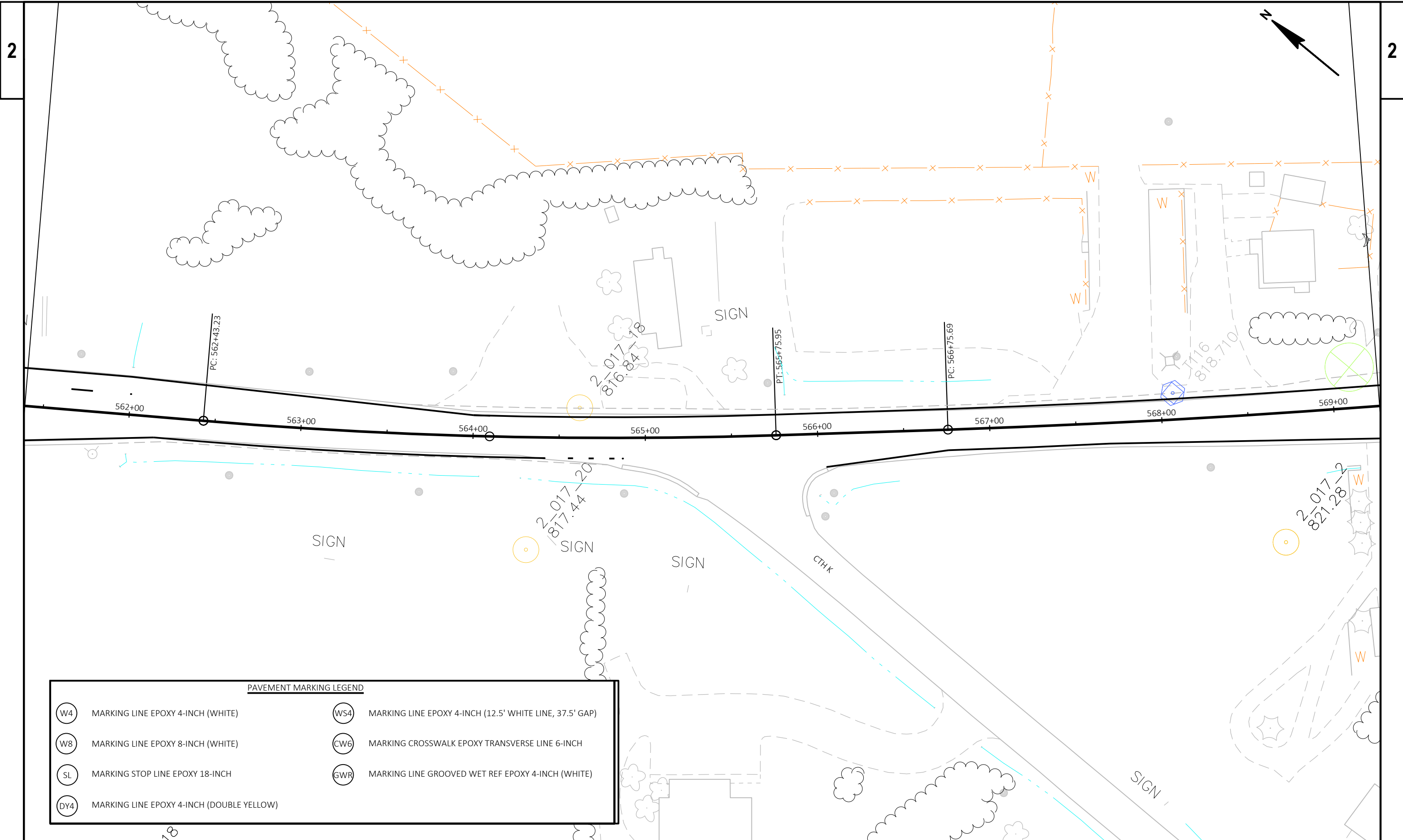
PAVEMENT MARKING LEGEND

W4	MARKING LINE EPOXY 4-INCH (WHITE)	WS4	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
W8	MARKING LINE EPOXY 8-INCH (WHITE)	CW6	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
SL	MARKING STOP LINE EPOXY 18-INCH	GWR	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
DY4	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		



PAVEMENT MARKING LEGEND

W4	MARKING LINE EPOXY 4-INCH (WHITE)	WS4	MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
W8	MARKING LINE EPOXY 8-INCH (WHITE)	CW6	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
SL	MARKING STOP LINE EPOXY 18-INCH	GWR	MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
DY4	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)		



PAVEMENT MARKING LEGEND	
(W4) MARKING LINE EPOXY 4-INCH (WHITE)	(WS4) MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP)
(W8) MARKING LINE EPOXY 8-INCH (WHITE)	(CW6) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
(SL) MARKING STOP LINE EPOXY 18-INCH	(GWR) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
(DY4) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)	



PAVEMENT MARKING LEGEND

- | | |
|---|---|
| (W4) MARKING LINE EPOXY 4-INCH (WHITE) | (WS4) MARKING LINE EPOXY 4-INCH (12.5' WHITE LINE, 37.5' GAP) |
| (W8) MARKING LINE EPOXY 8-INCH (WHITE) | (CW6) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH |
| (SL) MARKING STOP LINE EPOXY 18-INCH | (GWR) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE) |
| (DY4) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW) | |

PROJECT NO: 3575-02-73

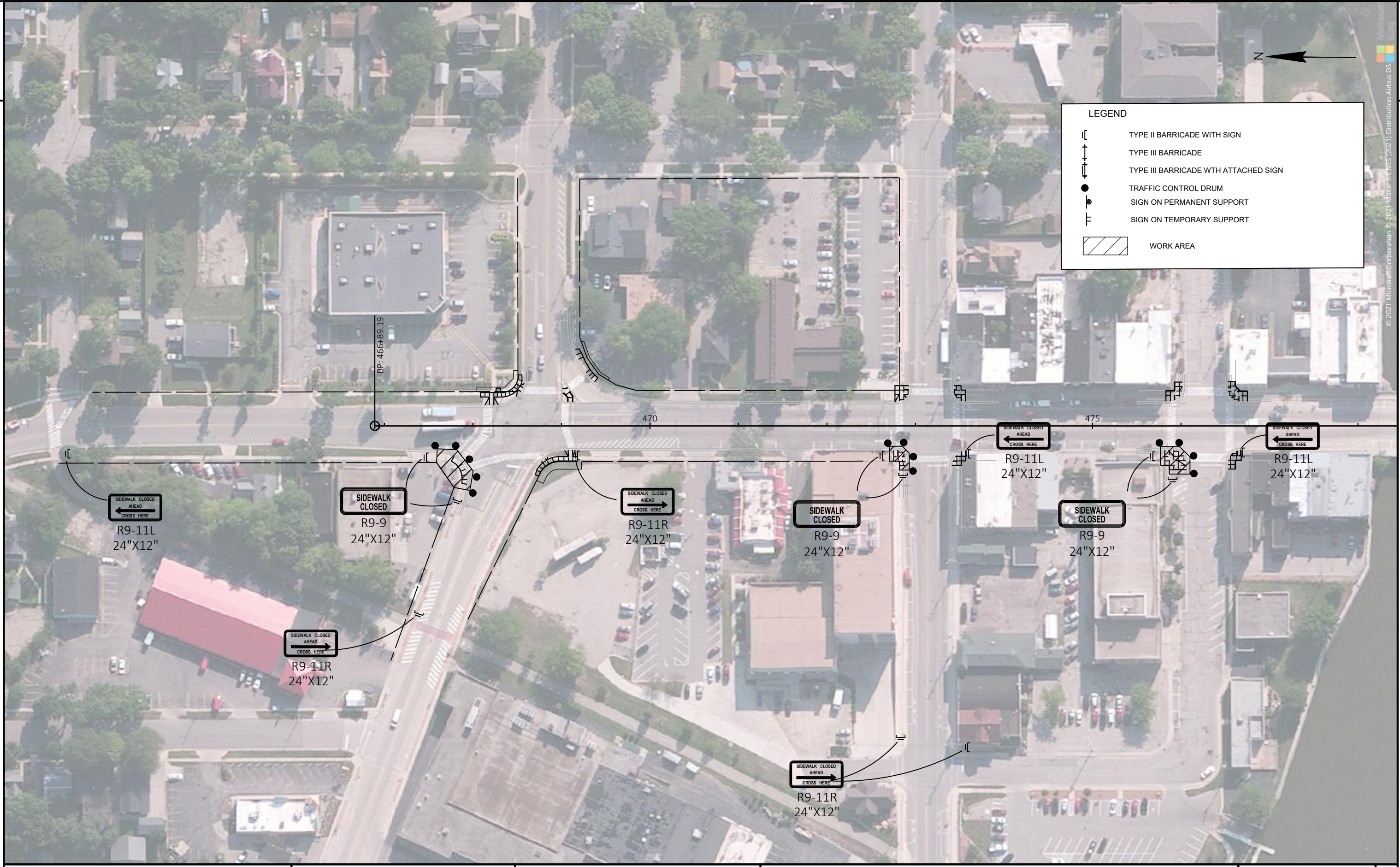
HWY: USH 12

COUNTY: JEFFERSON

PAVEMENT MARKING

SHEET

E



LEGEND

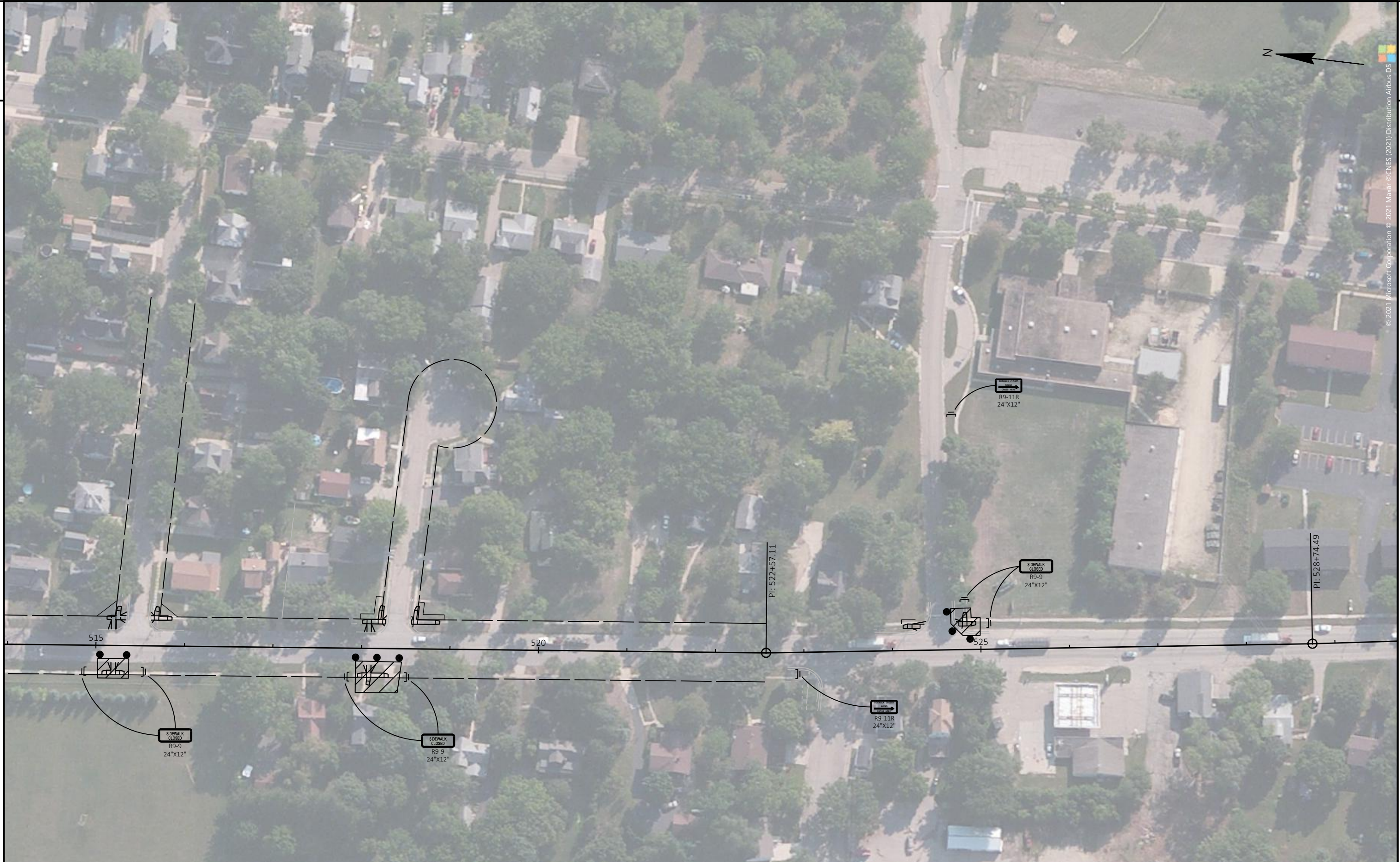
- TYPE II BARRICADE WITH SIGN
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- WORK AREA



PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING- NW RAMPS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING- NW RAMPS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING- NW RAMPS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING- NW RAMPS	SHEET E
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FILE NAME : N:\PDS\C3D\35750204\SHEETSPLAN\025100-TRAFFIC CONTROL\025100-TC.DWG PLOT DATE : 10/30/2022 10:13 PM PLOT BY : WHITE, JEREMY J PLOT NAME : PLOT SCALE : ##### WISDOT/CADD SHEET 42

LAYOUT NAME - 05



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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING - SW RAMPS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING - SW RAMPS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING - SW RAMPS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING - SW RAMPS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING - NE RAMP	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING - NE RAMPS	SHEET	E
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2

2



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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING - NE RAMPS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING- SE RAMPS	SHEET	E
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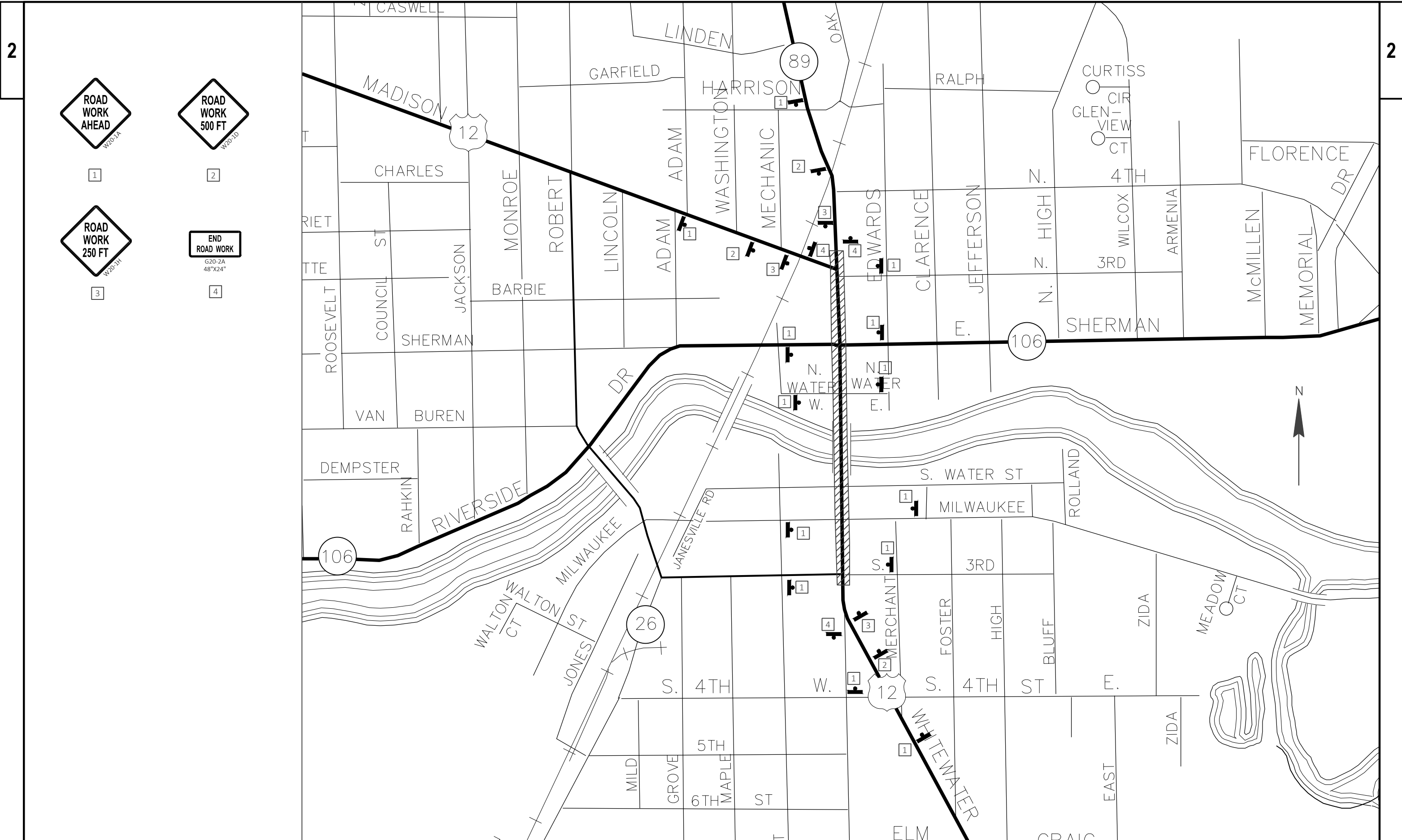
PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING- SE RAMPS	SHEET	E
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2

2

PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	CURB RAMP STAGING- SE RAMPS	SHEET	E
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PROJECT NO: 3575-02-73

HWY: USH 12

COUNTY: JEFFERSON

TRAFFIC CONTROL - STAGE 1

SHEET

E



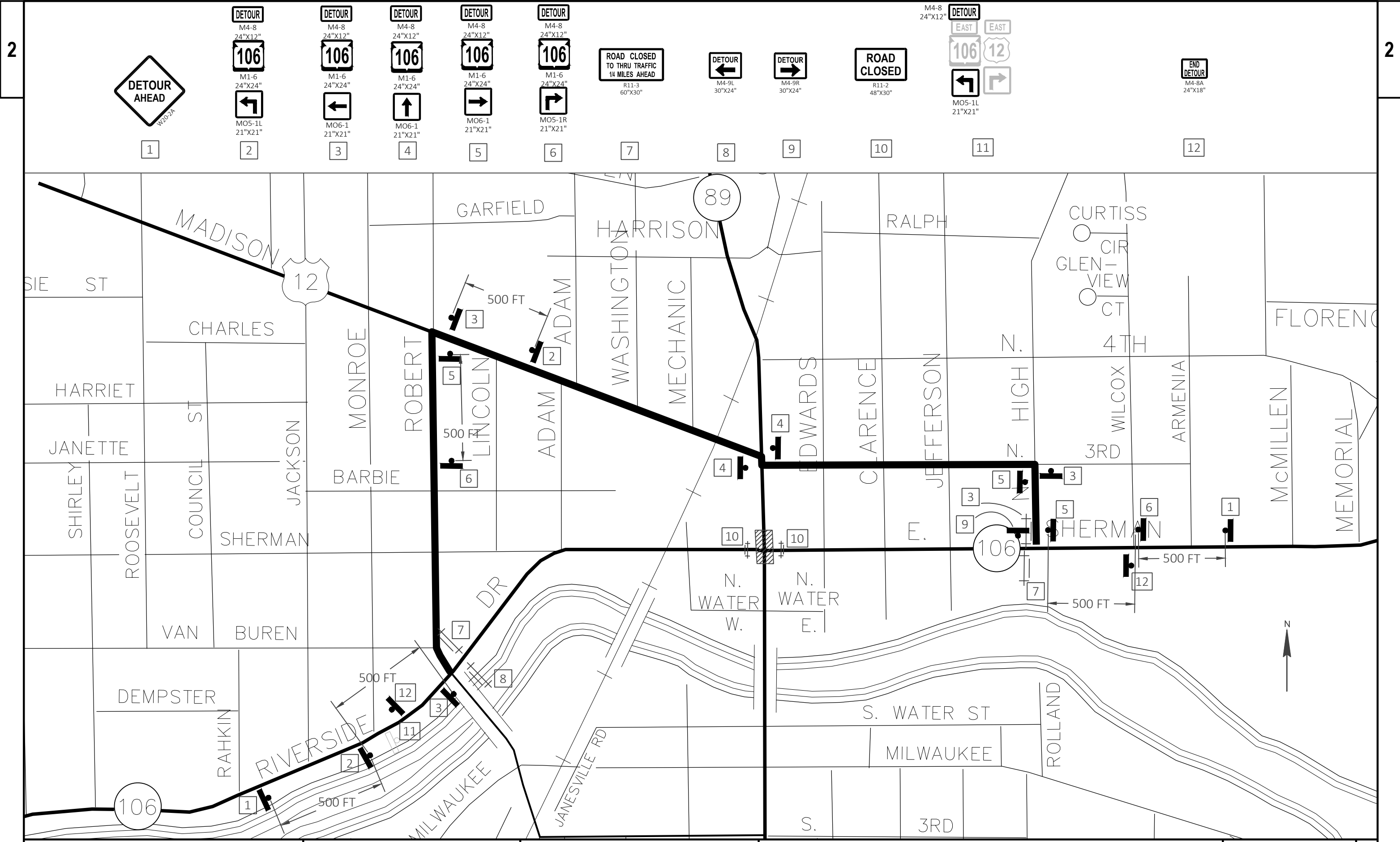
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4





PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	STH 106 DETOUR	SHEET	E
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Estimate Of Quantities

3575-02-73

Line	Item	Item Description	Unit	Total	Qty
0002	204.0110	Removing Asphaltic Surface	SY	427.000	427.000
0004	204.0120	Removing Asphaltic Surface Milling	SY	45,698.000	45,698.000
0006	204.0150	Removing Curb & Gutter	LF	1,793.000	1,793.000
0008	204.0155	Removing Concrete Sidewalk	SY	1,410.000	1,410.000
0010	213.0100	Finishing Roadway (project) 01. 3575-02-73	EACH	1.000	1.000
0012	305.0110	Base Aggregate Dense 3/4-Inch	TON	478.000	478.000
0014	305.0500	Shaping Shoulders	STA	51.000	51.000
0016	416.0610	Drilled Tie Bars	EACH	65.000	65.000
0018	455.0605	Tack Coat	GAL	6,401.000	6,401.000
0020	460.2000	Incentive Density HMA Pavement	DOL	5,740.000	5,740.000
0022	460.6224	HMA Pavement 4 MT 58-28 S	TON	8,961.000	8,961.000
0024	465.0125	Asphaltic Surface Temporary	TON	119.000	119.000
0026	601.0409	Concrete Curb & Gutter 30-Inch Type A	LF	218.000	218.000
0028	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	1,575.000	1,575.000
0030	601.0600	Concrete Curb Pedestrian	LF	298.000	298.000
0032	602.0410	Concrete Sidewalk 5-Inch	SF	14,205.000	14,205.000
0034	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	940.000	940.000
0036	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	24.000	24.000
0038	619.1000	Mobilization	EACH	1.000	1.000
0040	624.0100	Water	MGAL	4.780	4.780
0042	628.2004	Erosion Mat Class I Type B	SY	349.000	349.000
0044	628.7010	Inlet Protection Type B	EACH	2.000	2.000
0046	628.7015	Inlet Protection Type C	EACH	35.000	35.000
0048	628.7020	Inlet Protection Type D	EACH	5.000	5.000
0050	629.0210	Fertilizer Type B	CWT	0.280	0.280
0052	630.0140	Seeding Mixture No. 40	LB	13.000	13.000
0054	642.5201	Field Office Type C	EACH	1.000	1.000
0056	643.0300	Traffic Control Drums	DAY	720.000	720.000
0058	643.0420	Traffic Control Barricades Type III	DAY	176.000	176.000
0060	643.0900	Traffic Control Signs	DAY	2,741.000	2,741.000
0062	643.3105	Temporary Marking Line Paint 4-Inch	LF	5,714.000	5,714.000
0064	643.3120	Temporary Marking Line Epoxy 4-Inch	LF	2,859.000	2,859.000
0066	643.3205	Temporary Marking Line Paint 8-Inch	LF	1,926.000	1,926.000
0068	643.3220	Temporary Marking Line Epoxy 8-Inch	LF	963.000	963.000
0070	643.5000	Traffic Control	EACH	1.000	1.000
0072	646.1020	Marking Line Epoxy 4-Inch	LF	24,290.000	24,290.000
0074	646.1040	Marking Line Grooved Wet Ref Epoxy 4-Inch	LF	4,016.000	4,016.000
0076	646.3020	Marking Line Epoxy 8-Inch	LF	1,061.000	1,061.000
0078	646.5020	Marking Arrow Epoxy	EACH	26.000	26.000
0080	646.5120	Marking Word Epoxy	EACH	11.000	11.000
0082	646.6120	Marking Stop Line Epoxy 18-Inch	LF	305.000	305.000
0084	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	3,819.000	3,819.000
0086	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	1,793.000	1,793.000
0088	650.8000	Construction Staking Resurfacing Reference	LF	10,128.000	10,128.000
0090	650.9000	Construction Staking Curb Ramps	EACH	91.000	91.000
0092	650.9500	Construction Staking Sidewalk (project) 01. 3575-02-73	EACH	1.000	1.000
0094	650.9911	Construction Staking Supplemental Control (project) 01. 3575-02-73	EACH	1.000	1.000
0096	690.0150	Sawing Asphalt	LF	2,881.000	2,881.000
0098	690.0250	Sawing Concrete	LF	1,608.000	1,608.000

Estimate Of Quantities

3575-02-73

Line	Item	Item Description	Unit	Total	Qty
0100	SPV.0090	Special 01. Concrete Curb Pedestrian 12-Inch	LF	77.000	77.000
0102	SPV.0180	Special 01. Removing Distressed Pavement Milling	SY	2,286.000	2,286.000

3

REMOVALS

CATEGORY	STATION	TO	STATION	LOCATION	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY
STAGE 1					
0010	467+50	-	475+85		5,139
0010	482+57	-	490+45		4,437
STAGE 2					
0010	490+45	-	581+81		35,317
TOTAL 0010					44,893
0020	467+50	-	475+85		363
0020	482+57	-	490+45		348
0020	490+45	-	581+81		94
TOTAL 0020					805
PROJECT TOTAL					45,698

HMA

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4- INCH TON	305.0500 SHAPING SHOULDERS STA	455.0605 TACK COAT GAL	460.6224 HMA PAVEMENT 4 MT 58-28 S TON	465.0125 ASPHALTIC SURFACE TEMPORARY TON	624.0100 WATER MGAL	REMARKS
0010	467+50	-	475+85		-	-	720	1,008	26	-	Undistributed
0010	482+57	-	490+45		-	622	870	23	-		
0010	490+45	-	581+81		-	51	4,945	70	-		
0010	TOTAL 0010				111	51	6,287	8,801	119	1.11	
0020	467+50	-	475+85		-	-	51	72	-	-	
0020	482+57	-	490+45		-	-	49	69	-	-	
0020	490+45	-	581+81		-	-	14	19	-	-	
TOTAL 0020					0	0	114	160	0	0	
PROJECT TOTAL					111	51	6,401	8,961	119	1.11	

*ADDITIONAL QUANTITY LOCATED ELSEWHERE

REMOVING C&G AND SIDEWALK

CATEGORY	STATION	TO	STATION	LOCATION	204.0110 REMOVING ASPHALTIC SURFACE SY	204.0150 REMOVING CURB & GUTTER LF	204.0155 REMOVING CONCRETE SIDEWALK SY	REMARKS
STAGE 1								
0010	467+75	-	469+41	Madison Ave	10	41	19	NW
0010	-	-	-	-	13	62	65	NE
0010	-	-	-	-	10	61	44	SE
0010	-	-	-	-	11	36	28	SW
0010	472+72	-	473+60	Sherman Ave	8	26	19	NW
0010	-	-	-	-	8	28	28	NE
0010	-	-	-	-	8	25	19	SE
0010	-	-	-	-	7	47	19	SW
0010	475+79	-	476+76	N Water St	10	31	31	NW
0010	-	-	-	-	9	25	26	NE
0010	-	-	-	-	9	24	37	SE
0010	-	-	-	-	9	24	35	SW
0010	481+83	-	482+67	S Water St	9	24	30	NW
0010	-	-	-	-	10	26	28	NE
0010	-	-	-	-	11	33	44	SE
0010	-	-	-	-	11	31	37	SW
0010	483+80	-	484+73	Milwaukee Ave	8	32	32	NW
0010	-	-	-	-	8	32	31	NE
0010	-	-	-	-	10	31	36	SE
0010	-	-	-	-	9	30	27	SW
0010	487+33	-	488+49	S 3rd St	9	27	35	NW
0010	-	-	-	-	9	28	35	NE
0010	-	-	-	-	7	26	21	SE
0010	-	-	-	-	7	63	45	SW
SUBTOTAL					220	813	771	
STAGE 2								
0010	491+88	-	492+65	Main St	3	18	14	West
0010	-	-	-	-	6	23	0	Middle West
0010	-	-	-	-	6	23	62	Middle East
0010	-	-	-	-	3	15	11	East
0010	495+33	-	496+69	S 4th St	7	30	21	NW
0010	-	-	-	-	7	58	34	NE
0010	-	-	-	-	9	35	22	SE
0010	-	-	-	-	9	53	30	SW
0010	503+36	-	504+85	Foster St	3	15	7	NW
0010	-	-	-	-	11	85	37	NE
0010	-	-	-	-	6	29	18	SE
0010	505+69	-	507+10	Elm St	12	33	34	NW
0010	-	-	-	-	10	58	35	NE
0010	-	-	-	-	8	33	19	SE
0010	-	-	-	-	11	59	36	SW
0010	509+64	-	510+64	South St	5	30	18	NW
0010	-	-	-	-	8	50	26	SW
0010	510+23	-	511+93	High St	17	64	33	NE
0010	-	-	-	-	10	30	46	SE
0010	511+93	-	512+24	McComb St	8	61	0	NE
0010	515+06	-	515+85	Converse St	4	15	7	NW
0010	-	-	-	-	9	44	18	NE
0010	-	-	-	-	4	17	13	SE
0010	517+96	-	518+88	McKee Ct	3	15	7	NW
0010	-	-	-	-	9	38	22	NE
0010	-	-	-	-	5	17	19	SE
0010	524+15	-	524+94	Bark River Dr	5	16	11	NE
0010	-	-	-	-	5	16	16	SE
0010	536+09	-	536+82	Fox Hill Road	2	-	17	NE
0010	-	-	-	-	2	-	6	SE
SUBTOTAL					207	980	639	
TOTAL 0010					427	1,793	1,410	

3

CONCRETE CURB RAMPS

CATEGORY	STATION	TO	STATION	LOCATION	* 305.0110		416.0610		601.0409		601.0411		601.0600		602.0410		602.0505		602.0605		* 624.0100		SPV.0090.01 SPECIAL (01. CONCRETE CURB PEDESTRIAN 12-INCH)		REMARKS		
					BASE AGGREGATE DENSE 3/4-INCH TON	DRILLED TIE BARS EACH	CONCRETE CURB & GUTTER 30-INCH TYPE A LF	CONCRETE CURB & GUTTER 30-INCH TYPE D LF	CONCRETE CURB PEDESTRIAN LF	CONCRETE SIDEWALK 5- INCH SF	DETECTABLE WARNING FIELD YELLOW SF	DETECTABLE WARNING FIELD RADIAL YELLOW SF	WATER MGAL	CONCRETE CURB PEDESTRIAN 12-INCH) LF													
STAGE 1																											
0010	467+75	-	469+41	Madison Ave	5	-	-	41	-	198	10	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	11	-	-	62	-	428	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	11	-	-	61	32	453	40	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	-	-	-	-	9	-	-	36	-	340	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SW		
0010	472+72	-	473+60	Sherman Ave	5	-	-	26	-	177	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	6	-	-	28	-	244	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	5	-	-	25	-	175	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	-	-	-	-	5	-	-	47	-	168	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SW		
0010	475+79	-	476+76	N Water St	11	9	31	-	-	459	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	7	9	25	-	20	256	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	8	7	24	-	22	314	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	-	-	-	-	8	7	24	-	23	295	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SW		
0010	481+83	-	482+67	S Water St	6	7	24	-	20	250	20	24	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	8	7	26	-	20	294	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	10	10	33	-	25	379	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	-	-	-	-	8	9	31	-	26	316	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SW		
0010	483+80	-	484+73	Milwaukee Ave	6	-	-	32	-	238	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	7	-	-	32	-	284	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	10	-	-	31	20	386	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	-	-	-	-	8	-	-	30	-	298	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SW		
0010	487+33	-	488+49	S 3rd St	8	-	-	27	-	301	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	8	-	-	28	19	313	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	5	-	-	26	-	182	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	-	-	-	-	11	-	-	63	34	437	40	-	-	-	-	-	-	-	-	-	-	-	-	-	SW		
0010	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	1.86	-	-	-	-	-	-	-	B.A.D. Compact		
SUBTOTAL					186	65	218	595	261	7,185	510	24	-	-	-	2	-	-	-	-	-	-	-	-	-		
STAGE 2																											
0010	491+88	-	492+65	Main St.	5	-	-	18	-	180	10	-	-	-	-	-	-	-	-	-	-	-	-	-	West		
0010	-	-	-	-	6	-	-	23	-	212	10	-	-	-	-	-	-	-	-	-	-	-	-	-	Middle West		
0010	-	-	-	-	5	-	-	23	-	198	10	-	-	-	-	-	-	-	-	-	-	-	-	-	Middle East		
0010	-	-	-	-	4	-	-	15	-	137	10	-	-	-	-	-	-	-	-	-	-	-	-	-	East		
0010	495+33	-	496+69	S 4th St	6	-	-	30	37	229	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	8	-	-	58	-	321	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	10	-	-	35	-	397	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	-	-	-	-	9	-	-	53	-	369	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SW		
0010	503+36	-	504+85	Foster St	5	-	-	15	-	170	10	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	11	-	-	85	-	438	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	5	-	-	29	-	185	10	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	505+69	-	507+10	Elm St	9	-	-	33	-	339	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	8	-	-	58	-	333	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	7	-	-	33	-	283	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	-	-	-	-	6	-	-	59	-	238	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SW		
0010	509+64	-	510+64	South St	5	-	-	30	-	184	10	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	7	-	-	50	-	262	20	-	-	-	-	-	-	-	-	-	-	-	-	-	SW		
0010	510+23	-	511+93	High St	8	-	-	64	-	322	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	7	-	-	30	-	260	10	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	511+93	-	512+24	McComb St	6	-	-	61	-	251	10	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	515+06	-	515+85	Converse St	4	-	-	15	-	165	10	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	5	-	-	44	-	208	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	4	-	-	17	-	162	10	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	517+96	-	518+88	McKee Ct	6	-	-	15	-	246	10	-	-	-	-	-	-	-	-	-	-	-	-	-	NW		
0010	-	-	-	-	6	-	-	38	-	251	20	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	6	-	-	17	-	230	10	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	524+15	-	524+94	Bark River Dr	3	-	-	16	-	106	10	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	4	-	-	16	-	148	10	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	536+09	-	536+82	Fox Hill Road	4	-	-	-	-	145	10	-	-	-	-	-	-	-	-	-	-	-	-	-	NE		
0010	-	-	-	-	2	-	-	-	-	51	10	-	-	-	-	-	-	-	-	-	-	-	-	-	SE		
0010	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	1.81	-	-	-	-	-	-	-	-	B.A.D. Compact		
SUBTOTAL					181	0	0	980	37	7,020	430	0	-	-	-	1.81	-	-	-	-	-	-	-	-	-	-	
TOTAL 0010					367	65	218	1,575	298	14,205	940	24	-	-	-	3.67	-	-	-	-	-	-	-	-	-	-	-

*ADDITIONAL QUANTITY LOCATED ELSEWHERE

3

EROSION CONTROL

CATEGORY	STATION	TO	STATION	LOCATION	628.2004	628.7010	628.7015	628.7020	629.0210	630.0140	REMARKS
					EROSION MAT CLASS I TYPE B SY	INLET PROTECTION TYPE B EACH	INLET PROTECTION TYPE C EACH	INLET PROTECTION TYPE D EACH	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 40 LB	
STAGE 1											
0010	467+75	-	469+41		44	-	-	-	0.03	1	Madison Ave
0010	468+05			LT/RT	-	-	2	-	-	-	Madison Ave
0010	469+35			LT	-	-	-	1	-	-	Madison Ave
0010	472+52			LT/RT	-	-	2	-	-	-	Sherman Ave
0010	472+75	-	473+57		-	-	-	-	-	-	Sherman Ave
0010	475+80	-	476+75		-	-	-	-	-	-	N Water St
0010	475+86			LT	-	-	1	-	-	-	N Water St
0010	476+03			LT/RT	-	-	2	-	-	-	N Water St
0010	476+51			LT/RT	-	-	2	-	-	-	N Water St
0010	476+68			LT/RT	-	-	2	-	-	-	N Water St
0010	481+20			LT/RT	-	-	2	-	-	-	S Water St
0010	481+83	-	482+67		-	-	-	-	-	-	S Water St
0010	482+06			LT/RT	-	-	2	-	-	-	S Water St
0010	482+38			LT/RT	-	-	2	-	-	-	S Water St
0010	482+44			LT/RT	-	-	2	-	-	-	S Water St
0010	483+80	-	484+74		-	-	-	-	-	-	Milwaukee Ave
0010	484+00			LT/RT	-	-	2	-	-	-	Milwaukee Ave
0010	484+50			LT/RT	-	-	2	-	-	-	Milwaukee Ave
0010	486+51			LT/RT	-	-	1	-	-	-	S 3rd St
0010	487+33	-	488+49		4	-	-	-	0.01	1	S 3rd St
0010	487+56			LT/RT	-	-	2	-	-	-	S 3rd St
SUBTOTAL					48	0	26	1	0.04	2	
STAGE 2											
0010	490+96			LT	-	1	-	-	-	-	Main St
0010	492+30	-	492+56		45	-	-	-	0.03	1	Main St
0010	495+56	-	496+66		46	-	-	-	0.03	1	S 4th St
0010	495+98			LT	-	-	1	-	-	-	S 4th St
0010	496+36			LT	-	-	1	-	-	-	S 4th St
0010	503+38	-	504+85		22	-	-	-	0.02	1	Foster St
0010	503+76			LT	-	-	1	-	-	-	Foster St
0010	504+77			LT	-	-	1	-	-	-	Foster St
0010	505+69	-	506+85		51	-	-	-	0.04	1	Elm St
0010	506+00			RT	-	1	-	-	-	-	Elm St
0010	506+31			LT	-	-	1	-	-	-	Elm St
0010	509+68	-	510+60		13	-	-	-	0.01	1	South St
0010	509+82			RT	-	-	1	-	-	-	South St
0010	510+37			RT	-	-	1	-	-	-	South St
0010	510+23	-	510+87		44	-	-	-	0.03	1	High St
0010	511+41	-	512+14		18	-	-	-	0.02	1	McComb St
0010	512+12			LT	-	-	-	1	-	-	McComb St
0010	513+15			LT/RT	-	-	-	2	-	-	-
0010	515+06	-	515+85		18	-	-	-	0.02	1	Converse St
0010	515+28			LT	-	-	1	1	-	-	Converse St
0010	517+48			LT	-	-	1	-	-	-	McKee Ct
0010	517+95	-	518+88		26	-	-	-	0.02	1	McKee Ct
0010	524+11	-	524+94		10	-	-	-	0.01	1	Bark River Dr
0010	536+07	-	536+82		8	-	-	-	0.01	1	Fox Hill Rd
SUBTOTAL					301	2	9	4	0.24	11	
TOTAL 0010					349	2	35	5	0.28	13	

TEMPORARY TRAFFIC CONTROL

CATEGORY	DESCRIPTION	643.5000	643.0300	643.0420	643.0900	DURATION
		TRAFFIC CONTROL EACH	TRAFFIC CONTROL DRUMS DAY	TRAFFIC CONTROL BARRICADES TYPE III DAY	TRAFFIC CONTROL SIGNS DAY	
STAGE 1						
		1	-	-	-	PROJECT
	TRAFFIC CONTROL	-	-	-	-	
	STH 106 DETOUR	-	-	6	6	24 24 1 DAYS
0010	CURB RAMPS SW	-	60	5	-	12 DAYS
0010	CURB RAMPS SE	-	85	5	-	17 DAYS
0010	CURB RAMPS NE	-	75	5	-	15 DAYS
0010	CURB RAMPS NW	-	65	5	-	13 DAYS
0010	MILLING	-	20	10	4	2 2 DAYS
0010	PAVING	-	30	10	36	12 3 DAYS
0010	PAVEMENT MARKING	-	5	5	-	1 DAYS
SUBTOTAL		1	340	45	46	20 1,454 46
STAGE 2						
	TRAFFIC CONTROL	-	-	-	-	1,287 13 99 DAYS
0010	CURB RAMPS SW	-	35	5	-	7 DAYS
0010	CURB RAMPS SE	-	65	5	-	13 DAYS
0010	CURB RAMPS NE	-	60	5	-	12 DAYS
0010	CURB RAMPS NW	-	45	5	-	9 DAYS
0010	MILLING	-	50	10	10	2 5 DAYS
0010	PAVING	-	100	10	120	12 10 DAYS
0010	SHOULDERS	-	20	10	-	2 DAYS
0010	PAVEMENT MARKING	-	5	5	-	1 DAYS
SUBTOTAL		0	380	55	130	14 1,287 13
TOTAL 0010		1	720	100	176	34 2,741 59

3

PAVEMENT MARKING

CATEGORY	STATION	TO	STATION	LOCATION	646.1020 MARKING LINE EPOXY 4-INCH		646.1040	646.3020	646.5120	646.5020	646.6120	646.7420	643.3120	643.3105	643.3220	643.3205	REMARKS
					WHITE LF	YELLOW LF	MARKING LINE GROOVED WET REF EPOXY 4-INCH LF	MARKING LINE EPOXY 8-INCH LF	MARKING WORD EPOXY EACH	MARKING ARROW EPOXY EACH	MARKING STOP LINE EPOXY 18-INCH LF	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	TEMPORARY MARKING LINE EPOXY 4-INCH YELLOW LF	TEMPORARY MARKING LINE PAINT 4-INCH YELLOW LF	TEMPORARY MARKING LINE EPOXY 8-INCH LF	TEMPORARY MARKING LINE PAINT 8-INCH LF	
STAGE 1																	
0010	467+00	-	470+13		-	182	-	260	2	8	112	532	182	364	260	520	Madison Ave
0010	469+18	-	472+68		-	700	-	-	-	-	-	-	700	1,400	-	-	
0010	471+77	-	474+35		-	-	-	250	2	4	66	409	-	-	250	500	Sherman Ave
0010	473+60	-	475+87		-	456	-	-	-	-	-	-	456	912	-	-	
0010	475+22	-	477+30		-	-	-	64	1	2	-	401	-	-	64	128	N Water St
0010	476+65	-	481+88		-	-	-	-	-	-	-	-	-	-	-	-	
0010	481+21	-	482+99		-	-	-	42	1	1	-	354	-	-	42	84	S Water St
0010	482+57	-	483+70		-	232	-	-	-	-	-	-	232	464	-	-	
0010	483+39	-	485+14		-	-	-	150	2	3	63	386	-	-	150	300	Milwaukee Ave
0010	484+69	-	487+23		-	510	-	-	-	-	-	-	510	1,020	-	-	
0010	486+80	-	488+91		-	-	-	197	3	8	64	416	-	-	197	394	S 3rd St
0010	488+21	-	490+34		-	428	-	-	-	-	-	-	428	856	-	-	
				SUBTOTAL	0	2,508	0	963	11	26	305	2,498	2,508	5,016	963	1,926	
STAGE 2																	
0010	490+34	-	492+55		-	442	-	-	-	-	-	-	10	19	-	-	
0010	492+13	-	492+61		-	-	-	-	-	-	-	213	-	-	-	-	Main St
0010	492+61	-	495+62		-	602	-	-	-	-	-	-	13	26	-	-	
0010	495+45	-	496+36		-	-	-	-	-	-	-	310	-	-	-	-	S 4th St
0010	496+32	-	503+57		-	1,450	-	-	-	-	-	-	31	61	-	-	
0010	503+57	-	503+63		-	-	-	-	-	-	-	64	-	-	-	-	Foster St
0010	503+63	-	506+03		-	478	-	-	-	-	-	-	10	20	-	-	
0010	505+91	-	506+95		-	-	-	-	-	-	-	320	-	-	-	-	Elm St
0010	506+84	-	510+53		-	738	-	-	-	-	-	-	16	31	-	-	
0010	509+87	-	510+59		-	-	-	-	-	-	-	144	-	-	-	-	South St
0010	510+59	-	512+67		-	416	-	-	-	-	-	-	9	18	-	-	
0010	512+17	-	512+78		-	-	-	-	-	-	-	142	-	-	-	-	McComb St
0010	512+77	-	515+17		-	478	-	-	-	-	-	-	10	20	-	-	
0010	515+17	-	515+23		-	-	-	-	-	-	-	64	-	-	-	-	Converse St
0010	515+23	-	518+05		-	566	-	-	-	-	-	-	12	24	-	-	
0010	518+05	-	518+11		-	-	-	-	-	-	-	64	-	-	-	-	McKee Ct
0010	518+11	-	575+50		4,808	11,478	4,016	98	-	-	-	-	240	479	-	-	
				SUBTOTAL	4,808	16,648	4,016	98	0	0	0	1,321	351	698	0	0	
				TOTAL 0010	4,808	19,156	4,016	1,061	11	26	305	3,819	2,859	5,714	963	1,926	
0020	469+18	-	472+68		56	-	-	-	-	-	-	-	-	-	-	-	
0020	473+60	-	475+87		54	-	-	-	-	-	-	-	-	-	-	-	
0020	476+65	-	481+88		100	-	-	-	-	-	-	-	-	-	-	-	
0020	482+57	-	483+70		31	-	-	-	-	-	-	-	-	-	-	-	
0020	484+69	-	487+23		67	-	-	-	-	-	-	-	-	-	-	-	
0020	496+32	-	503+58		18	-	-	-	-	-	-	-	-	-	-	-	
				TOTAL 0020	326	0	0	0	0	0	0	0	0	0	0	0	
				PROJECT TOTAL	5,134	19,156	4,016	1,061	11	26	305	3,819	2,859	5,714	963	1,926	

3

3

CONSTRUCTION STAKING

CATEGORY	STATION TO	STATION	LOCATION	650.5500	650.8000	650.9000	650.9500.01	650.9911.01	REMARKS
				CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER	CONSTRUCTION STAKING RESURFACING REFERENCE	CONSTRUCTION STAKING CURB RAMPS	CONSTRUCTION STAKING SIDEWALK (PROJECT) (01. 3575-02-73)	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 3575-02-73)	
				LF	LF	EACH	EACH	EACH	
0010	467+50	575+50		-	10128	-	1	1	
STAGE 1									
0010	467+75	469+41	Madison Ave	41	-	1	-	-	NW
0010	-	-		62	-	2	-	-	NE
0010	-	-		61	-	4	-	-	SE
0010	-	-		36	-	2	-	-	SW
0010	472+72	473+60	Sherman Ave	26	-	2	-	-	NW
0010	-	-		28	-	2	-	-	NE
0010	-	-		25	-	2	-	-	SE
0010	-	-		47	-	1	-	-	SW
0010	475+79	476+76	N Water St	31	-	2	-	-	NW
0010	-	-		25	-	1	-	-	NE
0010	-	-		24	-	2	-	-	SE
0010	-	-		24	-	2	-	-	SW
0010	481+83	482+67	S Water St	24	-	1	-	-	NW
0010	-	-		26	-	2	-	-	NE
0010	-	-		33	-	2	-	-	SE
0010	-	-		31	-	2	-	-	SW
0010	483+80	484+73	Milwaukee Ave	32	-	2	-	-	NW
0010	-	-		32	-	2	-	-	NE
0010	-	-		31	-	2	-	-	SE
0010	-	-		30	-	2	-	-	SW
0010	487+33	488+49	S 3rd St	27	-	2	-	-	NW
0010	-	-		28	-	2	-	-	NE
0010	-	-		26	-	2	-	-	SE
0010	-	-		63	-	4	-	-	SW
			SUBTOTAL	813	-	48	-	-	
STAGE 2									
0010	491+88	492+65	Main St.	18	-	1	-	-	West
0010	-	-		23	-	1	-	-	Middle West
0010	-	-		23	-	1	-	-	Middle East
0010	-	-		15	-	1	-	-	East
0010	495+33	496+69	S 4th St	30	-	2	-	-	NW
0010	-	-		58	-	2	-	-	NE
0010	-	-		35	-	2	-	-	SE
0010	-	-		53	-	2	-	-	SW
0010	503+36	504+85	Foster St	15	-	1	-	-	NW
0010	-	-		85	-	2	-	-	NE
0010	-	-		29	-	1	-	-	SE
0010	505+69	507+10	Elm St	33	-	2	-	-	NW
0010	-	-		58	-	2	-	-	NE
0010	-	-		33	-	2	-	-	SE
0010	-	-		59	-	2	-	-	SW
0010	509+64	510+64	South St	30	-	1	-	-	NW
0010	-	-		50	-	2	-	-	SW
0010	510+23	511+93	High St	64	-	2	-	-	NE
0010	-	-		30	-	1	-	-	SE
0010	511+93	512+24	McComb St	61	-	1	-	-	NE
0010	515+06	515+85	Converse St	15	-	1	-	-	NW
0010	-	-		44	-	2	-	-	NE
0010	-	-		17	-	1	-	-	SE
0010	517+96	518+88	McKee Ct	15	-	1	-	-	NW
0010	-	-		38	-	2	-	-	NE
0010	-	-		17	-	1	-	-	SE
0010	524+15	524+94	Bark River Dr	16	-	1	-	-	NE
0010	-	-		16	-	1	-	-	SE
0010	536+09	536+82	Fox Hill Road	-	-	1	-	-	NE
0010	-	-		-	-	1	-	-	SE
			SUBTOTAL	980	-	43	-	-	
			TOTAL 0010	1,793	10,128	91	1	1	

3

3

SAWING PAVMENT

CATEGORY	STATION TO	STATION	LOCATION	690.0150	690.0250	REMARKS	
				SAWING ASPHALT LF	SAWING CONCRETE LF		
STAGE 1							
0010	467+79	-	469+38	Madison Ave	84	17	NW
0010					74	19	NE
0010					-	152	SE
0010					58	21	SW
0010					48	-	North Limit
0010					38	-	East Limit
0010					48	-	West Limit
0010	472+75	-	473+57	Sherman Ave	46	24	NW
0010					48	38	NE
0010					46	26	SE
0010					44	27	SW
0010					48	-	West Limit
0010					46	-	East Limit
0010	475+82	-	476+67	N Water St	20	88	NW
0010					22	55	NE
0010					20	61	SE
0010					12	67	SW
0010	481+83	-	482+67	S Water St	12	66	NW
0010					11	72	NE
0010					26	61	SE
0010					21	62	SW
0010	483+80	-	484+73	Milwaukee Ave	34	51	NW
0010					51	37	NE
0010					60	31	SE
0010					52	32	SW
0010					45	-	West Limit
0010					46	-	East Limit
0010	487+33	-	488+49	S 3rd St	58	25	NW
0010					75	27	NE
0010					45	27	SE
0010					-	150	SW
0010					-	21	West Limit
0010					36	-	East Limit
SUBTOTAL 0010					1,274	1,257	

CATEGORY	STATION TO	STATION	LOCATION	690.0150	690.0250	REMARKS	
				SAWING ASPHALT LF	SAWING CONCRETE LF		
STAGE 2							
0010	491+97	-	492+50	Main St	33	16	West
0010					48	26	Middle
0010					24	16	East
0010	495+38	-	496+39	S 4th St	42	13	NW
0010					46	13	NE
0010					50	13	SE
0010					58	8	SW
0010					33	-	West Limit
0010					25	-	East Limit
0010	503+08	-	504+85	Foster St	23	13	NW
0010					65	13	NE
0010					38	15	SE
0010	505+69	-	506+85	Elm St	71	16	NW
0010					58	13	NE
0010					45	13	SE
0010					63	13	SW
0010	509+68	-	510+64	South St	30	13	NW
0010					80	13	SW
0010	510+41	-	510+83	High St	101	9	NE
0010	511+42	-	512+25	McComb St	103	13	NE
0010	515+06	-	515+80	Converse St	27	13	NW
0010					54	13	NE
0010					28	13	SE
0010	518+03	-	518+88	McKee Ct	25	15	NW
0010					53	13	NE
0010					30	13	SE
0010	524+15	-	524+94	Bark River Dr	35	5	NE
0010					32	13	SE
0010	536+09	-	536+82	Fox Hill Road	23	-	NE
0010					12	4	SE
0010					64	-	West Limit
0010					27	-	East Limit
0010	560+00	-	570+00	E Highland Ave	44	-	West Limit
0010	565+50	-	566+00	CTH K	36	-	West Limit
0010	571+25	-	571+80	CTH M	57	-	East Limit
0010	575+50				24	-	South Limit
SUBTOTAL 0020					1,607	351	
TOTAL 0010					2,881	1,608	

DISTRESSED PAVEMENT MILLING

CATEGORY	LOCATION	SPV.0180.01	REMARKS
		SPECIAL (01. REMOVING DISTRESSED PAVEMENT MILLING) SY	
0010	STAGE 1	515	UNDISTRIBUTED
0010	STAGE 2	1,771	UNDISTRIBUTED
TOTAL 0010		2,286	

3

3

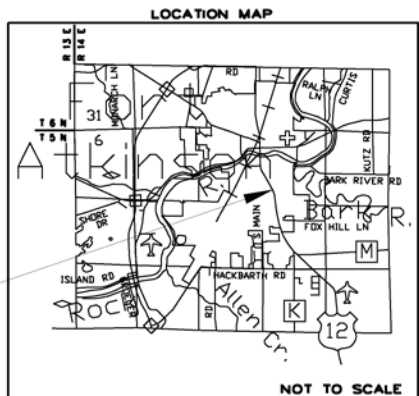
TRANSPORTATION PROJECT PLAT NO: 3575-02-22 - 4.01

THAT PART OF LOT 23, BLOCK 14, IN THE NW 1/4-SW 1/4 AND THE SW 1/4-SW 1/4 OF SECTION 3, ALSO LOTS 1 AND 2, BLOCK 17, IN THE SW 1/4-SW 1/4 OF SECTION 3, ALL IN GEORGE P. MARSTON'S ADDITION TO FORT ATKINSON, BEING A PART OF TOWNSHIP 5 NORTH, RANGE 14 EAST, IN THE CITY OF FORT ATKINSON, JEFFERSON COUNTY, WISCONSIN.

RELOCATION ORDER USH 12 CITY OF FORT ATKINSON WHITEWATER AVE. (MADISON AVENUE TO CTH M), JEFFERSON COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE CITY OF FORT ATKINSON DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE-NAMED PROJECT, TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 62.22, WISCONSIN STATUTES, THE CITY OF FORT ATKINSON HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE-NAMED PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE CITY FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE CITY OF FORT ATKINSON, PURSUANT TO THE PROVISIONS OF SUBSECTION 62.22, WISCONSIN STATUTES.



HWY	BASIS OF EXISTING R/W	R/W WIDTH	YEAR
USH 12	GEORGE P. MARSTON'S ADDITION	66'	1858
SOUTH ST	GEORGE P. MARSTON'S ADDITION	60'	1858

CITY OF FORT ATKINSON

raSmith
CREATIVITY BEYOND
ENGINEERING

I, SHANE M. ZODROW, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE CITY OF FORT ATKINSON, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



Shane M. Zodrow 07/21/2021
DATE
SHANE M. ZODROW
AGENT FOR R.A. SMITH
P.L.S. NUMBER 2869

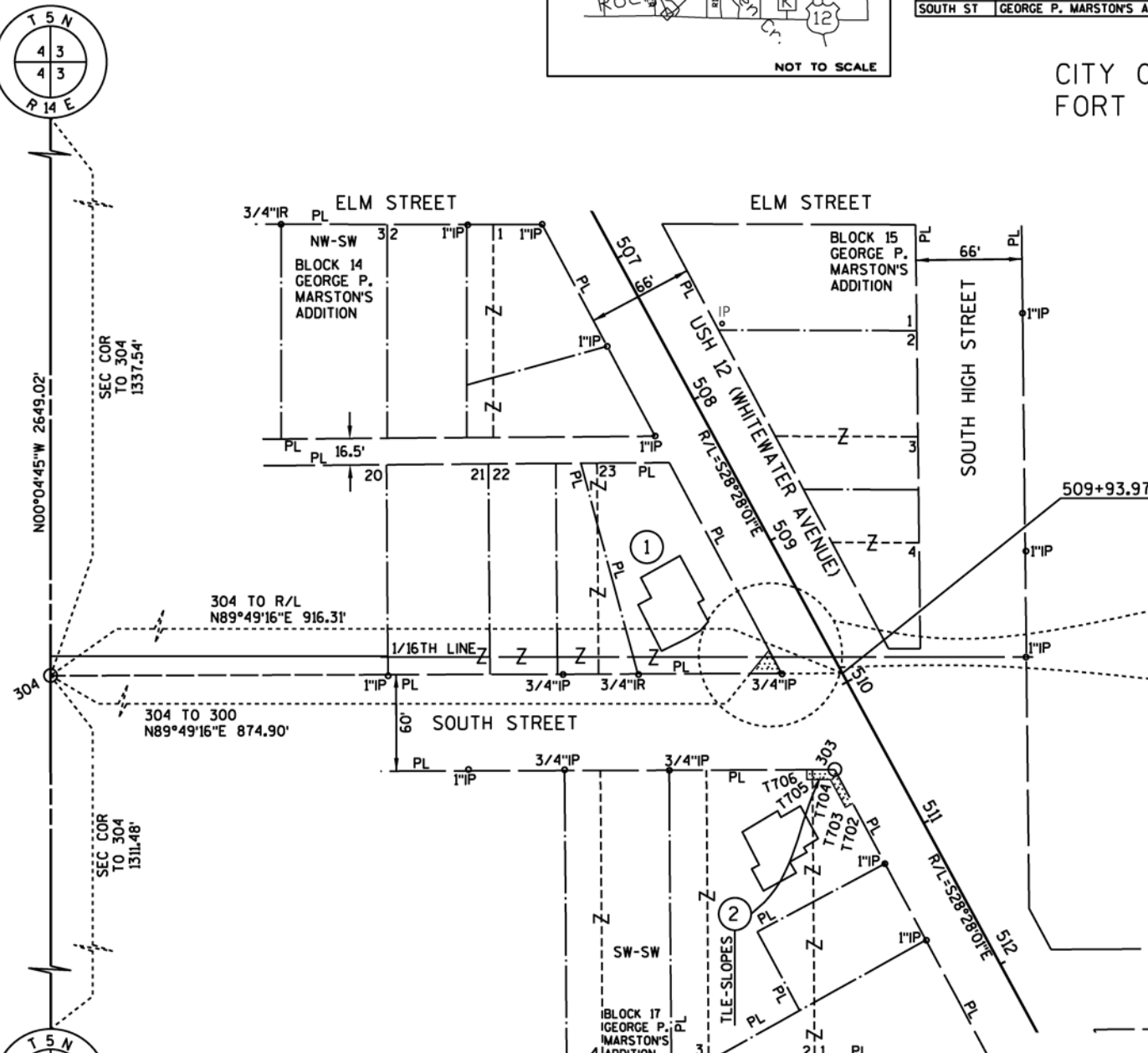
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE
CITY OF FORT ATKINSON.
Andy Selle, PE
PRINTED NAME SIGNATURE DATE: 10/14/21

1453553

Office of Register of Deeds
Jefferson County, WI
RECEIVED FOR RECORD
10/15/2021 01:25:43 PM
Staci M. Hoffman
Total Pages: 2
REC FEE: 25.00
**The above recording information
verifies that this document has
been electronically recorded
and returned to the submitter**

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 3575-02-22 - 4.01
AMENDMENT NO:
SHEET 1 OF 2

PK NAIL
Y=529124.573
X=847399.772



NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES (WCCS), JEFFERSON COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

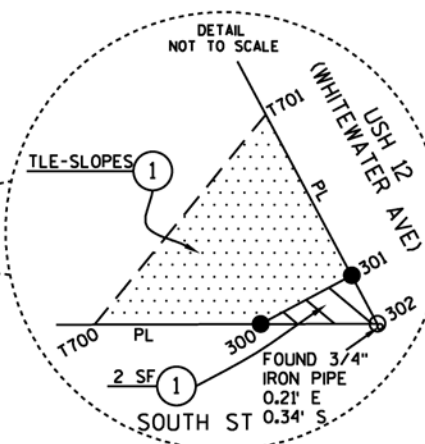
FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE CITY OF FORT ATKINSON.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2.

Station	Offset	Distance
300	509+74.35	36.46' RT
301	509+74.37	33.41' RT
302	509+75.99	33.42' RT
303	510+44.44	32.83' RT

Station	Offset	Distance
T700	509+66.74	50.61' RT
T701	509+59.62	33.30' RT
T702	510+68.22	32.98' RT
T703	510+68.19	38.26' RT
T704	510+48.50	38.16' RT
T705	510+41.49	51.06' RT
T706	510+36.18	48.18' RT

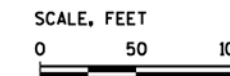
Station	Bearing	Distance
300-301	N61°49'55"E	3.05'
301-302	S28°03'33"E	1.62'
302-300	S89°49'16"W	3.45'
302-303	S28°57'26"E	68.46'



COMPUTED FROM COUNTY SURVEYOR PUBLISHED NAD 83/91 STATE PLANE COORDINATES TRANSLATED/ROTATED TO ADJACENT SECTION CORNER MONUMENTS
Y=526475.556
X=847403.429

PARCEL NUMBER	OWNER	INTEREST REQUIRED	AREAS S.F. REQUIRED			TLE S.F.	P.L.S. S.F.
			NEW	EXISTING	TOTAL		
1	RYAN T. & ASHLEY E. CORTEZ	FEE, TLE	2	-	2	139	-
2	D&L TRIEBOLD JOINT REVOCABLE TRUST	TLE	-	-	-	212	-

POT	Y	X
509+93.97	527,789.894	848,317.925



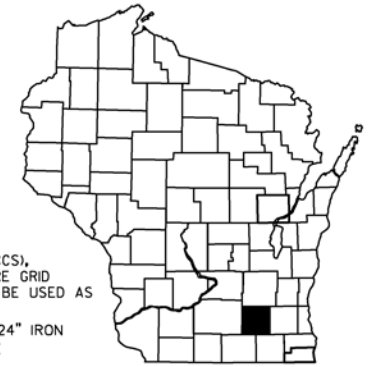
STATE OF WISCONSIN CITY OF FORT ATKINSON

TRANSPORTATION PROJECT PLAT TITLE SHEET

PROJECT NO. 3575-02-22

(MADISON AVENUE TO CTH M)

USH 12 WHITEWATER AVENUE JEFFERSON COUNTY



NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES (WCCS), JEFFERSON COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A **TEMPORARY LIMITED EASEMENT (TLE)** IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLE'S) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION CONTACT THE CITY OF FORT ATKINSON.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

CONVENTIONAL SYMBOLS			
SECTION LINE		PARCEL NUMBER	UTILITY NUMBER
QUARTER LINE		SECTION CORNER	R/W MONUMENT NON-MONUMENTED R/W POINT
SIXTEENTH LINE		NOTATION FOR COMBUSTIBLE FLUIDS	FOUND IRON PIN
NEW REFERENCE LINE		NOTATION FOR HIGH VOLTAGE TRANSMISSION LINES	VALVE (GAS, WATER, ETC.)
NEW R/W LINE			SIGN
EXISTING R/W LINE			OFF-PREMISE SIGN
PROPERTY LINE			
LOT, TIE, AND OTHER MINOR LINES			
SLOPE INTERCEPT			
CORPORATE LIMITS			
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)			
FEE ACQUISITION AREA (HATCHING VARIES BY OWNER)			
TEMP. LIMITED EASEMENT AREA		ACCESS CONTROLLED BY ACQUISITION	
EASEMENT AREA (HIGHWAY, PERMANENT LIMITED, OR RESTRICTED DEVELOPMENT)		NO ACCESS (BY STATUTORY AUTHORITY)	
TRANSMISSION STRUCTURES		ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	
BUILDING		NO ACCESS (NEW HIGHWAY)	
BUILDING (TO BE REMOVED)		NATIONAL GEODETIC SURVEY MONUMENT	
BRIDGE		SIXTEENTH CORNER MONUMENT	
		PARALLEL OFFSETS	

CONVENTIONAL UTILITY SYMBOLS

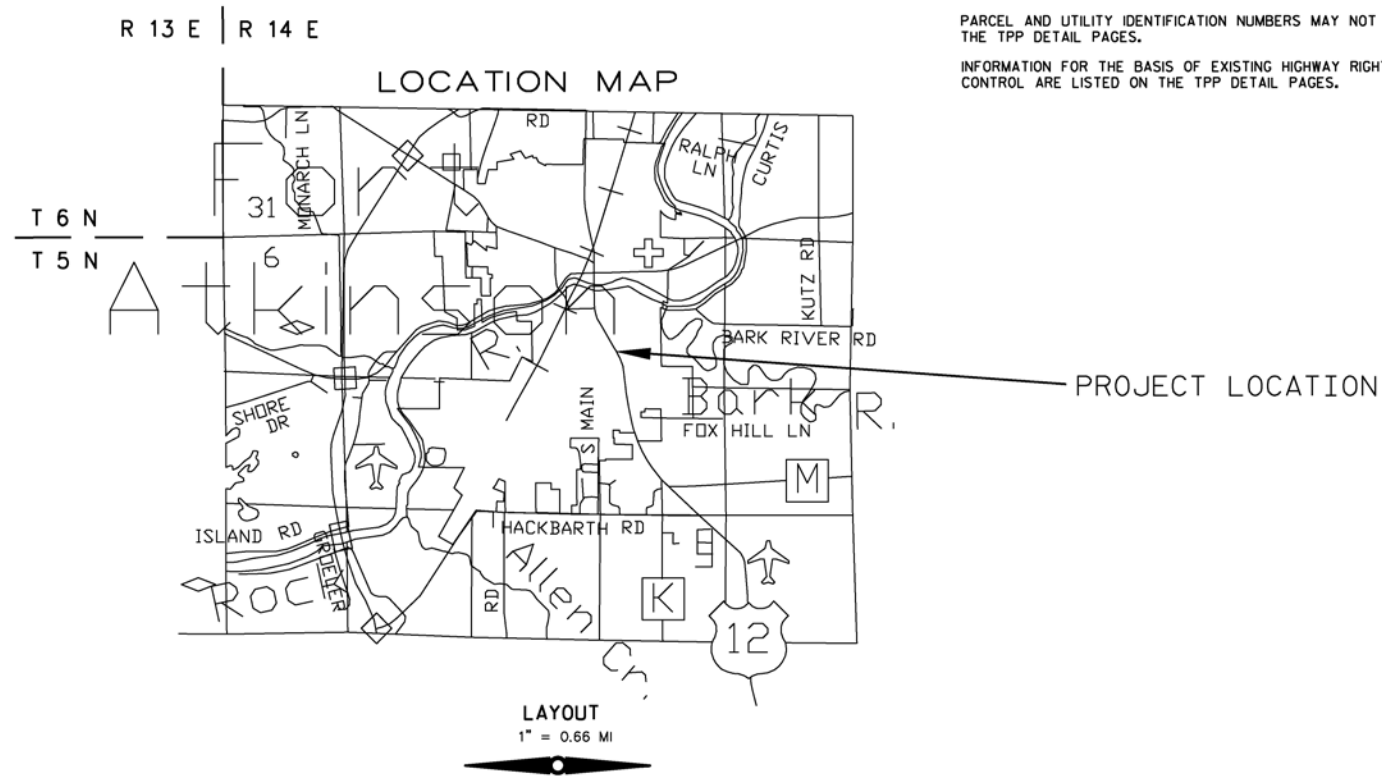
WATER	—W—
GAS	—G—
TELEPHONE	—T—
OVERHEAD TRANSMISSION LINES	—OH—
ELECTRIC	—E—
CABLE TELEVISION	—TV—
FIBER OPTIC	—FO—
SANITARY SEWER	—SAN—
STORM SEWER	—SS—
ELECTRIC TOWER	
POWER POLE	•
TELEPHONE POLE	•
TELEPHONE PEDESTAL	•

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL ABBREVIATIONS

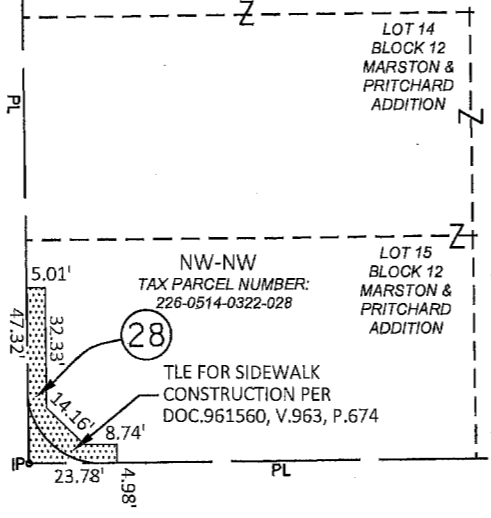
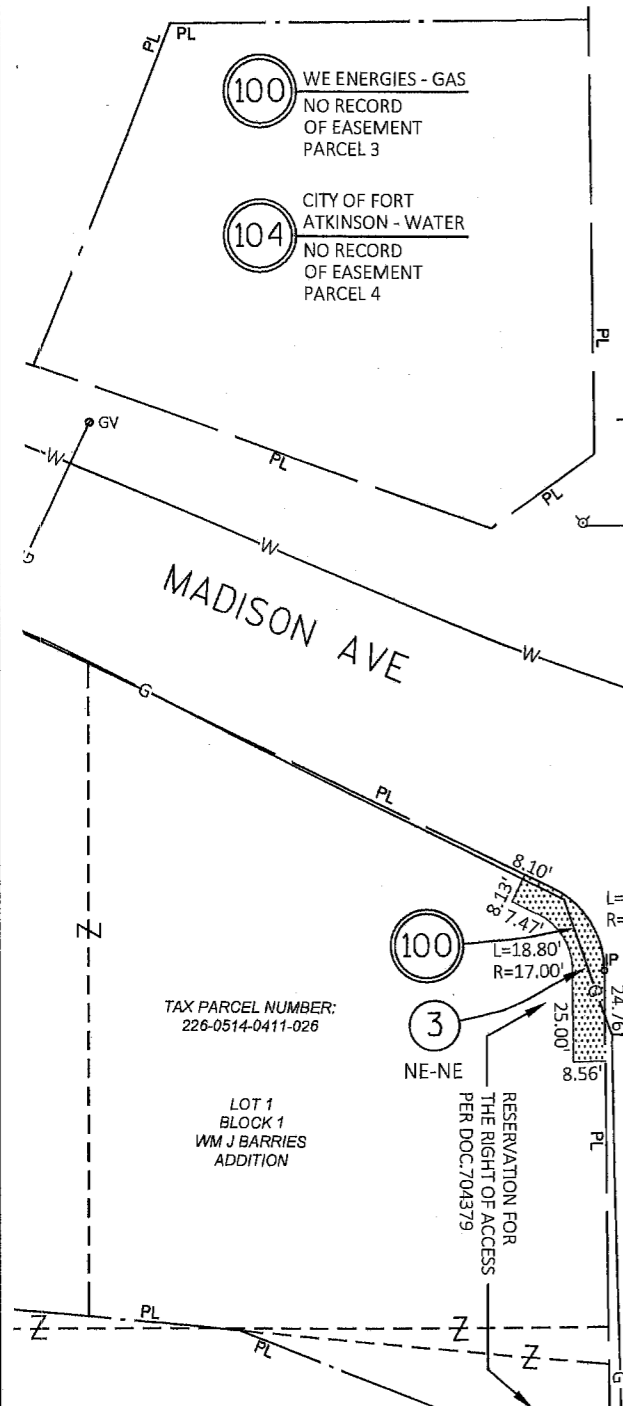
ACCESS RIGHTS	AR	OUTLOT	OL
ACRES	AC	PAGE	P
AHEAD	AH	POINT OF TANGENCY	PT
ALUMINUM	ALUM	PROPERTY LINE	PL
AND OTHERS	ET AL	RECORDED AS (100')	
BACK	BK	REEL / IMAGE	R/I
BLOCK	BLK	REFERENCE LINE	R/L
CENTERLINE	C/L	PERMANENT LIMITED EASEMENT	PLE
CERTIFIED SURVEY MAP	CSM	POINT OF BEGINNING	POB
CONCRETE	CONC	POINT OF CURVATURE	PC
COUNTY	CO	POINT OF COMPOUND CURVE	PCC
COUNTY TRUNK HIGHWAY	CTH	POINT OF INTERSECTION	PI
DISTANCE	DIST	REMAINING	REM
CORNER	COR	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
DOCUMENT NUMBER	DOC	RIGHT	RT
EASEMENT	EASE	RIGHT OF WAY	R/W
EXISTING	EX	SECTION	SEC
GAS VALVE	GV	SEPTIC VENT	SEP
GRID NORTH	GN	SQUARE FEET	SF
HIGHWAY EASEMENT	HE	STATE TRUNK HIGHWAY	STH
IDENTIFICATION	ID	STATION	STA
LAND CONTRACT	LC	TELEPHONE PEDESTAL	TP
LEFT	LT	TEMPORARY LIMITED EASEMENT	TLE
MONUMENT	MON	TRANSPORTATION PROJECT PLAT	TPP
NATIONAL GEODETIC SURVEY	NGS	UNITED STATES HIGHWAY	USH
NUMBER	NO	VOLUME	V



R/W PROJECT NUMBER: 3575-02-22 EXHIBIT NUMBER:1
 TLE ACQUISITION EXHIBIT
 CITY OF FORT ATKINSON, WHITEWATER AVENUE
 MADISON AVE TO CTH M JEFFERSON COUNTY
 USH 12
 PART OF THE NE 1/4 OF THE NE 1/4 OF SECTION 4 AND THE NW 1/4 OF THE NW 1/4 OF SECTION 3, ALL IN T5N, R14E, CITY OF FORT ATKINSON, JEFFERSON COUNTY, WISCONSIN.

NOTES:
 THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY. REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.

PURPOSE FOR ALL TLE'S IS FOR GRADING, UNLESS OTHERWISE NOTED.



SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE CITY.

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
28	GNI OF FORT ATKINSON, LLC	TLE	381
3	BADGER47, LLC	TLE	474
4	FORT ATKINSON AREA CHAMBER OF COMMERCE	TLE	149

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
100	WE ENERGIES - GAS	RELEASE OF RIGHTS
104	CITY OF FORT ATKINSON - WATER	RELEASE OF RIGHTS

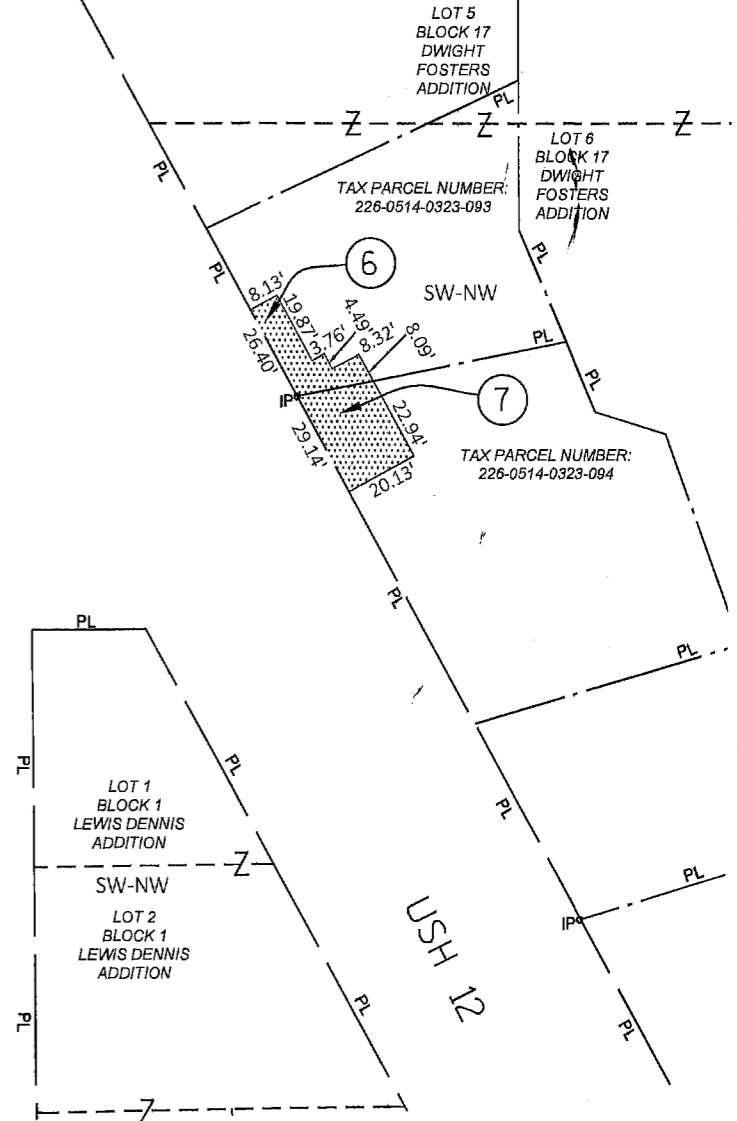
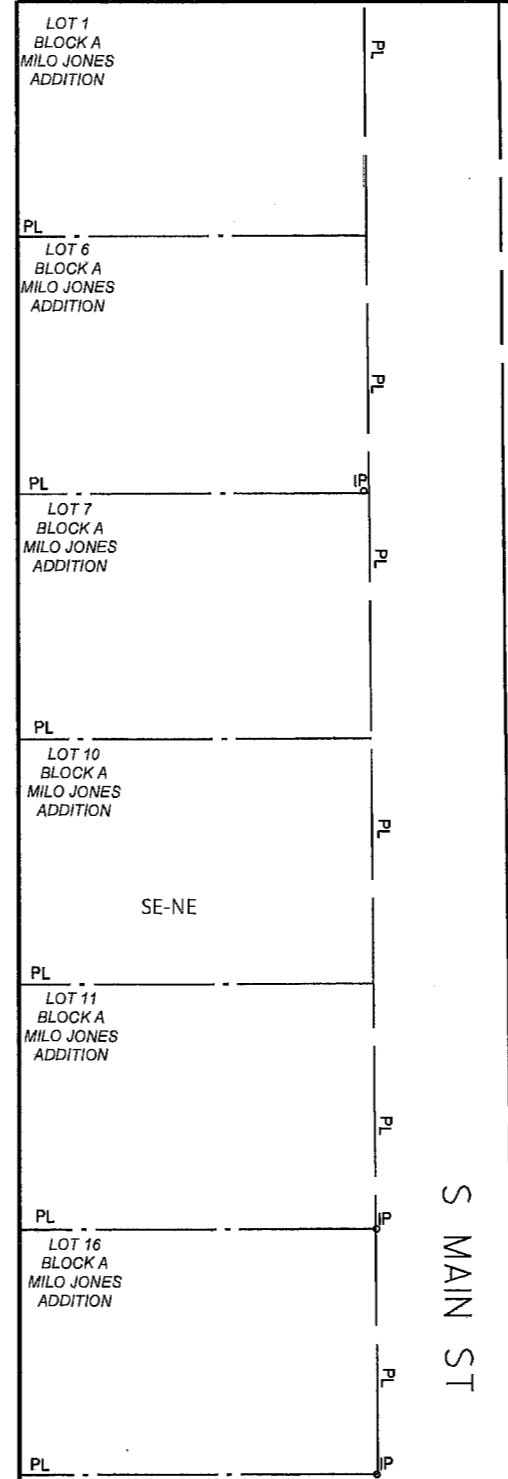
THIS MAP IS APPROVED FOR THE CITY OF FORT ATKINSON.
 REVISD 04-15-22

SIGNATURE: *S/L* DATE: 4/11/22
 PRINT NAME: A. Sella

R/W PROJECT NUMBER: 3575-02-22 EXHIBIT NUMBER:2
 TLE ACQUISITION EXHIBIT
 CITY OF FORT ATKINSON, WHITEWATER AVENUE
 MADISON AVE TO CTH M JEFFERSON COUNTY
 USH 12
 PART OF THE SW 1/4 OF THE NW 1/4 OF SECTION 3, T5N, R14E, CITY OF FORT ATKINSON, JEFFERSON COUNTY, WISCONSIN.

NOTES:
 THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY. REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.

PURPOSE FOR ALL TLE'S IS FOR GRADING, UNLESS OTHERWISE NOTED.



SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE CITY.

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
6	KEVIN J DIECE	TLE	318
7	ASSEMBLY OF PENTECOSTAL CHURCH	TLE	524

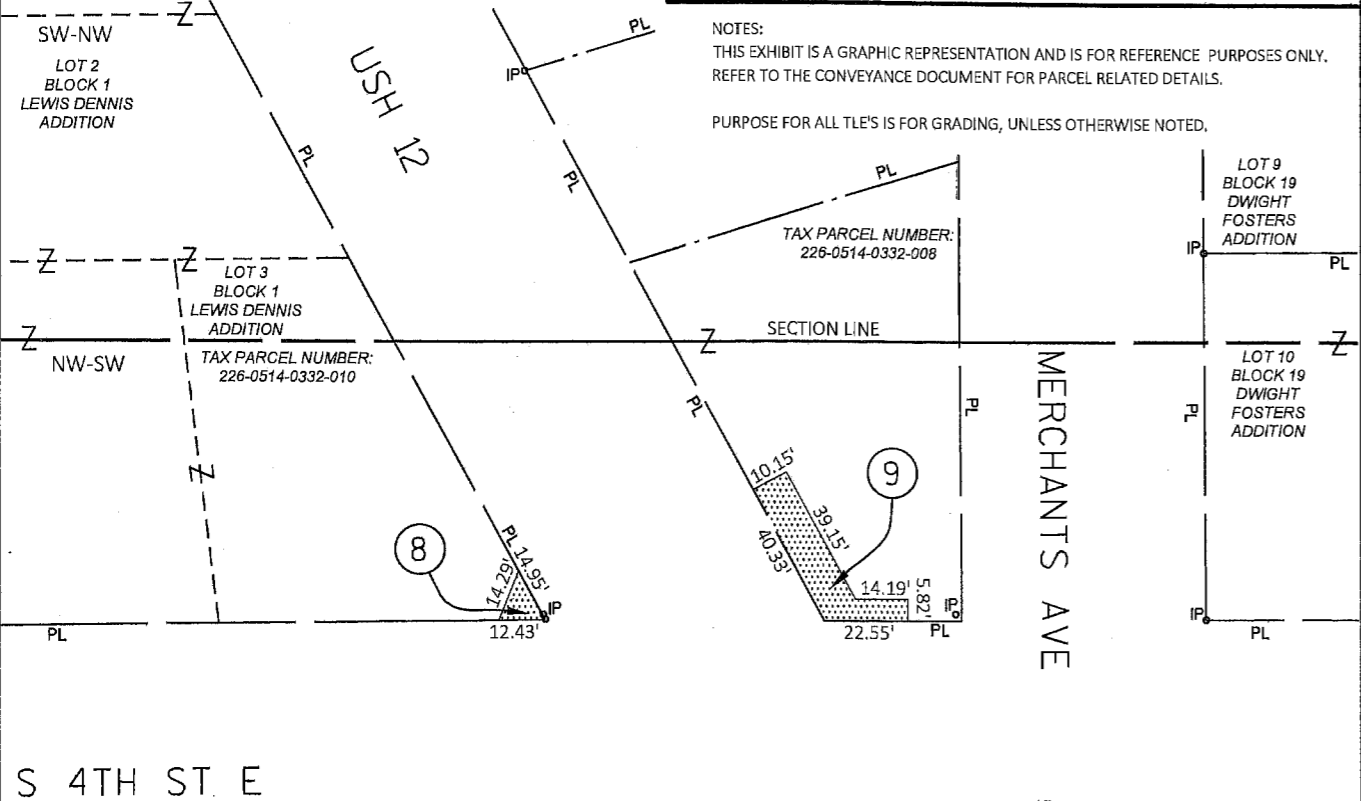
UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A

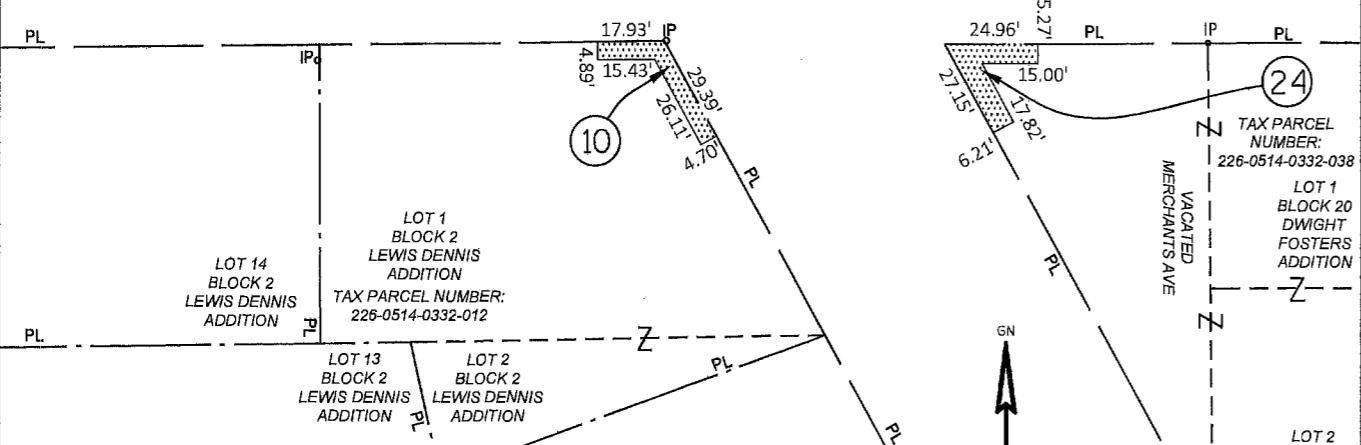
THIS MAP IS APPROVED FOR THE CITY OF FORT ATKINSON.
 REVISD 04-15-22

SIGNATURE: *S/L* DATE: 4/11/22
 PRINT NAME: A. Sella

R/W PROJECT NUMBER: 3575-02-22 EXHIBIT NUMBER:3
 TLE ACQUISITION EXHIBIT
 CITY OF FORT ATKINSON, WHITEWATER AVENUE
 MADISON AVE TO CTH M JEFFERSON COUNTY
 PART OF THE NW 1/4 OF THE SW 1/4 OF SECTION 3, T5N, R14E, CITY OF FORT ATKINSON, JEFFERSON COUNTY, WISCONSIN.



NOTES:
 THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY.
 REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.
 PURPOSE FOR ALL TLE'S IS FOR GRADING, UNLESS OTHERWISE NOTED.



SCHEDULE OF LANDS & INTERESTS REQUIRED

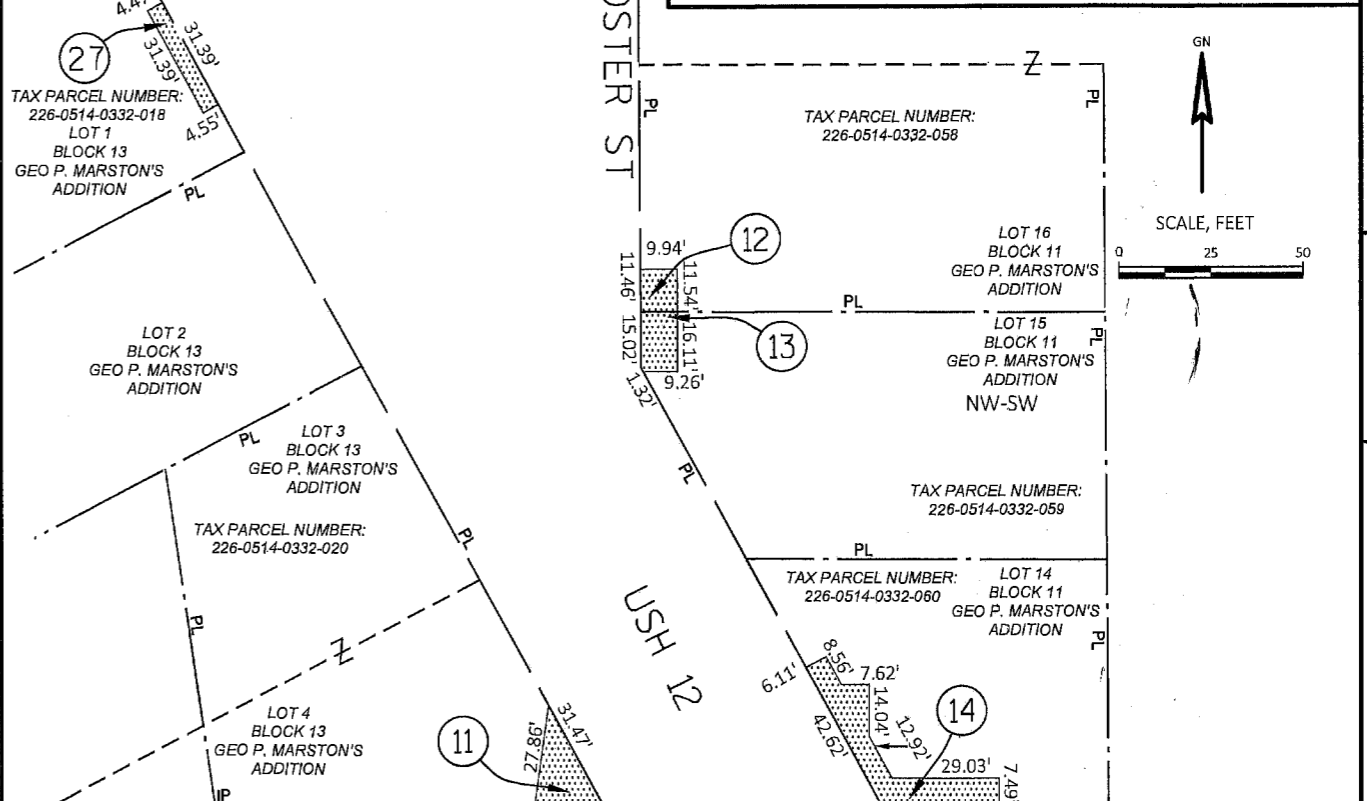
PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
8	CASEY'S MARKETING COMPANY	TLE	82
9	MATTHEW G BIENFANG	TLE	511
10	TIMOTHY A SMILLIE	TLE	213
24	CITY OF FORT ATKINSON	TLE	245

UTILITY INTERESTS REQUIRED

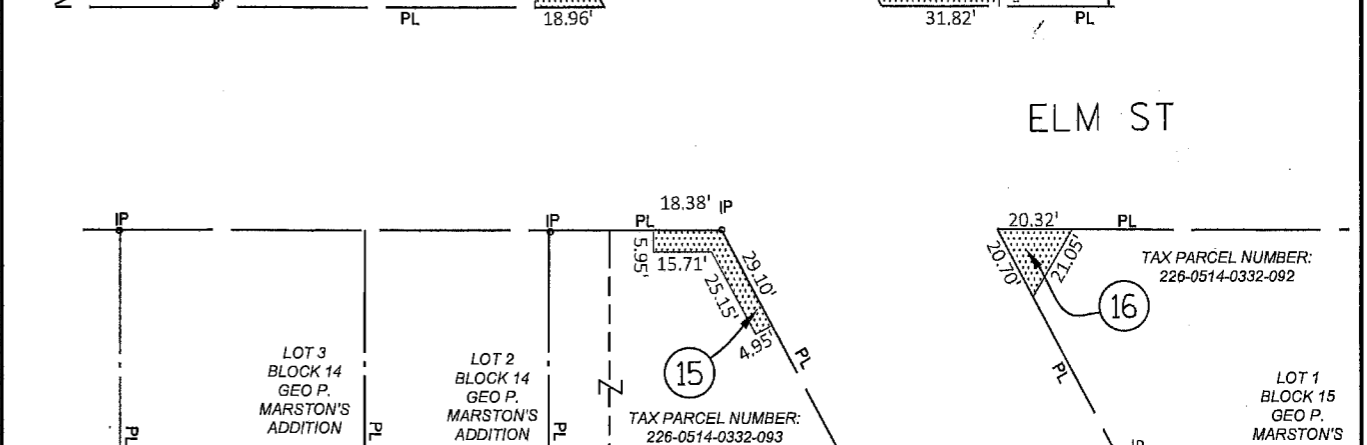
UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE CITY.
 SCALE, FEET
 0 25 50
 THIS MAP IS APPROVED FOR THE CITY OF FORT ATKINSON.
 SIGNATURE: *S/L* DATE: 5/11/22
 PRINT NAME: A. Selk

R/W PROJECT NUMBER: 3575-02-22 EXHIBIT NUMBER:4
 TLE ACQUISITION EXHIBIT
 CITY OF FORT ATKINSON, WHITEWATER AVENUE
 MADISON AVE TO CTH M JEFFERSON COUNTY
 PART OF THE NW 1/4 OF THE SW 1/4 OF SECTION 3, T5N, R14E, CITY OF FORT ATKINSON, JEFFERSON COUNTY, WISCONSIN.



NOTES:
 THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY.
 REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.
 PURPOSE FOR ALL TLE'S IS FOR GRADING, UNLESS OTHERWISE NOTED.



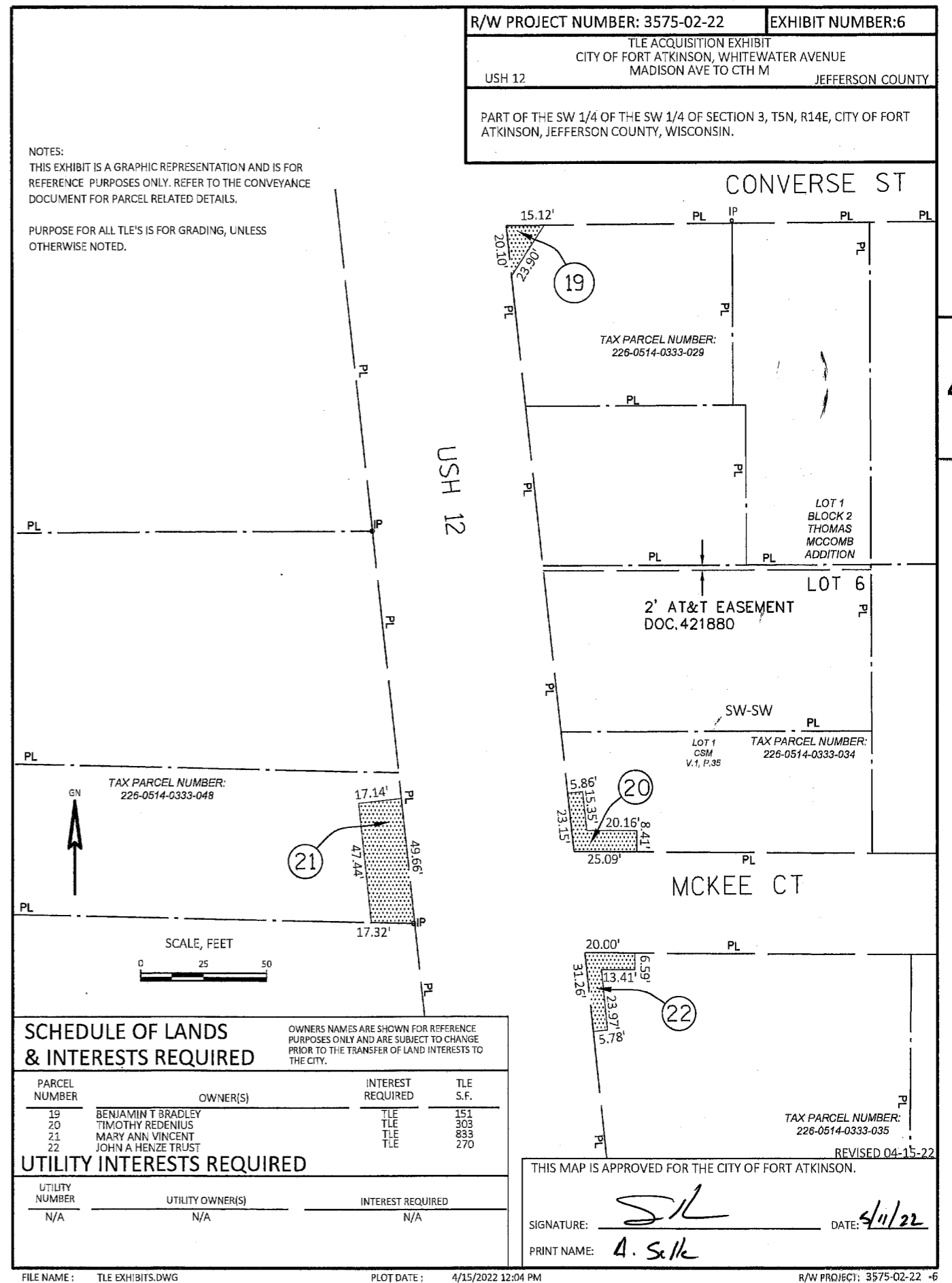
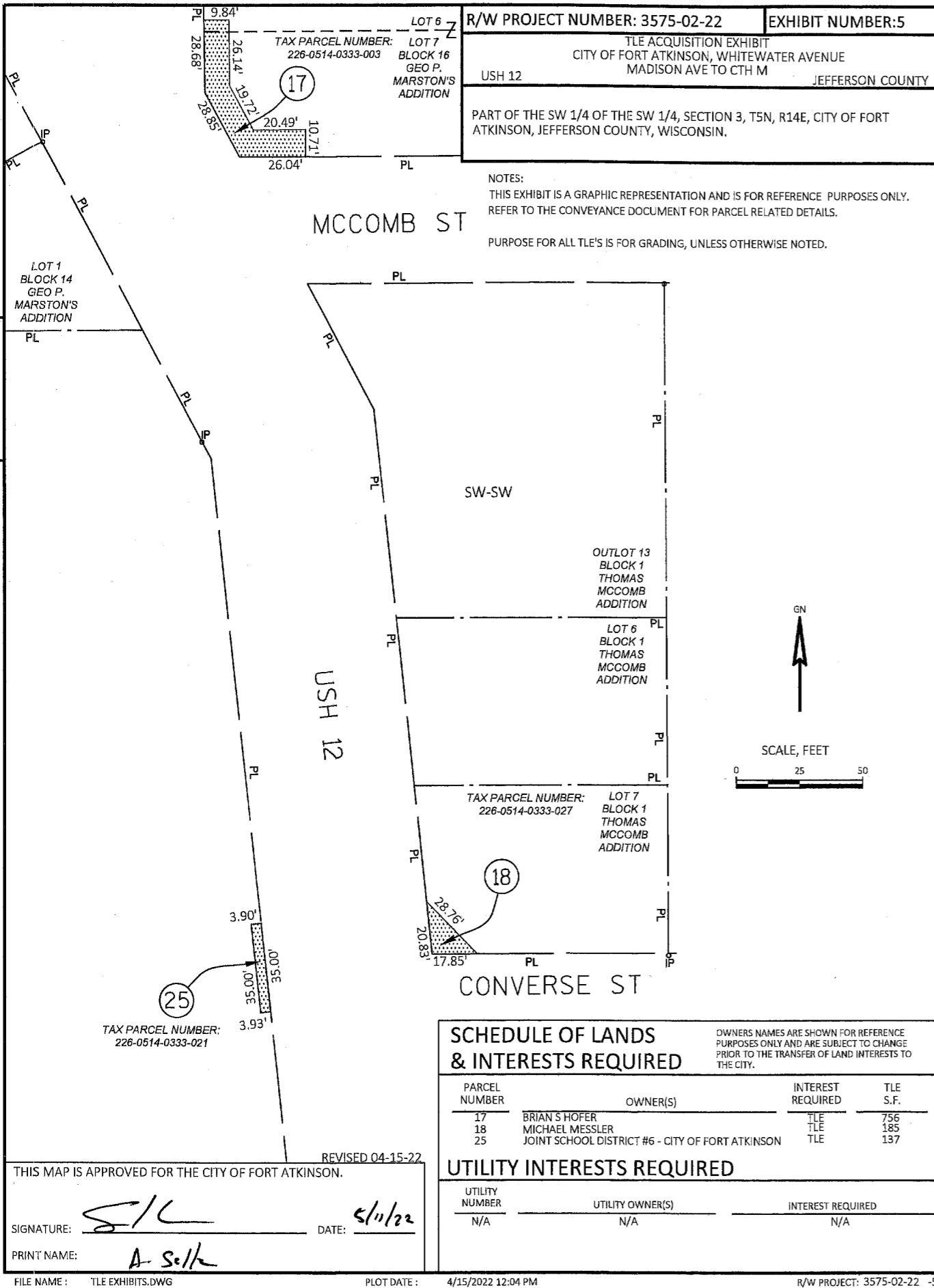
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
11	LUKE W WALSH	TLE	262
12	JERRY D SPAUDE	TLE	114
13	KAYLEE M BURHANS	TLE	160
14	JORDAN L BURROCHER	TLE	523
15	REBECCA REED	TLE	239
16	FIDEL SALAZAR	TLE	185
27	DENNIS L AND MARY LYNN VOGEL LIVING TRUST	TLE	142

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A

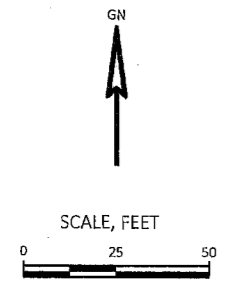
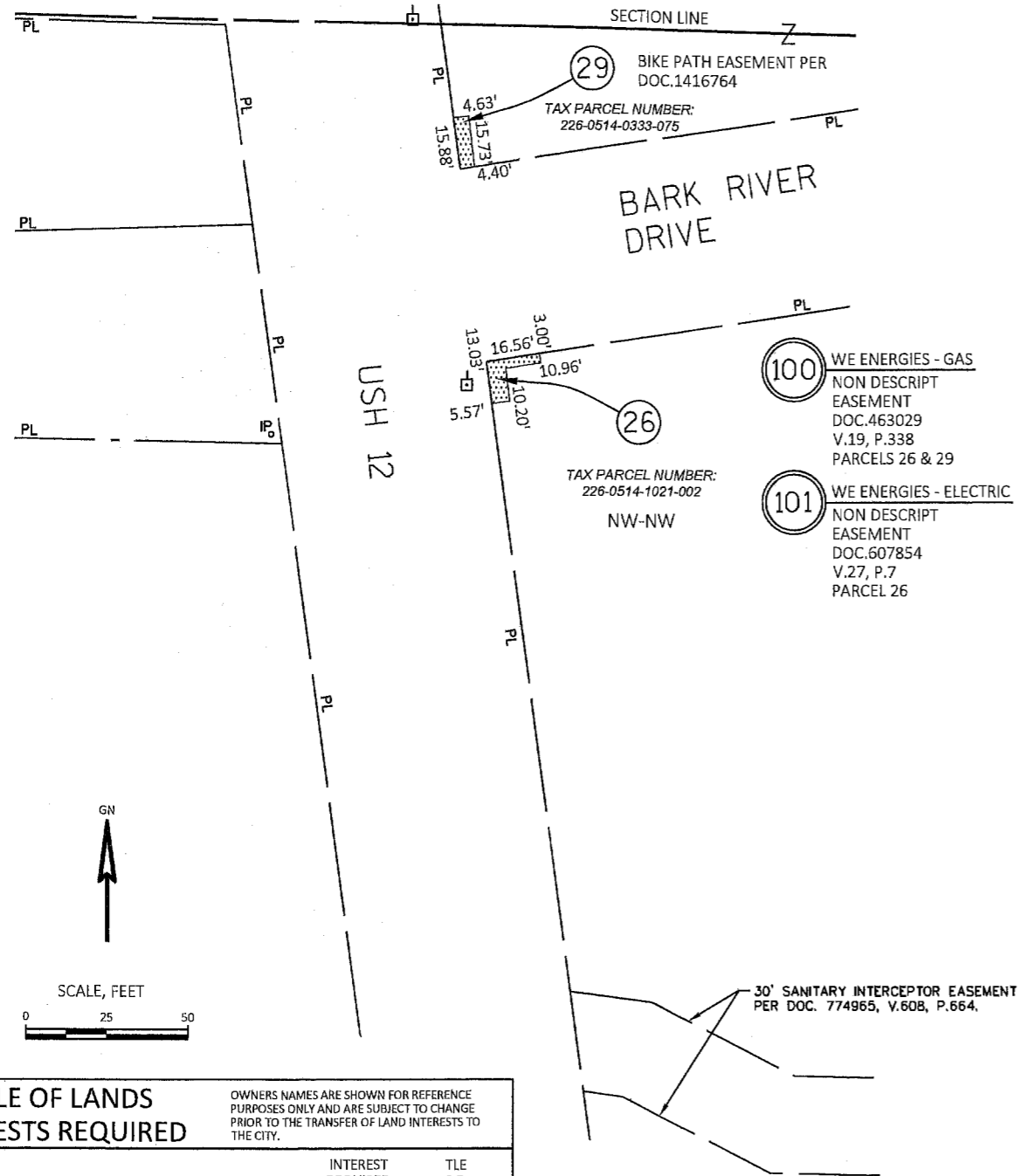
OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE CITY.
 SCALE, FEET
 0 25 50
 THIS MAP IS APPROVED FOR THE CITY OF FORT ATKINSON.
 SIGNATURE: *S/L* DATE: 5/11/22
 PRINT NAME: A. Selk



NOTES:
THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY.
REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.

PURPOSE FOR ALL TLE'S IS FOR GRADING, UNLESS OTHERWISE NOTED.

R/W PROJECT NUMBER: 3575-02-22 EXHIBIT NUMBER:7
TLE ACQUISITION EXHIBIT
CITY OF FORT ATKINSON, WHITEWATER AVENUE
MADISON AVE TO CTH M JEFFERSON COUNTY
PART OF THE NW 1/4 OF THE NW 1/4, SECTION 10, T5N, R14E, CITY OF FORT ATKINSON, JEFFERSON COUNTY, WISCONSIN.



SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE CITY.

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
26 (2)	STATE OF WISCONSIN - ADJUTANT GENERALS OFFICE	TLE	107
29 (2)	THE CITY OF FORT ATKINSON	TLE	71

UTILITY INTERESTS REQUIRED

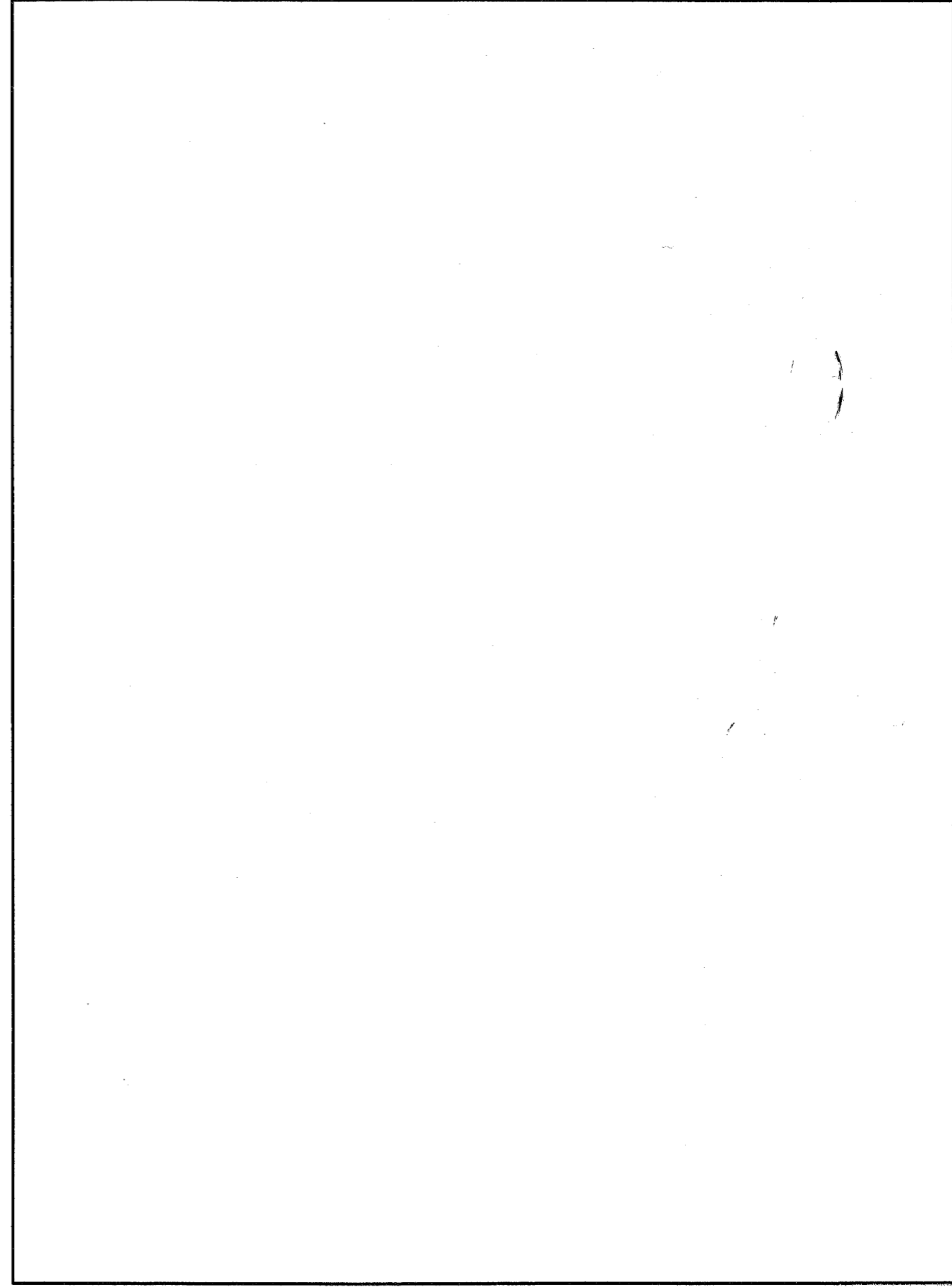
UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
100	WE ENERGIES - GAS	RELEASE OF RIGHTS
101	WE ENERGIES - ELECTRIC	RELEASE OF RIGHTS

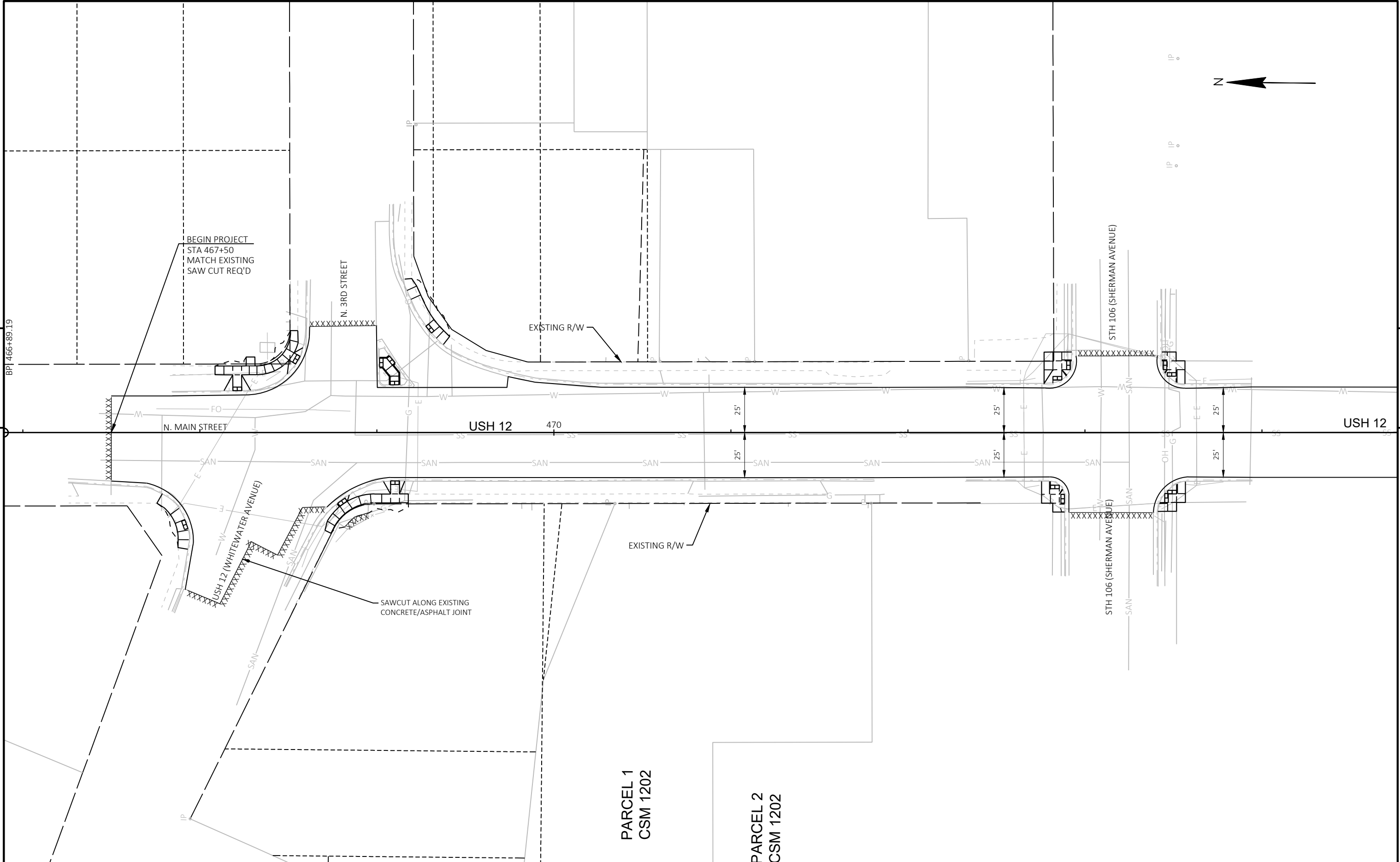
THIS MAP IS APPROVED FOR THE CITY OF FORT ATKINSON. REVISED 04-15-22

SIGNATURE: *[Signature]* DATE: 5/11/22
PRINT NAME: A. S. H.

4

4

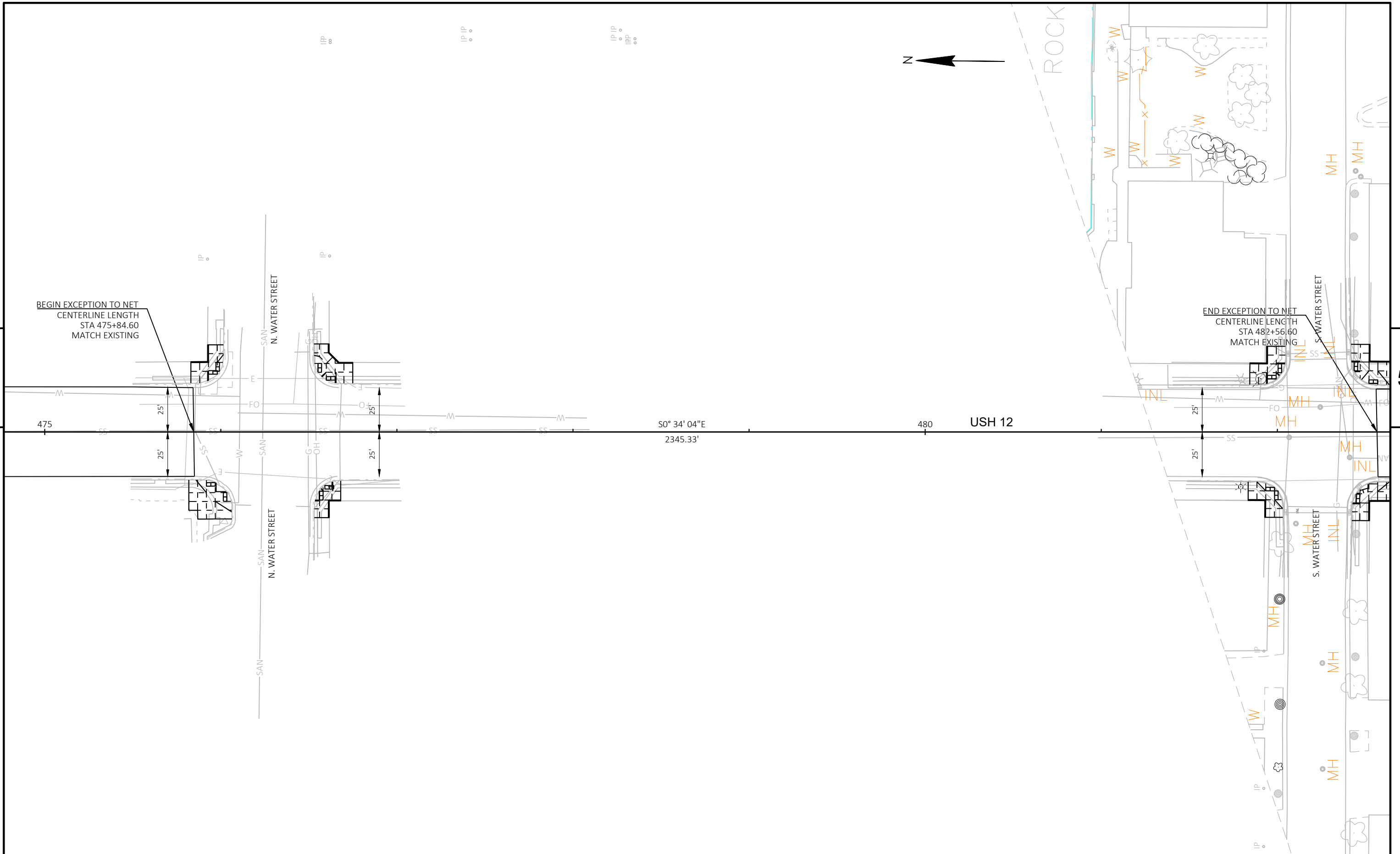




5
BPT 466+89.19

5

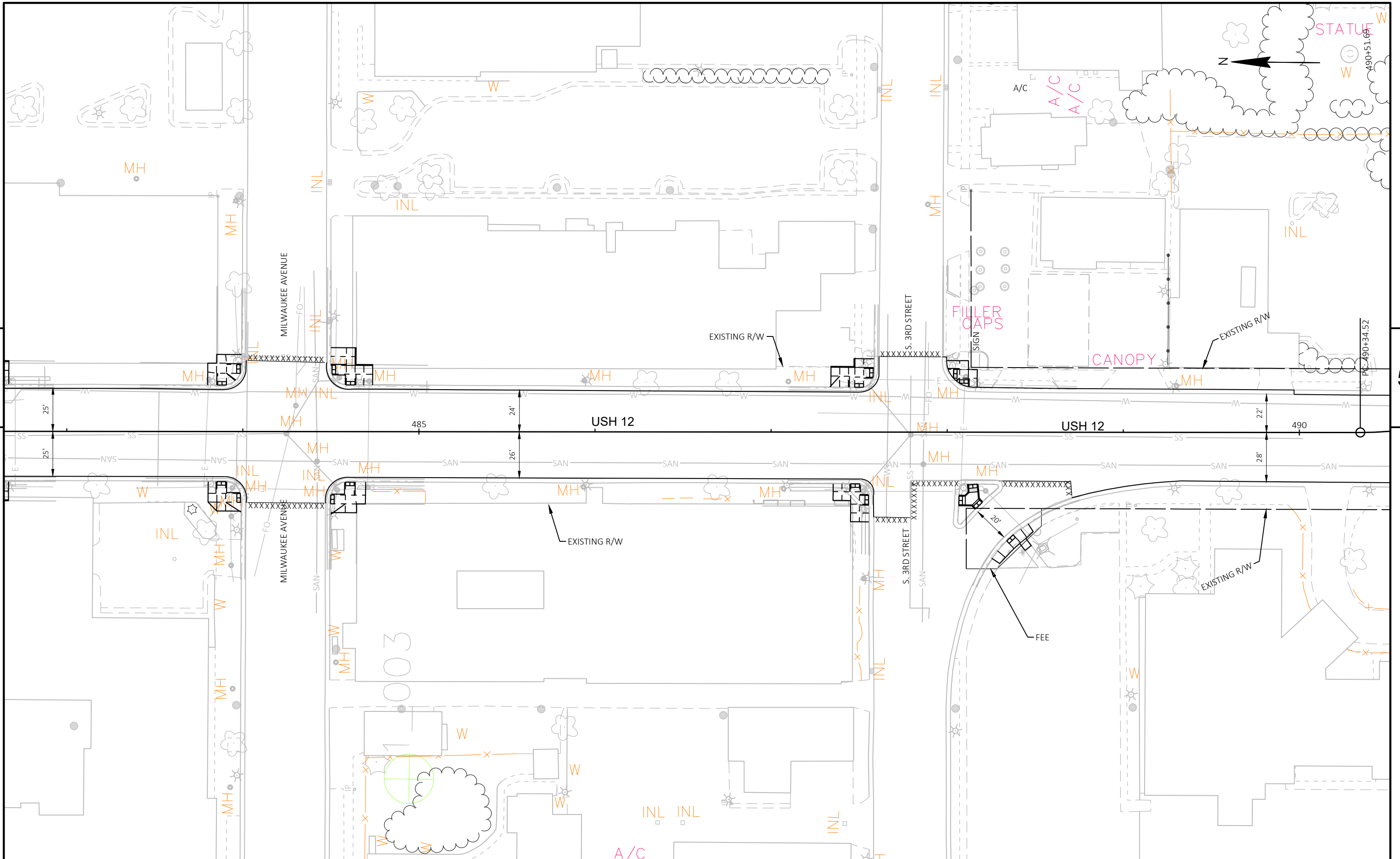
PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	PLAN SHEETS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	PLAN SHEETS	SHEET	E
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PROJECT NO: 3575-02-73

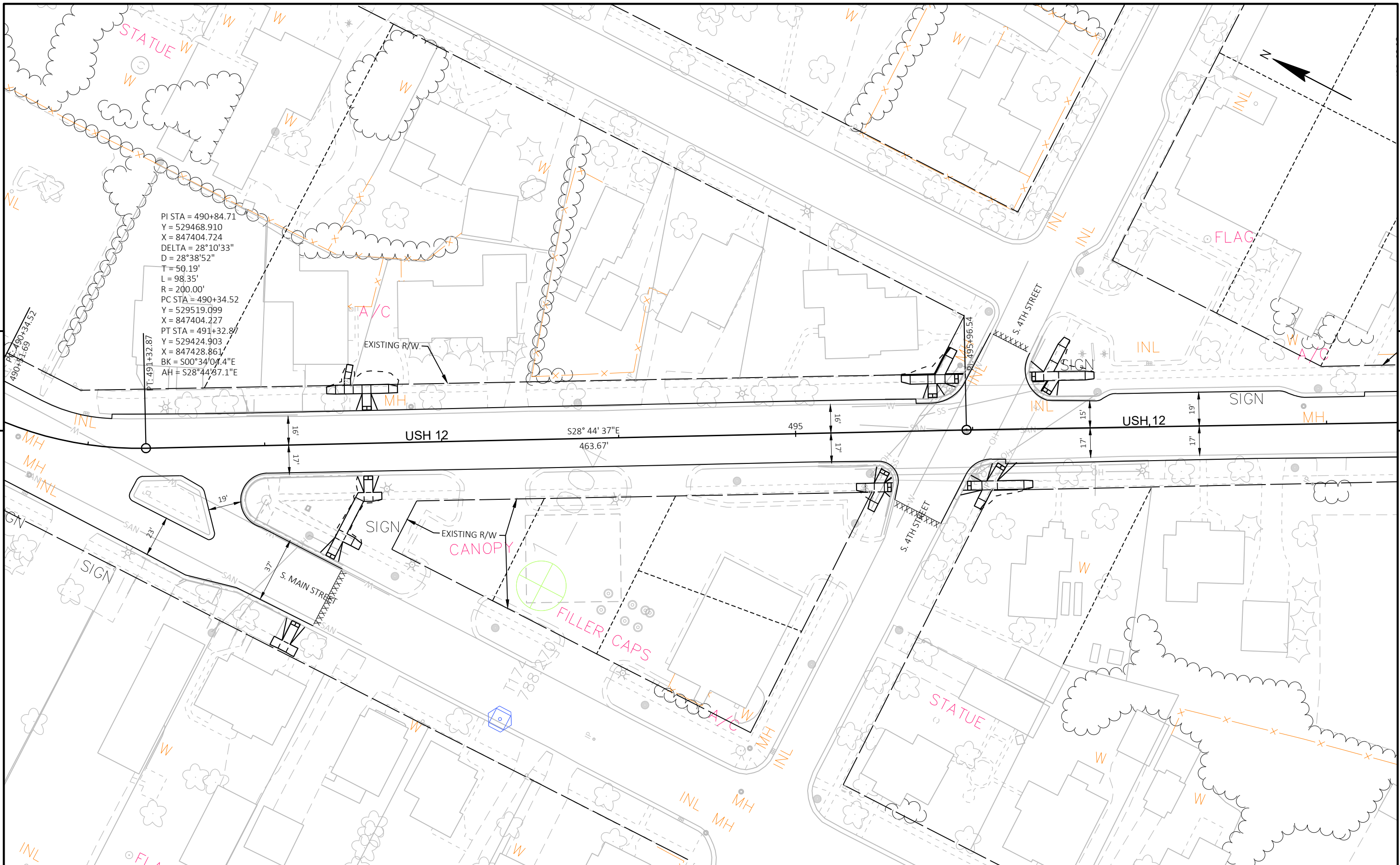
HWY: USH 12

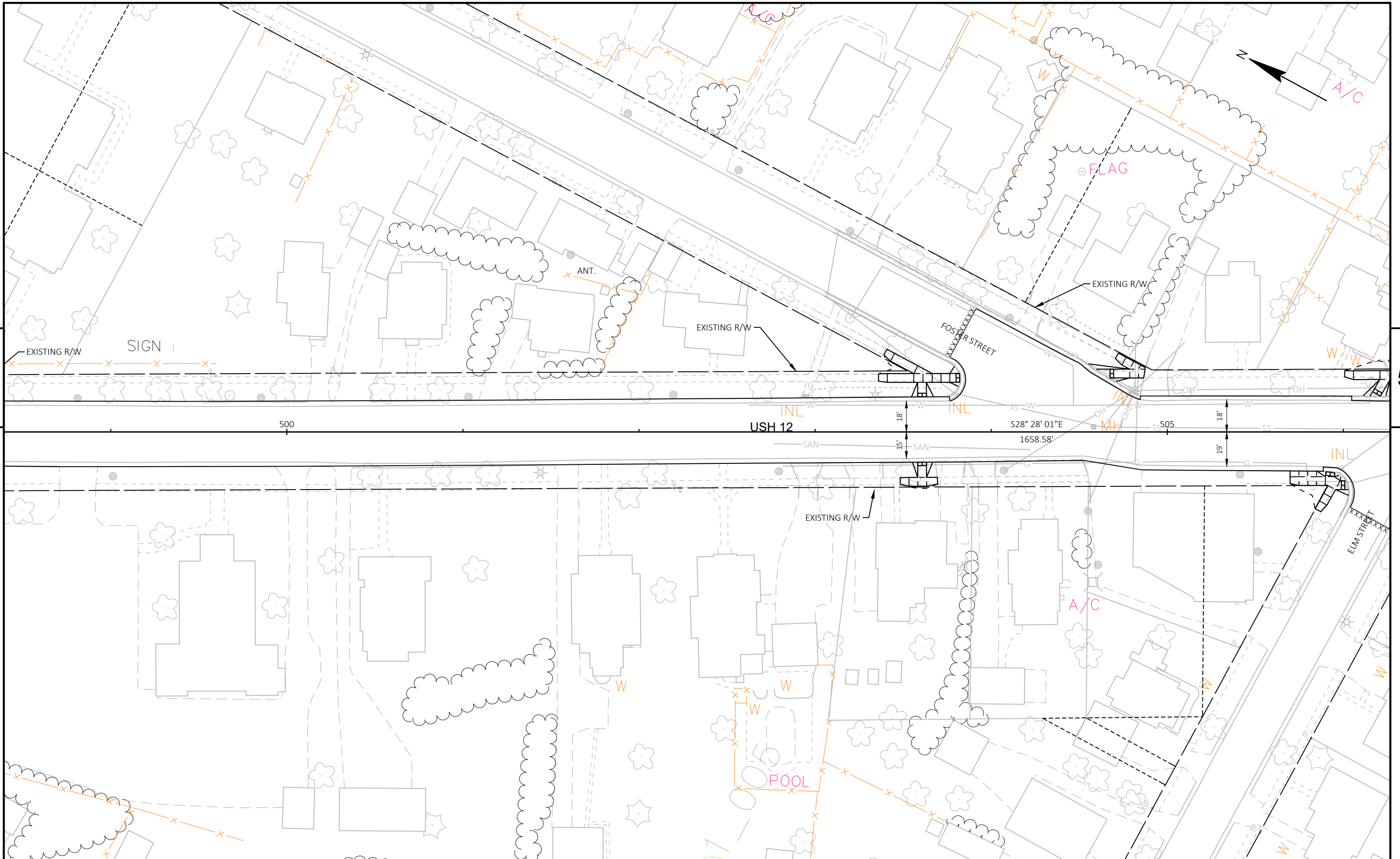
COUNTY: JEFFERSON

PLAN SHEETS

SHEET

E

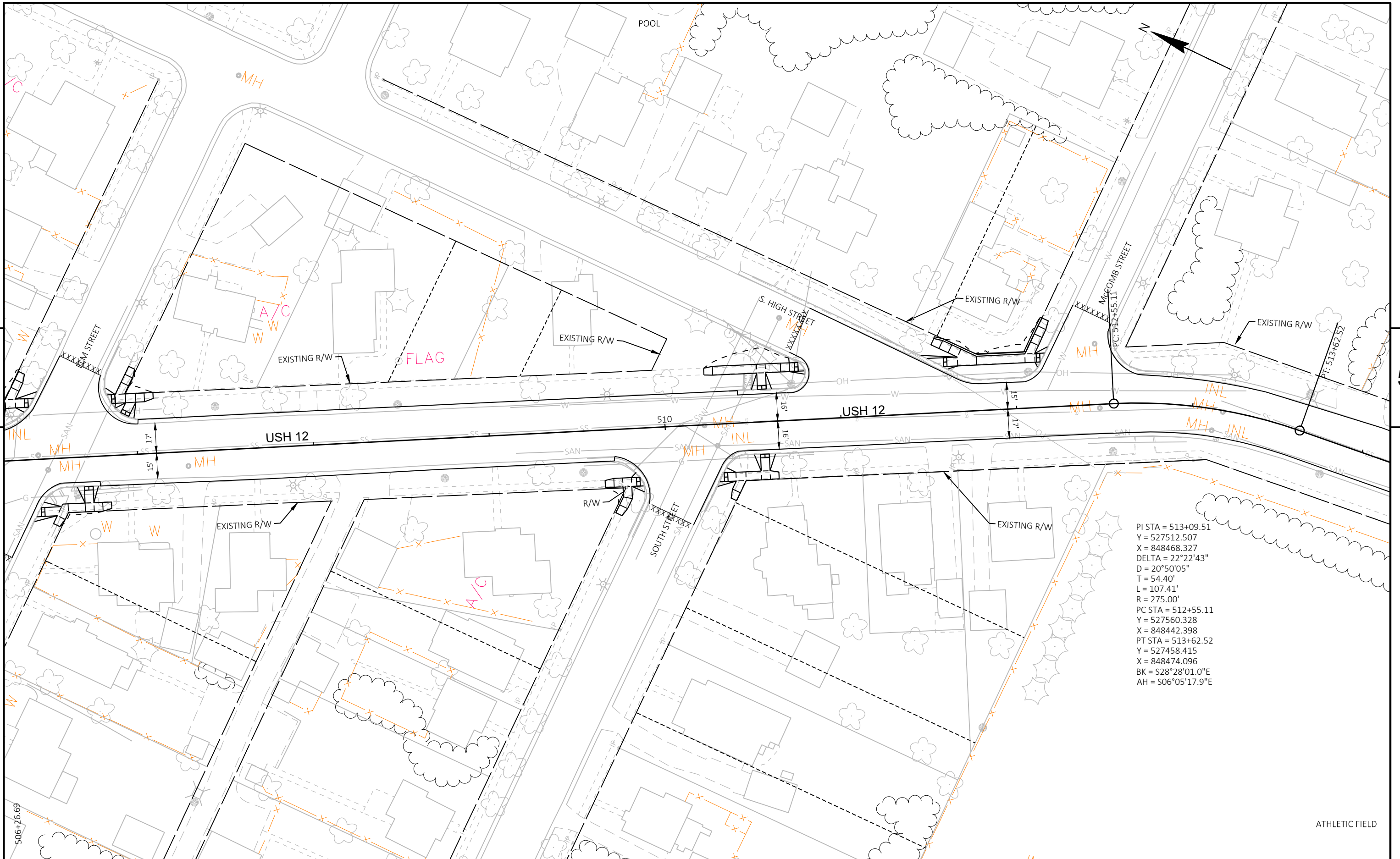




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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	PLAN SHEETS	SHEET	E
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PROJECT NO: 3575-02-73

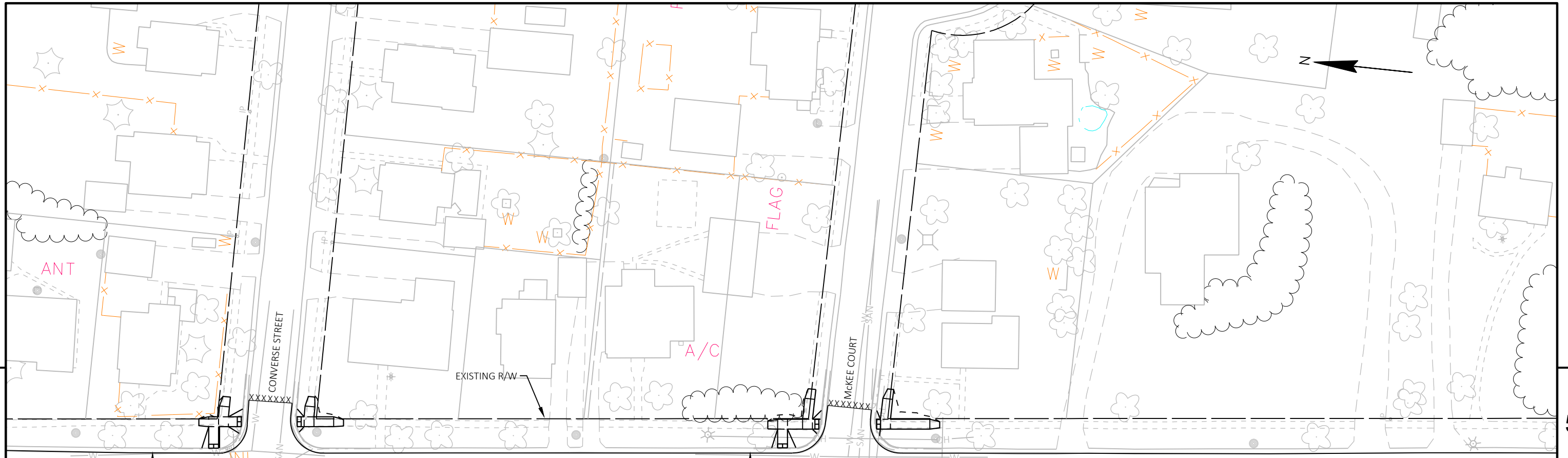
HWY: USH 12

COUNTY: JEFFERSON

PLAN SHEETS

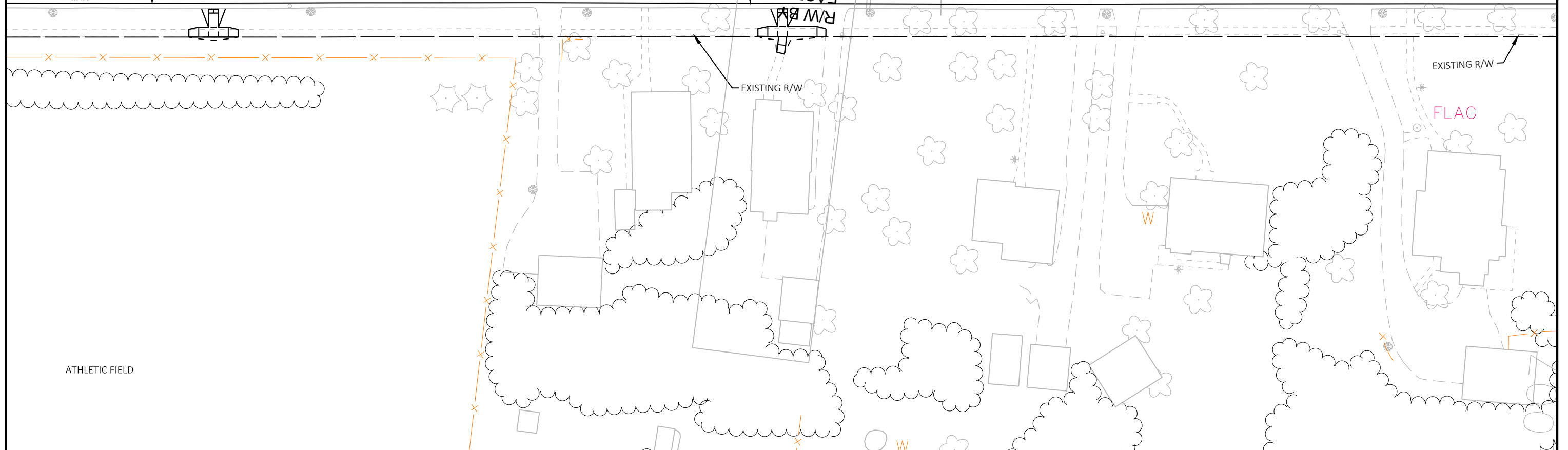
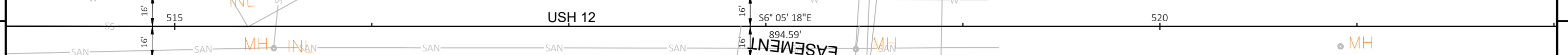
SHEET

E

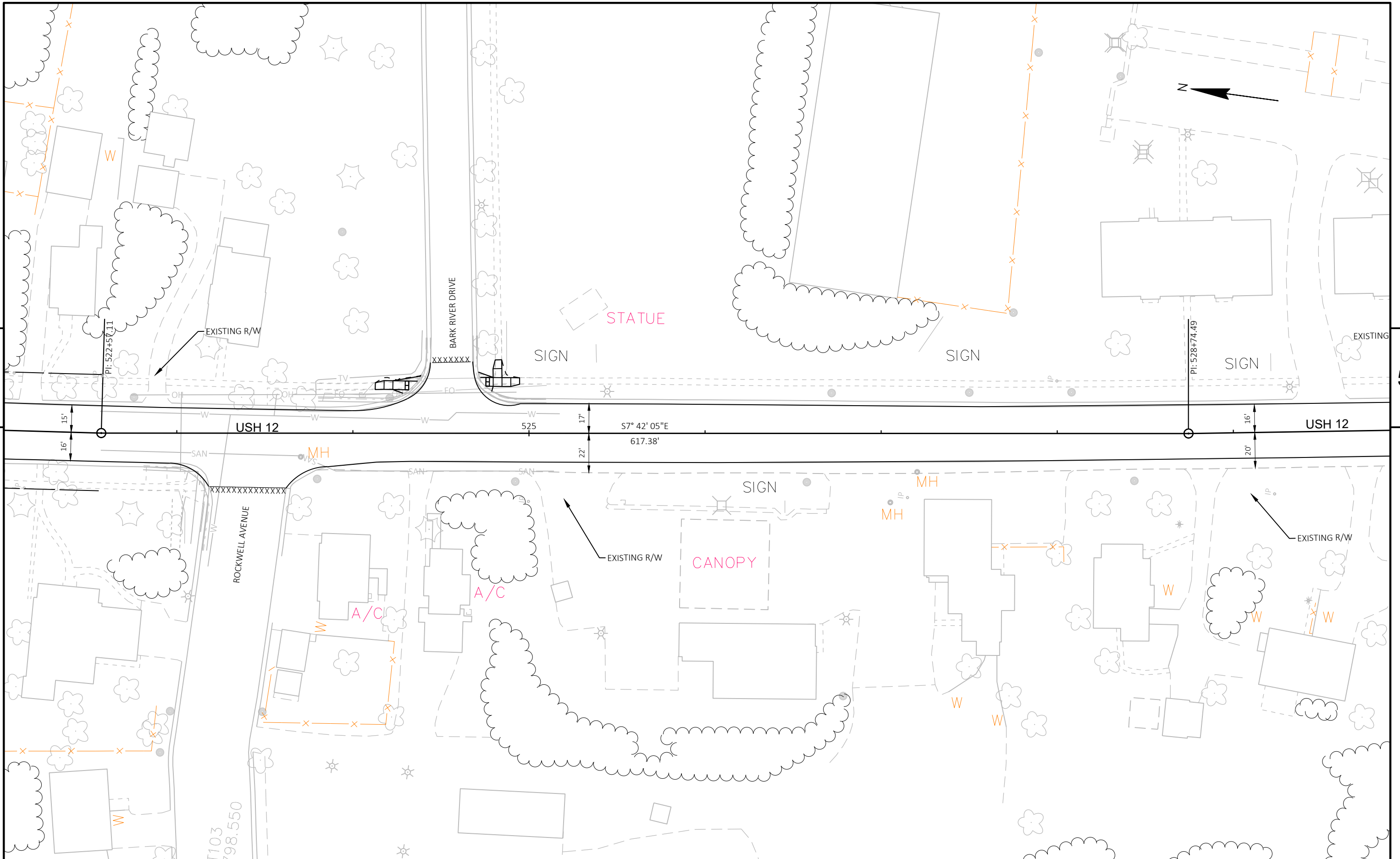


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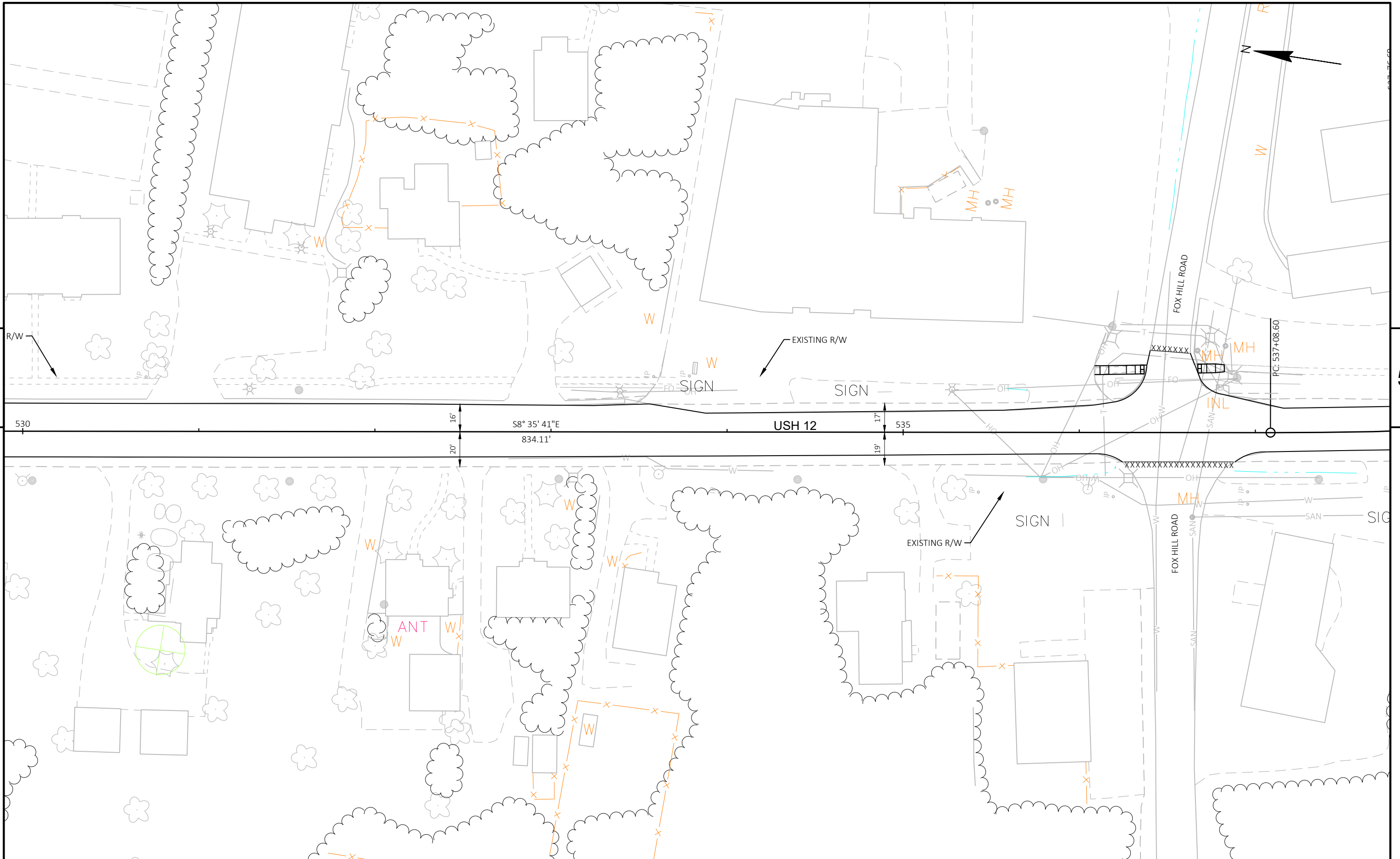
PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	PLAN SHEETS	SHEET	E
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PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	PLAN SHEETS	SHEET	E
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5

5

530 16' 17' 535 19' 834.11' S8° 35' 41"E

PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	PLAN SHEETS	SHEET	E
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FILE NAME : N:\PDS\C3D\35750204\SHEETSPLAN\050201-PLAN SHEETS\050201-PN.DWG PLOT DATE : 12/13/2022 8:54 AM PLOT BY : WHITE, JEREMY J PLOT NAME : PLOT SCALE : ##### WISDOT/CADD SHEET 44

PI STA = 539+44.15
 Y = 524899.412
 X = 848811.566
 DELTA = 10°45'54"
 D = 2°17'31"
 T = 235.55'
 L = 469.71'
 R = 2500.00'
 PC STA = 537+08.60
 Y = 525132.317
 X = 848776.365
 PT STA = 541+78.32
 Y = 524677.181
 X = 848889.650
 BK = S08°35'40.6"E
 AH = S19°21'34.8"E



5

5

USH 12

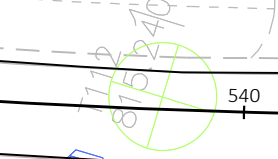
SIGN

SIGN

SIGN

SIGN

EXISTING R/W



PT: 541+78.32

16
18

545

S19°21'34.8"E
754.26'

SIGN

SIGN

SIGN

SIGN

SIGN

SIGN

EXISTING R/W

EXISTING R/W

CANOPY

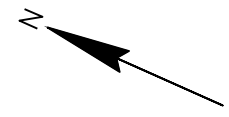
A/C

ANT

A/C

INL

553+51.69



PI STA = 551+12.74
Y = 523795.588
X = 849199.411
DELTA = 15°25'46"
D = 4°18'29"
T = 180.17'
L = 358.16'
R = 1330.00'
PC STA = 549+32.57
PT STA = 552+90.74

5

5



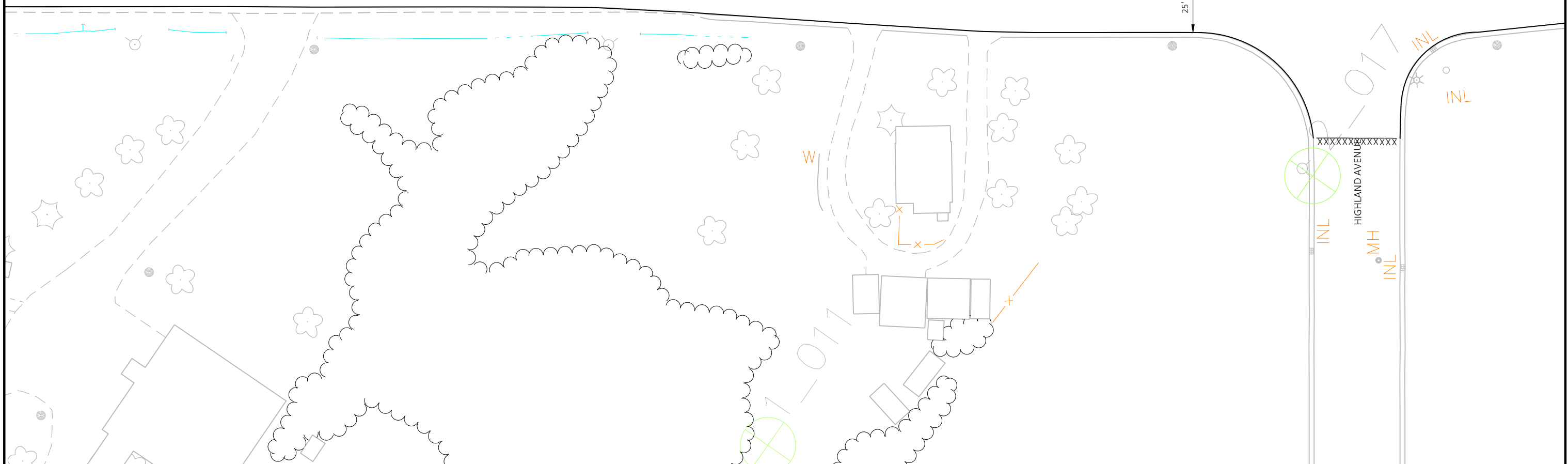
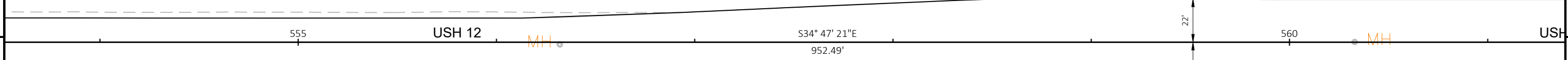
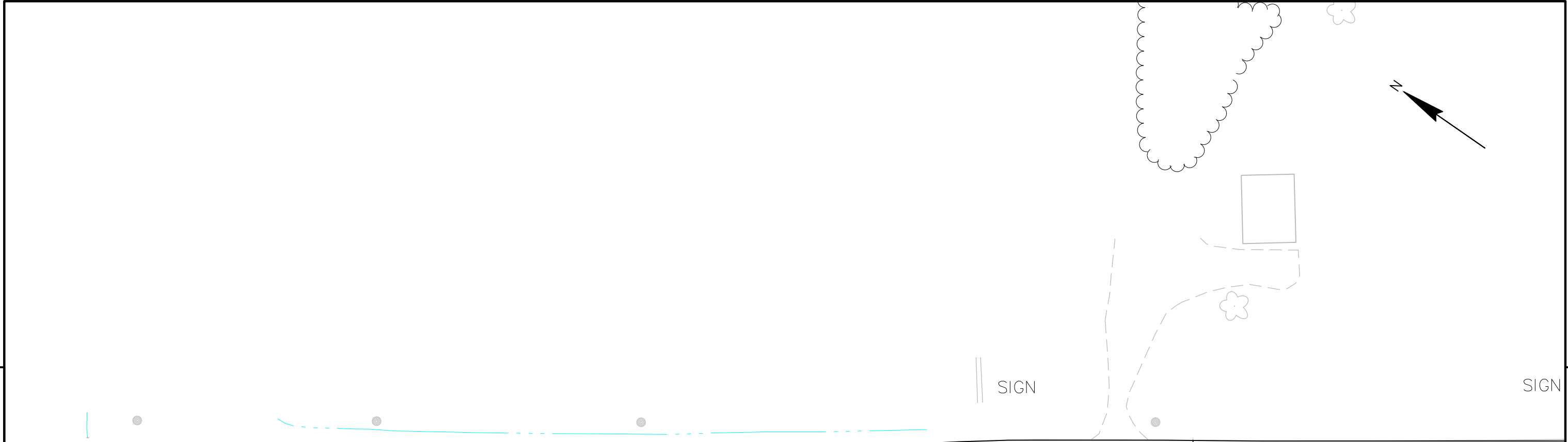
PROJECT NO: 3575-02-73 HWY: USH 12 COUNTY: JEFFERSON PLAN SHEETS SHEET E

FILE NAME : N:\PDS\C3D\35750204\SHEETSPLAN\050201-PLAN SHEETS\050201-PN.DWG PLOT DATE : 12/13/2022 8:54 AM PLOT BY : WHITE, JEREMY J PLOT NAME : PLOT SCALE : #####

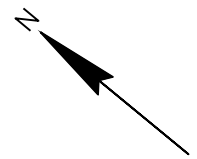
WISDOT/CADD SHEET 44

5

5



PROJECT NO: 3575-02-73	HWY: USH 12	COUNTY: JEFFERSON	PLAN SHEETS	SHEET	E
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569+26.69

PI STA = 564+09.78
 Y = 522728.600
 X = 849940.689
 DELTA = 6°39'15"
 D = 2°00'00"
 T = 166.55'
 L = 332.73'
 R = 2864.97'
 PC STA = 562+43.23
 Y = 522865.381
 X = 849845.662
 PT STA = 565+75.95
 Y = 522603.752
 X = 850050.925
 BK = S34°47'21.0"E
 AH = S41°26'35.9"E

PC: 562+43.23

PT: 565+75.95

PC: 566+75.69

5

5

12

13' 17'

565

S41° 26' 36"E
99.73'

USH 12

SIGN

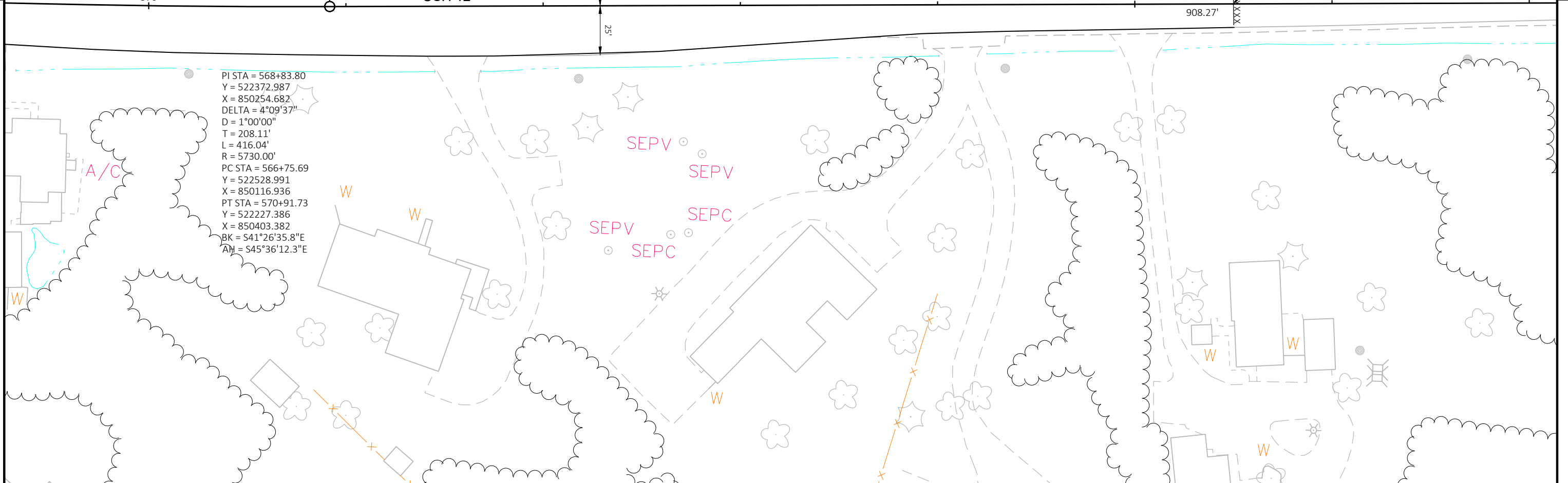
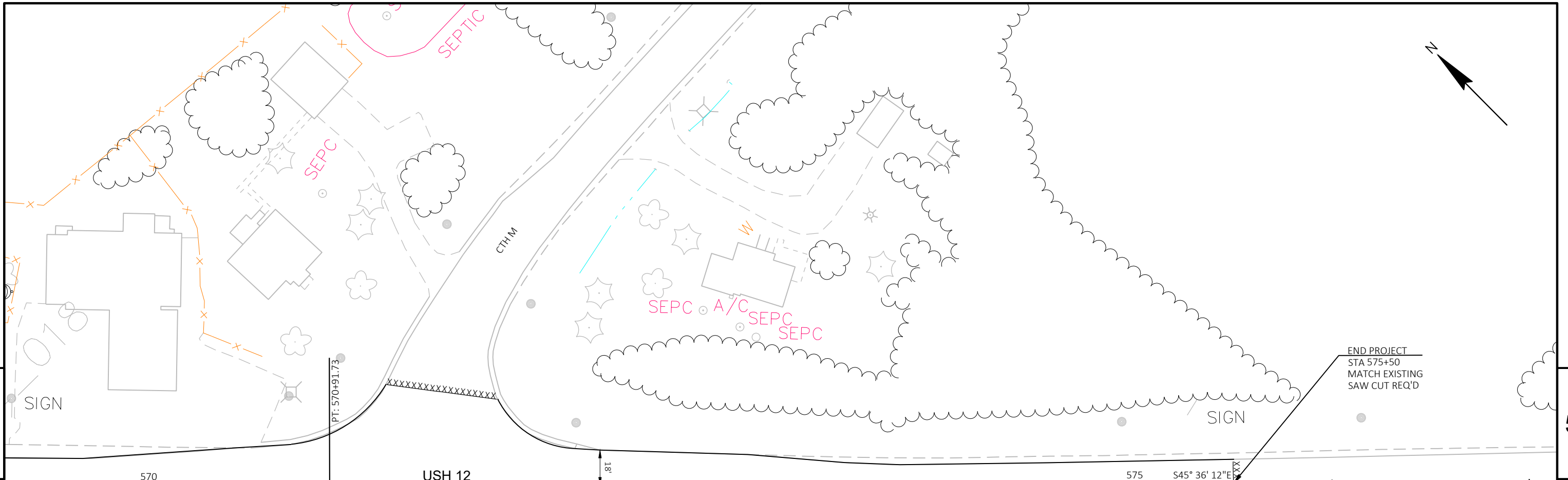
SIGN

SIGN

CHK

SIGN

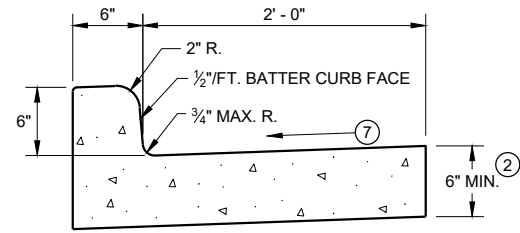
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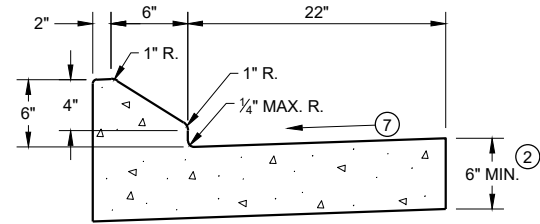
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Standard Detail Drawing List

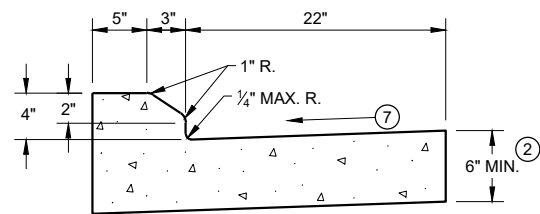
08D01-22A	CONCRETE CURB & GUTTER
08D01-22B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
13C01-19	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
13C19-03	HMA LONGITUDINAL JOINTS
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C07-15A	PAVEMENT MARKING SYMBOLS
15C07-15B	PAVEMENT MARKING WORDS
15C07-15C	PAVEMENT MARKING ARROWS
15C08-22A	LONGITUDINAL MARKING (MAINLINE)
15C08-22B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-22C	PAVEMENT MARKING (TURN LANES)
15C08-22D	PAVEMENT MARKING (TURN LANES)
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C12-09B	TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C36-01	PARKING STALL MARKING
15D30-07A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-07C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
16A01-07	LANDMARK REFERENCE MONUMENTS AND COVERS



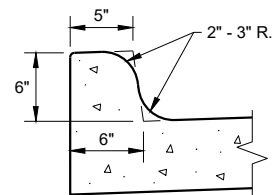
TYPES A^① & D



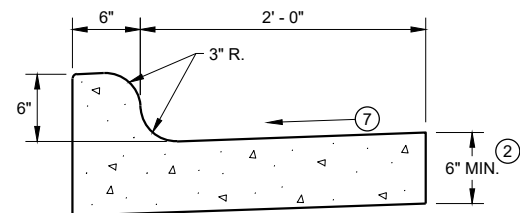
6" SLOPED CURB TYPES G^① & J



4" SLOPED CURB TYPES G^① & J

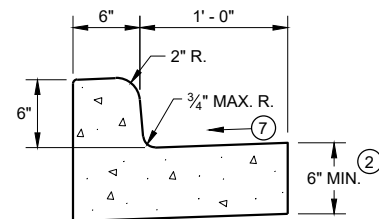


TYPES K^① & L
(OPTIONAL CURB SHAPE)



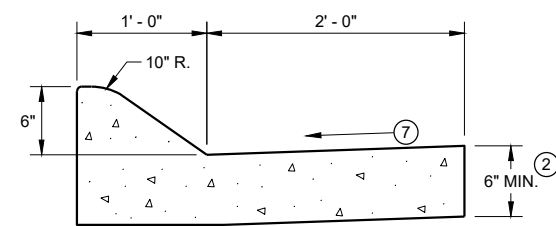
TYPES K^① & L

CONCRETE CURB AND GUTTER 30"

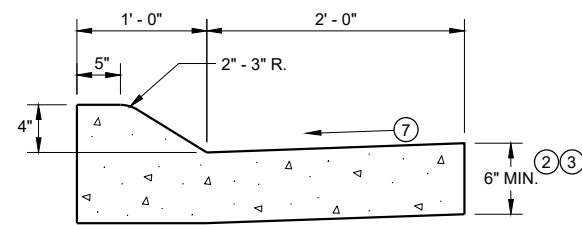


TYPES A^① & D

CONCRETE CURB AND GUTTER 18"

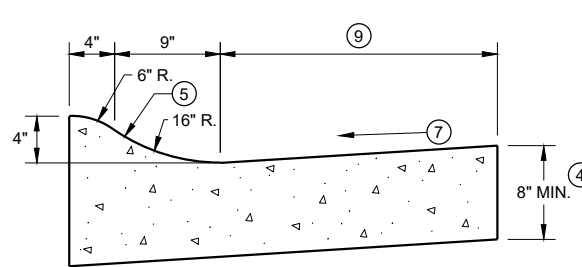


6" SLOPED CURB TYPES A^① & D



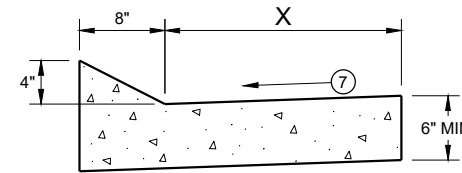
4" SLOPED CURB TYPES A^① & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

TBT & TBTT	X
30"	22"
36"	28"

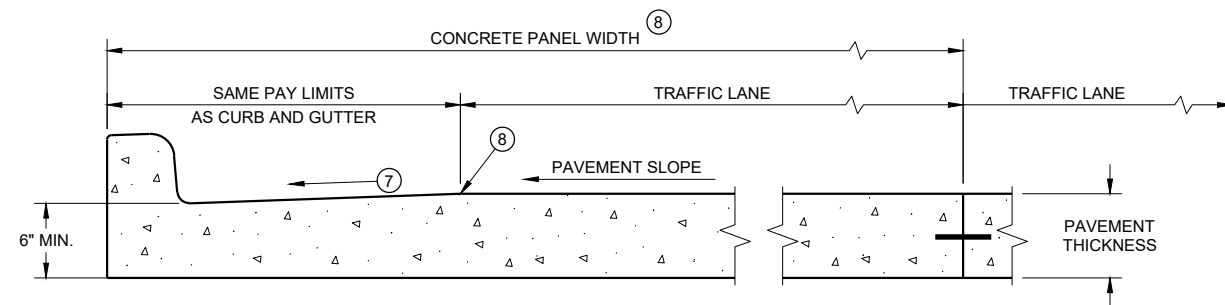


TYPES TBT & TBTT^①

CONCRETE CURB AND GUTTER

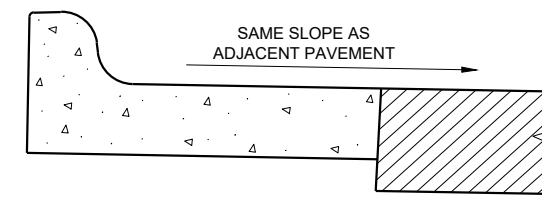
PAVEMENT THICKNESS
AND MAXIMUM CONCRETE
PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

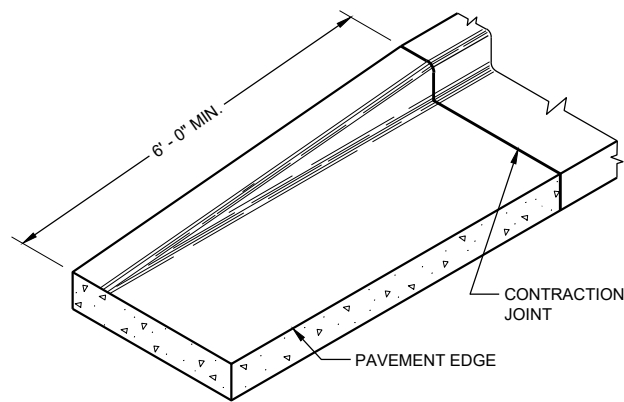
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

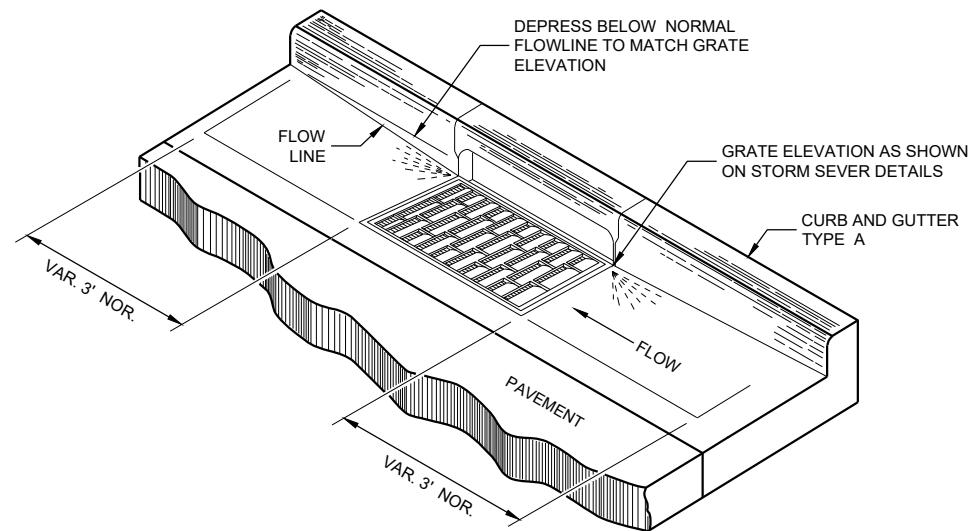
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

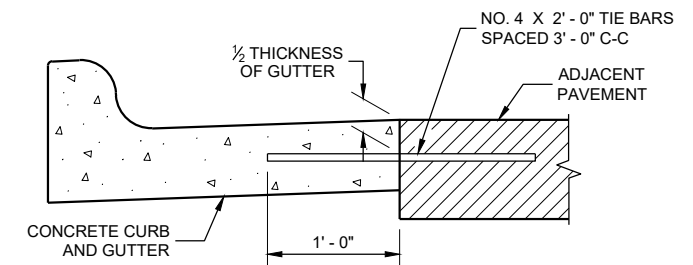
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

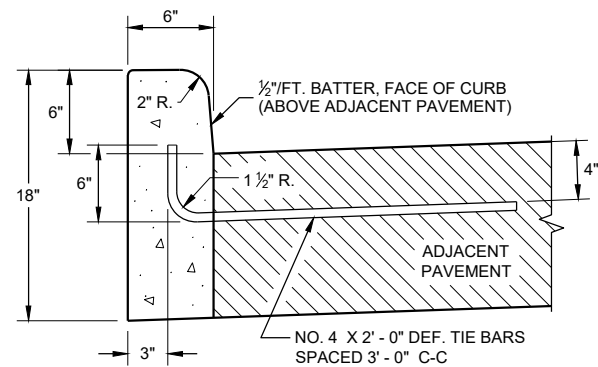
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

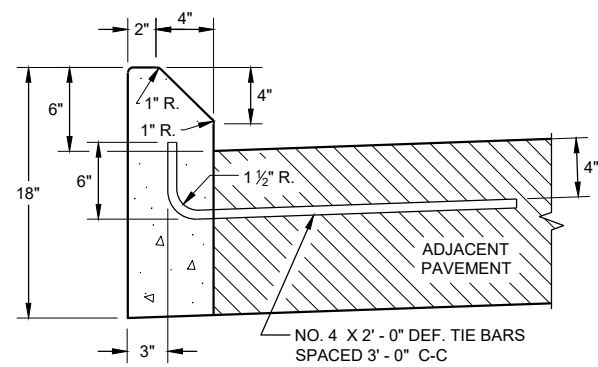
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑨ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION ①

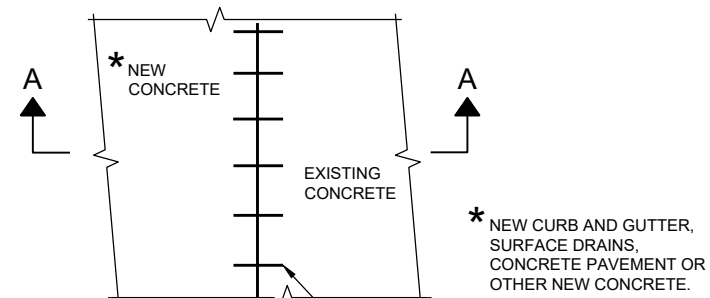


TYPES A ① & D

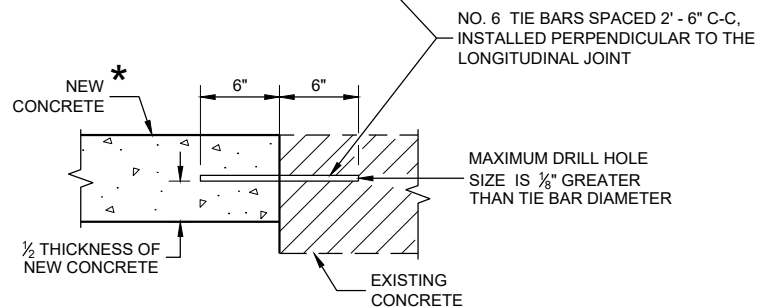


TYPES G ① & J

CONCRETE CURB

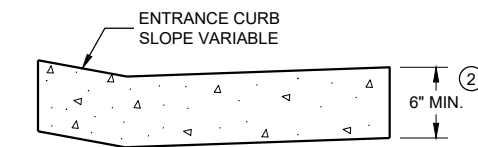


PLAN VIEW



SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



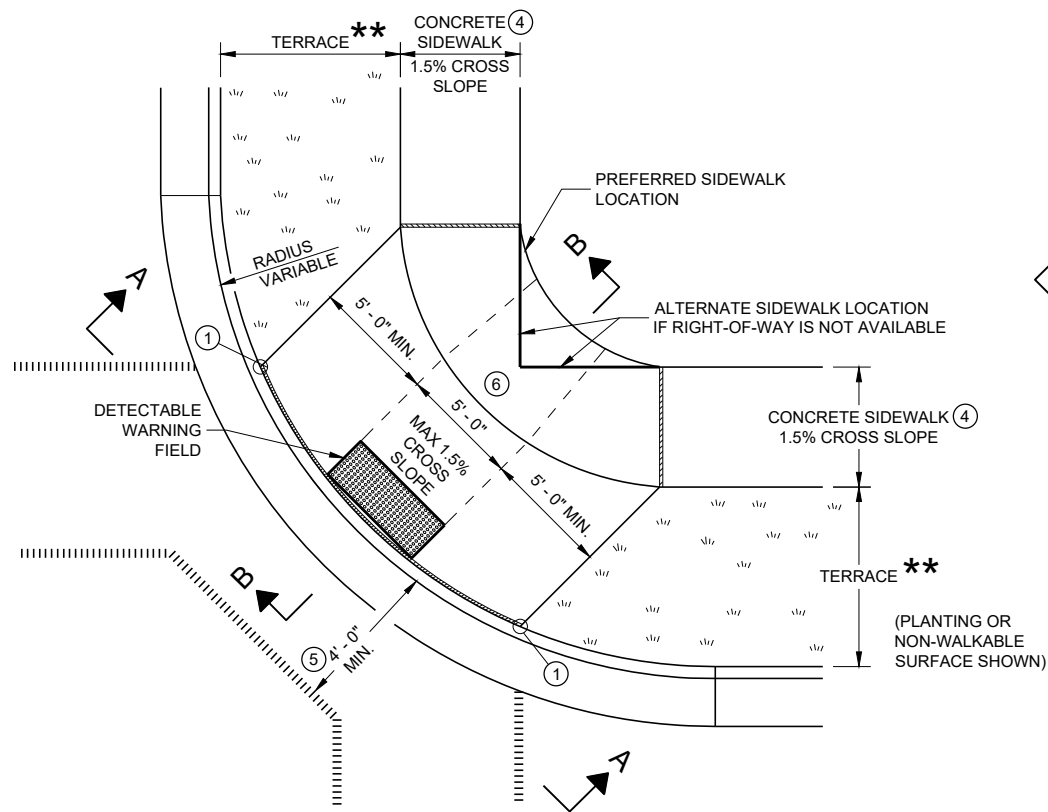
DRIVEWAY ENTRANCE CURB ⑨
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

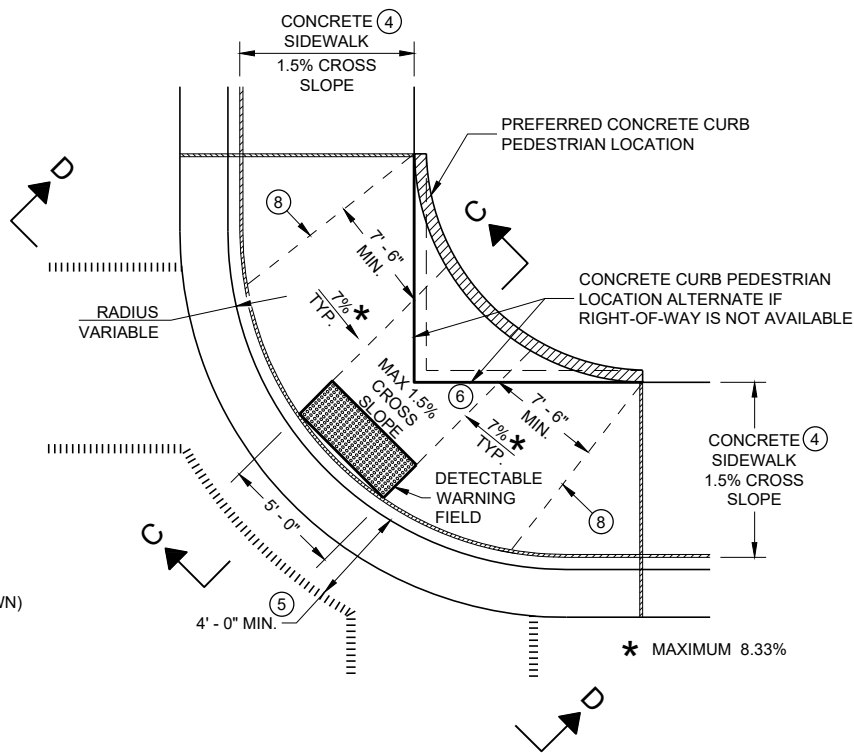
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

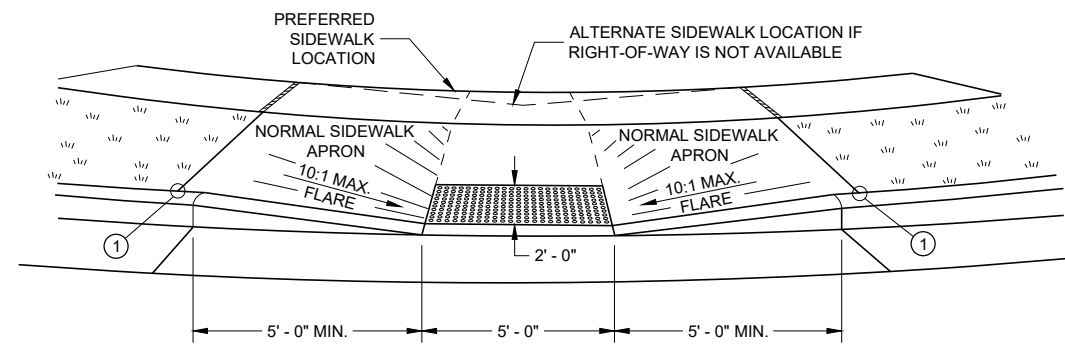
FHWA



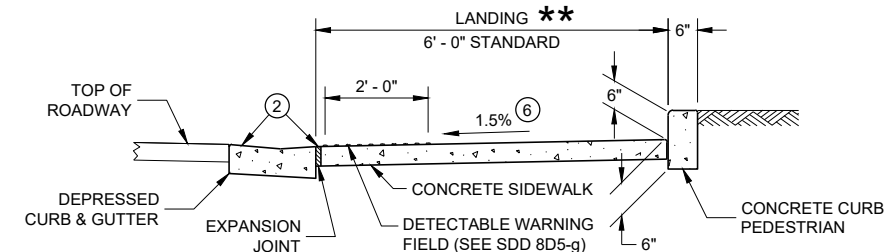
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



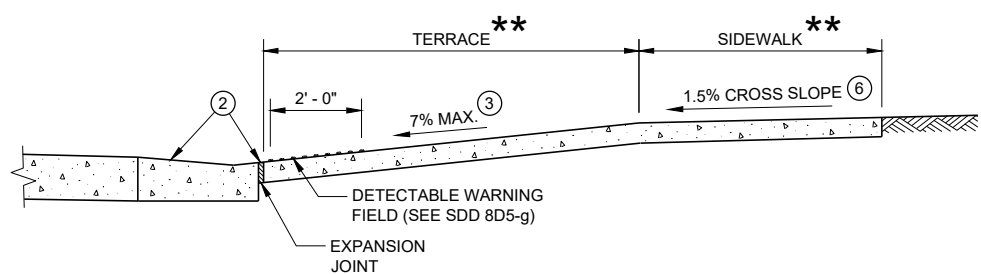
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



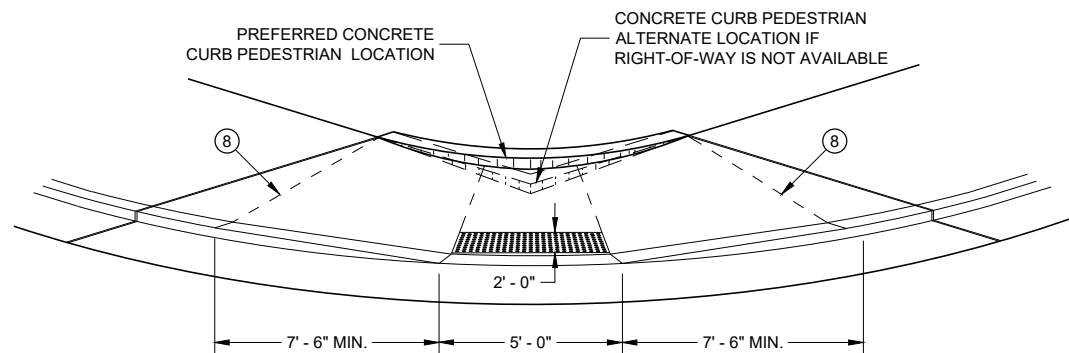
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

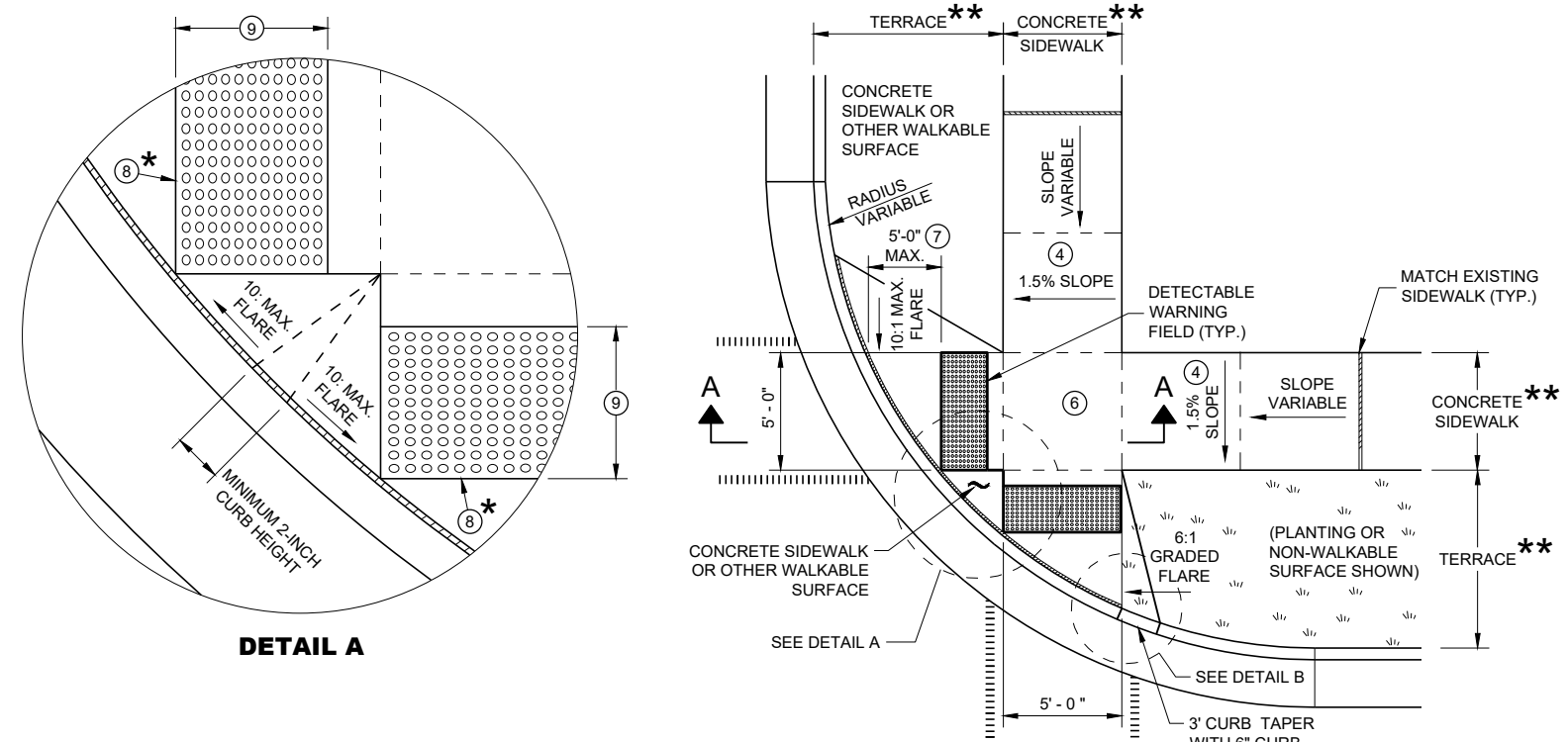
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

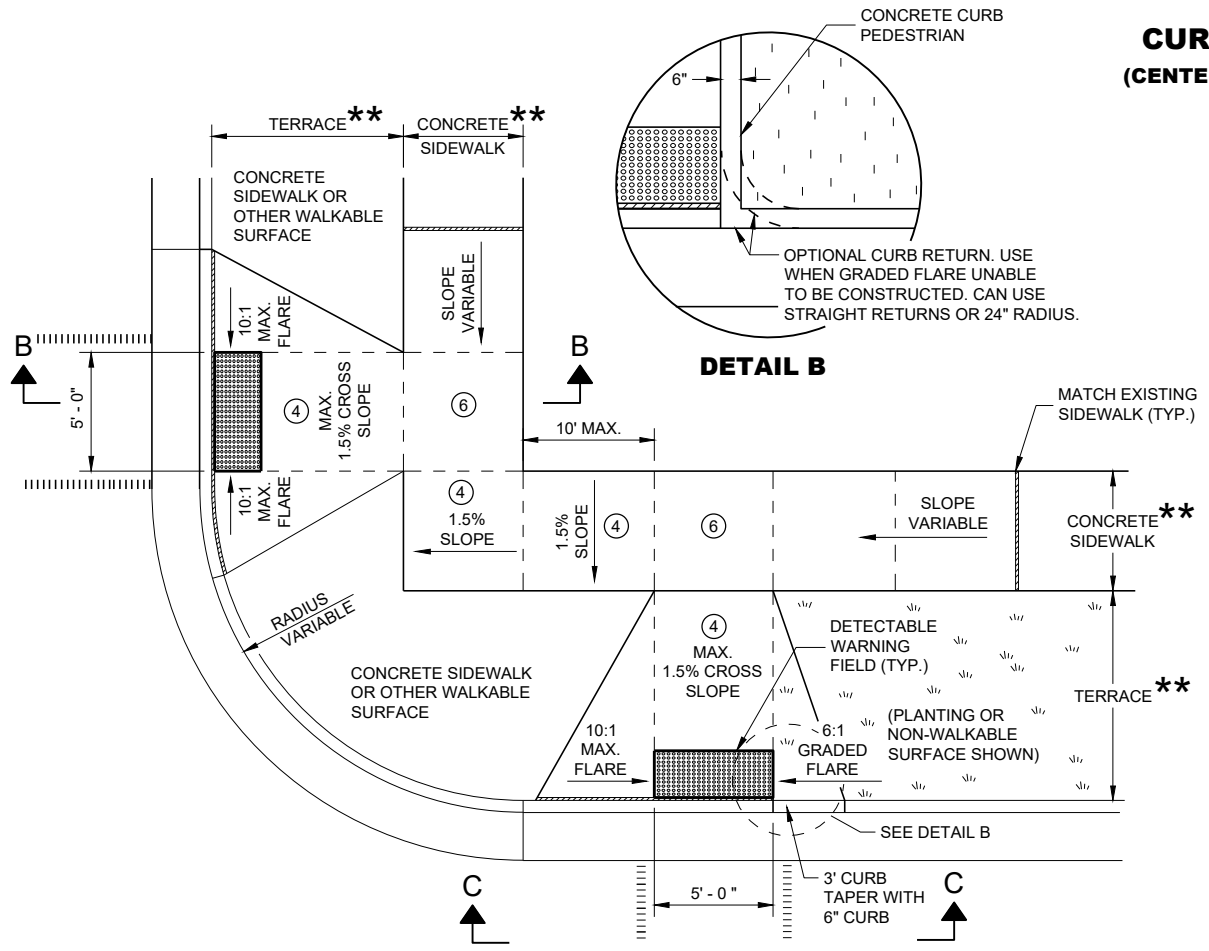
- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

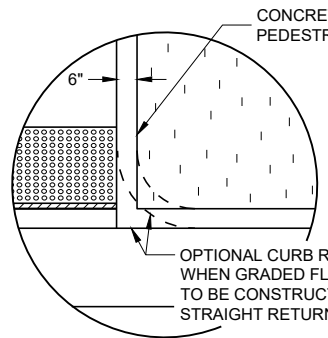
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



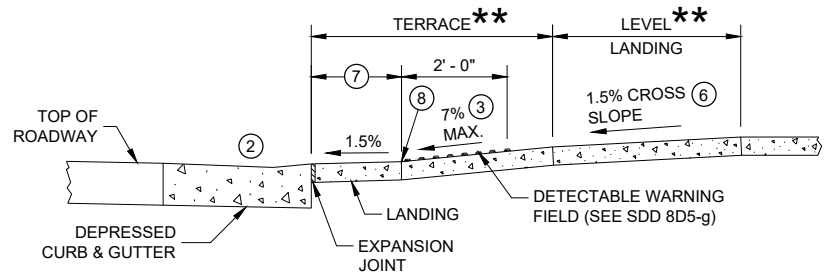
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



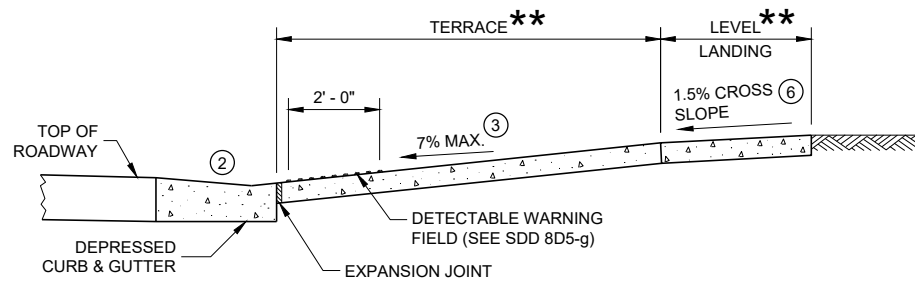
DETAIL B

GENERAL NOTES

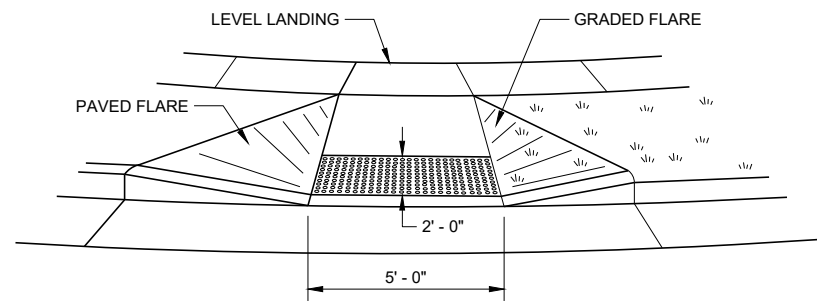
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

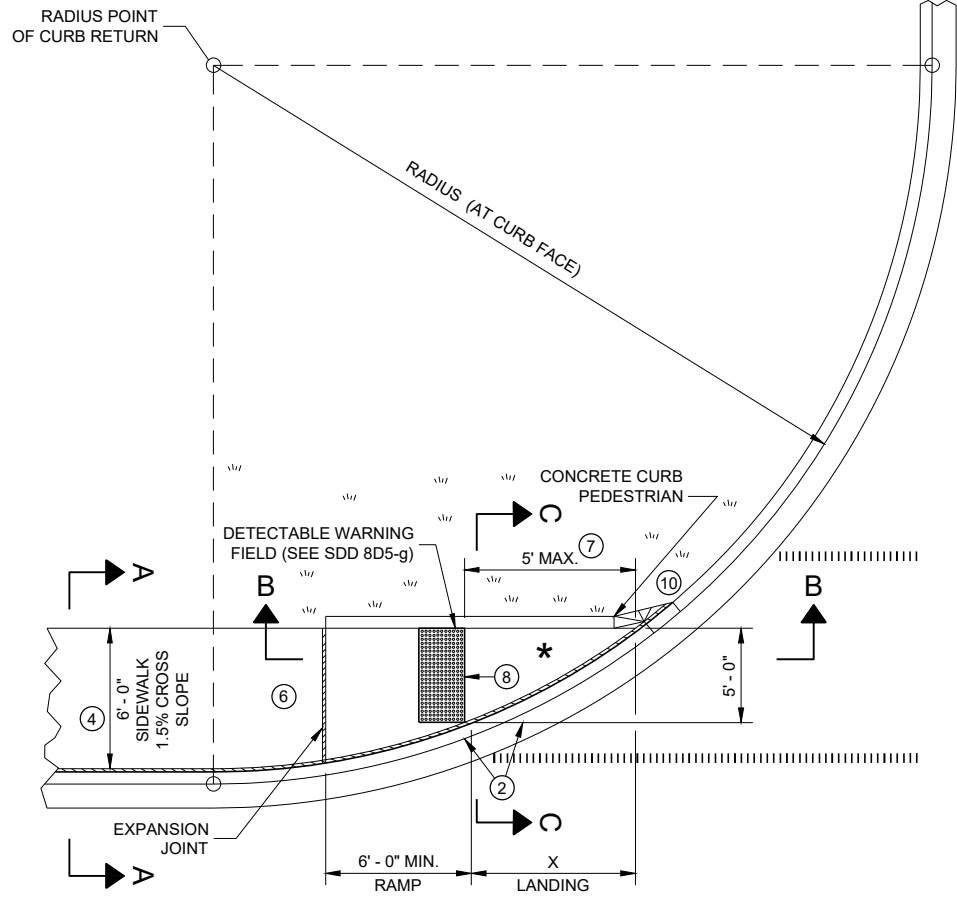
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

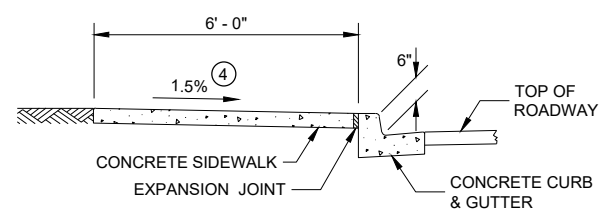
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 4A

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



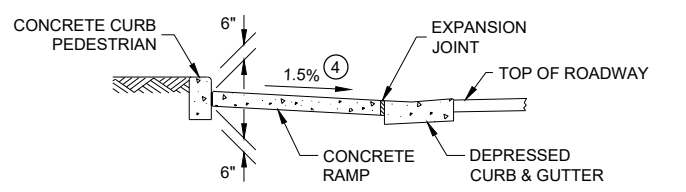
SECTION A - A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

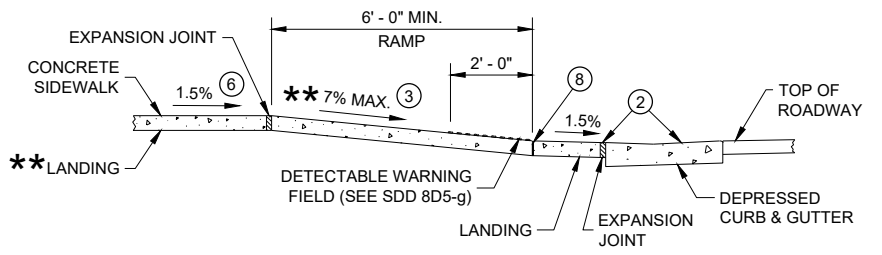
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



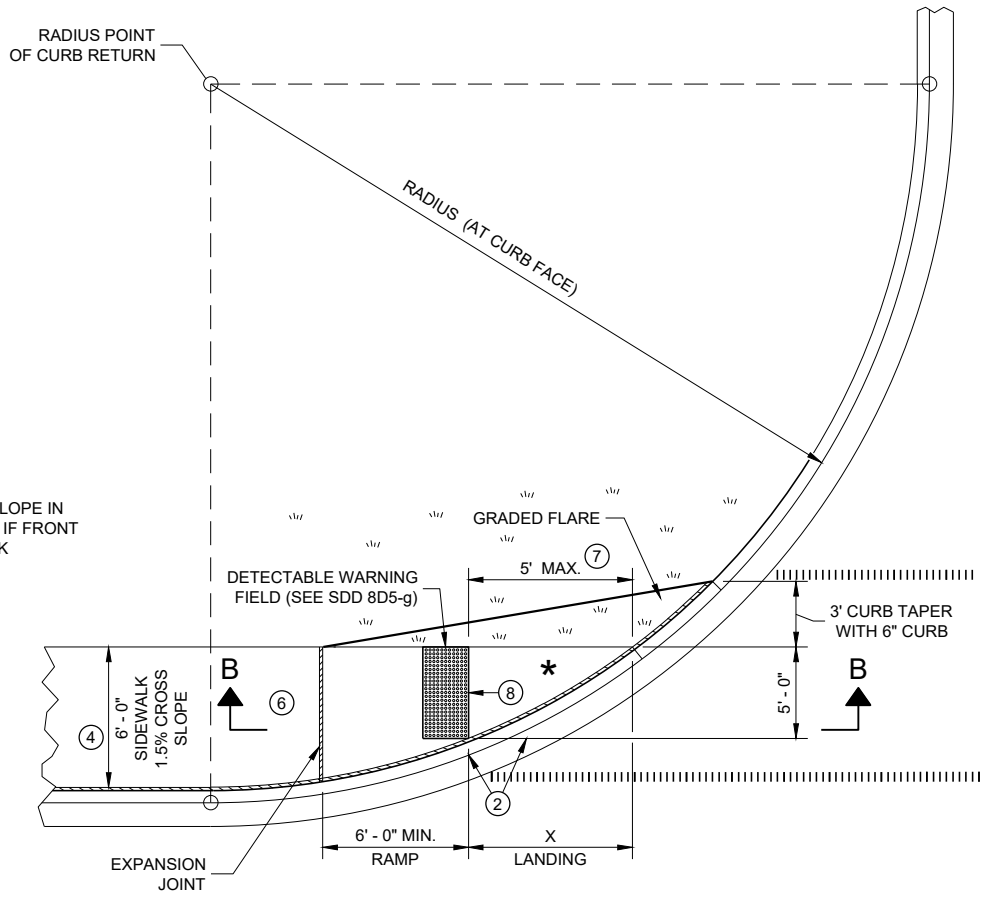
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

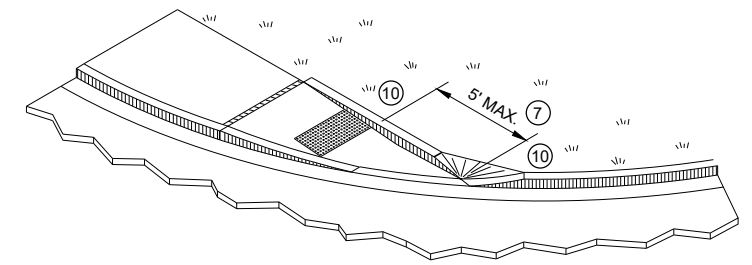


SECTION B - B FOR TYPE 4A AND TYPE 4A1

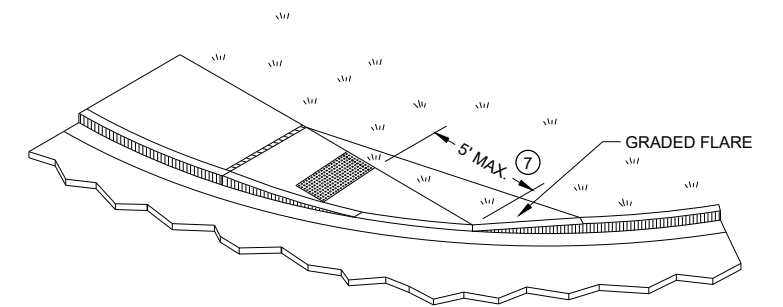
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



PLAN VIEW CURB RAMP TYPE 4A1



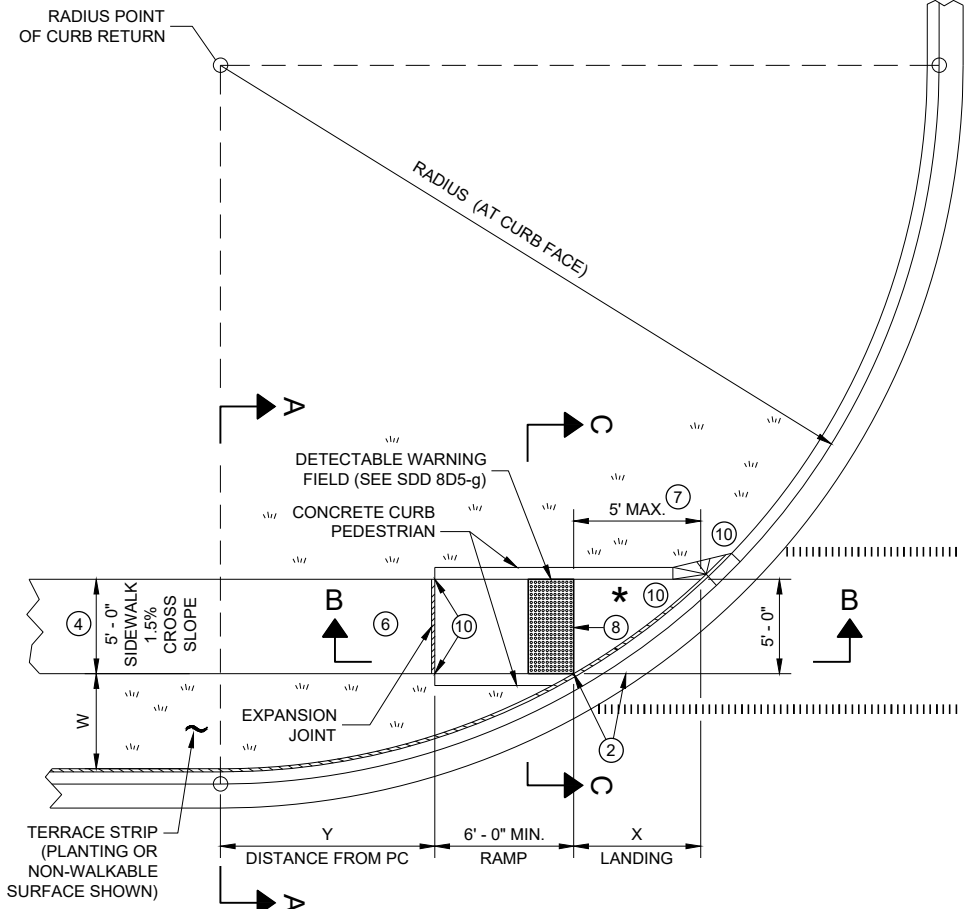
ISOMETRIC VIEW FOR TYPE 4A



ISOMETRIC VIEW FOR TYPE 4A1

CURB RAMPS TYPE 4A AND 4A1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**PLAN VIEW
CURB RAMP TYPE 4B**

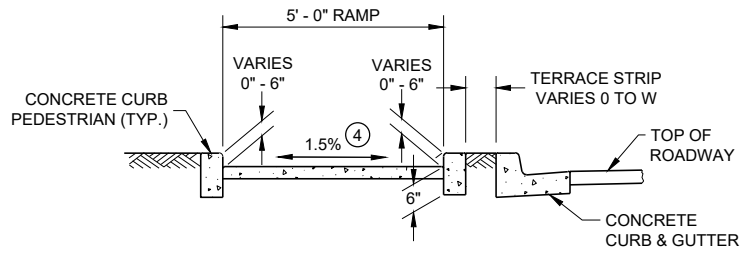
RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 3/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

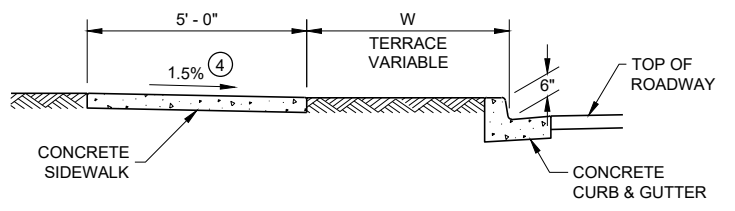
- LEGEND**
- ===== 1/2" EXPANSION JOINT SIDEWALK
 - - - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

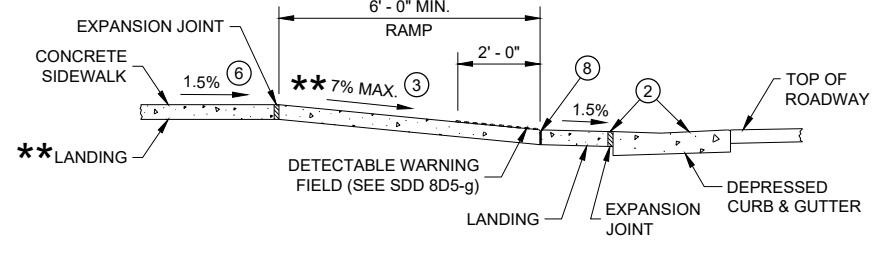


SECTION C - C FOR TYPE 4B



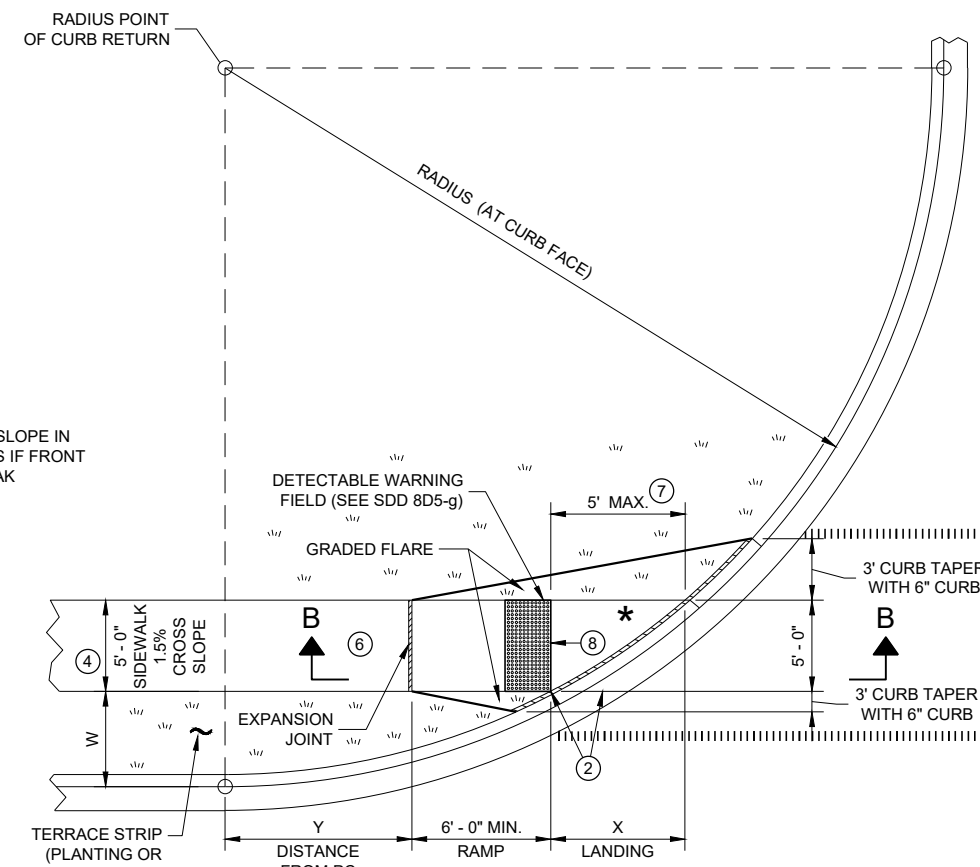
SECTION A - A FOR TYPE 4B

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

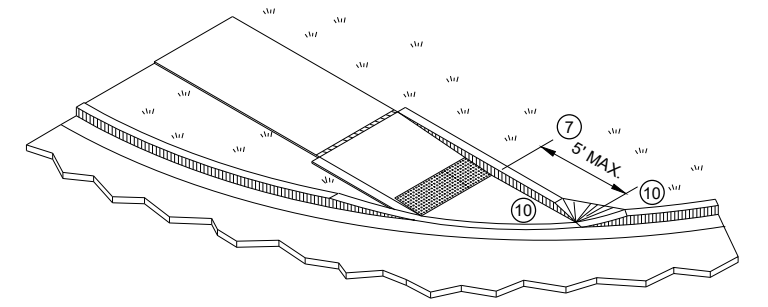


**SECTION B - B FOR
TYPE 4B AND TYPE 4B1**

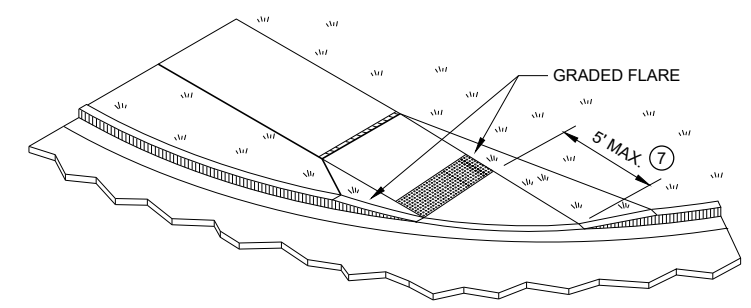
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW
CURB RAMP TYPE 4B1**



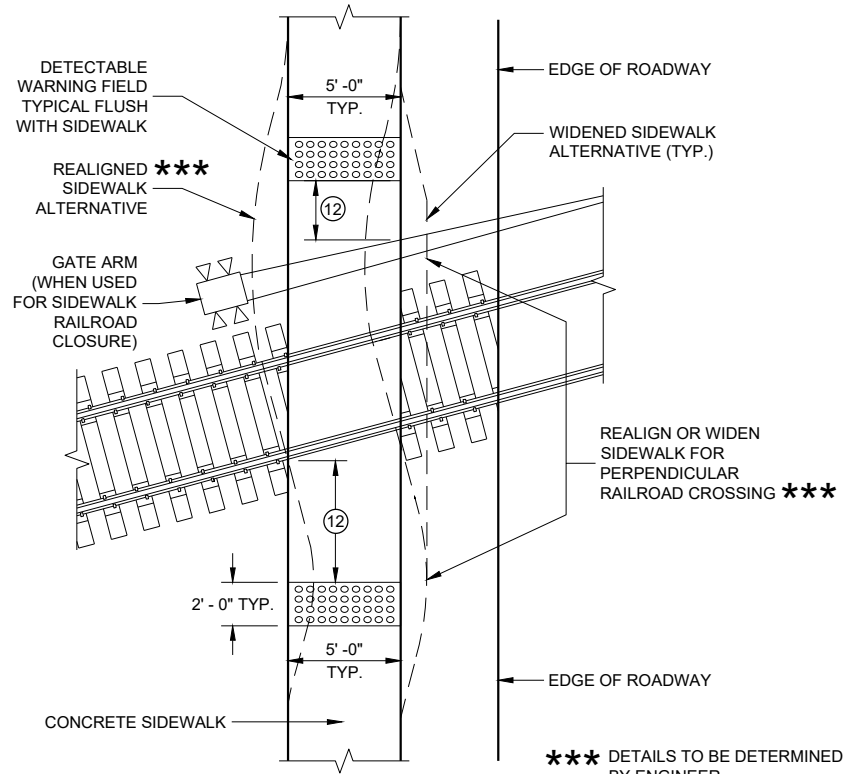
ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

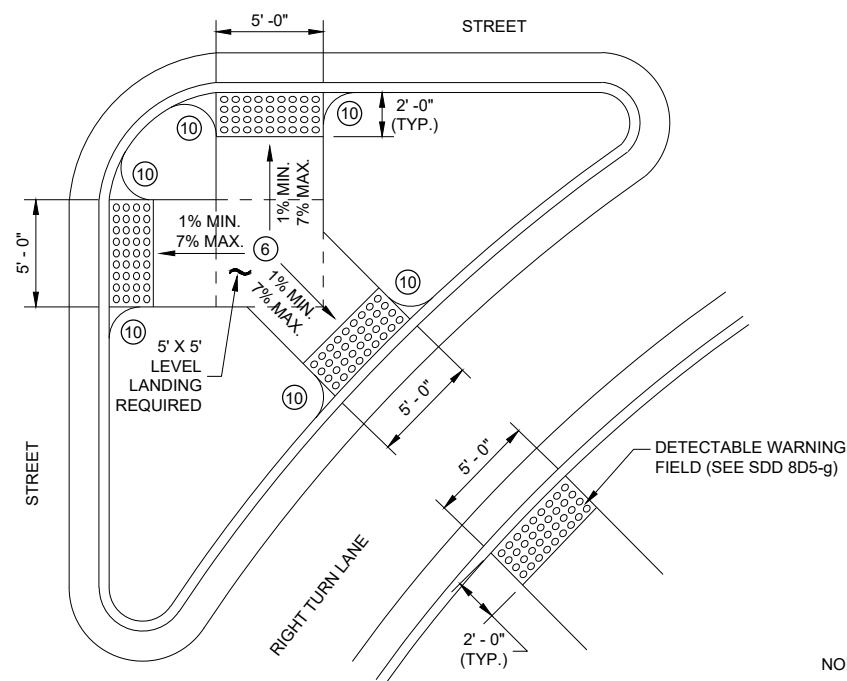
**CURB RAMPS
TYPE 4B AND 4B1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CURB RAMP TYPE 8

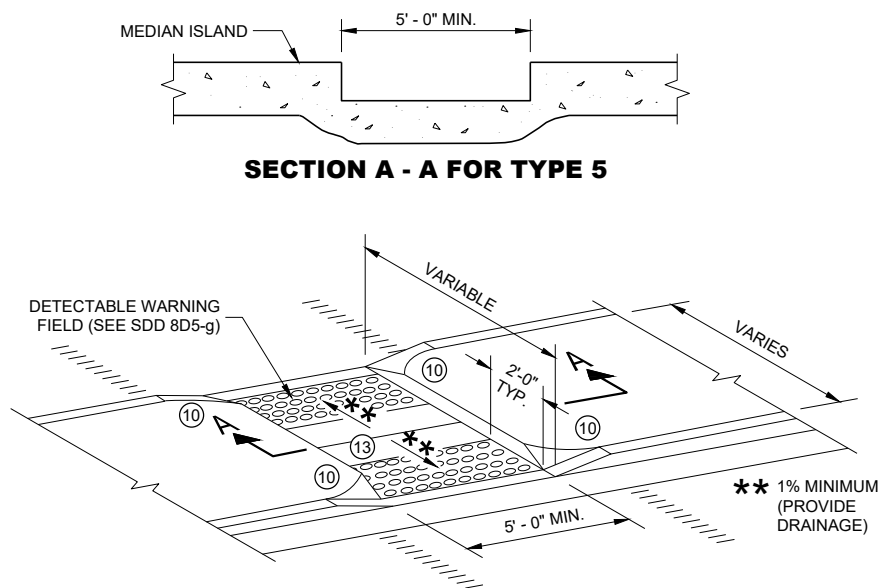
DETECTABLE WARNINGS AT RAILROAD CROSSING



CURB RAMP TYPE 6

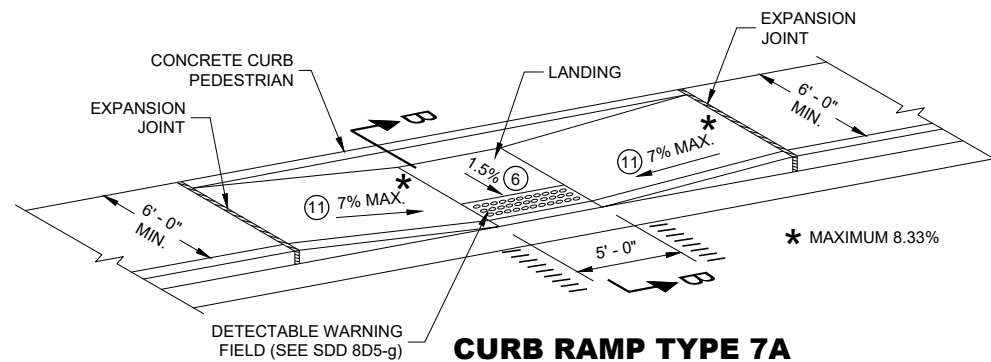
DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



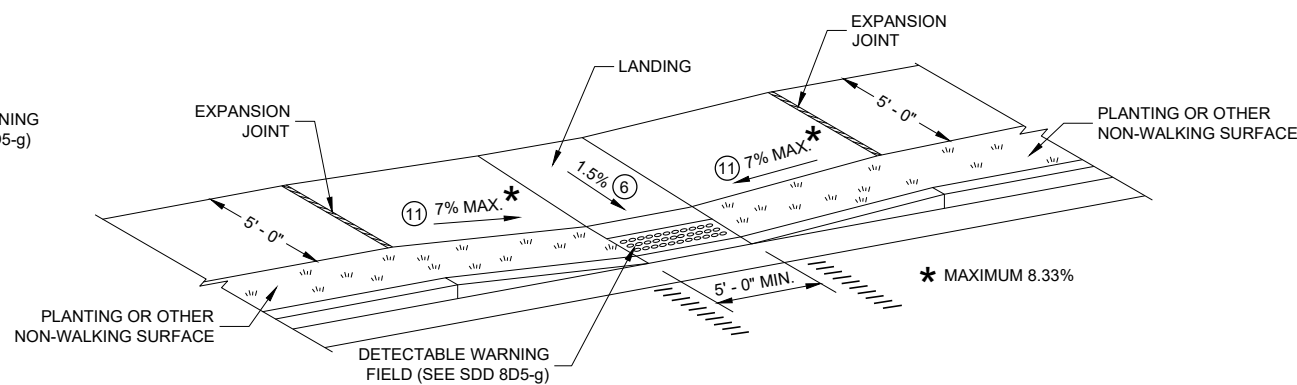
CURB RAMP TYPE 5

MEDIAN ISLAND NON-ELEVATED PEDESTRIAN CROSSING



CURB RAMP TYPE 7A

MID BLOCK CROSSING



CURB RAMP TYPE 7B

MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

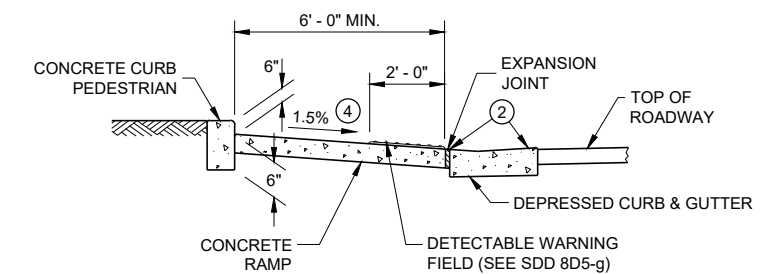
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- (10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- (11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- (12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- (13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

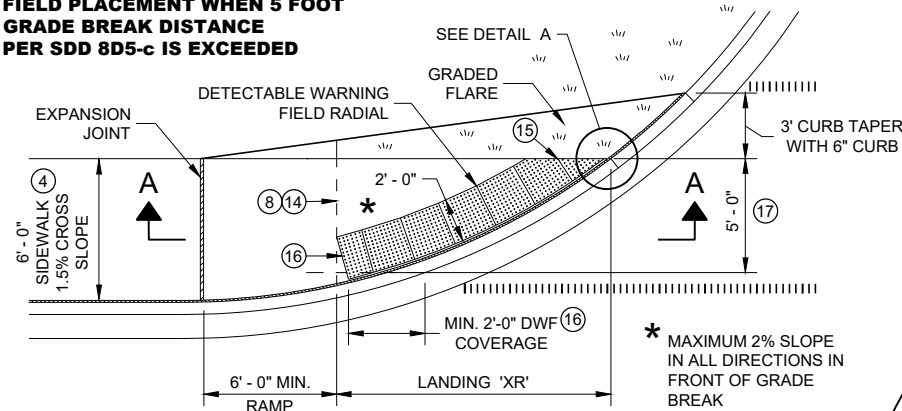


SECTION B - B FOR TYPE 7A

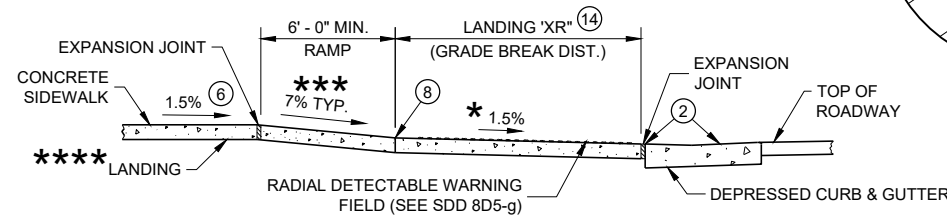
**CURB RAMPS
TYPE 5, 6, 7A, 7B & 8**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)

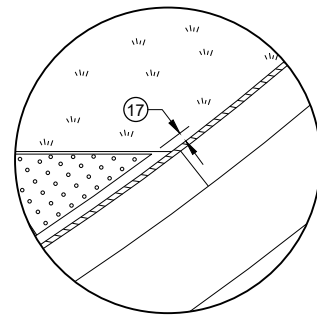


SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

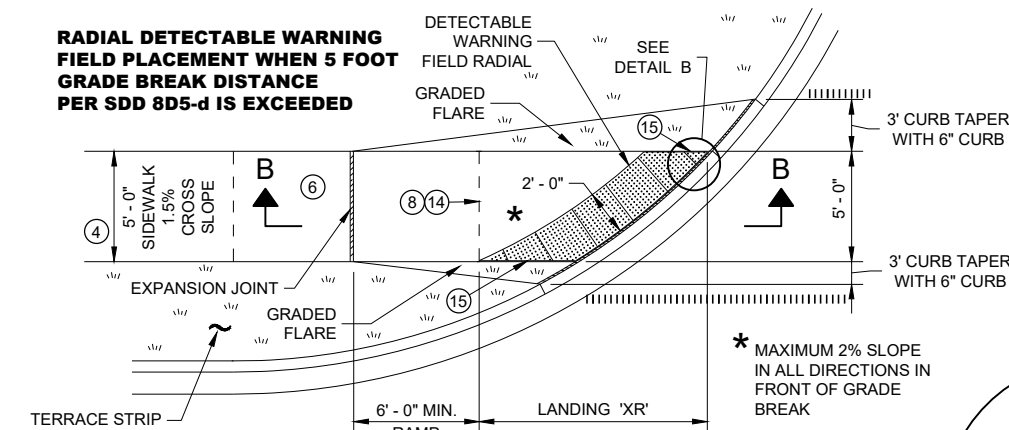


DETAIL A

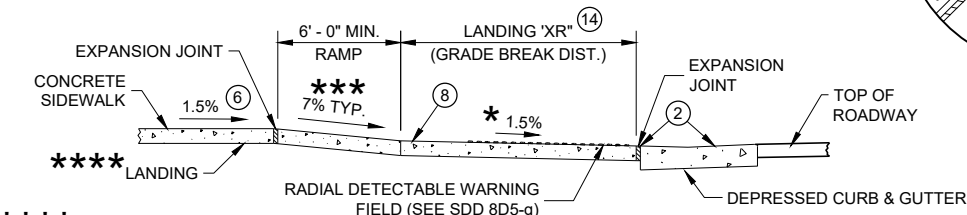
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



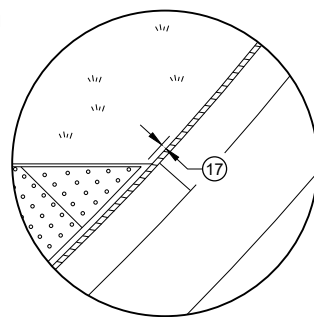
PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



SECTION B - B FOR TYPE 4B1

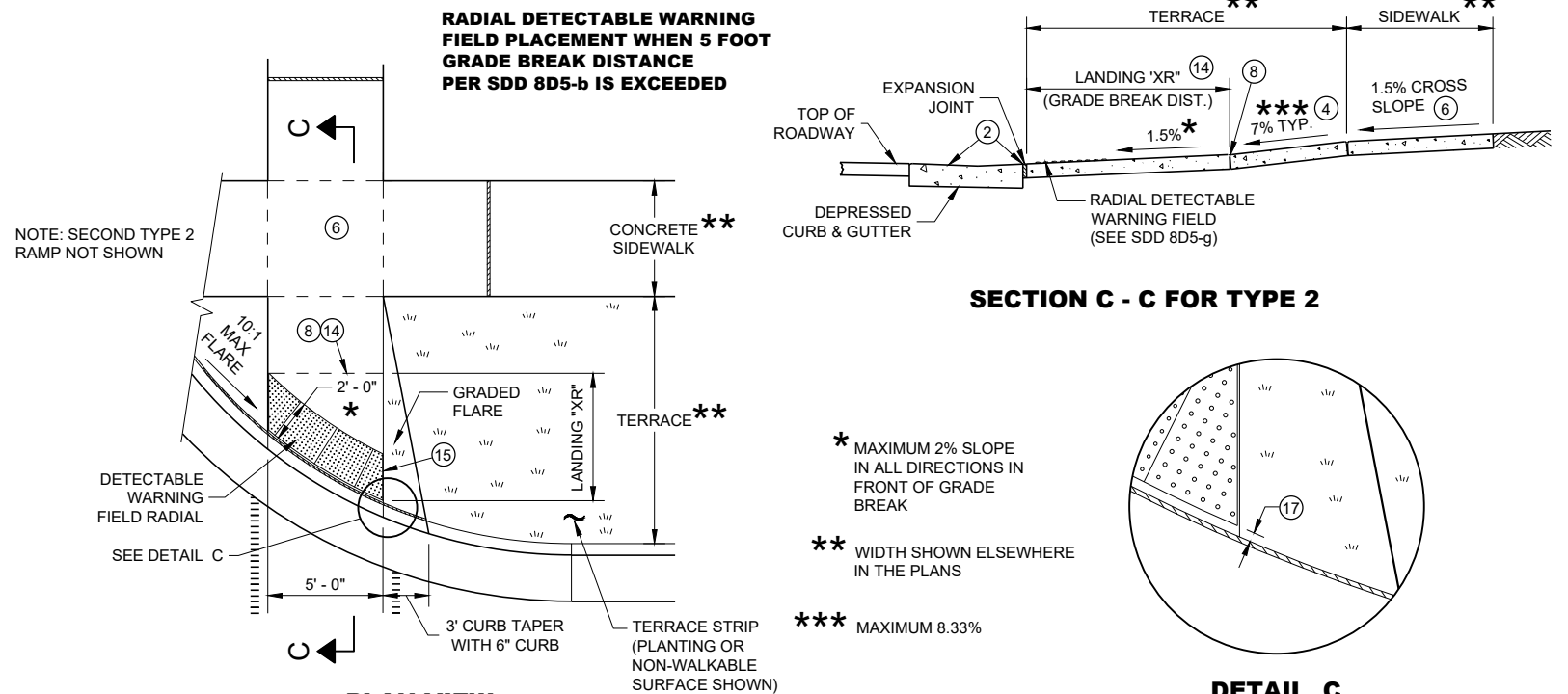
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



DETAIL B

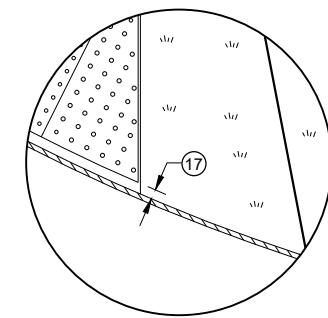
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 2 (GRADE BREAK DISTANCE GREATER THAN 5 FEET) (ON LINE WITH SIDEWALK)

NOTE: SECOND TYPE 2 RAMP NOT SHOWN

- * MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- *** MAXIMUM 8.33%



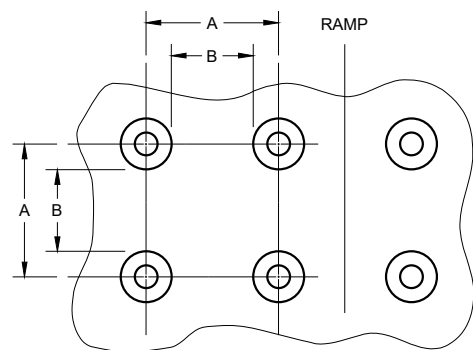
DETAIL C

CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS

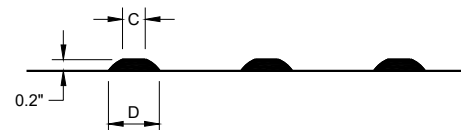
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

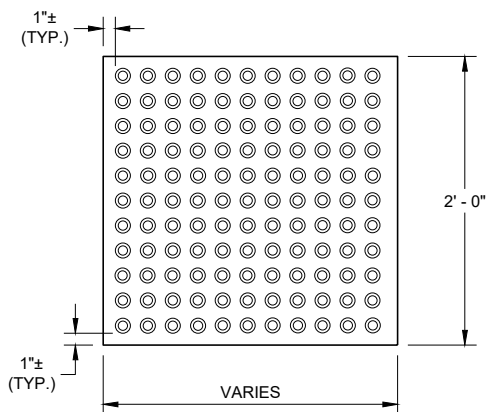


PLAN VIEW

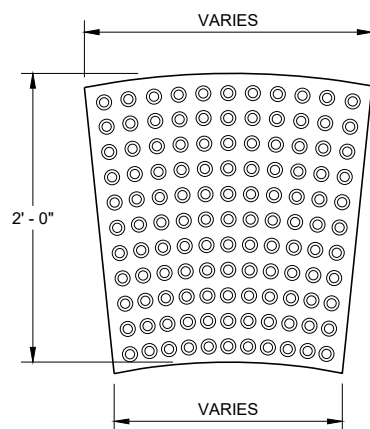


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

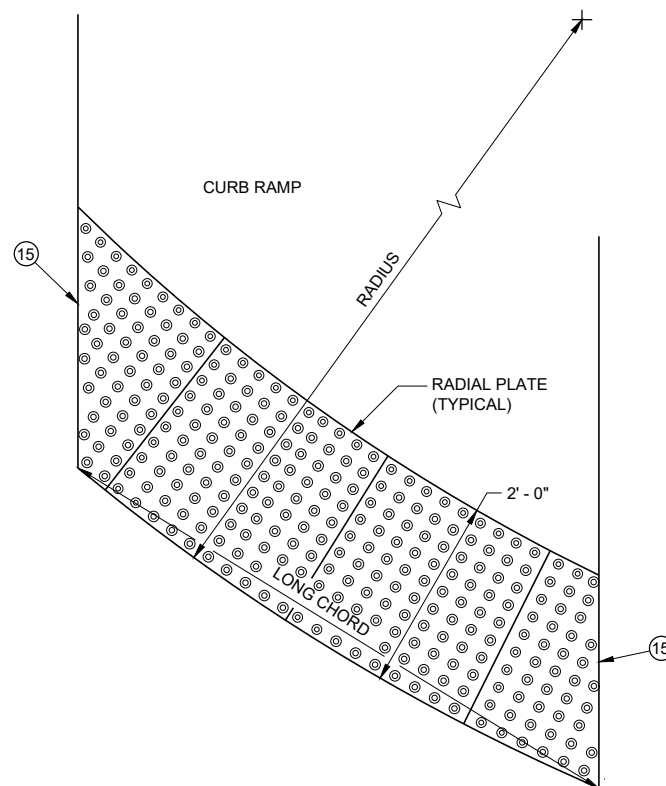


RECTANGULAR
PLATES

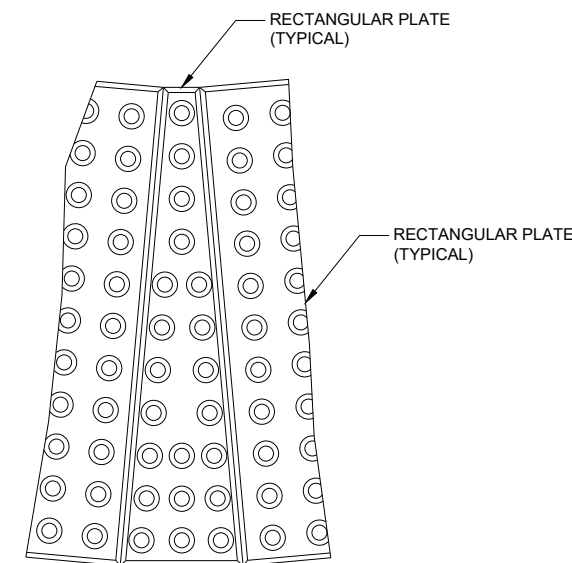


RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

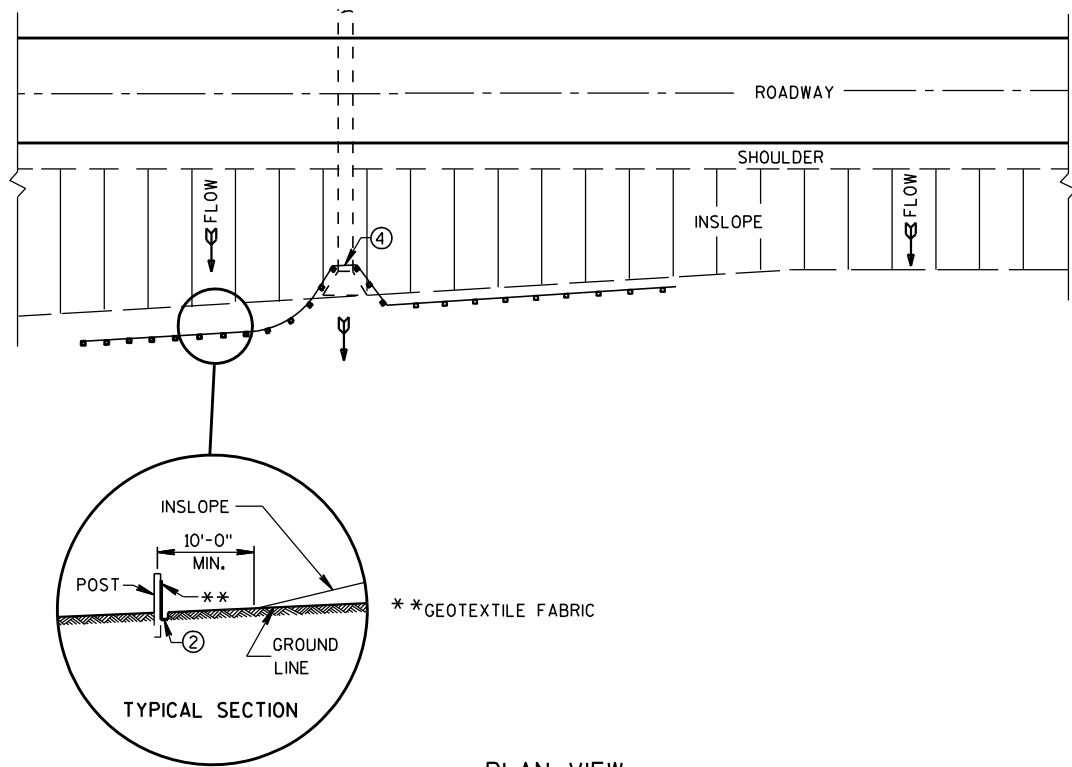
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

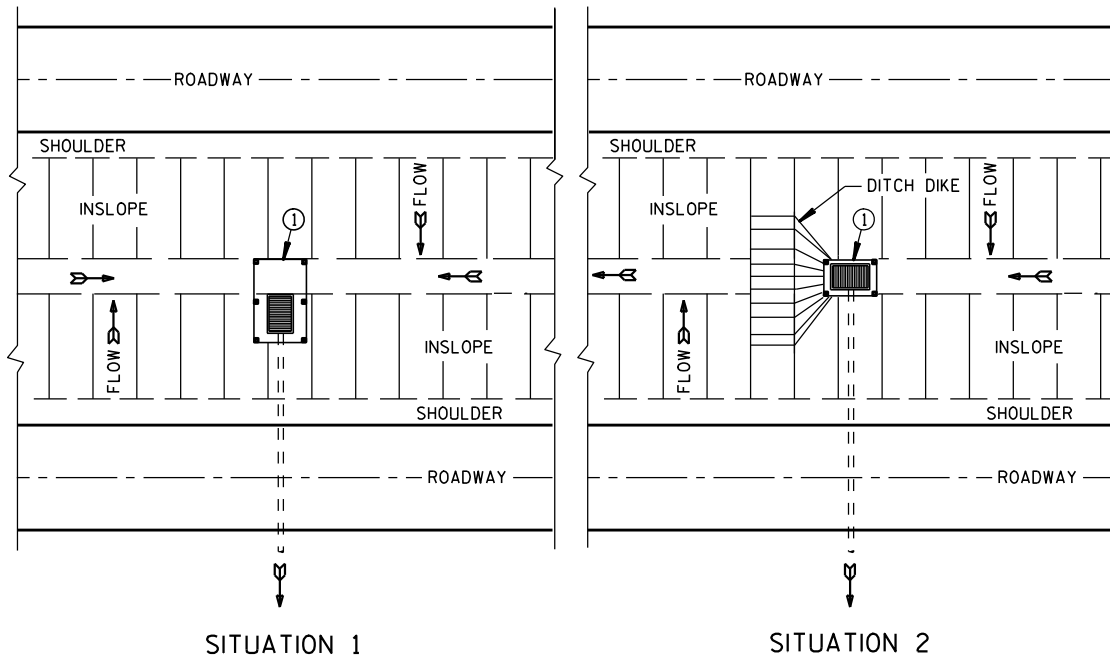
DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

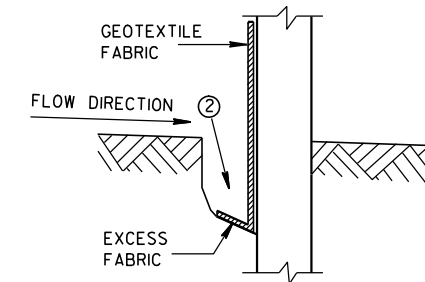


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

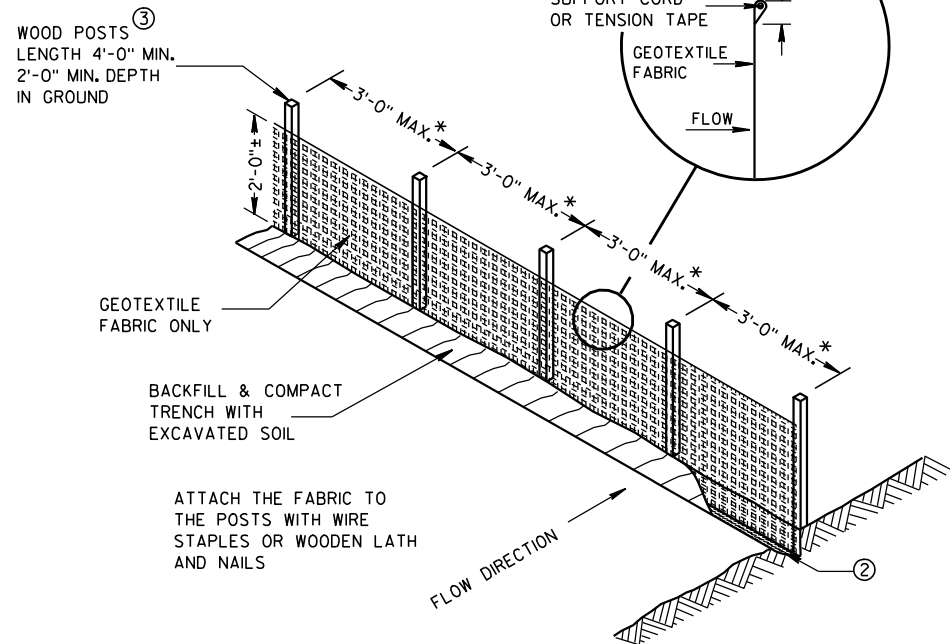
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



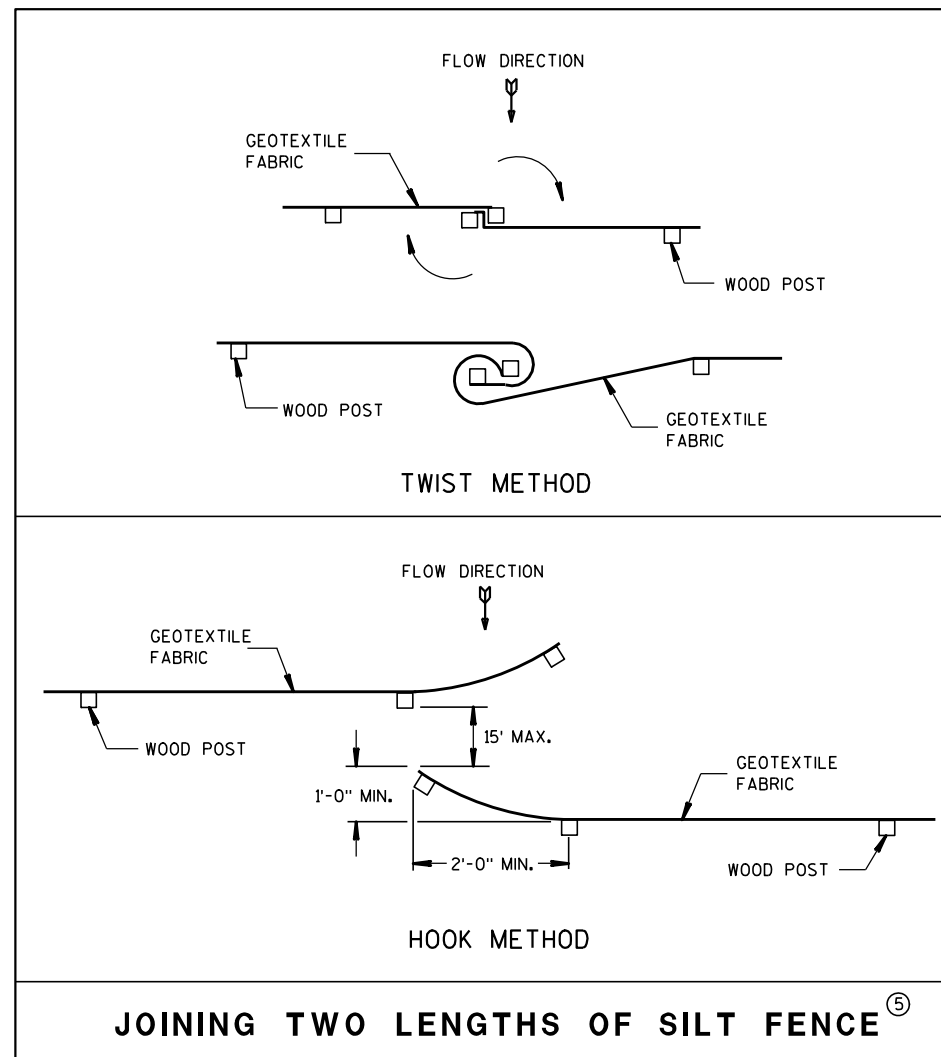
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

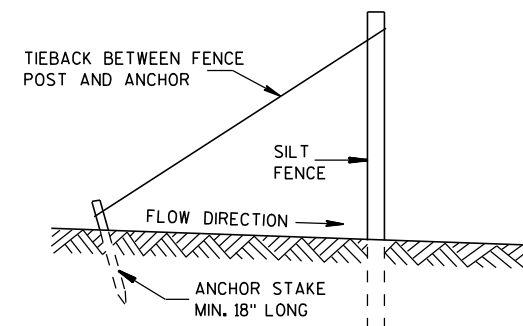


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

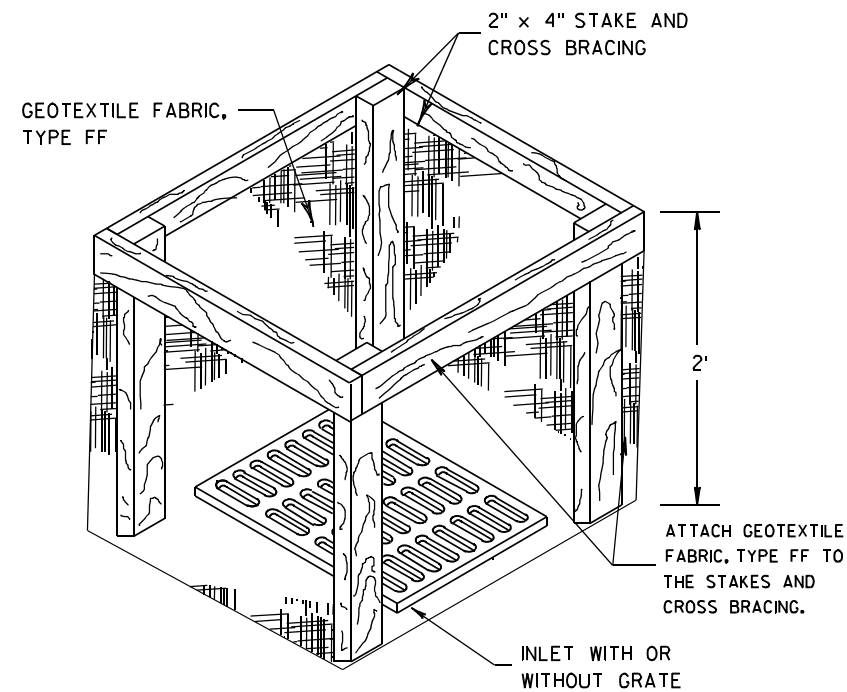
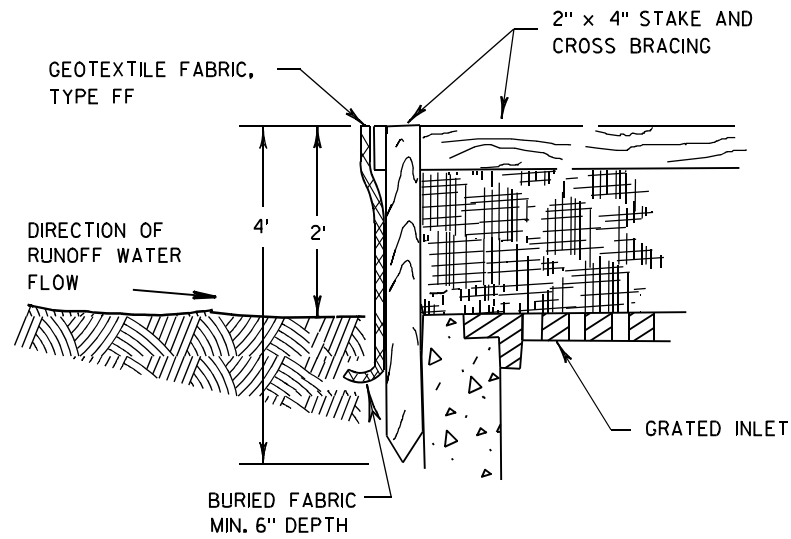
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

4-29-05
DATE

FHWA

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER



INLET PROTECTION, TYPE A

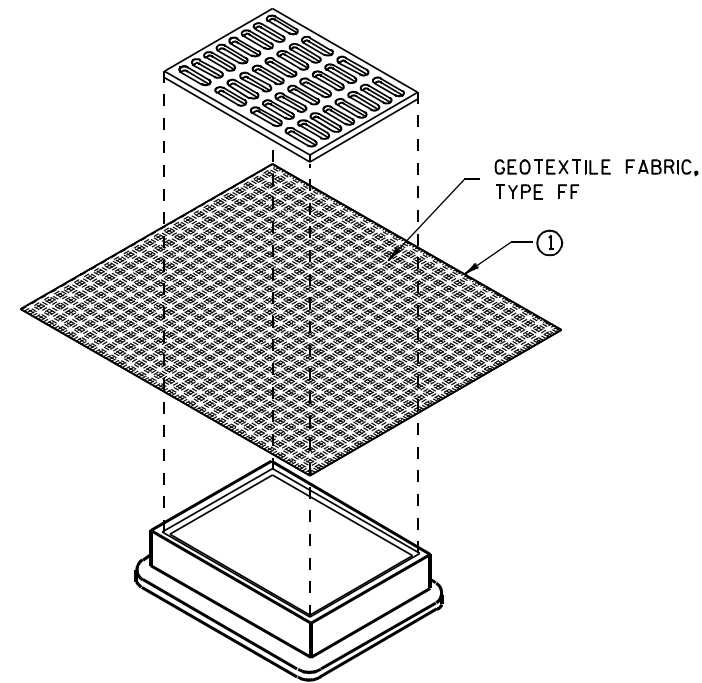
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

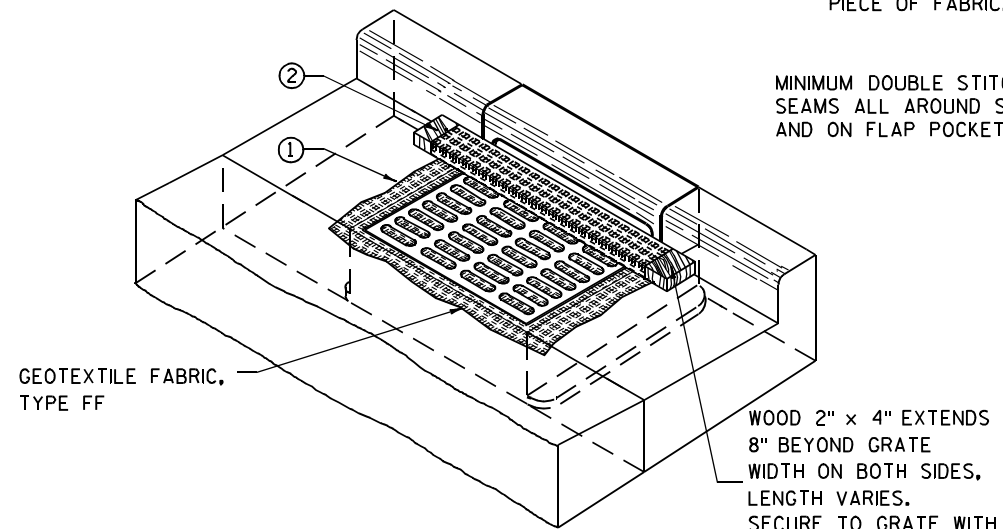
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

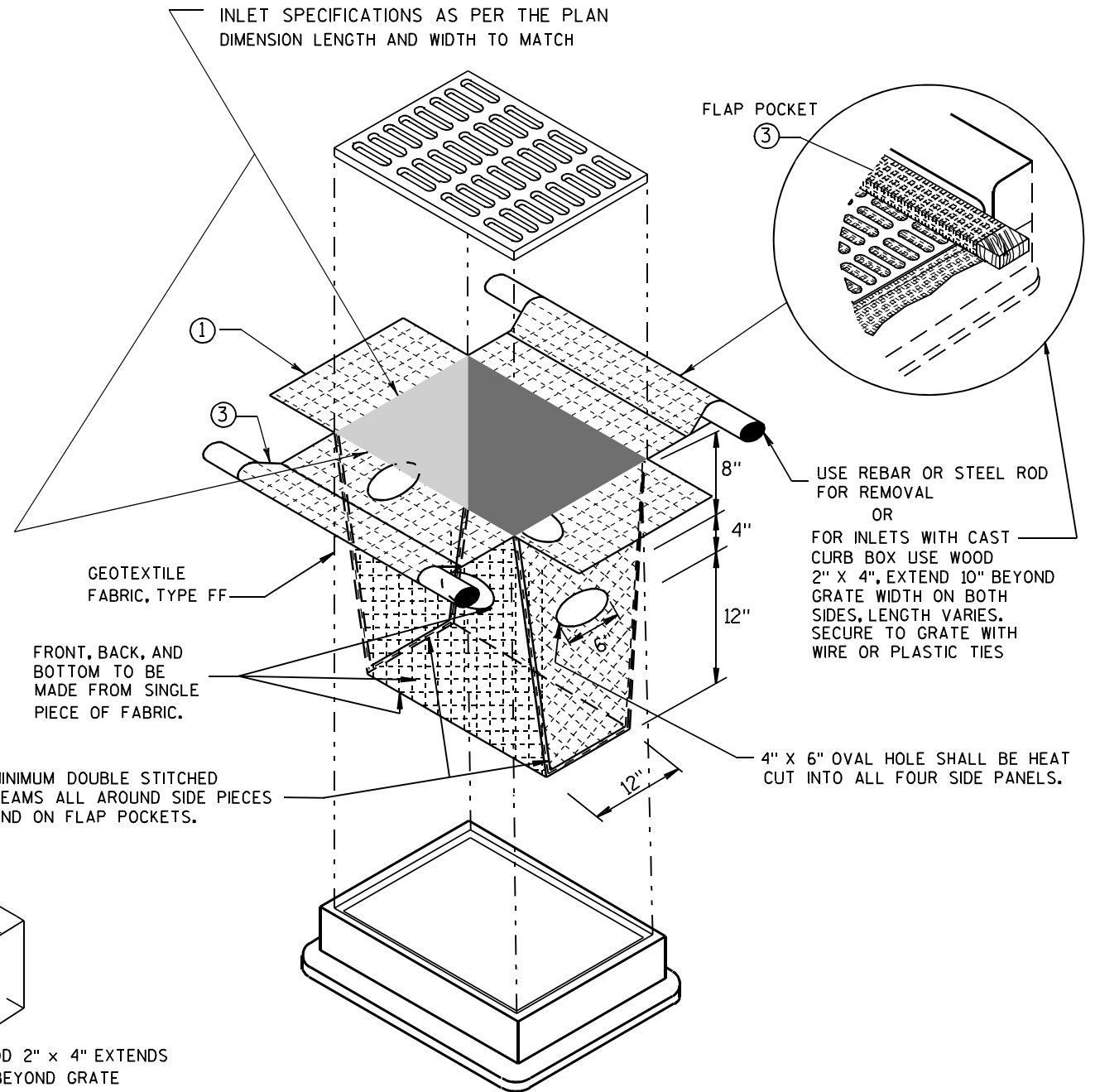
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



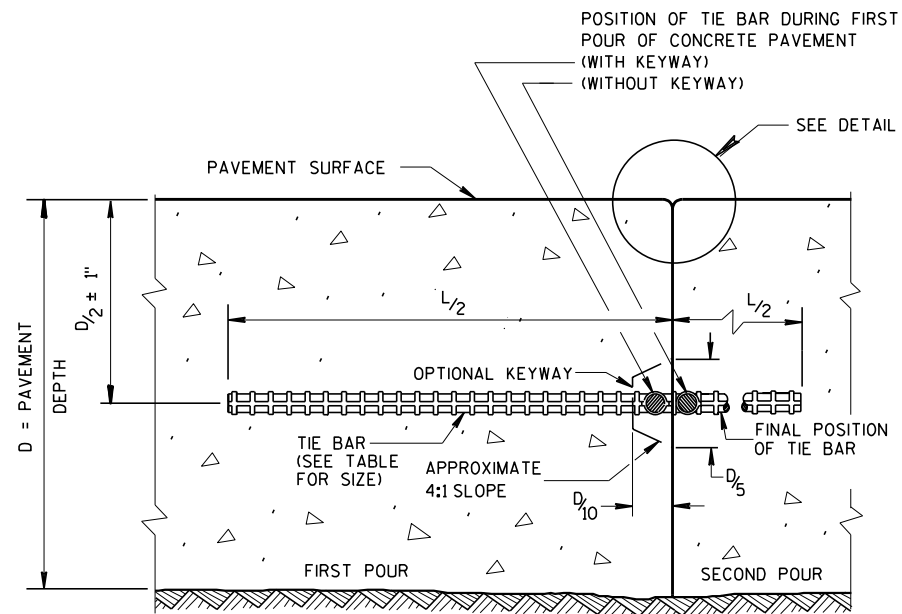
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

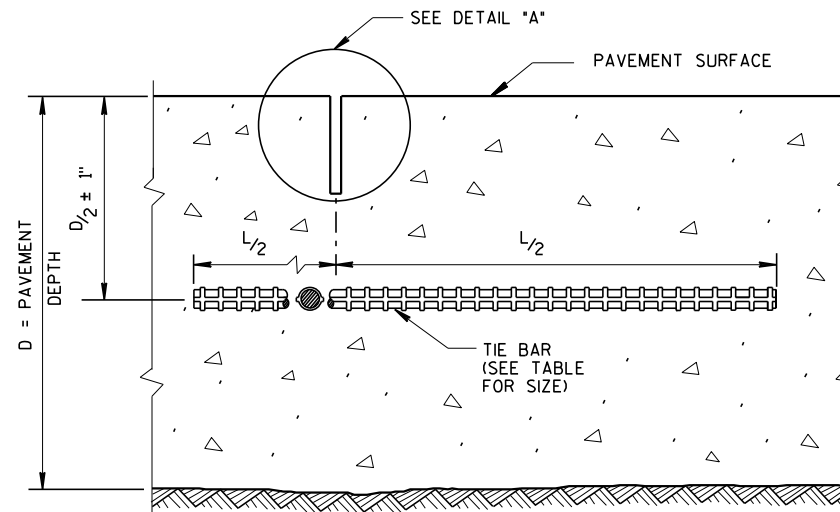
**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Connestra
DATE
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



CONSTRUCTION JOINT



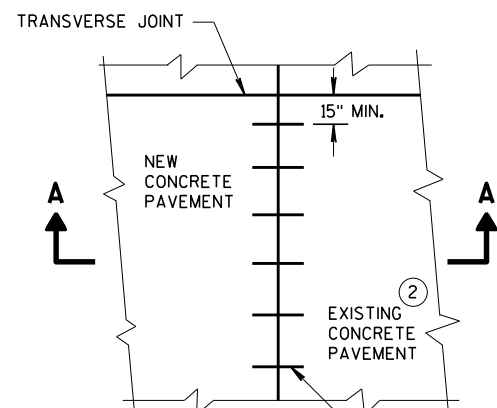
SAWED JOINT

GENERAL NOTES

CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.

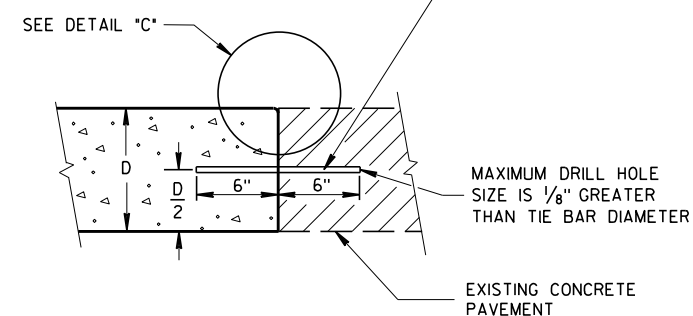
CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

- ① ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.
- ② PAVEMENT THAT WAS IN PLACE PRIOR TO THE CONTRACT.

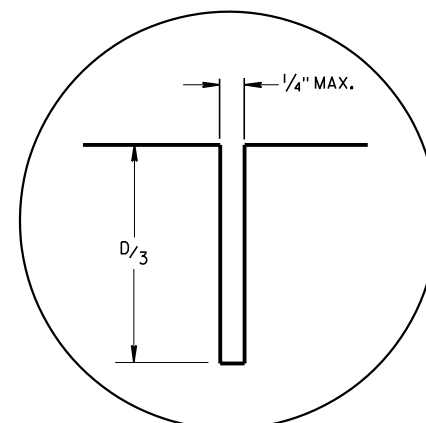


PLAN VIEW

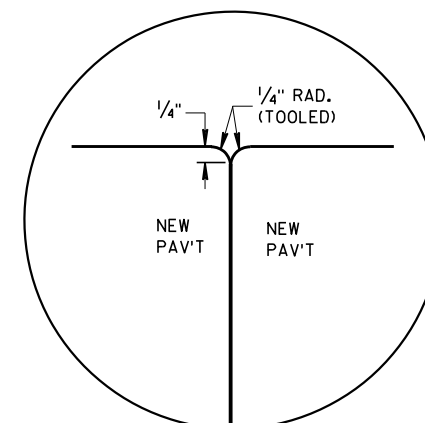
NO. 6 TIE BARS SPACED 30" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT. ①



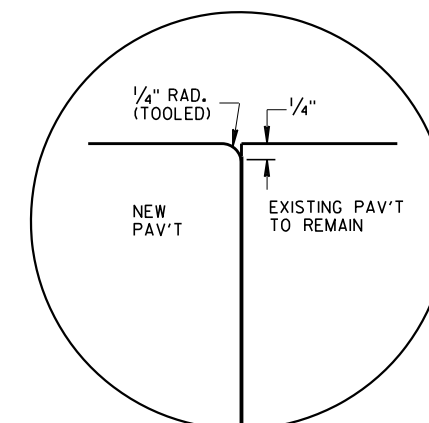
**SECTION A-A
LONGITUDINAL CONSTRUCTION JOINT
TIE BARS ANCHORED
INTO EXISTING PAVEMENT**



DETAIL "A"



DETAIL "B"



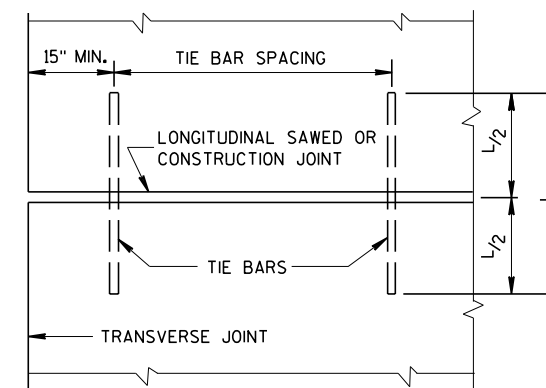
DETAIL "C"

TIE BAR TABLE

PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
	NO. 4 *	30"	24" **

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

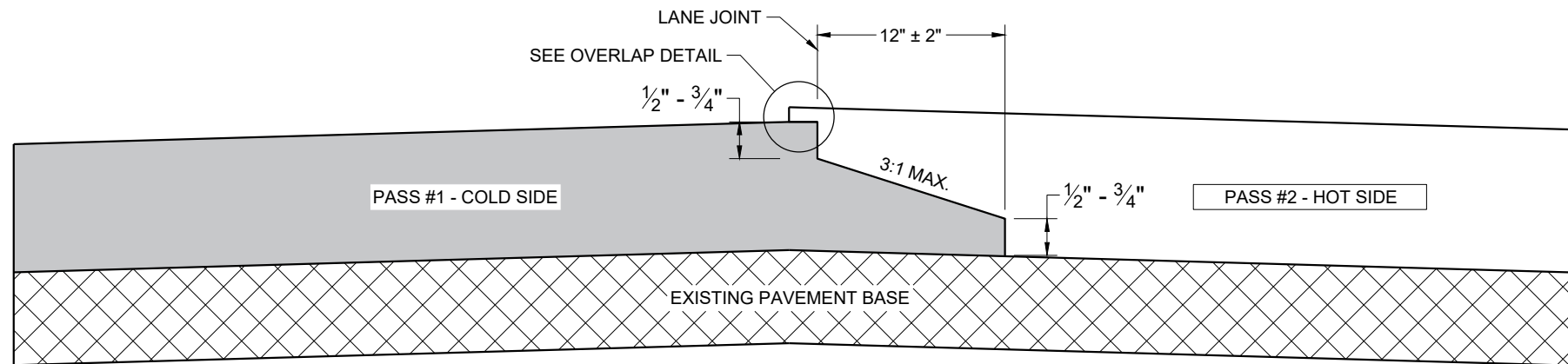


**PLAN VIEW
SHOWING LOCATION OF TIE BARS**

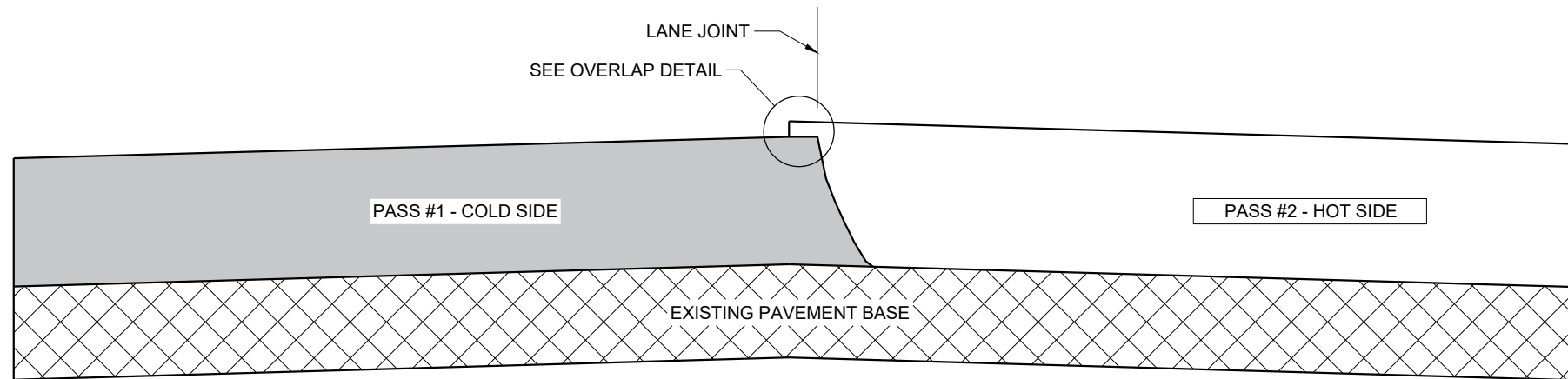
**CONCRETE PAVEMENT
LONGITUDINAL JOINTS AND TIES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

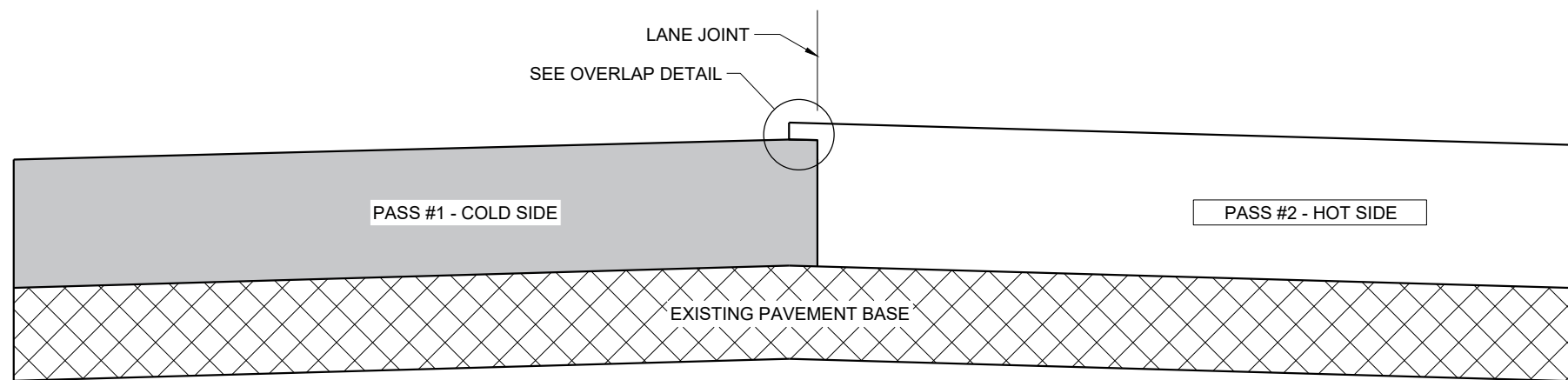
APPROVED
March 2018 /S/ Peter Kemp, P.E.
DATE PAVEMENT SUPERVISOR
FHWA



TYPICAL PAVEMENT CROSS SECTION NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION VERTICAL JOINT (MILLED)

GENERAL NOTES

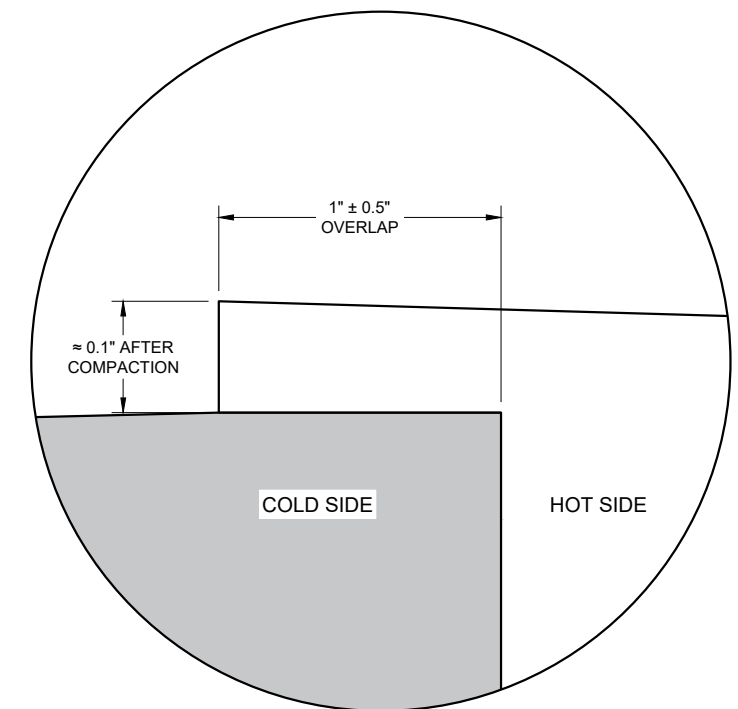
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY $0.1"$ AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO $2"$ FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

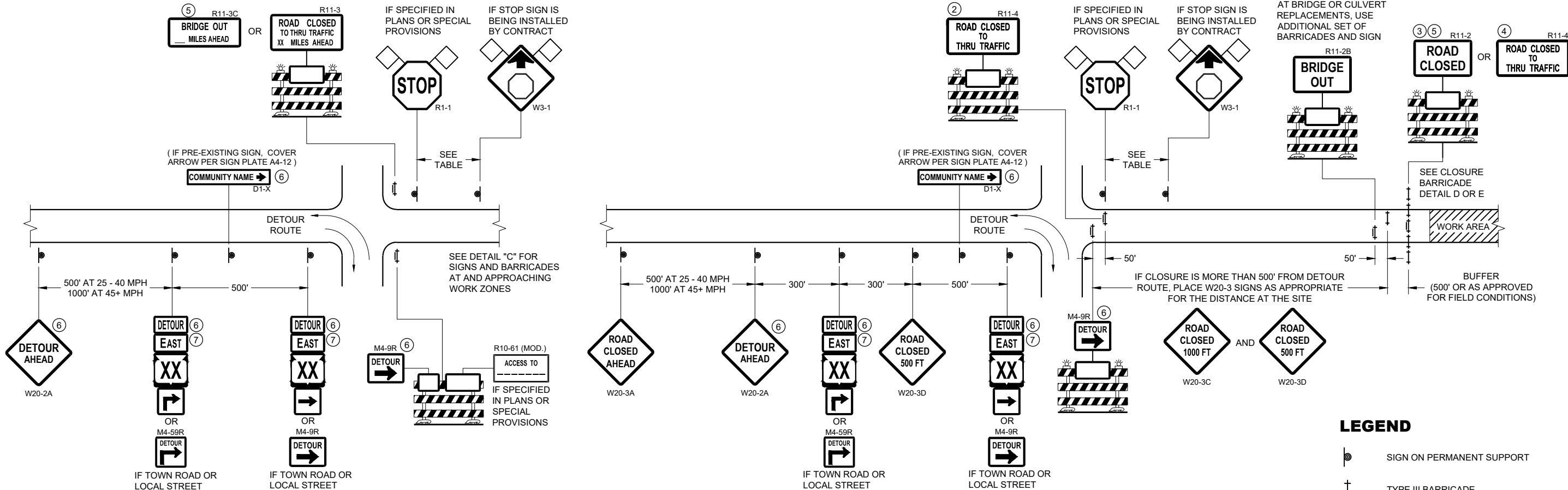
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6

SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

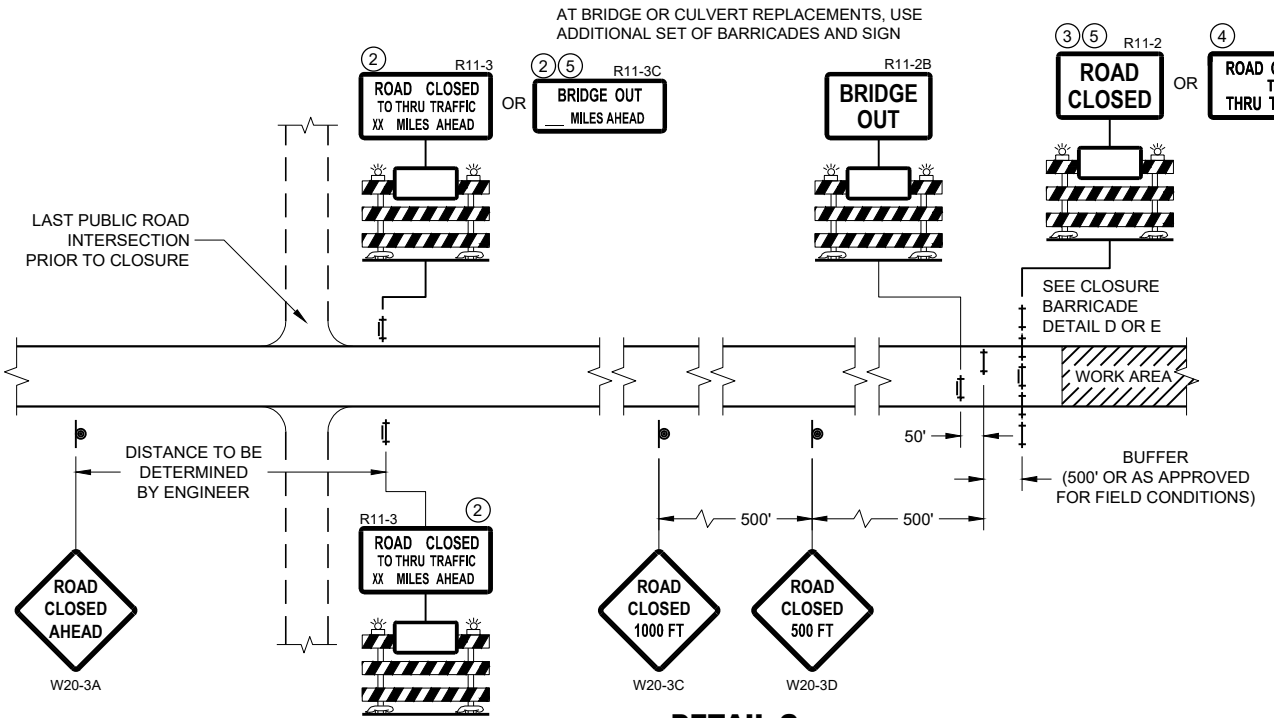
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



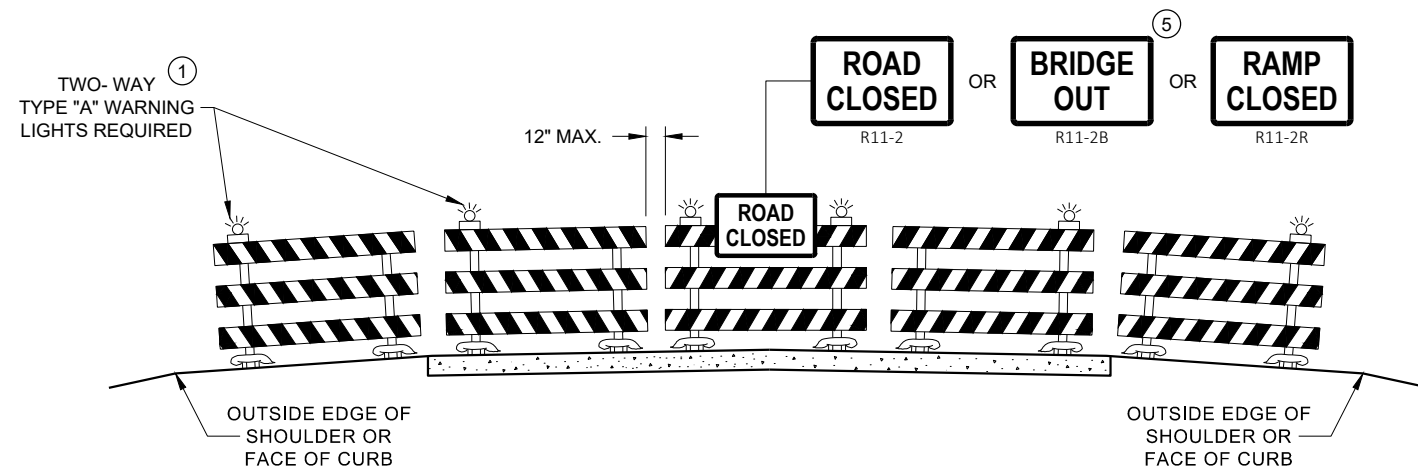
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

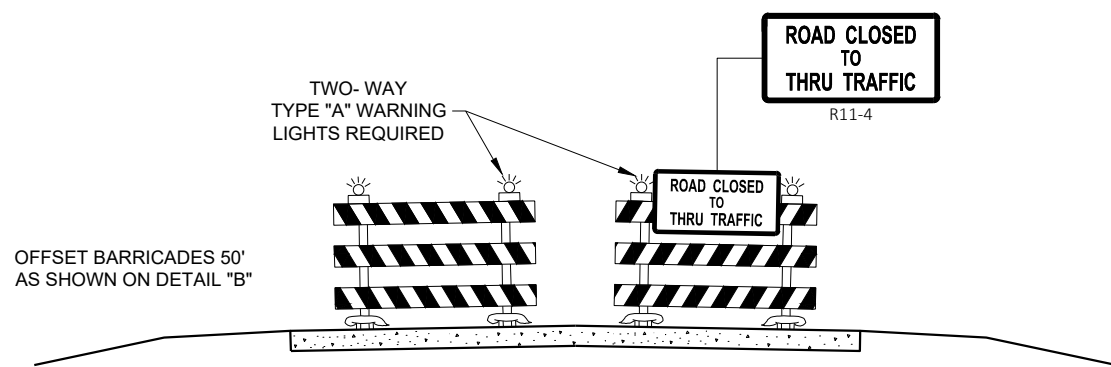
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER
FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

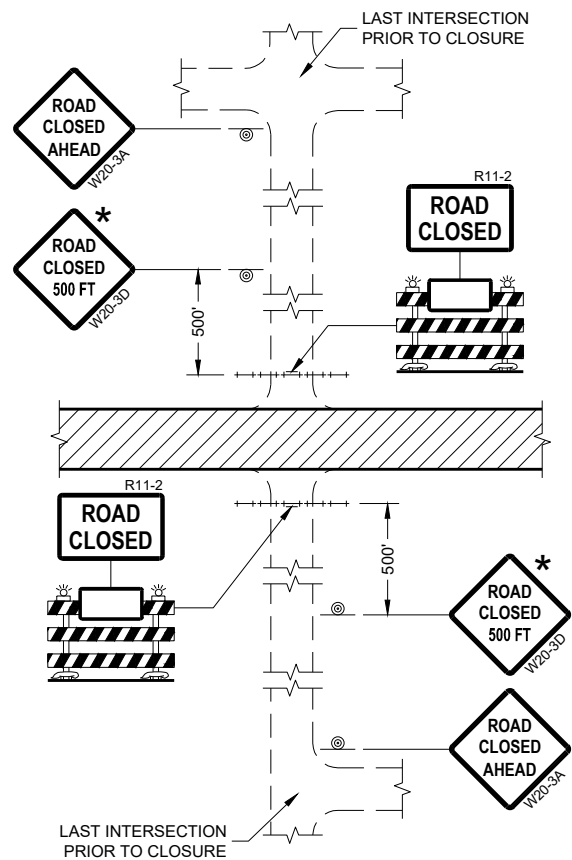
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

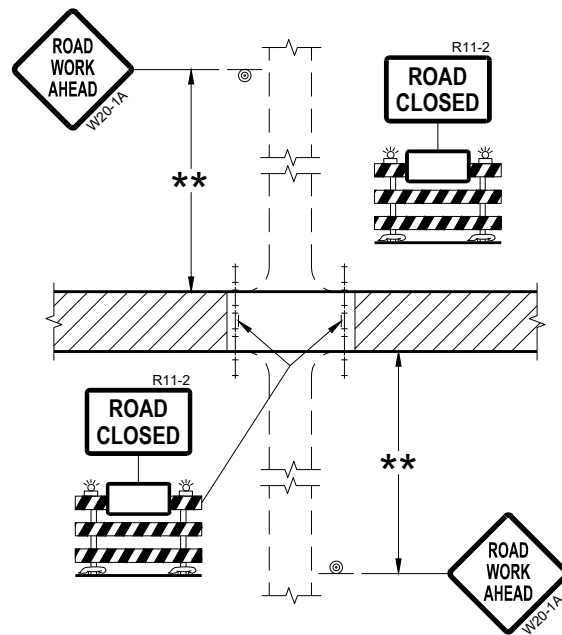
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

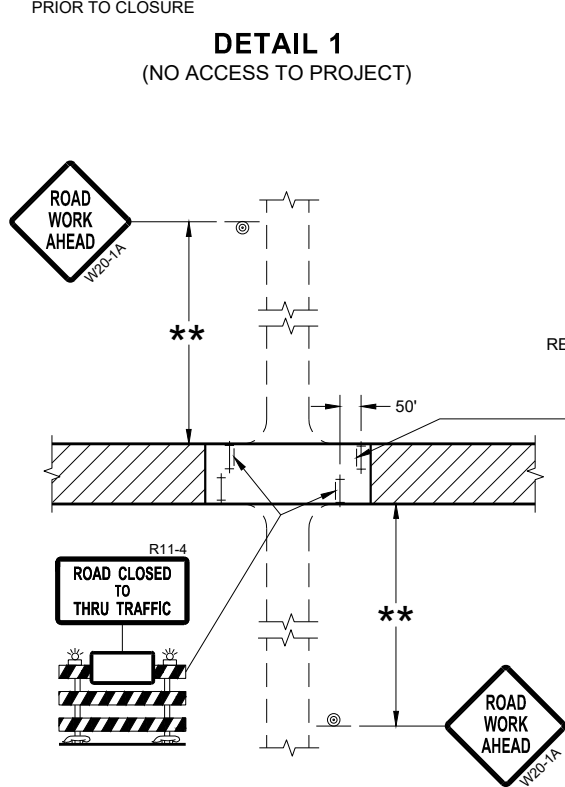
FHWA



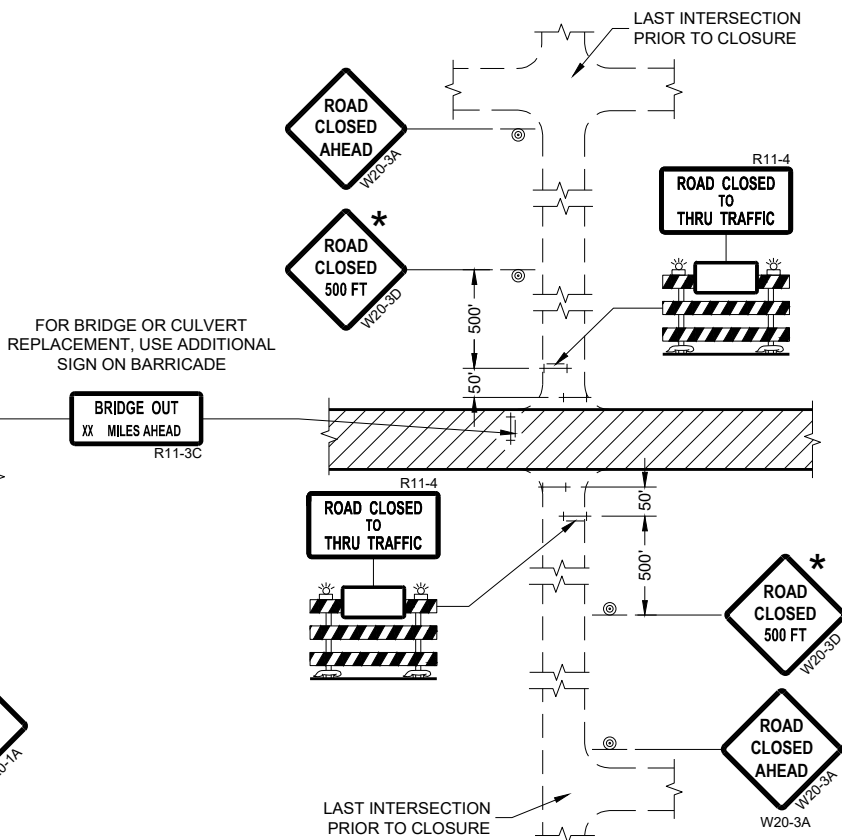
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

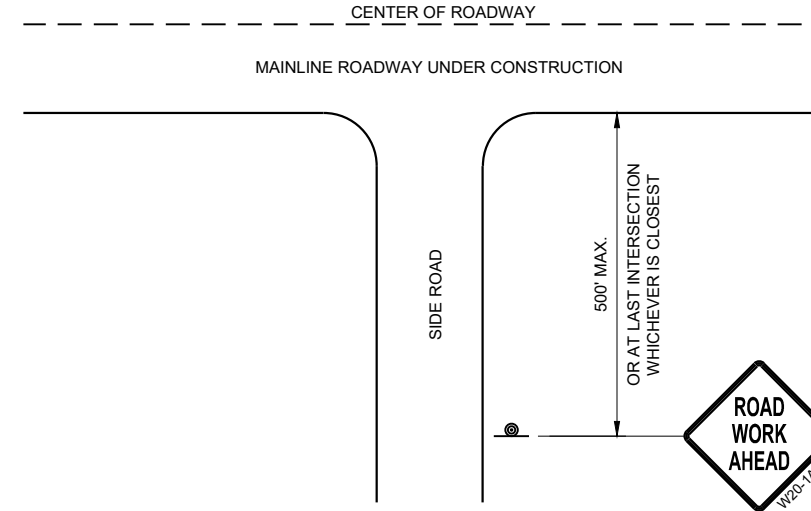
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

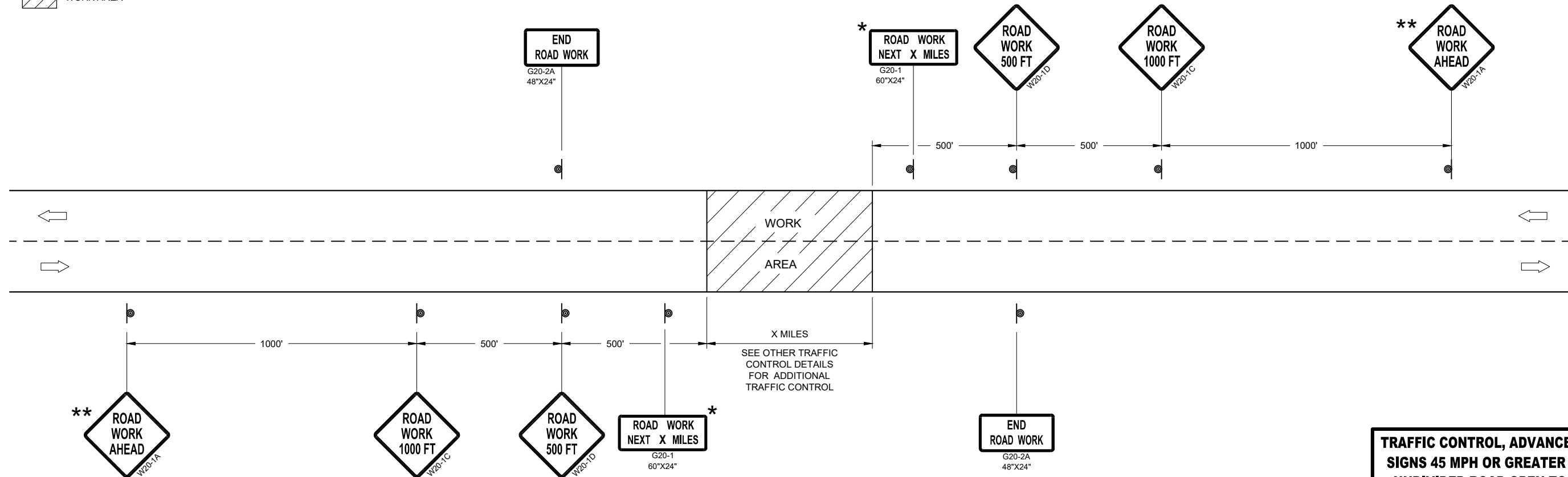
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 45 MPH OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

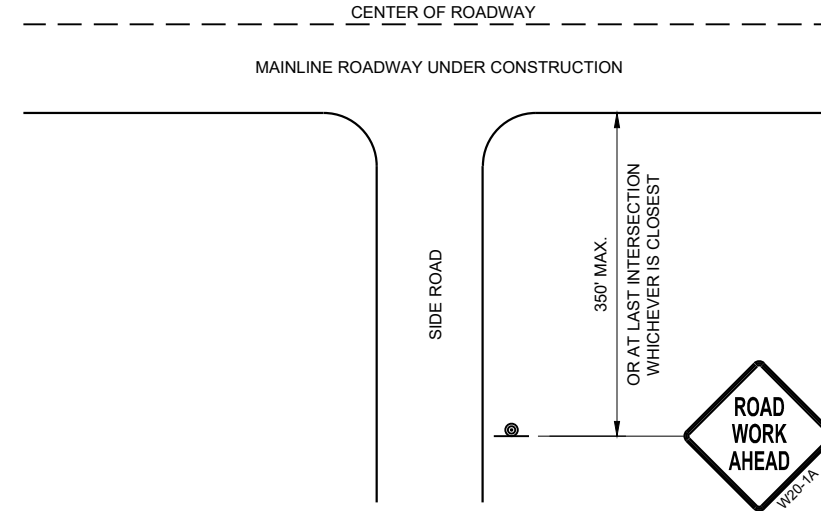
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

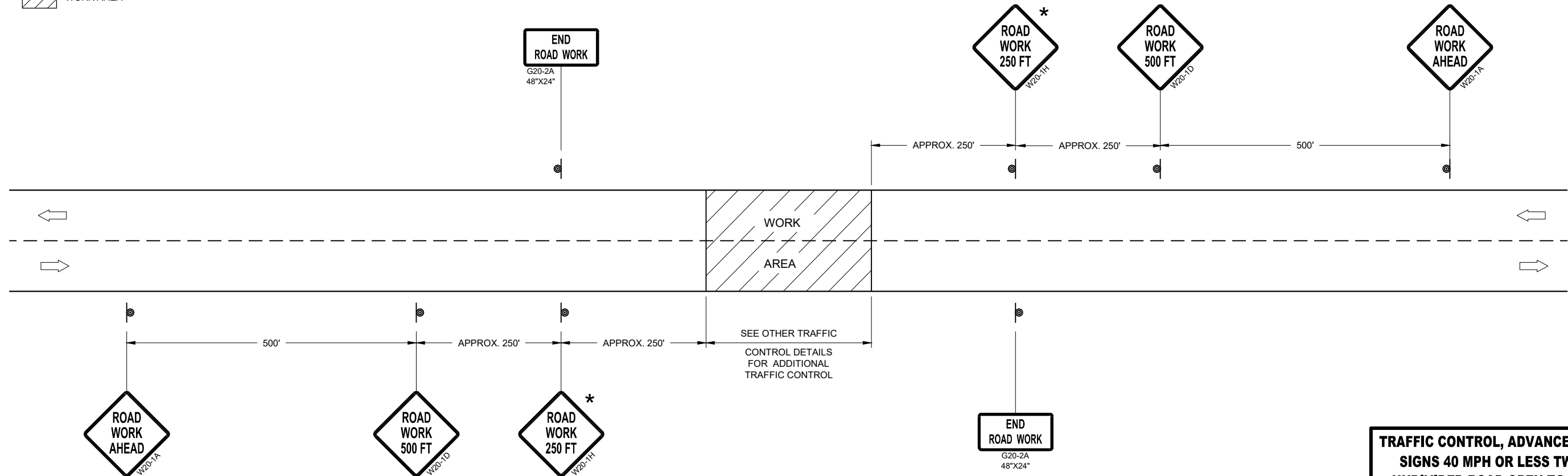
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

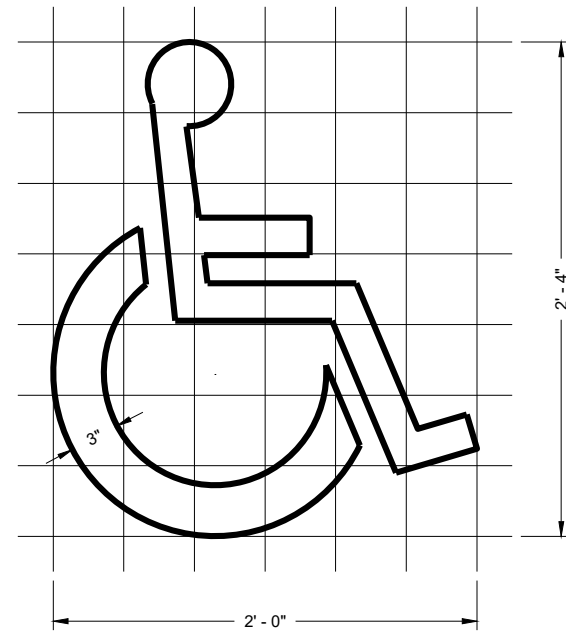
STATE OF WISCONSIN
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APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

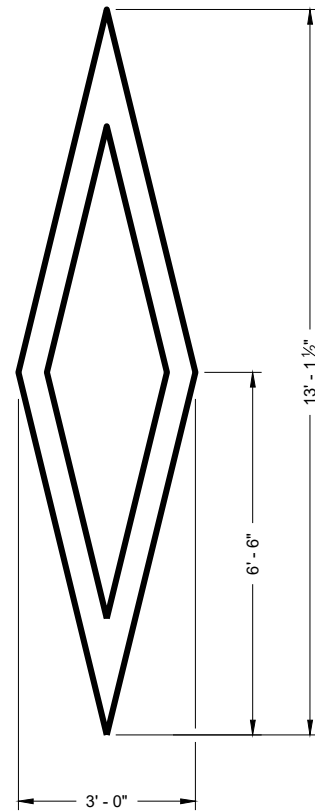
FHWA

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



HANDICAP SYMBOL



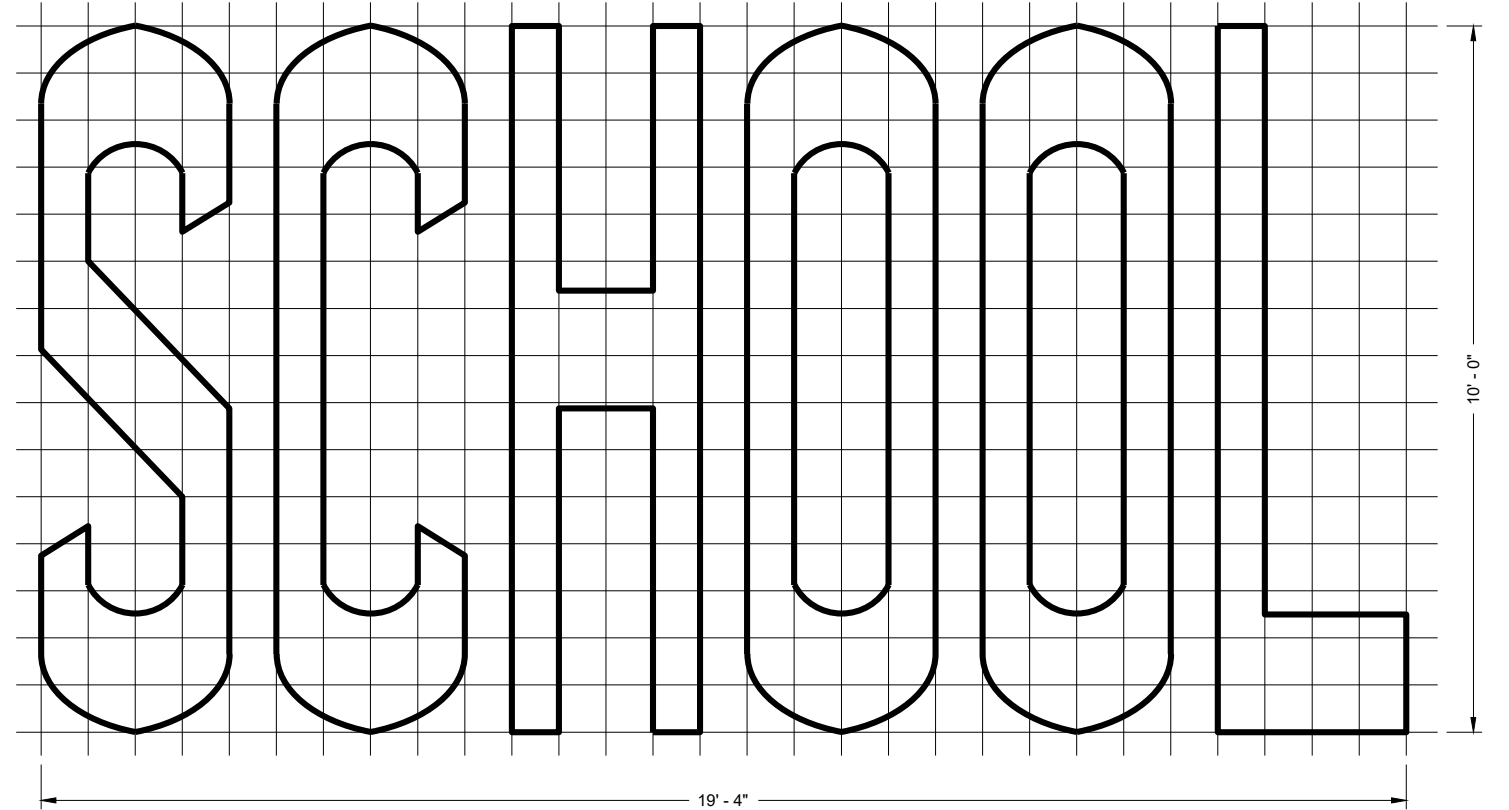
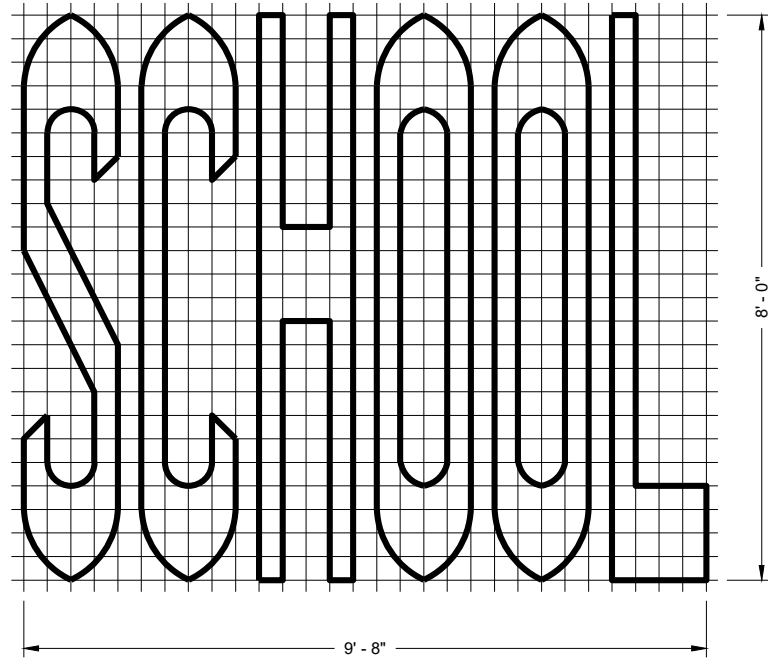
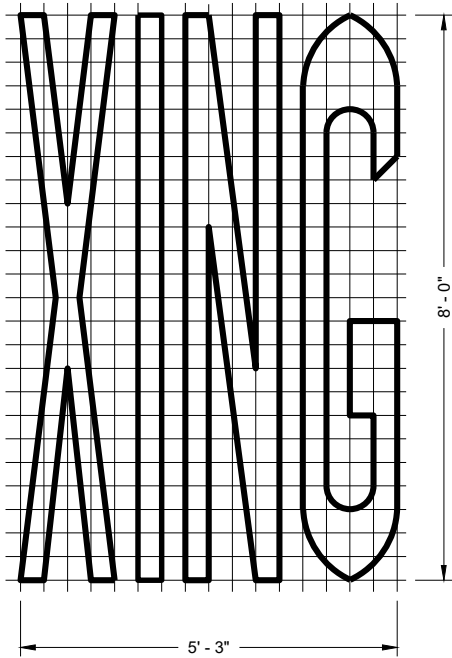
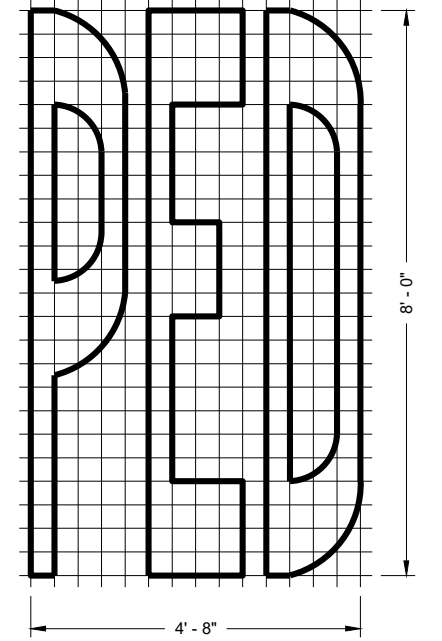
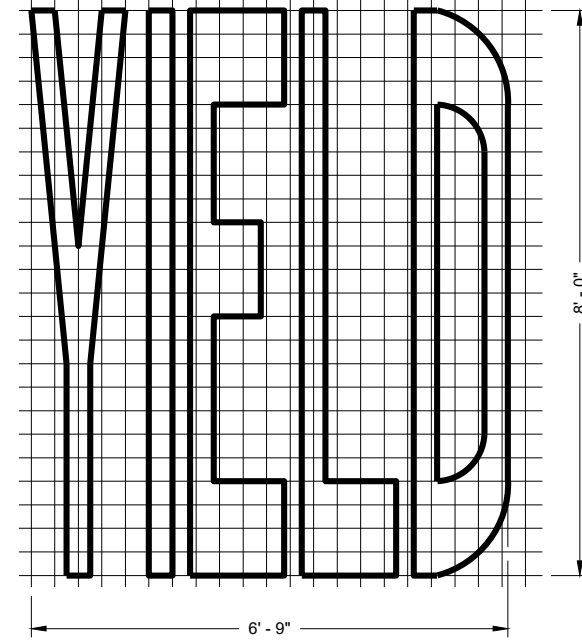
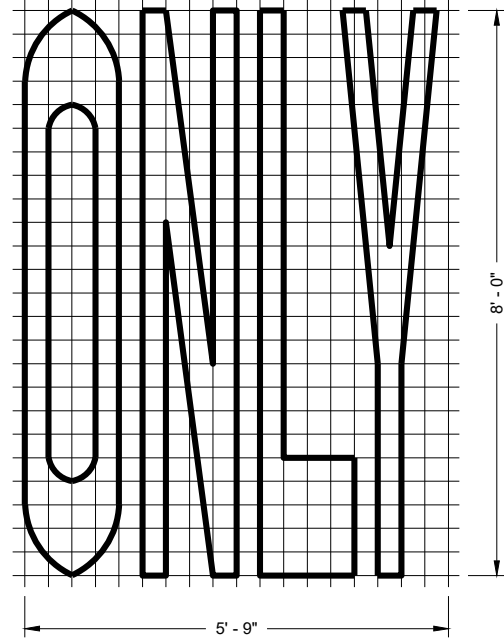
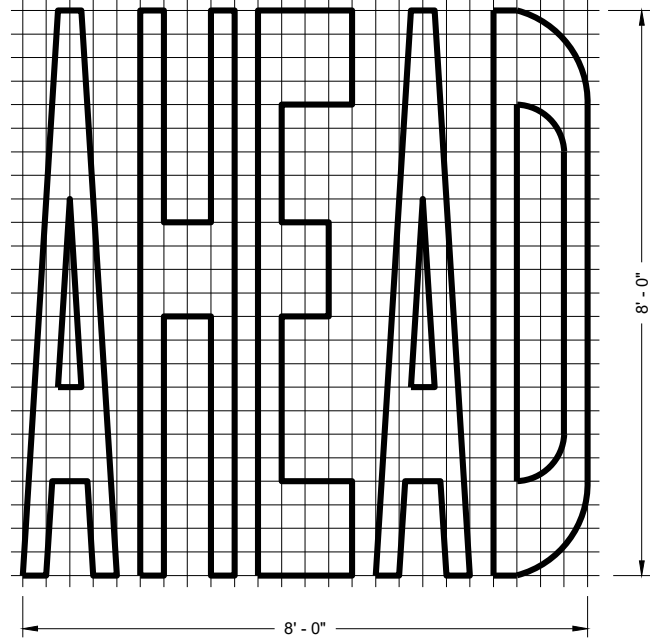
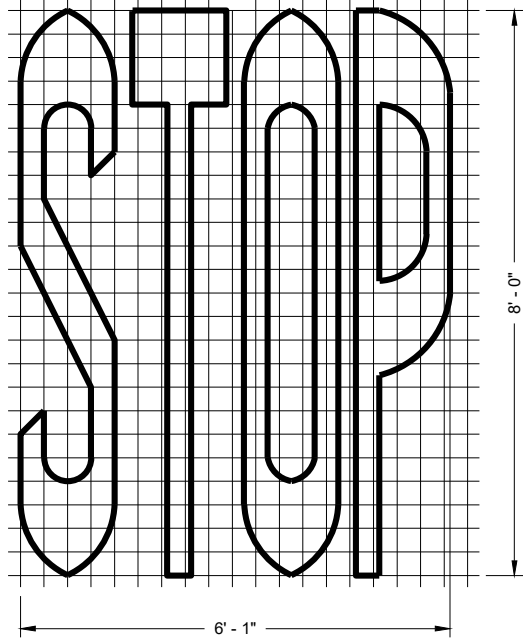
PREFERENTIAL LANE SYMBOL

PAVEMENT MARKING SYMBOLS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

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SINGLE LANE

TWO - LANE

GENERAL NOTES

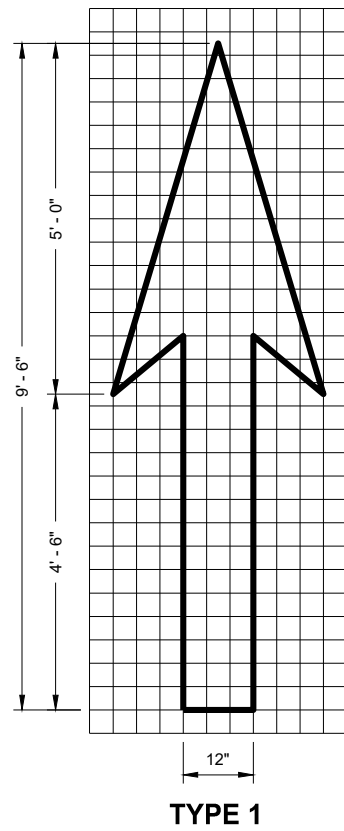
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING WORDS

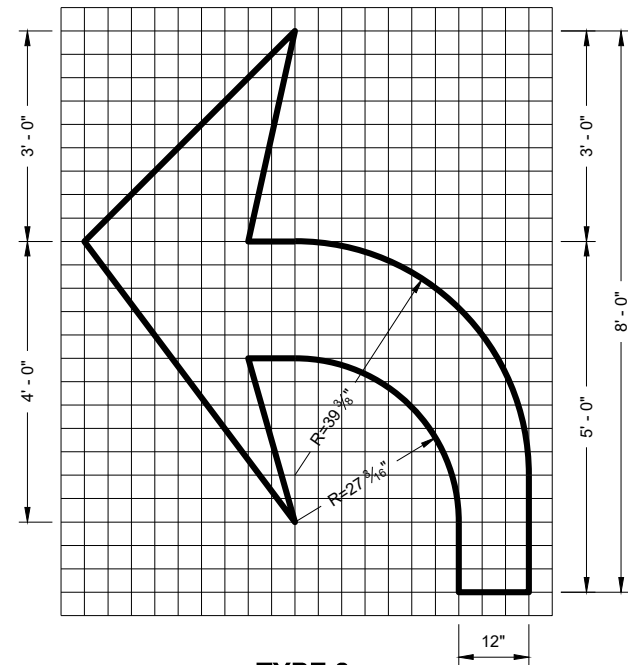
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

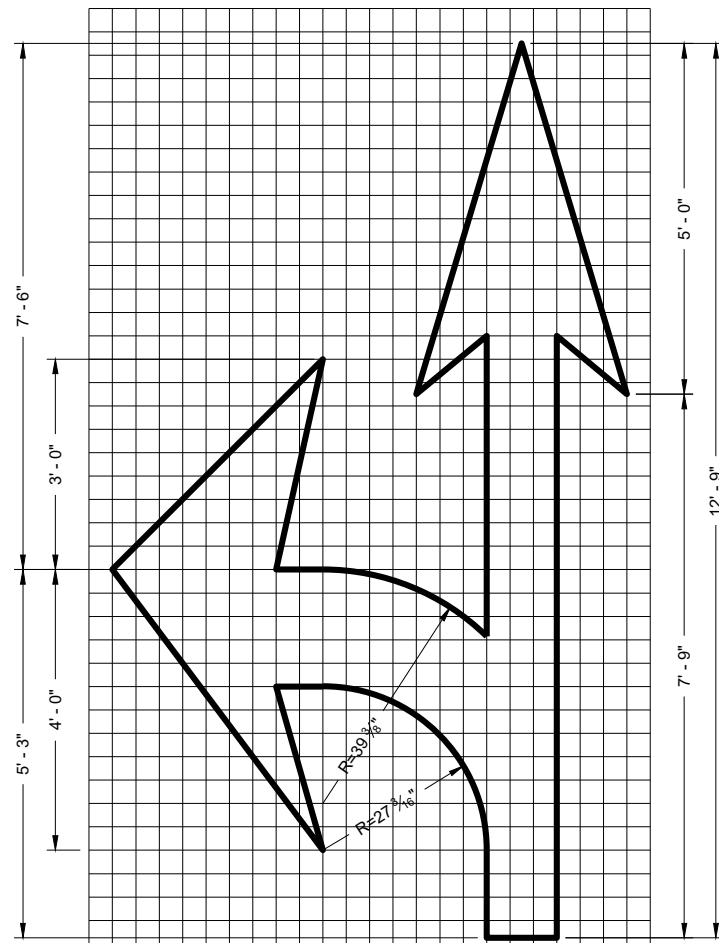
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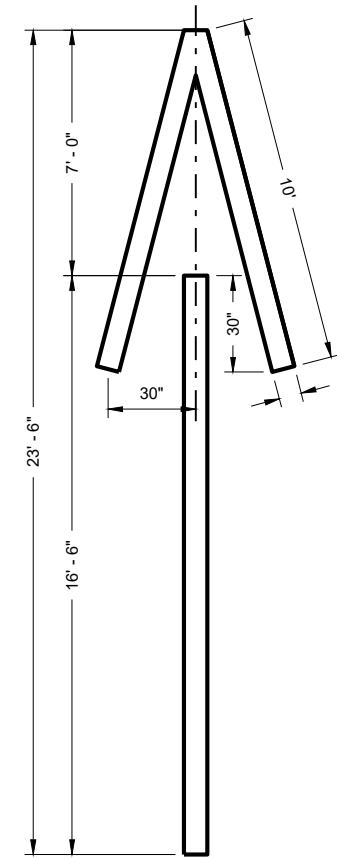
TYPE 1



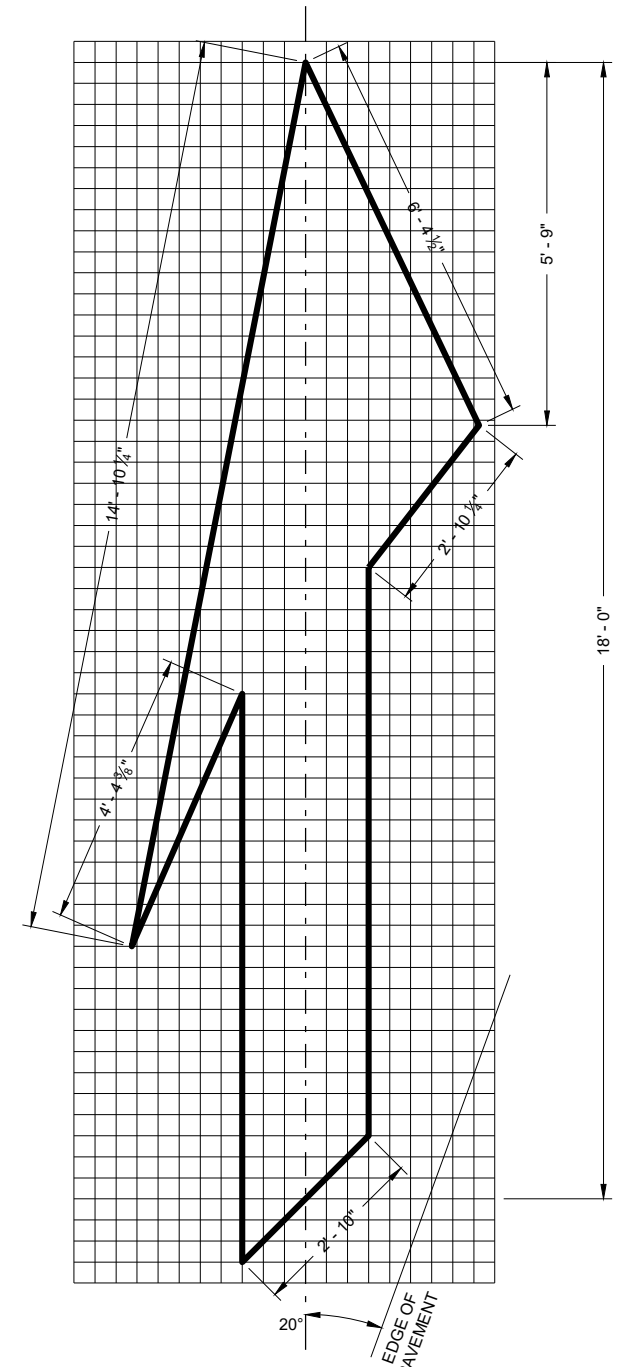
TYPE 2



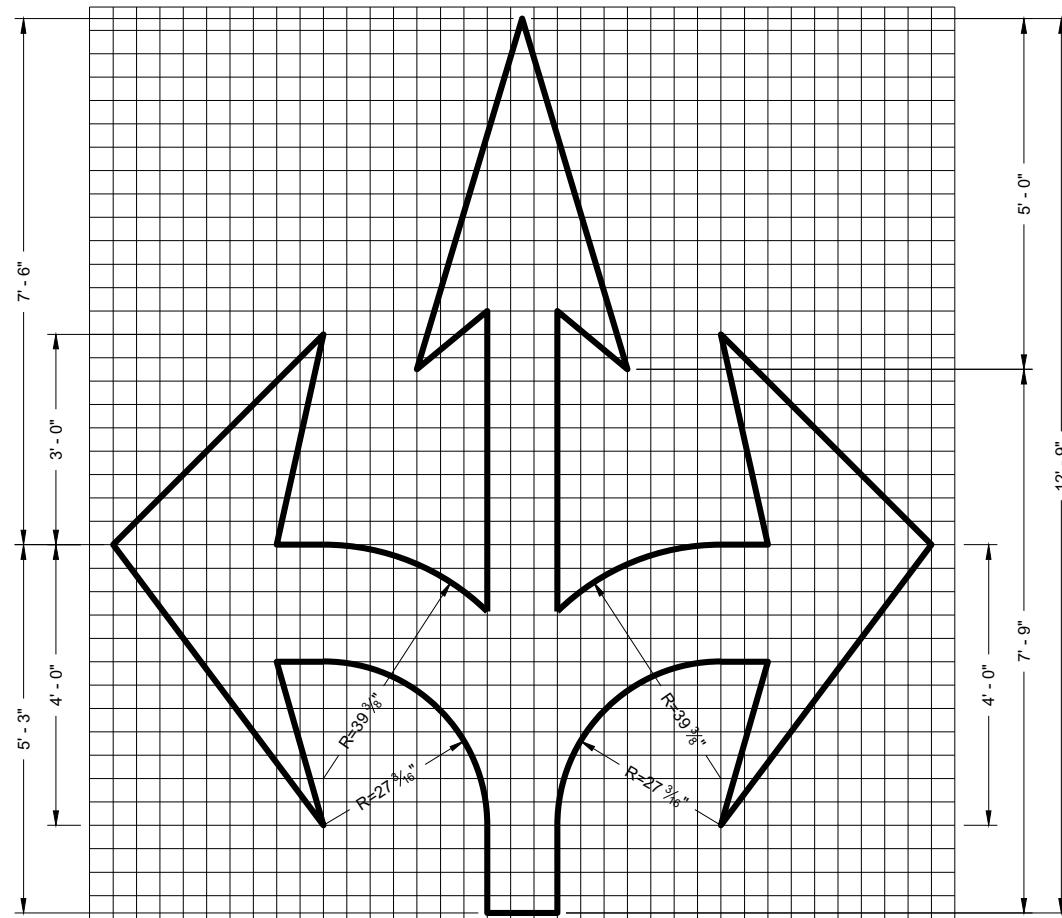
TYPE 3



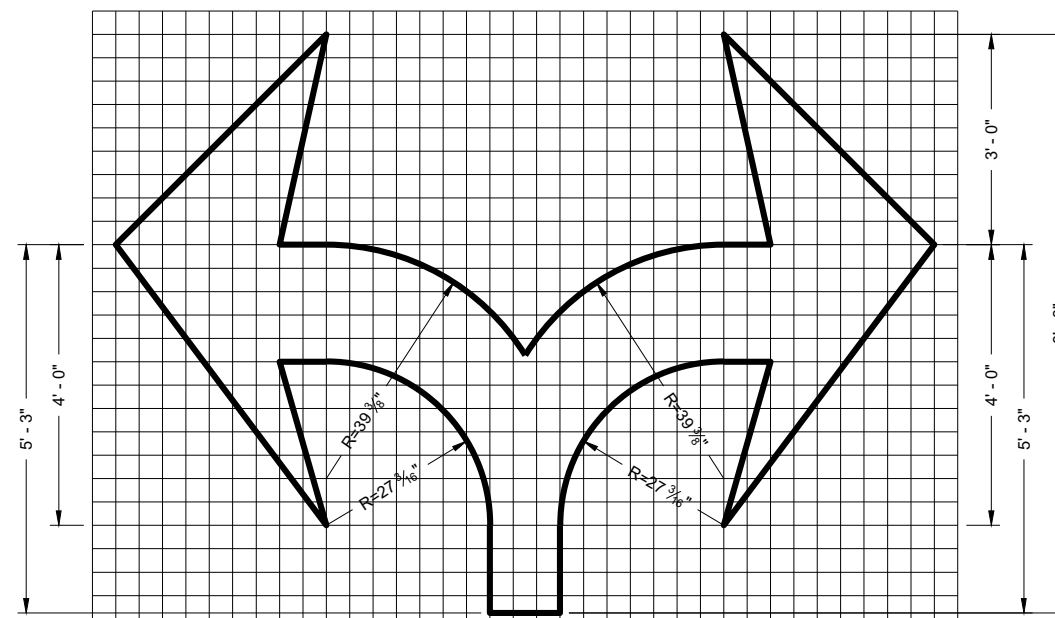
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED	/s/ Matthew Rauch
November 2019	STATE SIGNING AND MARKING ENGINEER
DATE	




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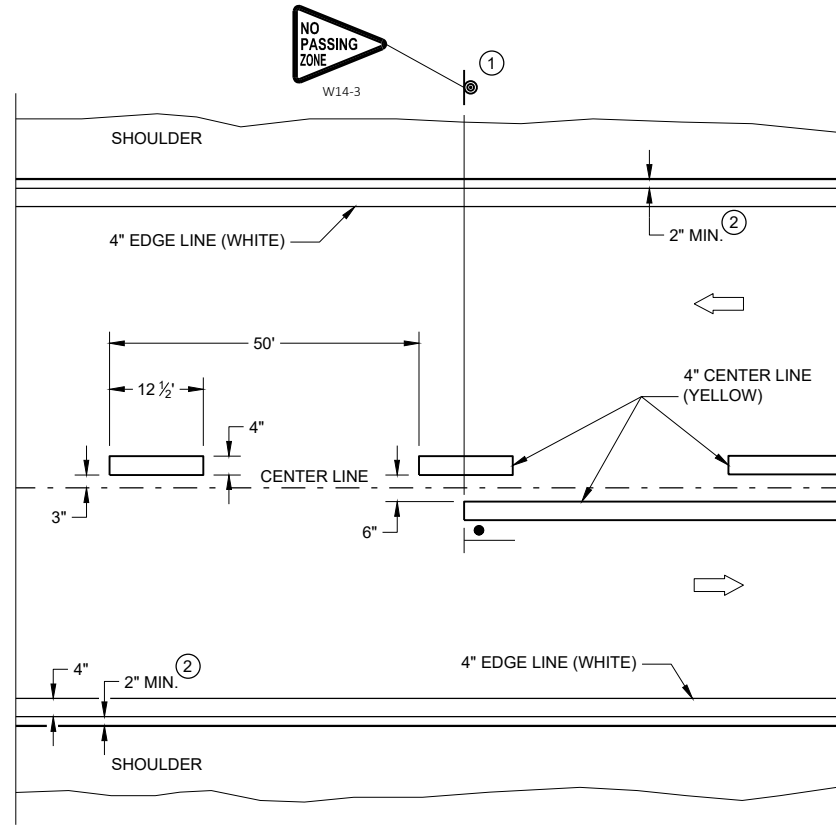
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

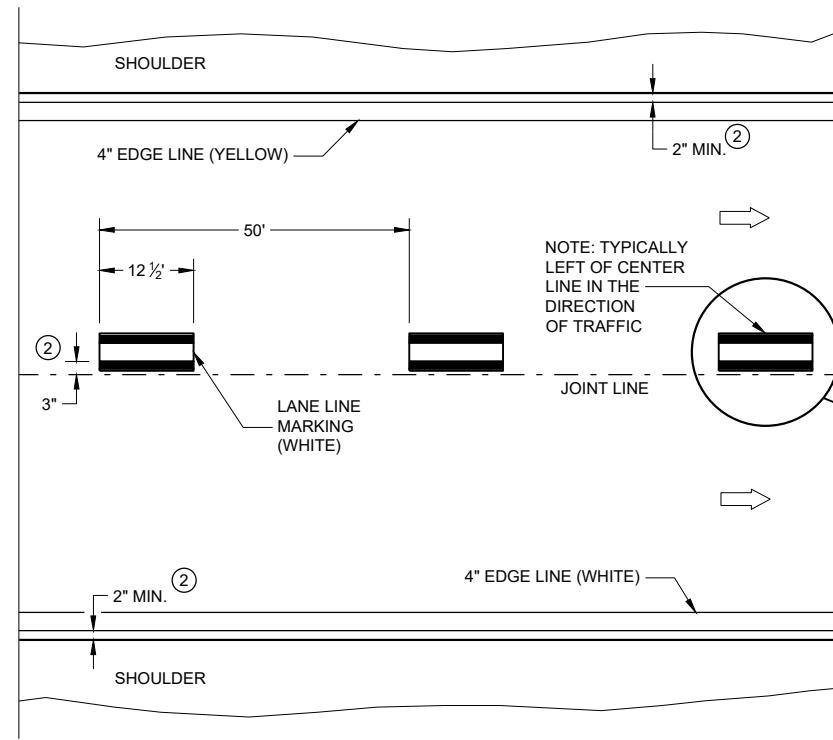
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC



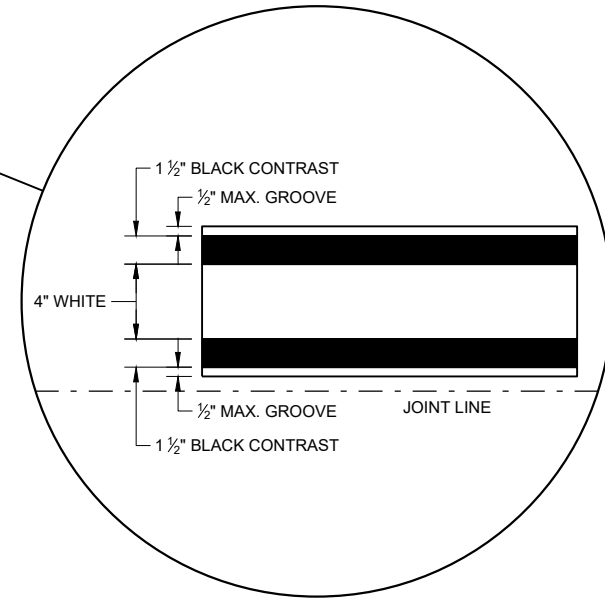
TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

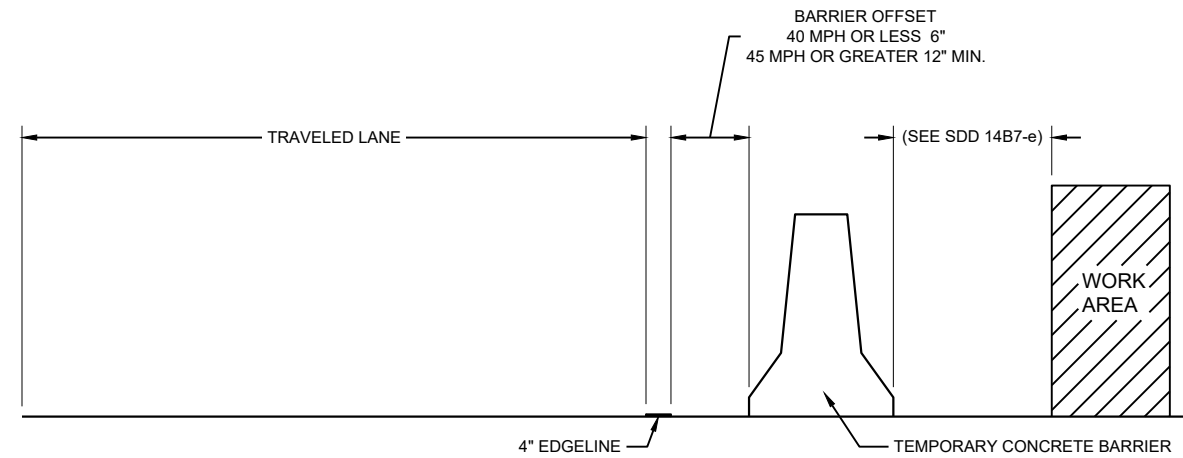
NOTE: TYPICALLY LEFT OF CENTER LINE IN THE DIRECTION OF TRAFFIC



PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2022 /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER



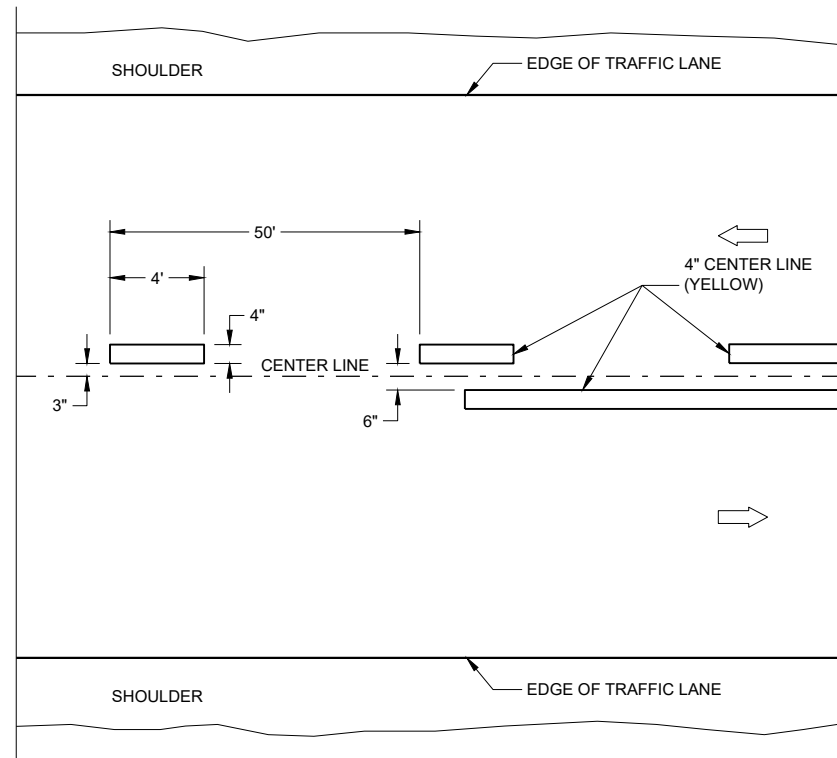
TEMPORARY BARRIER OFFSET FROM EDGELINE

GENERAL NOTES

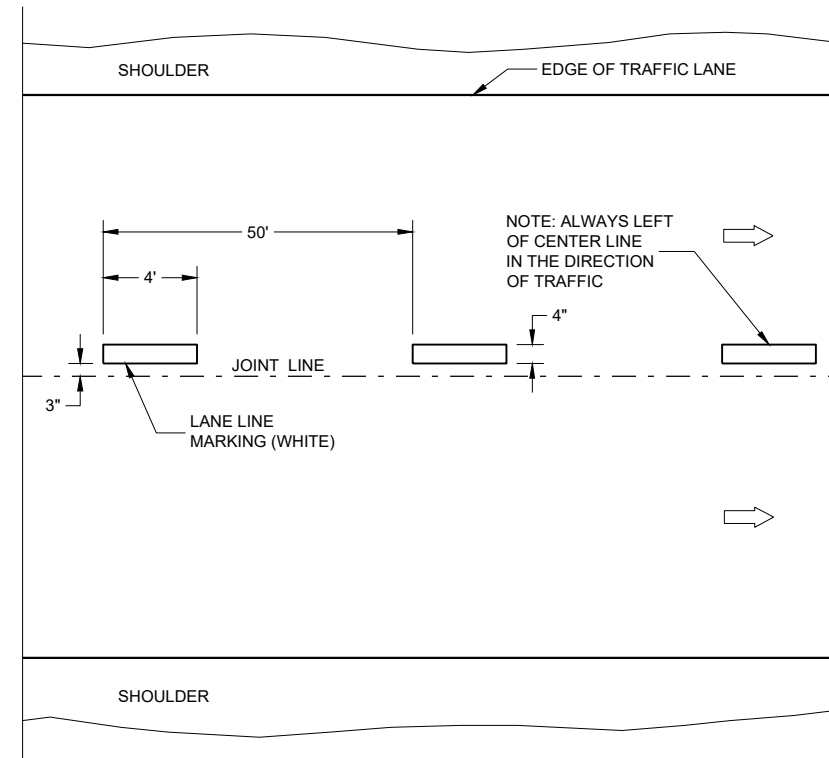
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

LEGEND

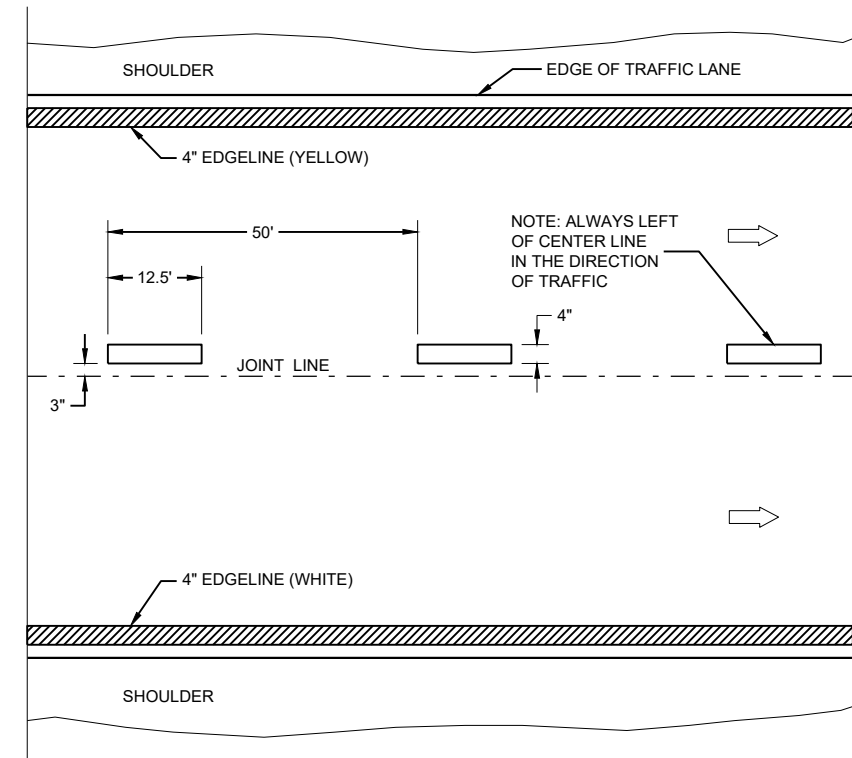
➡ DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

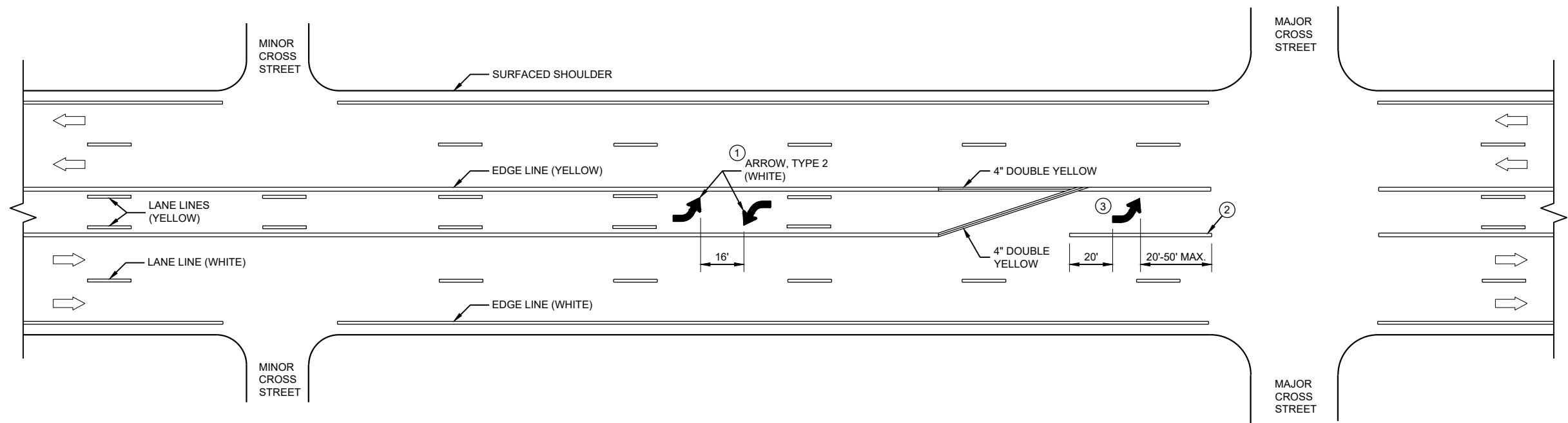
APPROVED
May 2022 DATE /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER

FHWA

GENERAL NOTES

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 8" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC



TWO WAY LEFT TURN LANE

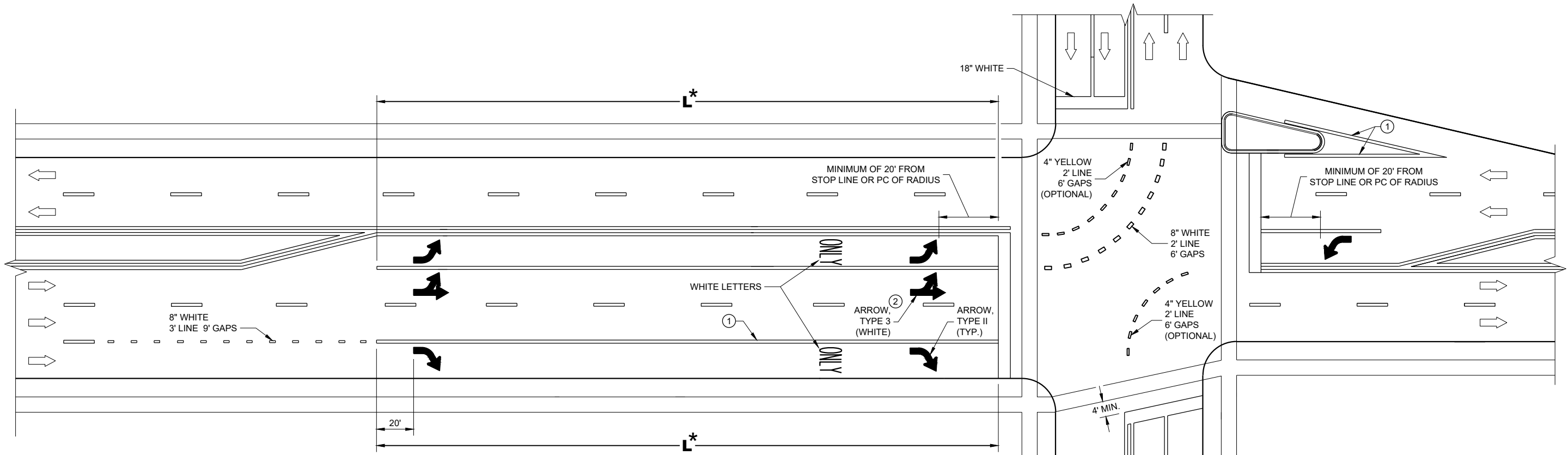
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SDD 15C08 - 22c

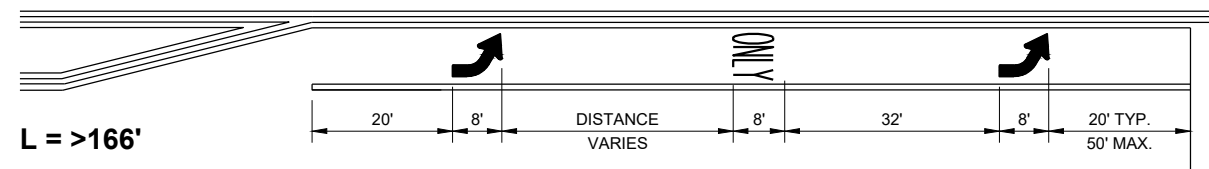
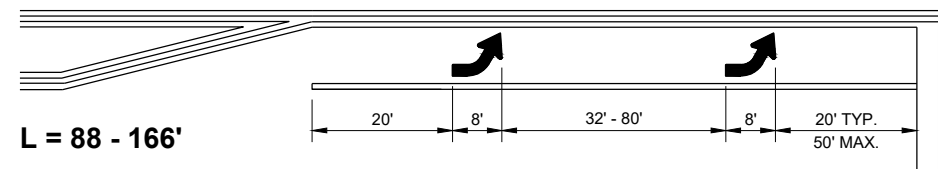
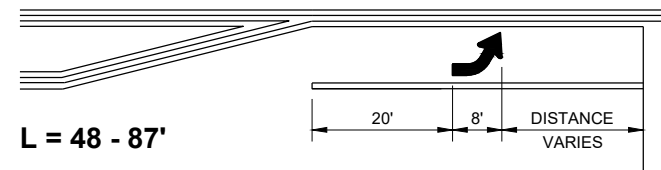
SDD 15C08 - 22c

<p>PAVEMENT MARKING (TURN LANES)</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TURN LANE OPTIONS

LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.






➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

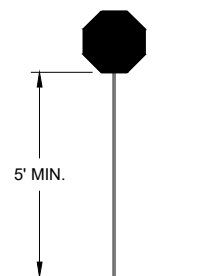
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



STOP/SLOW PADDLE ON SUPPORT STAFF

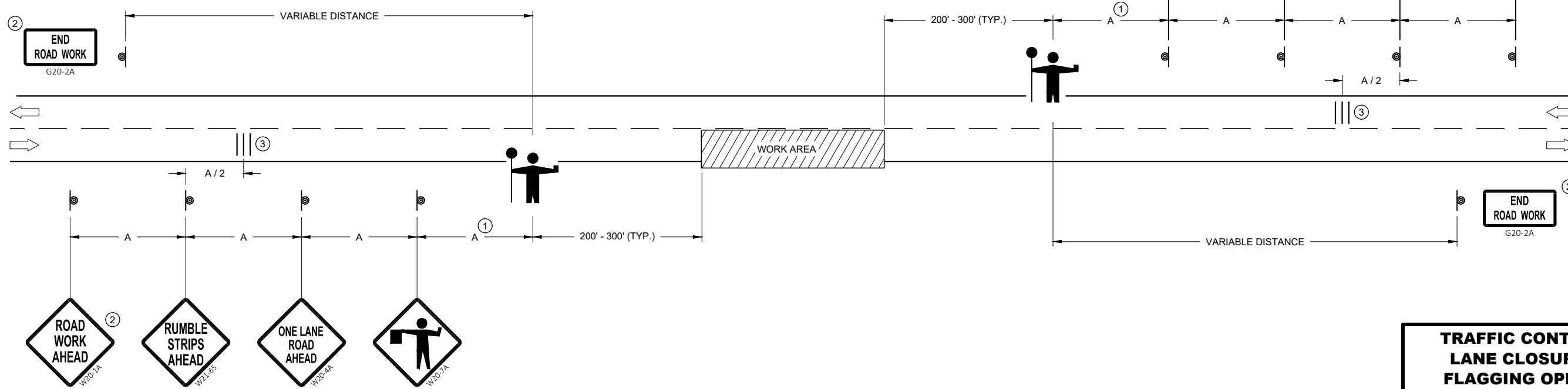
SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



W03-4

USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



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SDD 15C12 - 09a

SDD 15C12 - 09a

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL CONE 42-INCH
-  TRAFFIC CONTROL DRUM
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  **AFAD** AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD)

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

IF THE AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD) STOPS WORKING, FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

- ① SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- ② IF FLAGGERS ARE PHYSICALLY NEEDED TO FLAG, REPLACE WO3-4 SIGNS WITH W20-7A SIGNS.

TEMPORARY PORTABLE RUMBLE STRIPS

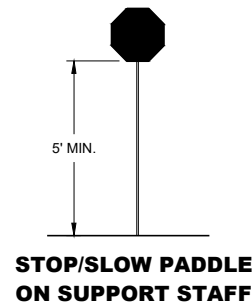
UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

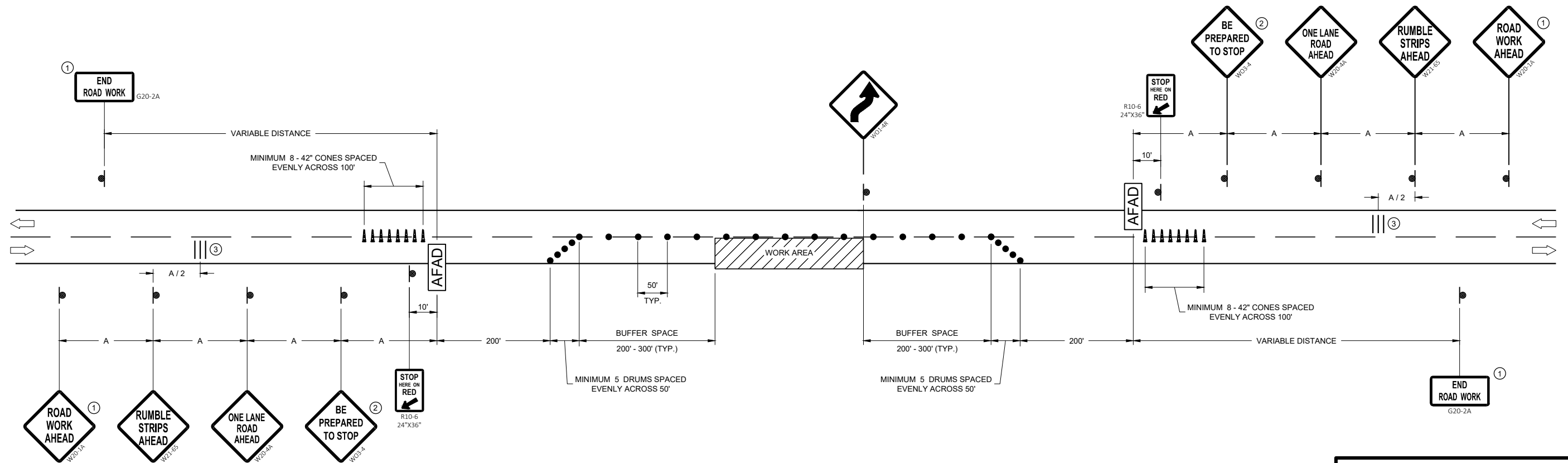
DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSELY AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER.



SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

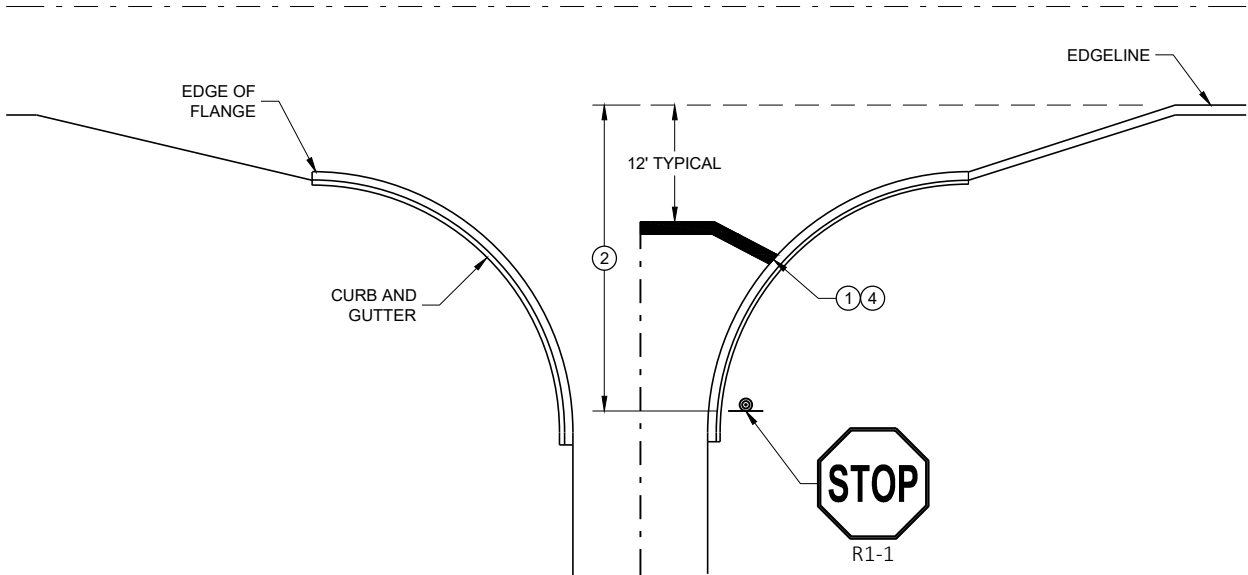
APPROVED
May 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

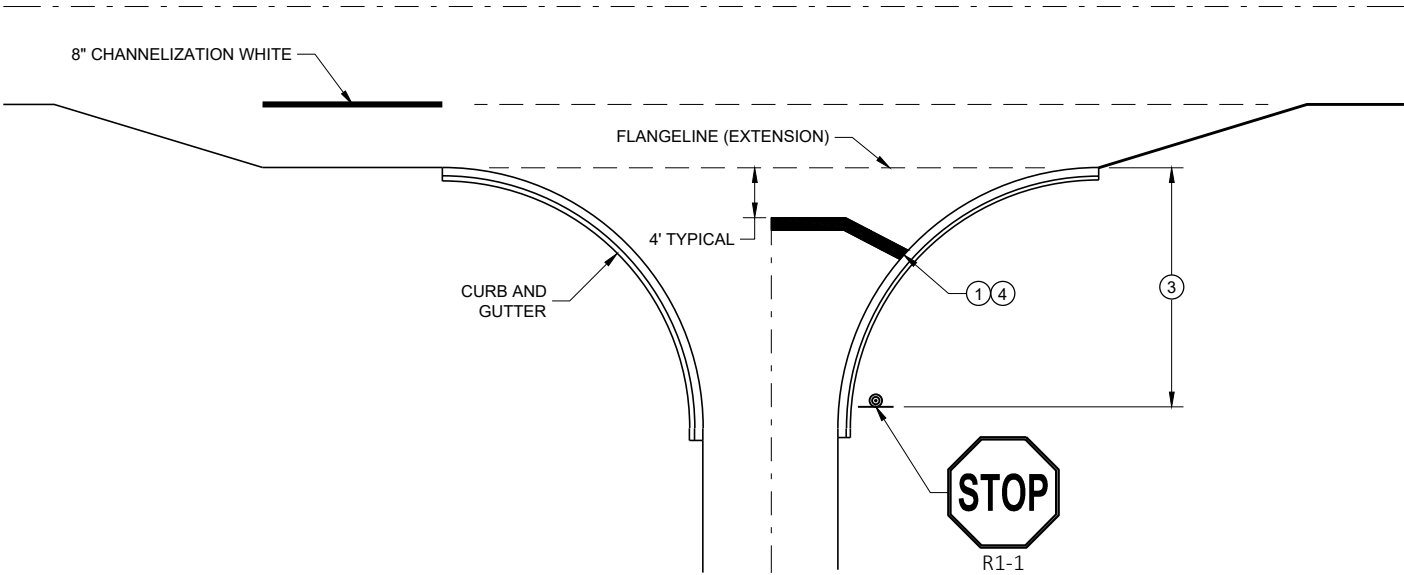
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

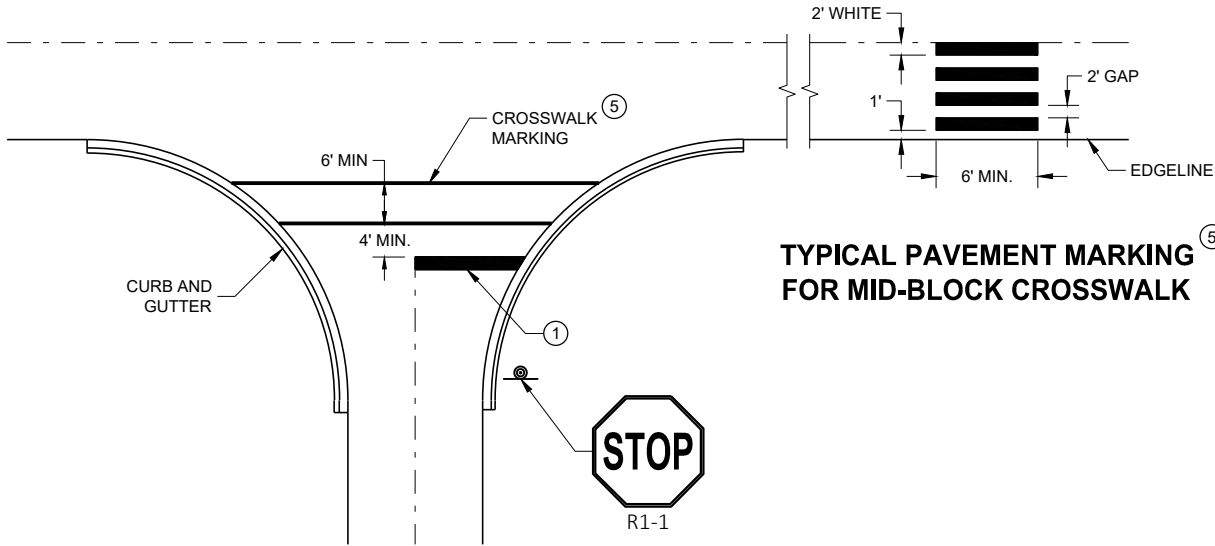
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



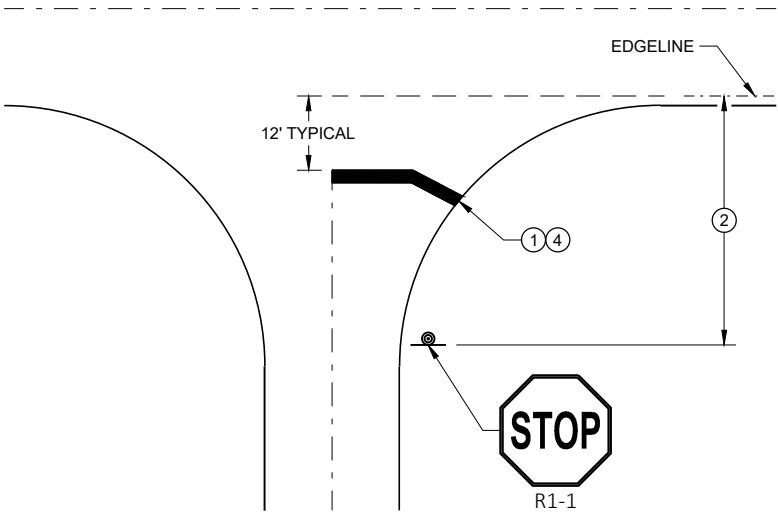
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



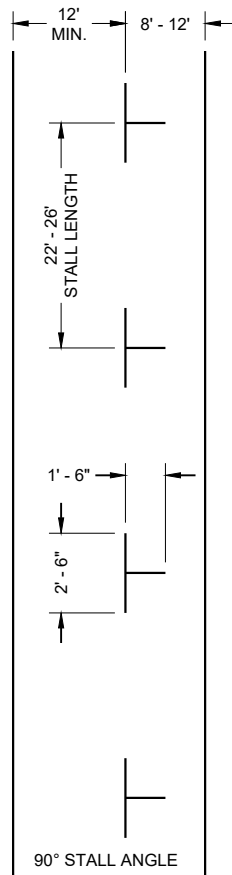
TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

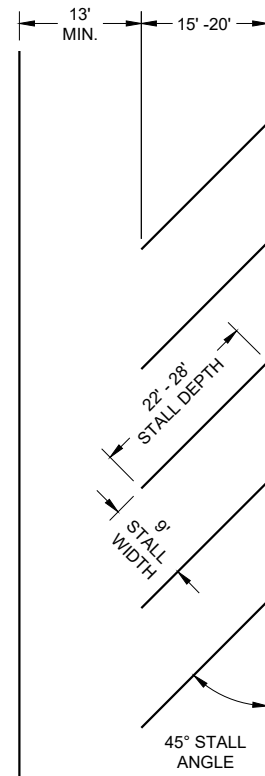
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

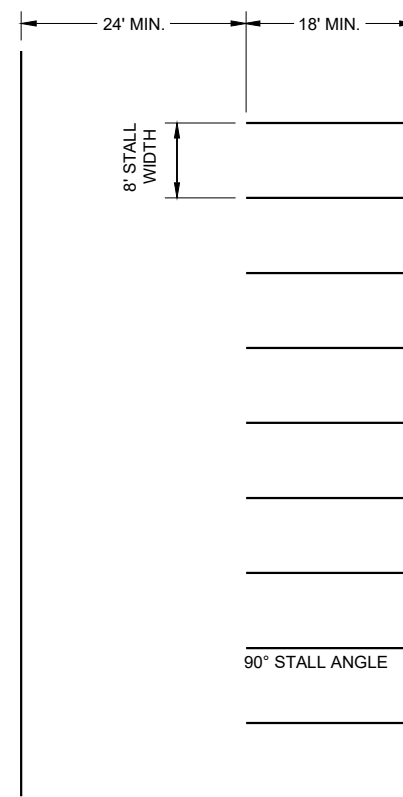
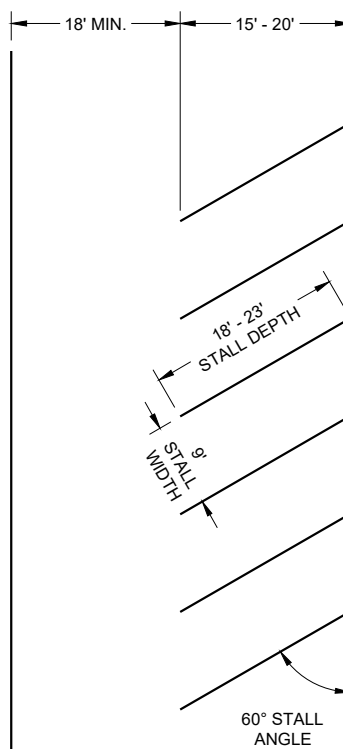


PARALLEL PARKING



ANGLED PARKING

(ANGLED PARKING IS NOT ALLOWED ON STATE HIGHWAYS UNLESS A DESIGN JUSTIFICATION HAS BEEN COMPLETED.)



PARKING LOTS

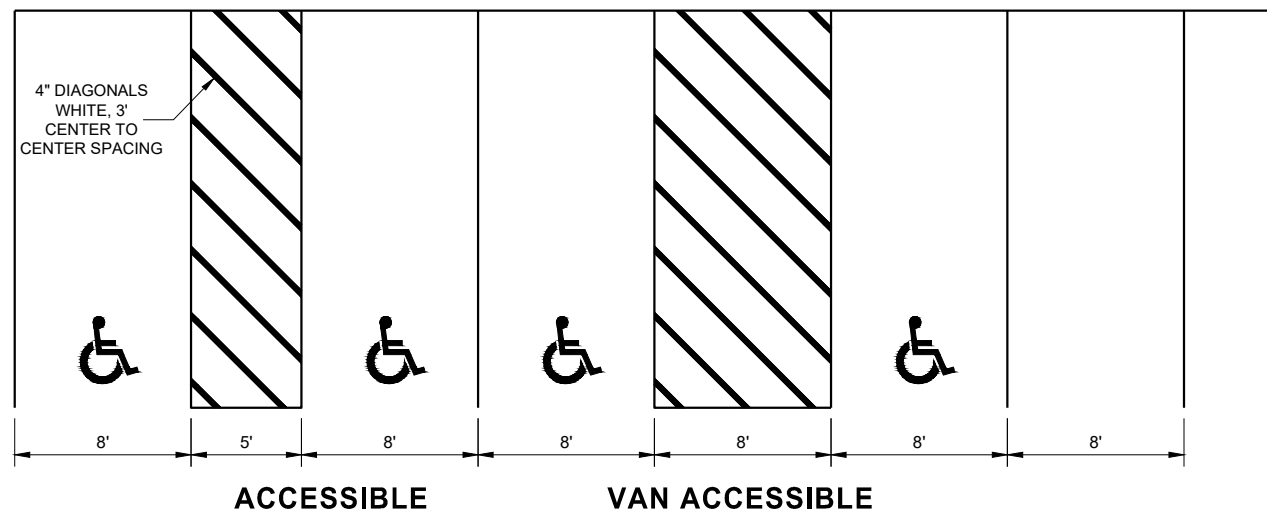
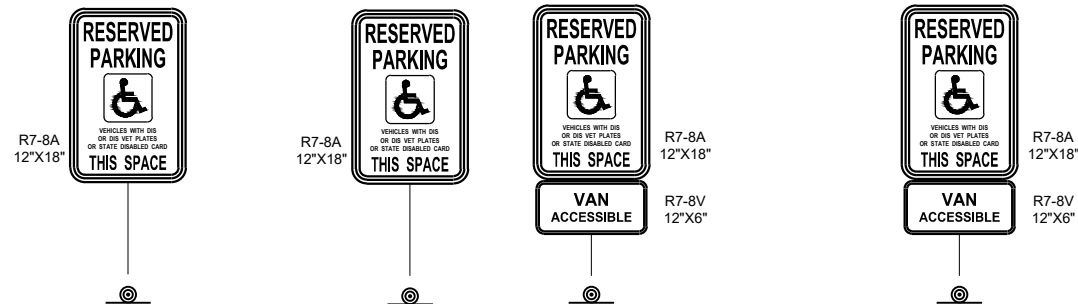
GENERAL NOTES

ALL LINES 4" WHITE (UNLESS OTHERWISE NOTED)

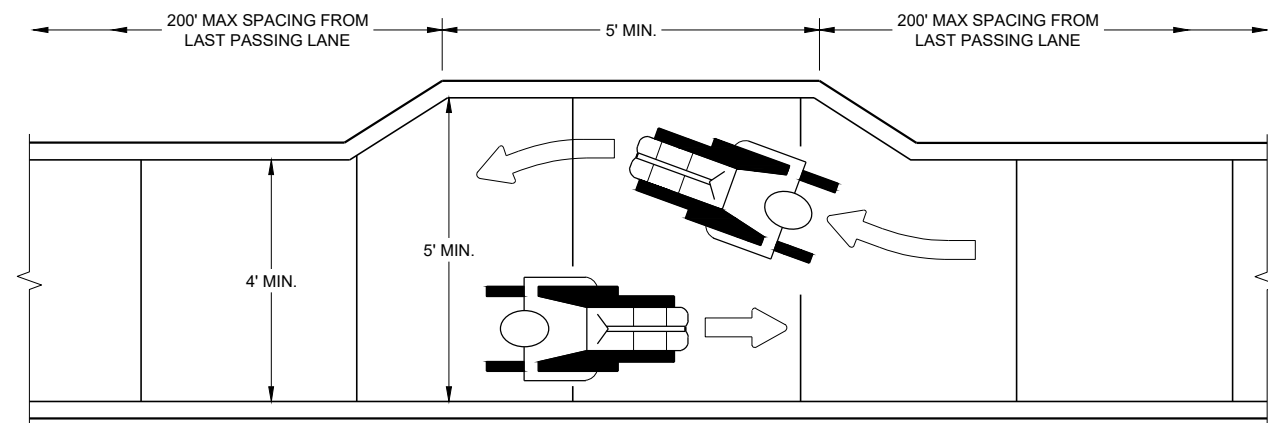
LAST PARKING STALL IS A MINIMUM OF 15' FROM THE CROSSWALK.

LEGEND

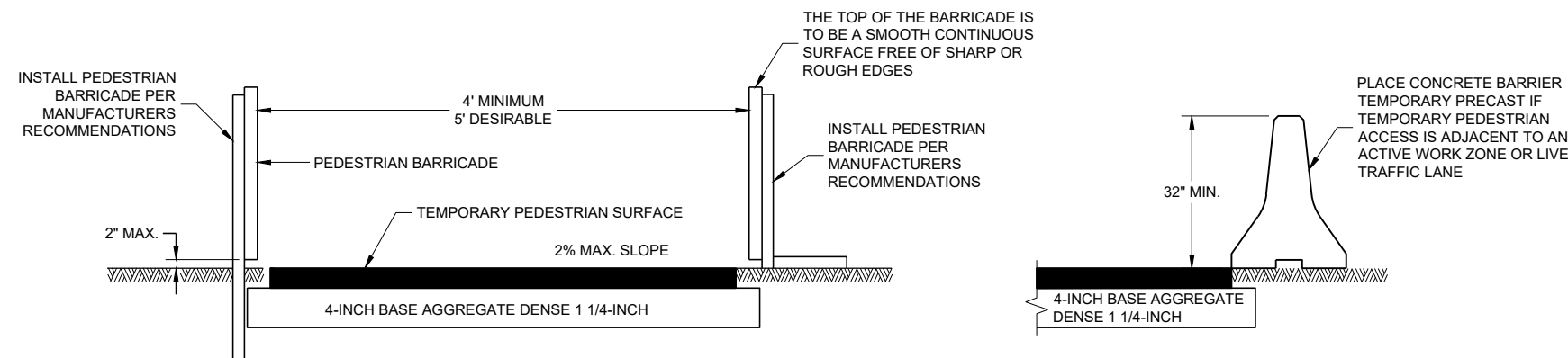
⊙ SIGN ON PERMANENT SUPPORT



PARKING STALL MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED August 2019 DATE	/s/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
<small>FHWA</small>	



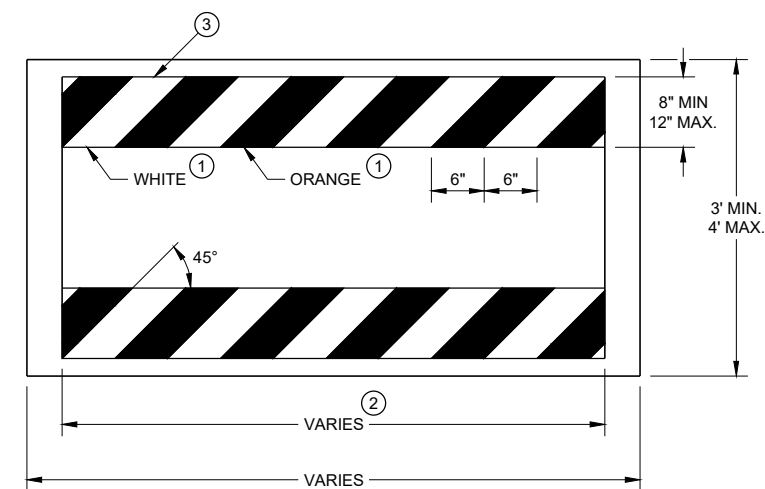
NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

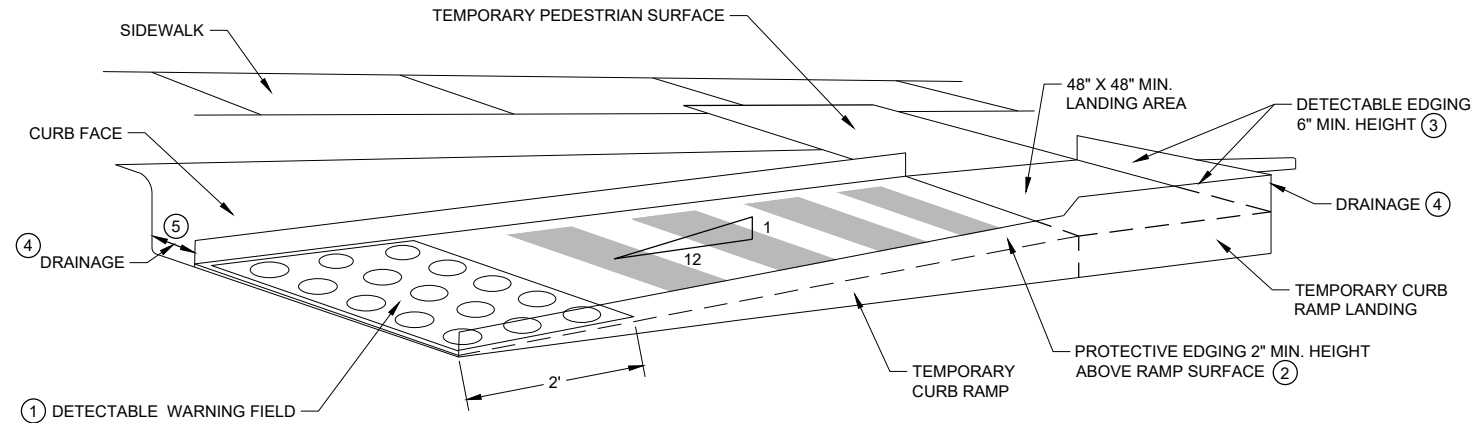


TEMPORARY PEDESTRIAN BARRICADE*

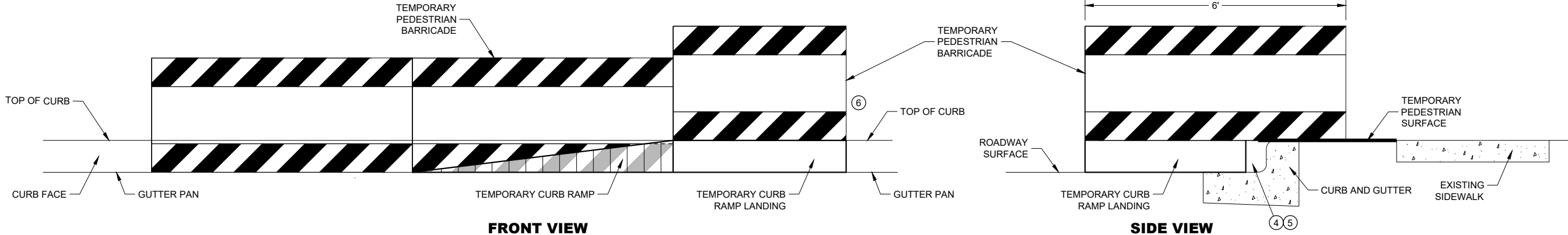
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ 6" MINIMUM BETWEEN CURB FACE AND EDGE OF RAMP
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW

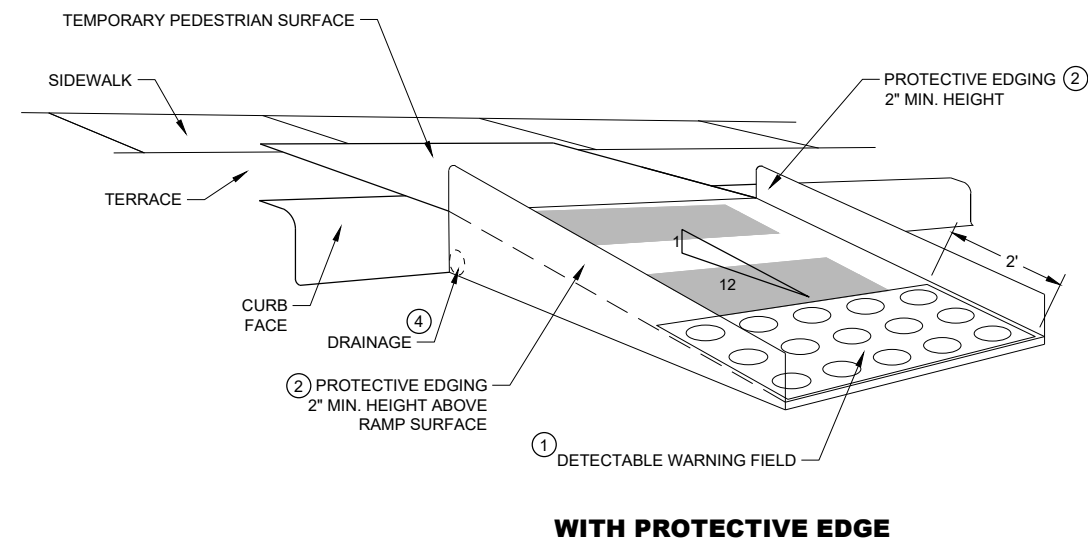
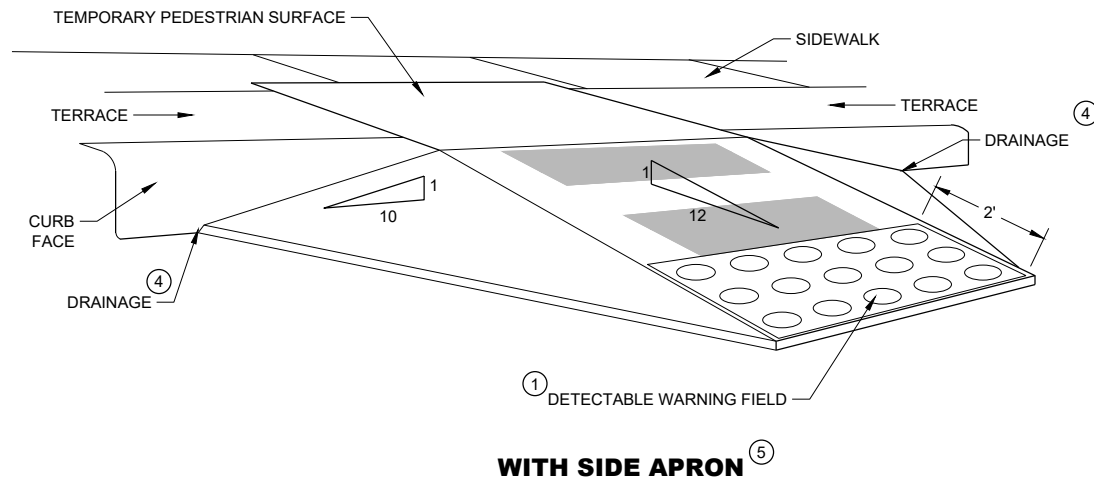


FRONT VIEW

SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

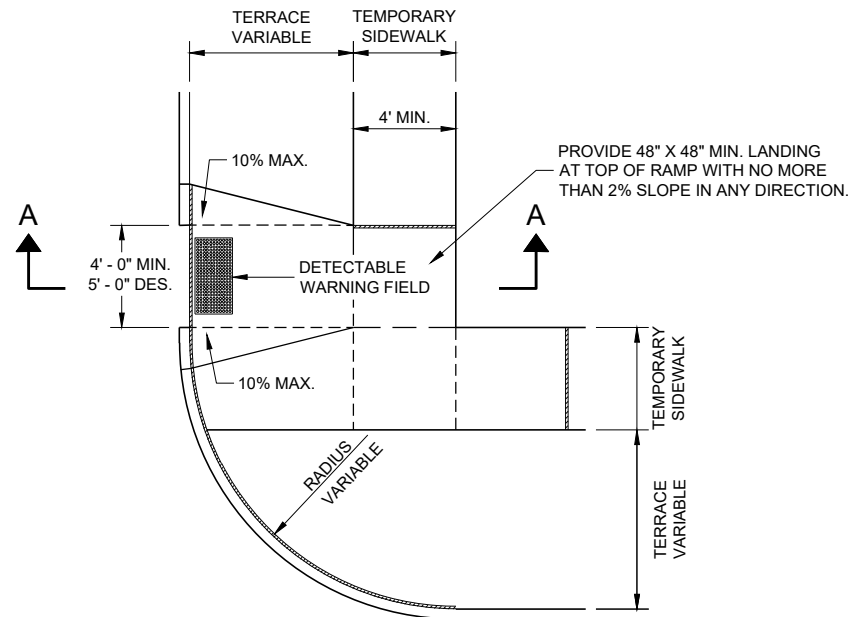
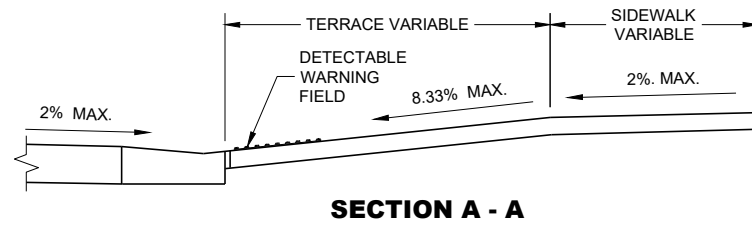
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



PLAN VIEW
TEMPORARY TYPE 3 RAMP
 (OUTSIDE OF CROSSWALK AREA)

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SDD 15D30 - 07d

SDD 15D30 - 07d

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

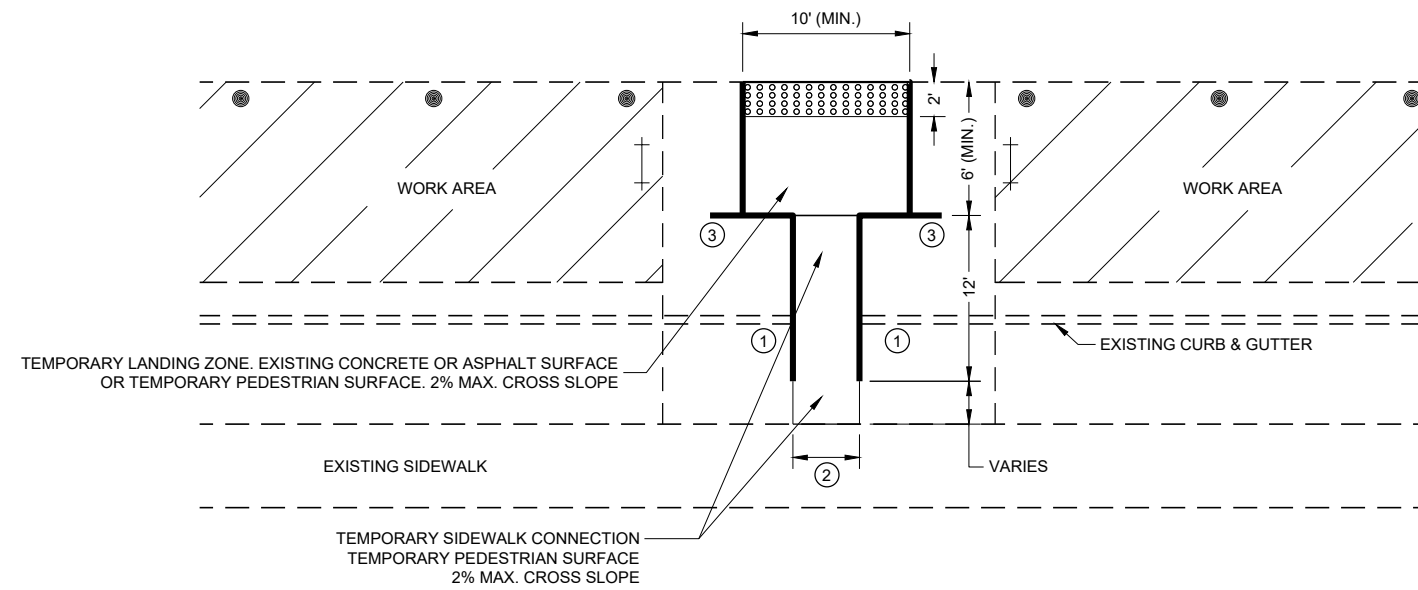
DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

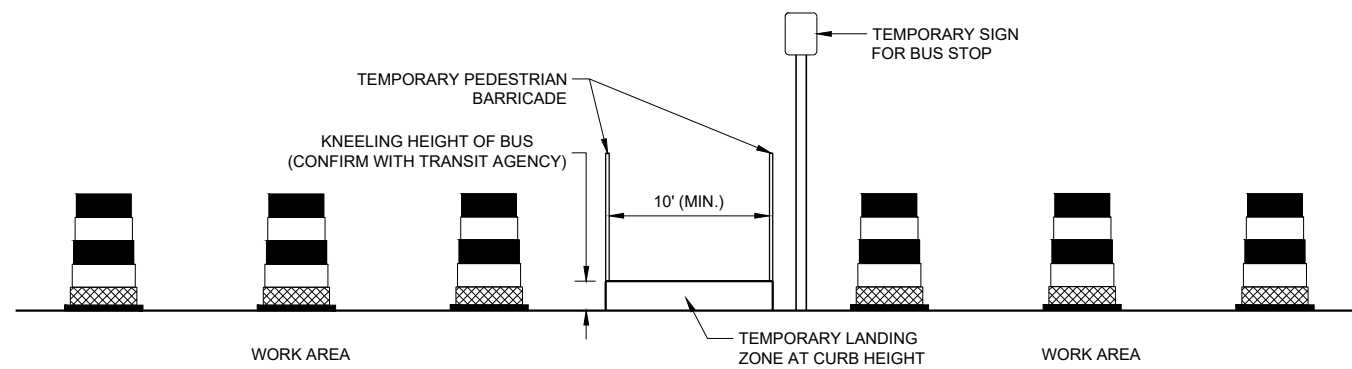
CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.



PLAN VIEW



**PROFILE VIEW
TEMPORARY BUS STOP PAD**







LEGEND

- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY DETECTABLE WARNING FIELD
- WORK AREA

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

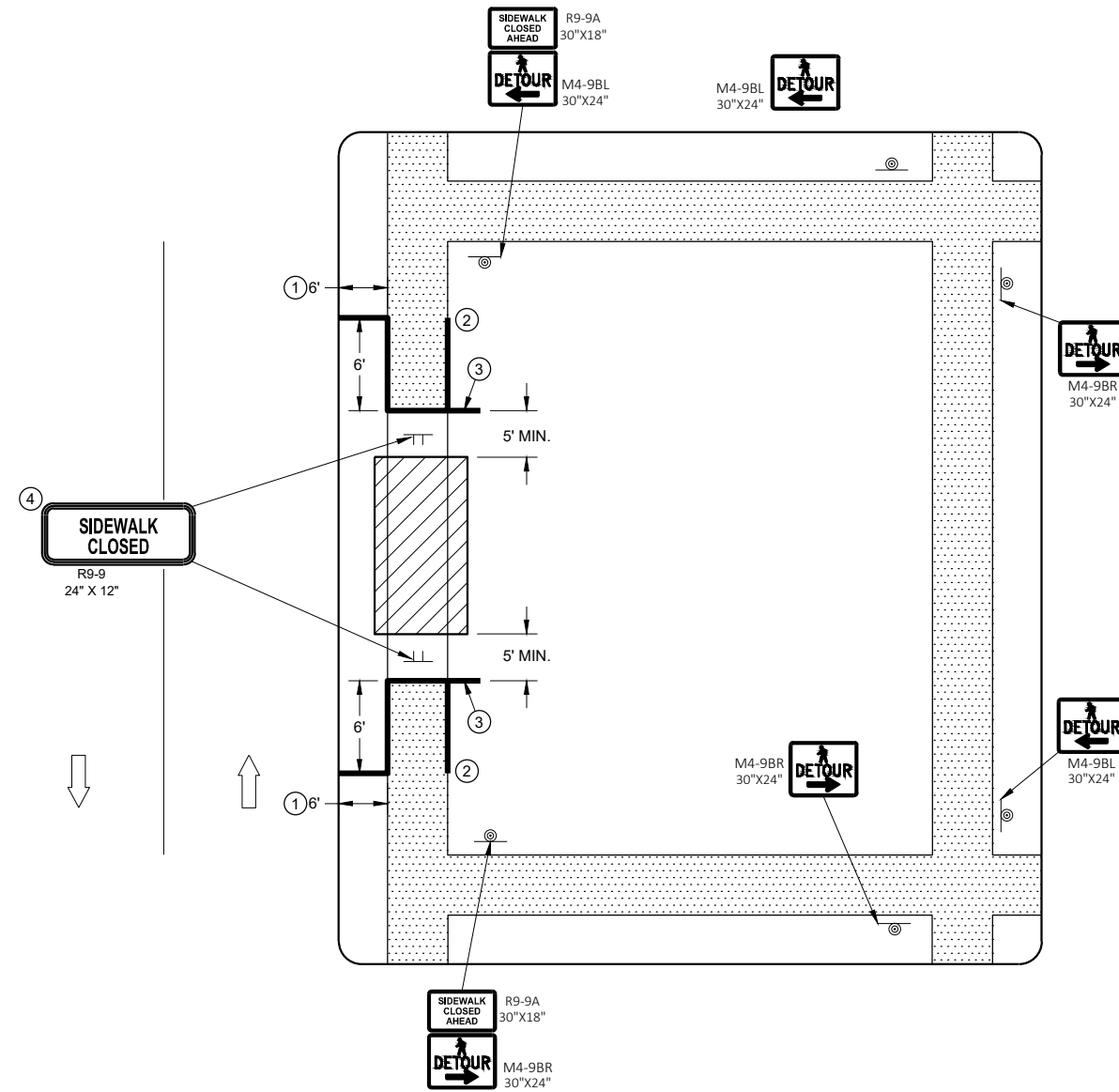
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES








- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

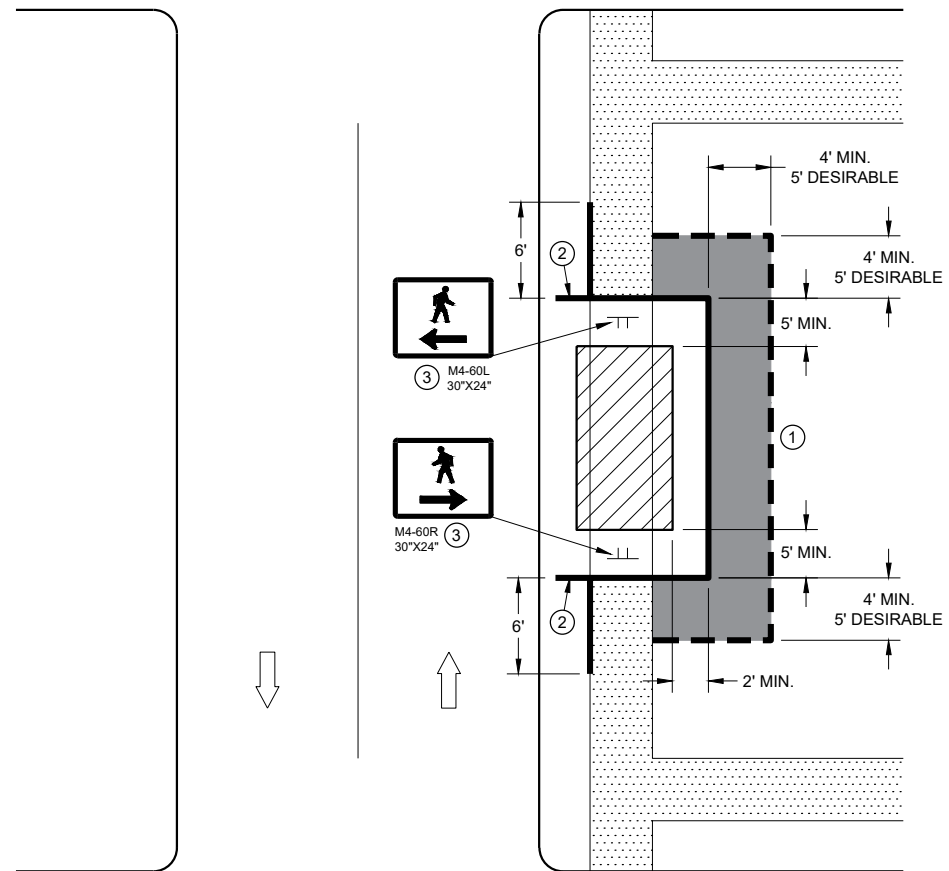
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC










GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



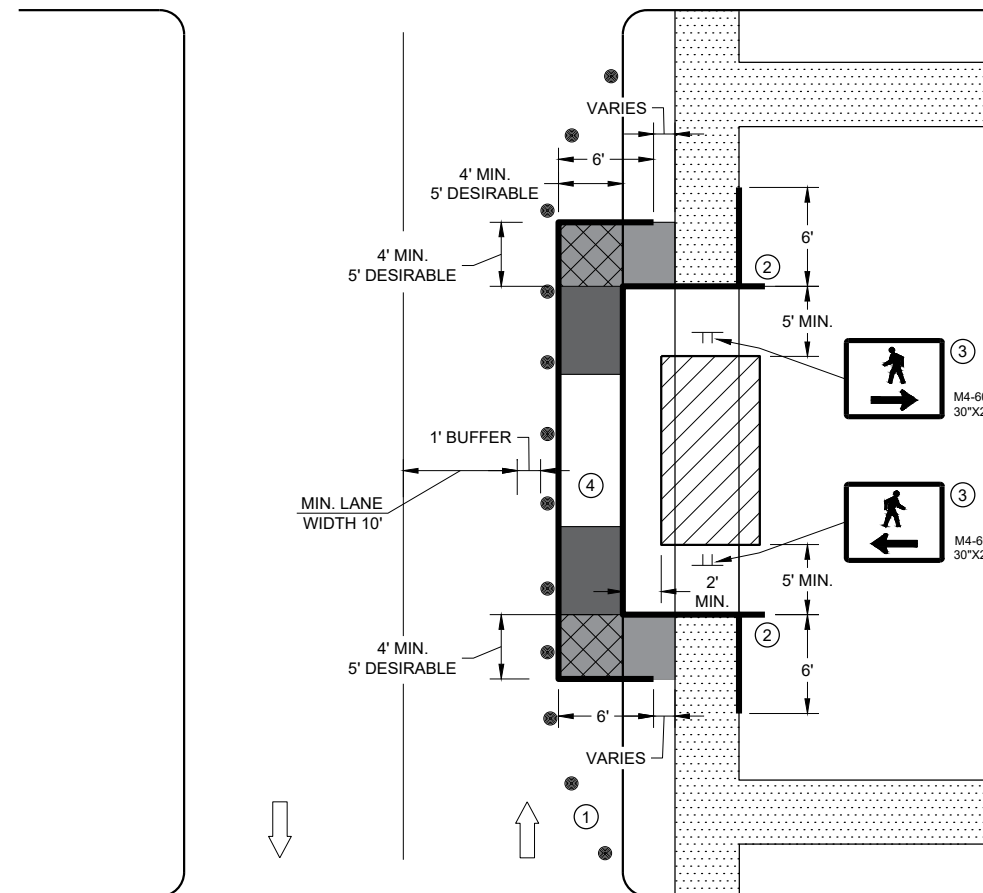
**SIDEWALK DIVERSION
SINGLE SIDE**

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

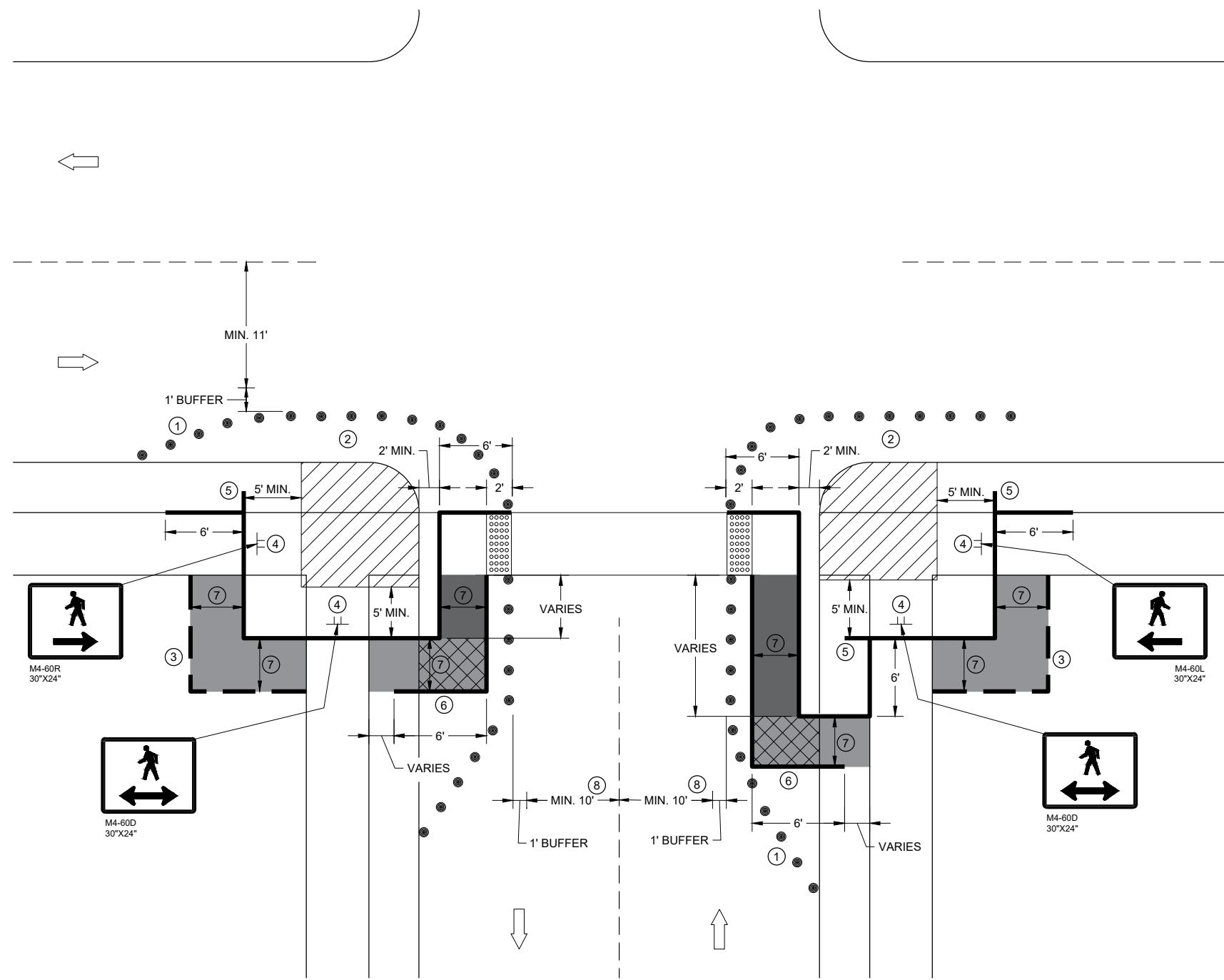
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

6

SDD 15D30 - 07i

SDD 15D30 - 07i

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

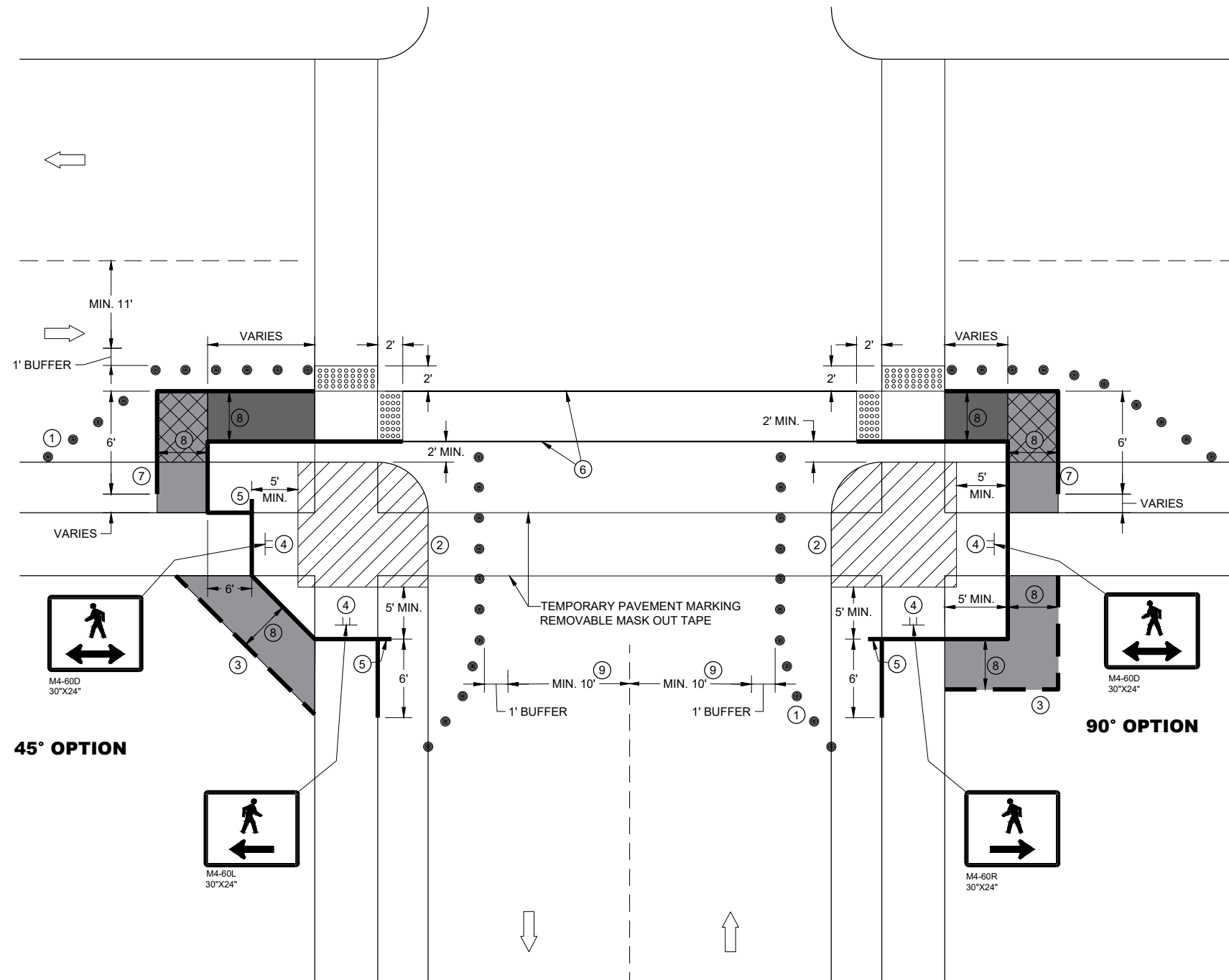
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



CURB RAMP PEDESTRIAN TRAFFIC CONTROL

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

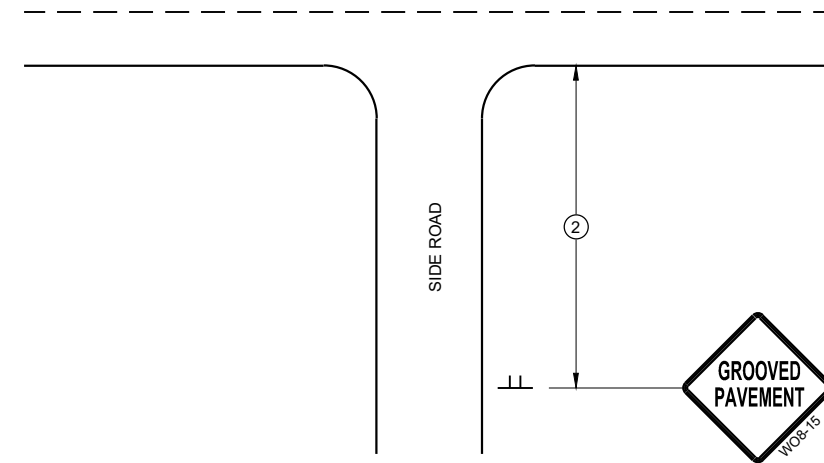
SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

- ① PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

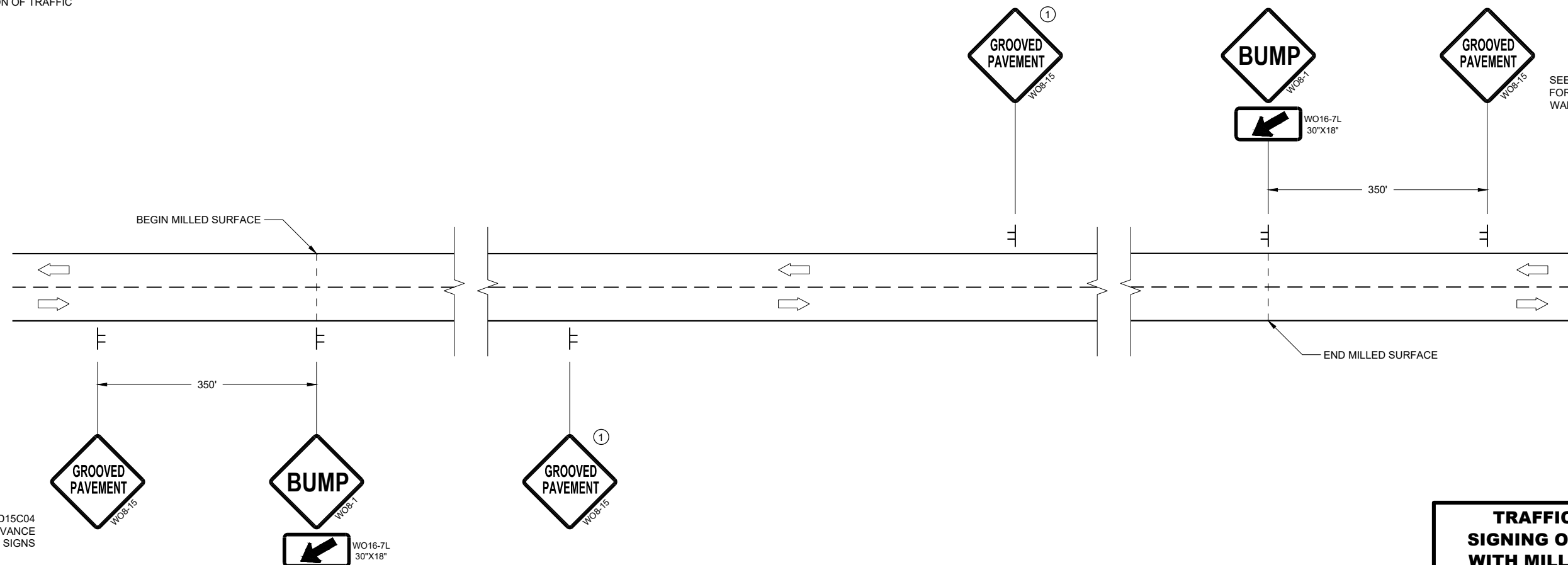
LEGEND

⊥ SIGN ON TEMPORARY SUPPORT

⇨ DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



SEE SDD15C04 FOR ADVANCE WARNING SIGNS

SEE SDD15C04 FOR ADVANCE WARNING SIGNS

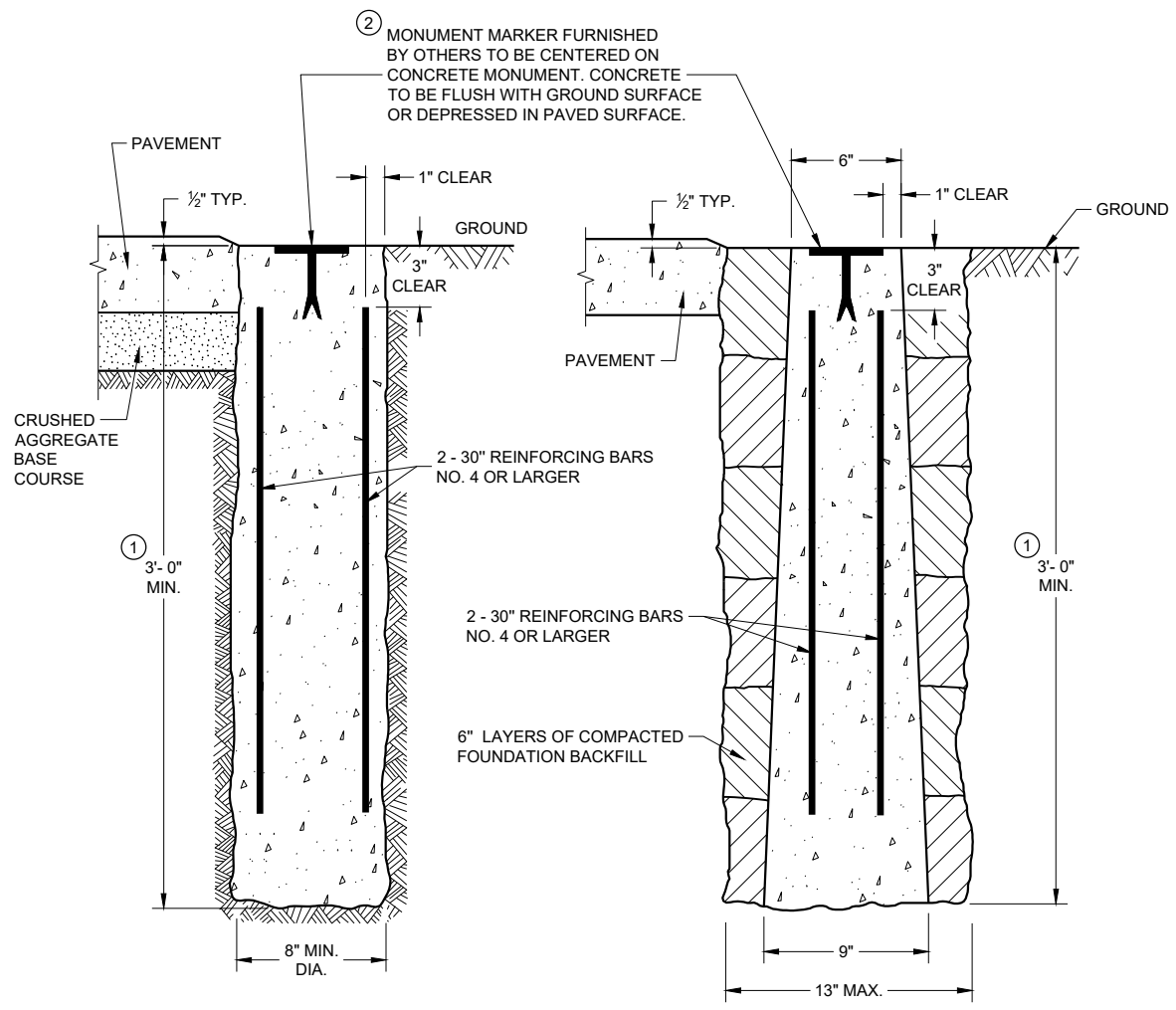
DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

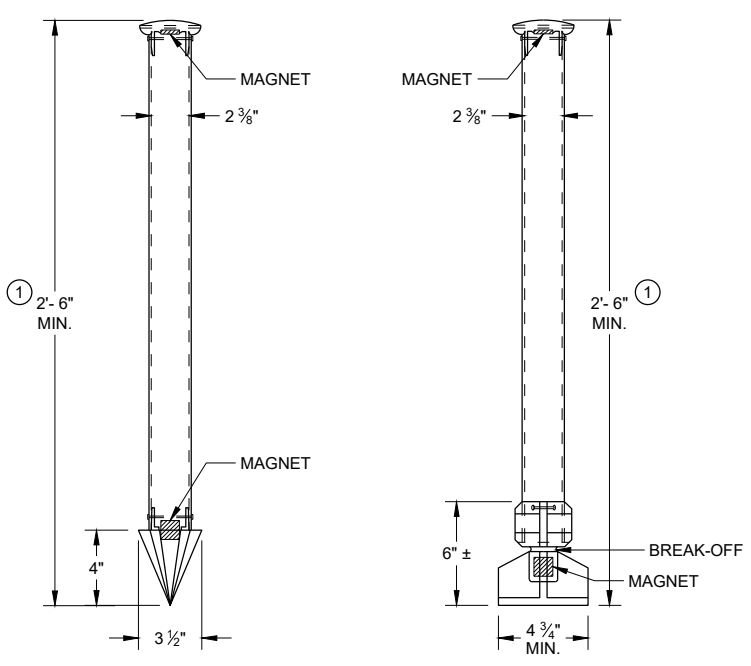
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



**CAST-IN-PLACE
CONCRETE MONUMENTS
TYPE A**



**TYPE C
DRIVEN MONUMENT**
**TYPE D
NON-DRIVEN
BREAK-OFF MONUMENT**
**ALUMINUM MONUMENTS
(INCLUDES MARKER)**

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAILED DRAWINGS OF PROPOSED ALTERNATE DESIGNS FOR METAL MONUMENTS OR MONUMENT COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

PERMANENT MAGNETS SHALL BE INSERTED NEAR THE TOP AND BOTTOM OF ALL ALUMINUM MONUMENTS SO THE MONUMENT CAN EASILY BE DETECTED BY A METAL DETECTOR.

THE CAST IRON MONUMENT COVER SHALL BE A "NON-ROCKING" TYPE. ADJUSTMENT OF THE COVER TO GRADE MAY BE ACCOMPLISHED BY THE USE OF MORTAR AND BRICK, OR BY EITHER PRECAST OR CAST-IN-PLACE REINFORCED CONCRETE GRADE RINGS.

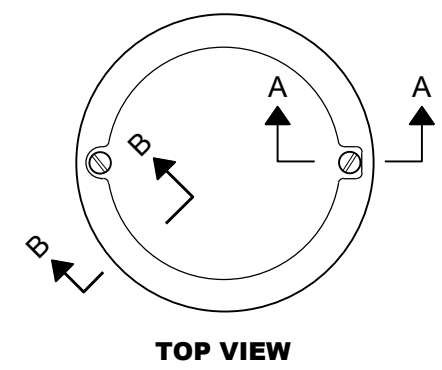
MONUMENTS SHALL BE LOCATED AND PLACED AT THE DIRECTION OF THE ENGINEER.

ALUMINUM MONUMENTS AND MONUMENT COVERS SHALL BE MADE FROM AN ALUMINUM AND MAGNESIUM ALLOY AS DETERMINED BY THE MANUFACTURER.

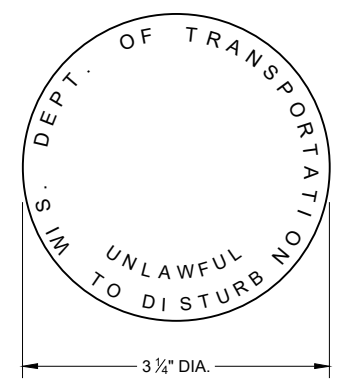
THE MONUMENT COVERS DETAILED ON THIS DRAWING ARE NOT EQUAL ALTERNATES. MONUMENT COVERS SHALL BE CAST IRON UNLESS ALUMINUM IS SPECIFIED ELSEWHERE IN THE CONTRACT.

MONUMENT SHALL BE CAST-IN-PLACE CONCRETE UNLESS PRECAST CONCRETE OR ALUMINUM MONUMENTS ARE SPECIFIED IN THE CONTRACT OR PERMITTED BY THE ENGINEER.

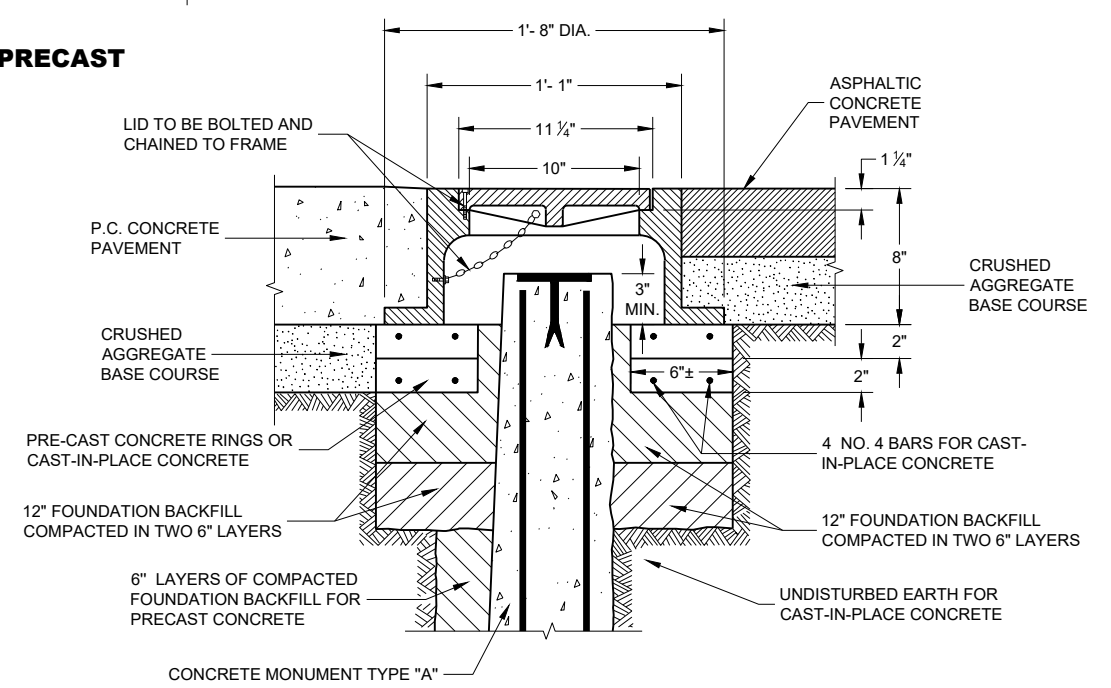
- ① MINIMUM LENGTH SHALL BE 4'-0" FOR MONUMENTS INSTALLED IN PAVED AREAS.
- ② AN OFFICIAL COUNTY MONUMENT MARKER SUPPLIED BY A COUNTY MAY BE REQUIRED FOR SOME SECTION CORNERS AND WITNESS MONUMENTS INSTEAD OF THIS WISDOT MARKER.



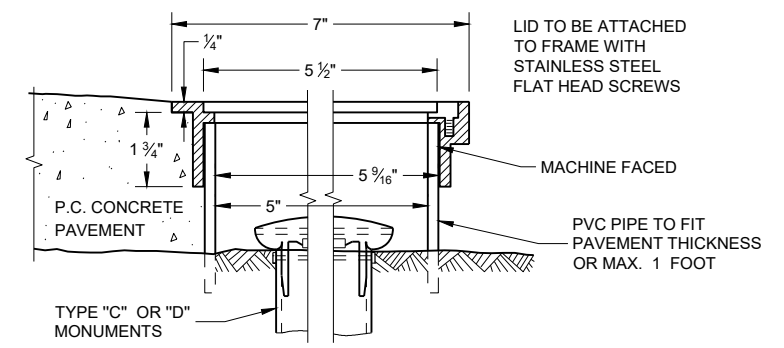
TOP VIEW



② **WIS DOT MONUMENT MARKER LOGO**
FOR TYPES "A", "C" & "D"



CAST IRON MONUMENT COVER
(APPROXIMATE WEIGHT 95 LBS)



SECTION B-B SECTION A-A
ALUMINUM MONUMENT COVER
(APPROXIMATE WEIGHT 2 LBS)
(FOR CONCRETE PAVEMENT ONLY)

LANDMARK REFERENCE MONUMENTS AND COVERS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED March 2018 DATE	/s/ Raymond A. Kumapayil CHIEF SURVEYING AND MAPPING ENGINEER
FHWA	

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>