WIS MARCH 2023

PROJECT ID: 7397-01-70

WITH: N/A

COUNTY: WOOD

30

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details (Includes Erosion Control Plans)
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 40

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

T ROCK, LYNN LINE ROAD

EAST FORK BLACK RIVER BRIDGE

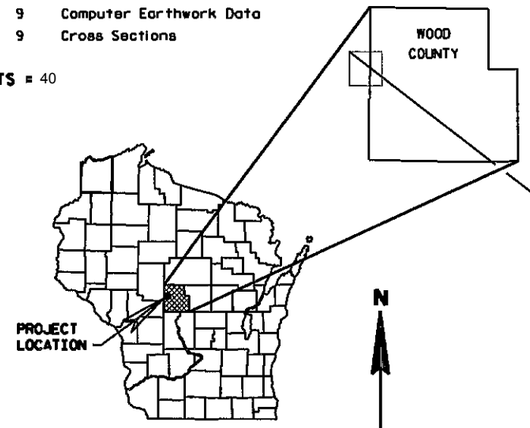
LOC STR

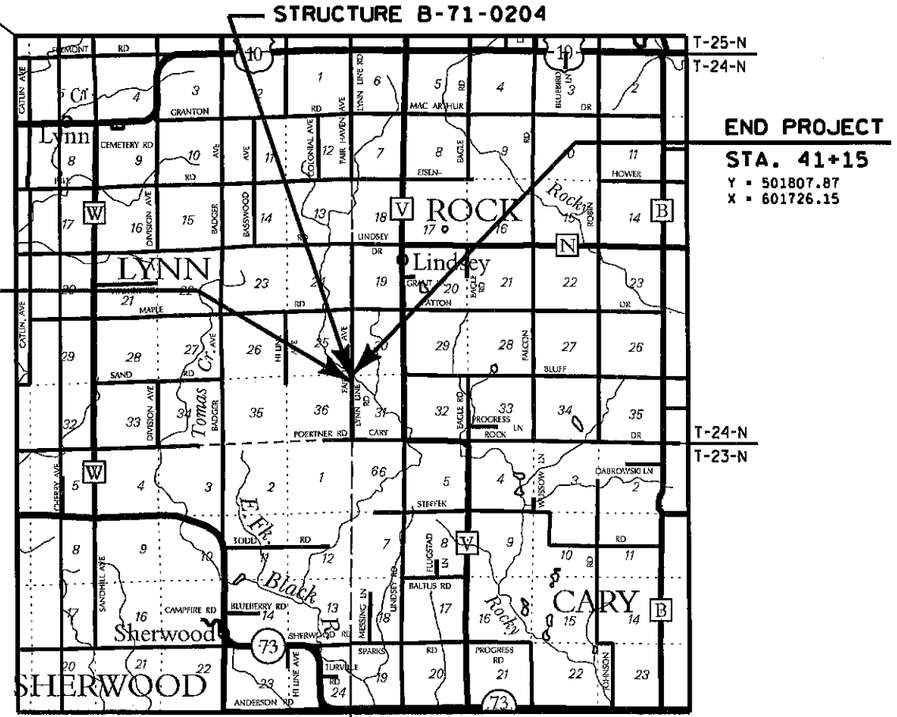
WOOD COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7397-01-70	WISC 2023293	1

STATE PROJECT NUMBER

7397-01-70





LAYOUT

SCALE 0 1 MI.

DESIGN DESIGNATION

A.A.D.T. (2023)	=	<100
A.A.D.T. (2043)	=	<100
D.H.V.	=	10
D.	=	50/50
T.	=	5%
DESIGN SPEED	=	20 MPH
ESALS	=	N/A

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE (PL + 58.)
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- HIGH VOLTAGE (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- OVERHEAD (OH)
- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

TOTAL NET LENGTH OF CENTERLINE = 0.057 MI.

SURVEY PERFORMED IN 2021.
COORDINATES ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), WOOD COUNTY.

ACCEPTED FOR

County of Wood

10-24-22 Date

Highway Commissioner

ORIGINAL PLANS PREPARED BY

AYRES 3433 Oakwood Hills Parkway Eau Claire, WI 54701 www.AyresAssociates.com

WISCONSIN KAREN L. WALDERA E-39158 TAYLOR WI PROFESSIONAL ENGINEER

DATE 10/24/2022

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	AYRES ASSOCIATES INC
Designer	AYRES ASSOCIATES INC
PROJECT MANAGER	JASON SCHAEFFER, PE
Regional Examiner	N/A
Regional Supervisor	DANIEL ERVA, PE

APPROVED FOR THE DEPARTMENT

DATE: 10/24/2022 (Signature)

E

2

GENERAL NOTES

EROSION CONTROL ITEMS TO BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.

NO TREES AND/OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCLUSIVE OF THE ROADBED, SHALL BE FERTILIZED, SEEDED, AND MULCHED AS DIRECTED BY THE ENGINEER.

WHEN THE QUANTITY OF THE ITEM OF BASE AGGREGATE DENSE IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

TOPSOIL SHALL BE PLACED ON THE SLOPES, TO THE POINT OF INTERCEPT WITH THE ORIGINAL GROUND SHOWN ON THE CROSS SECTIONS.

THE DEPARTMENT OF TRANSPORTATION WILL FURNISH THE CONTRACTOR WITH A MONUMENT TO BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM (NAVD 88).

WETLANDS EXIST IN THE PROJECT AREA. NO DISTURBANCE IS ALLOWED OUTSIDE THE SLOPE INTERCEPTS.

UTILITIES

CLARK ELECTRIC COOPERATIVE
124 NORTH MAIN STREET
PO BOX 190
GREENWOOD, WI 54437
ATTN: JOSH BURNS
715-268-6188
jburns@cecoop.com

** DENOTES UTILITIES THAT ARE NOT DIGGERS HOTLINE MEMBERS

DIGGERS HOTLINE
Dial 811 or (800) 242-8511
www.DiggersHotline.com

WISCONSIN DEPARTMENT OF NATURAL RESOURCES CONTACT:

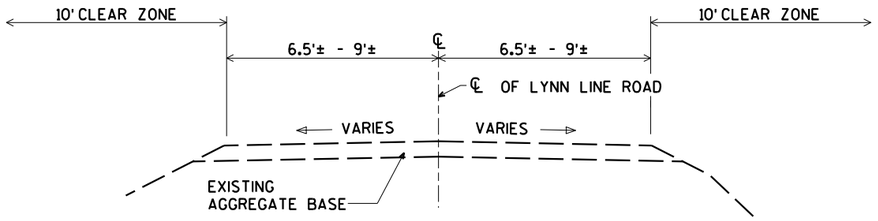
BRADLEY BETTHAUSER
910 HWY 54 E
BLACK RIVER FALLS, WI 54615
715-213-9064
bradley.betthauser@wisconsin.gov

DESIGNER

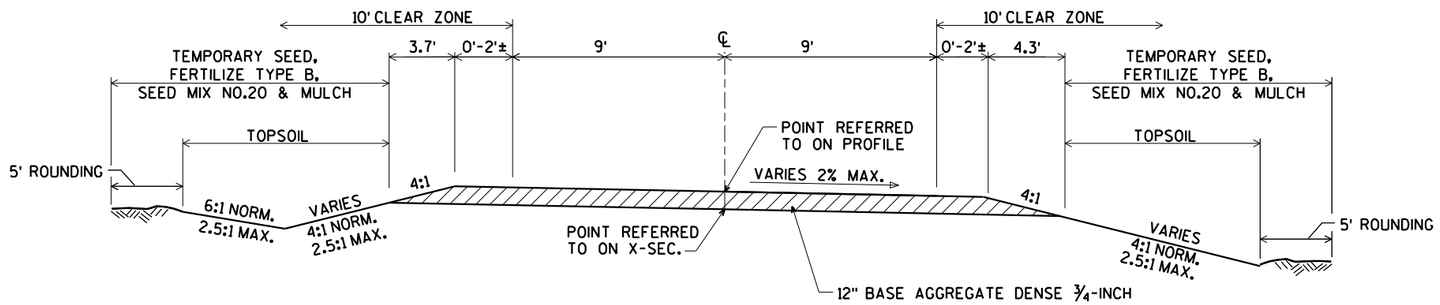
AYRES ASSOCIATES
3433 OAKWOOD HILLS PARKWAY
EAU CLAIRE, WI 54701
ATTN: KAREN WALDERA
715-834-3161
walderak@AyresAssociates.com

COUNTY CONTACT:

WOOD COUNTY, HIGHWAY COMMISSIONER
555 17TH AVENUE NORTH
WISCONSIN RAPIDS, WI 54495-1966
ATTN: ROLAND HAWK
715-421-8875
rhawk@co.wood.wi.us

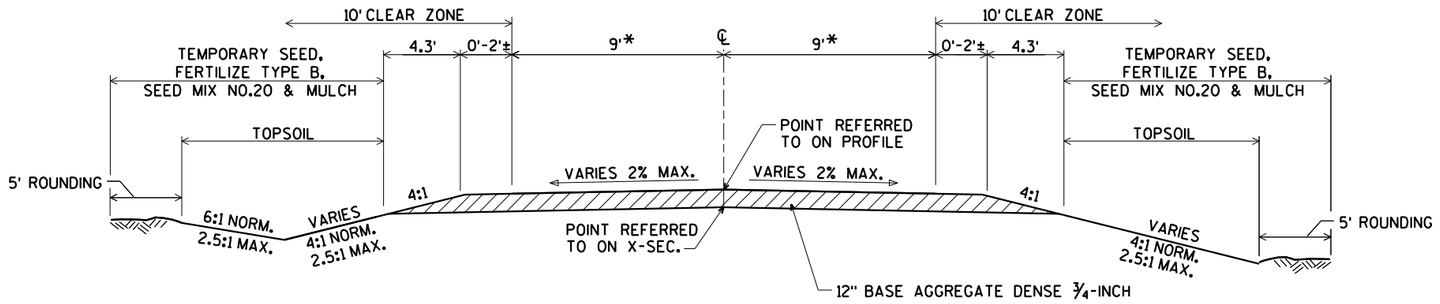


EXISTING TYPICAL SECTION



FINISHED TYPICAL SECTION

STA. 38+15 - STA. 39+58.75



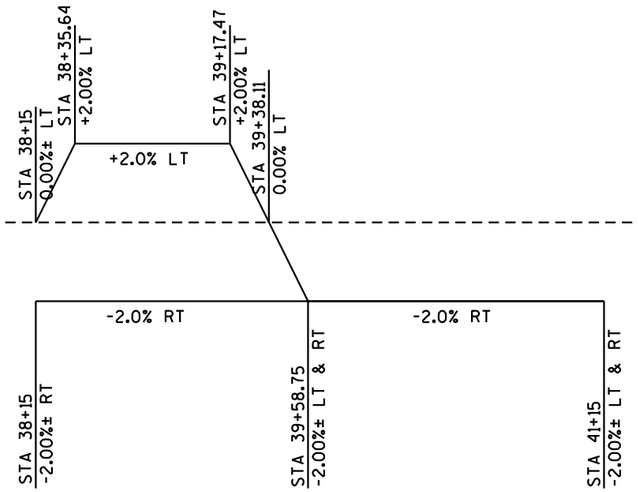
FINISHED TYPICAL SECTION

STA. 39+58.75- STA. 39+42.75
STA. 40+25.25 - STA. 41+15

* THE BASE AGGREGATE LANE WIDTH WILL TAPER FROM 13.25' AT THE END OF THE STRUCTURE TO 9'± AT 50' FROM END OF DECK OF BRIDGE

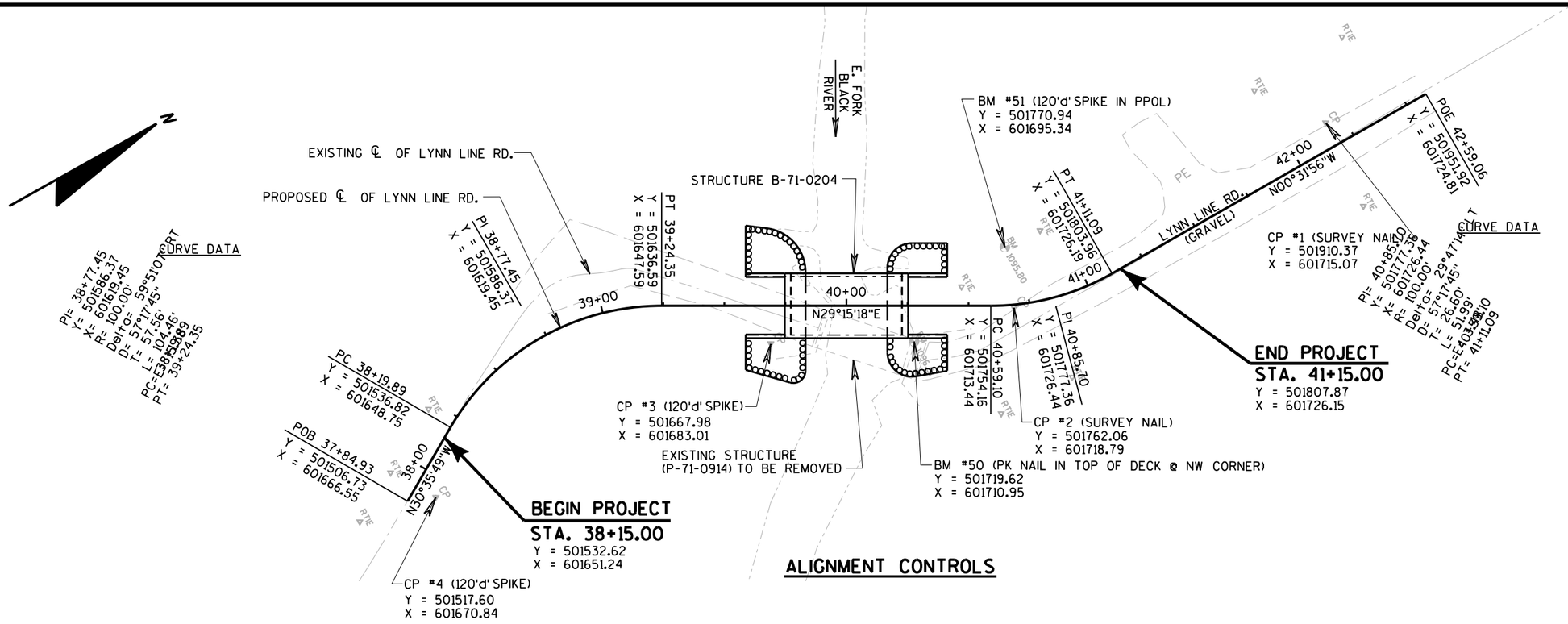
PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD	TYPICAL SECTIONS	SHEET 3	E

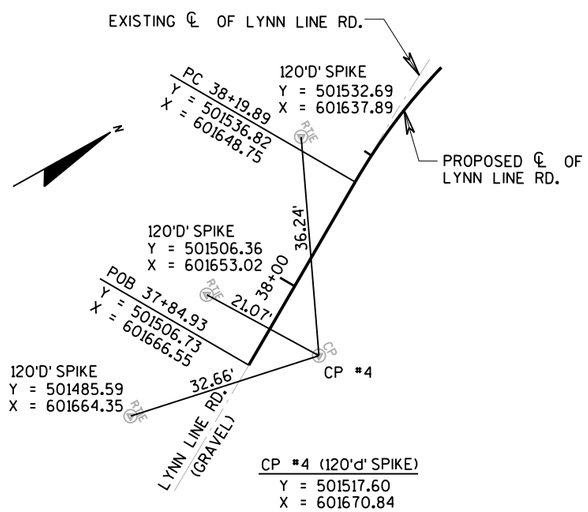
SUPERELEVATION:

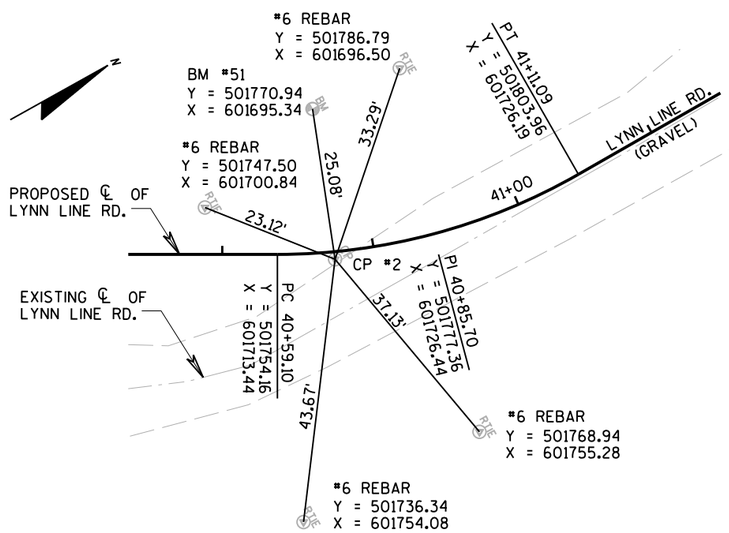


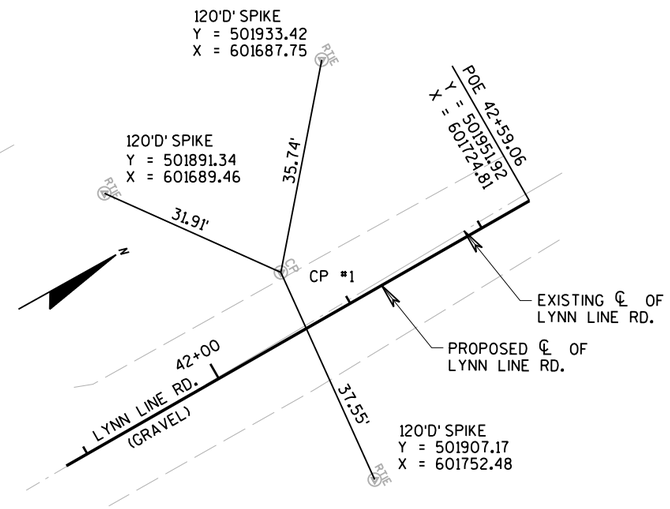
SUPERELEVATION DIAGRAM

PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD	CONSTRUCTION DETAILS	SHEET 4	E



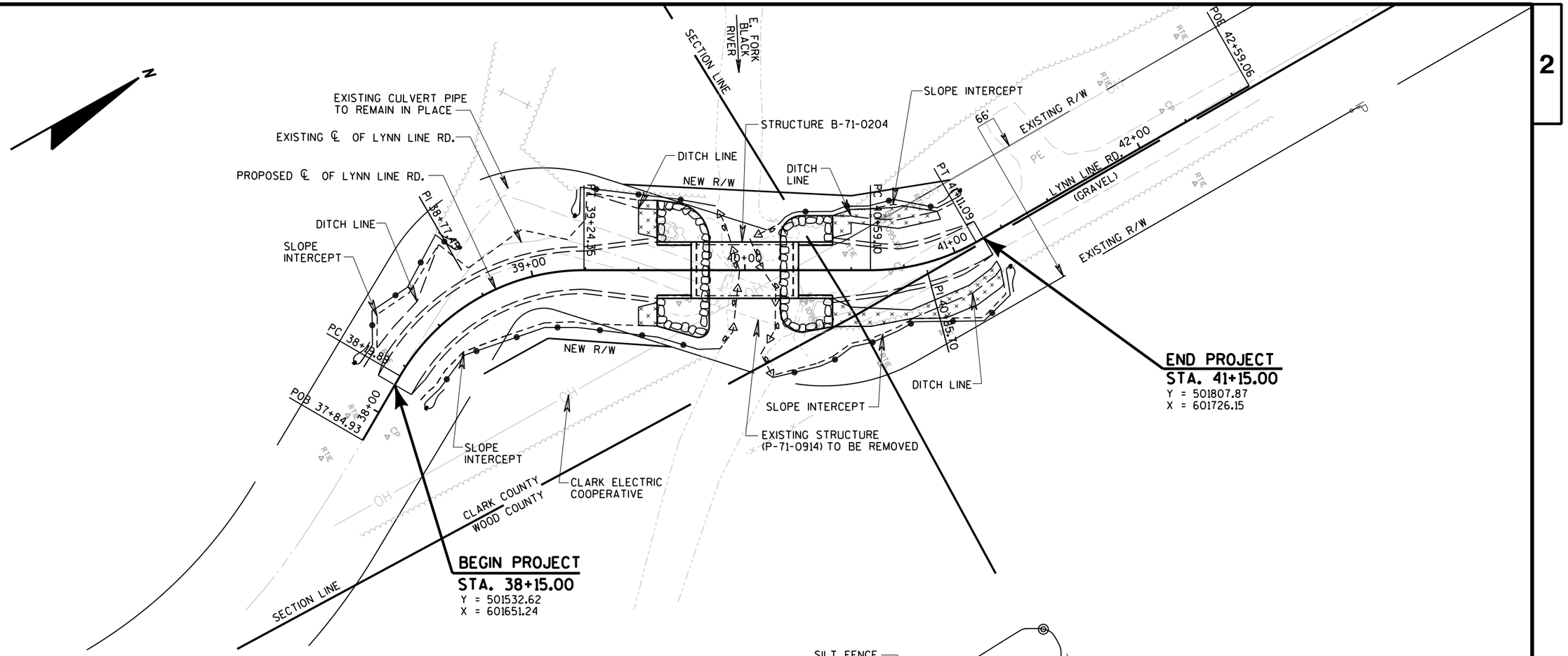






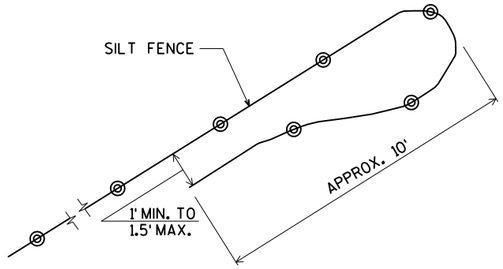
ALIGNMENT TIES

PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD	ALIGNMENT CONTROLS	SHEET 5	E



	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 0.556 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.318 ACRES



SILT FENCE END DETAIL

(TURNAROUNDS - TO REDIRECT AMPHIBIANS AND REPTILES AWAY FROM CONSTRUCTION ZONE)

HIGH WATER$_2$ EL. 1093.72

NOTE: NO DISTURBANCE OR TOPSOIL STOCKPILING IS ALLOWED OUTSIDE OF THE SLOPE INTERCEPTS. WETLANDS EXIST IN THE PROJECT AREA.

LEGEND

- EROSION MAT CLASS II TYPE C
- SILT FENCE
- TURBIDITY BARRIER
- RIPRAP HEAVY

PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD	EROSION CONTROL	SCALE, FEET 0 25 50	SHEET 6	E

7397-01-70

Line	Item	Item Description	Unit	Total	Qty
0002	201.0205	Grubbing	STA	3.000	3.000
0004	203.0250	Removing Structure Over Waterway Remove Debris (structure) 01. P-71-0914	EACH	1.000	1.000
0006	205.0100	Excavation Common	CY	243.000	243.000
0008	206.1001	Excavation for Structures Bridges (structure) 01. B-71-0204	EACH	1.000	1.000
0010	210.1500	Backfill Structure Type A	TON	390.000	390.000
0012	213.0100	Finishing Roadway (project) 01. 7397-01-70	EACH	1.000	1.000
0014	305.0110	Base Aggregate Dense 3/4-Inch	TON	465.000	465.000
0016	502.0100	Concrete Masonry Bridges	CY	195.000	195.000
0018	502.3200	Protective Surface Treatment	SY	225.000	225.000
0020	505.0400	Bar Steel Reinforcement HS Structures	LB	3,660.000	3,660.000
0022	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	25,880.000	25,880.000
0024	506.0105	Structural Steel Carbon	LB	462.000	462.000
0026	513.4061	Railing Tubular Type M	LF	169.400	169.400
0028	516.0500	Rubberized Membrane Waterproofing	SY	16.000	16.000
0030	550.0020	Pre-Boring Rock or Consolidated Materials	LF	48.000	48.000
0032	550.0500	Pile Points	EACH	6.000	6.000
0034	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	150.000	150.000
0036	606.0300	Riprap Heavy	CY	155.000	155.000
0038	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	140.000	140.000
0040	618.0100	Maintenance And Repair of Haul Roads (project) 01. 7397-01-70	EACH	1.000	1.000
0042	619.1000	Mobilization	EACH	1.000	1.000
0044	623.0200	Dust Control Surface Treatment	SY	650.000	650.000
0046	624.0100	Water	MGAL	5.000	5.000
0048	625.0100	Topsoil	SY	720.000	720.000
0050	627.0200	Mulching	SY	1,090.000	1,090.000
0052	628.1504	Silt Fence	LF	755.000	755.000
0054	628.1520	Silt Fence Maintenance	LF	1,510.000	1,510.000
0056	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0058	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	4.000
0060	628.2027	Erosion Mat Class II Type C	SY	205.000	205.000
0062	628.6005	Turbidity Barriers	SY	170.000	170.000
0064	629.0210	Fertilizer Type B	CWT	1.000	1.000
0066	630.0120	Seeding Mixture No. 20	LB	37.000	37.000
0068	630.0200	Seeding Temporary	LB	37.000	37.000
0070	630.0500	Seed Water	MGAL	29.000	29.000
0072	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	4.000	4.000
0074	637.2230	Signs Type II Reflective F	SF	12.000	12.000
0076	638.2602	Removing Signs Type II	EACH	3.000	3.000
0078	638.3000	Removing Small Sign Supports	EACH	3.000	3.000
0080	642.5001	Field Office Type B	EACH	1.000	1.000
0082	643.0420	Traffic Control Barricades Type III	DAY	1,260.000	1,260.000
0084	643.0705	Traffic Control Warning Lights Type A	DAY	1,960.000	1,960.000
0086	643.0900	Traffic Control Signs	DAY	980.000	980.000
0088	643.5000	Traffic Control	EACH	1.000	1.000
0090	645.0111	Geotextile Type DF Schedule A	SY	90.000	90.000
0092	645.0120	Geotextile Type HR	SY	295.000	295.000
0094	650.4500	Construction Staking Subgrade	LF	250.000	250.000
0096	650.6501	Construction Staking Structure Layout (structure) 01. B-71-0204	EACH	1.000	1.000
0098	650.9911	Construction Staking Supplemental Control (project) 01. 7397-01-70	EACH	1.000	1.000

7397-01-70

Line	Item	Item Description	Unit	Total	Qty
0100	650.9920	Construction Staking Slope Stakes	LF	250.000	250.000
0102	715.0502	Incentive Strength Concrete Structures	DOL	1,170.000	1,170.000
0104	999.2005.S	Maintaining Bird Deterrent System (station) 01. 40+00	EACH	1.000	1.000
0106	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	300.000	300.000
0108	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	600.000	600.000
0110	SPV.0195	Special 01. Select Crushed Material for Riprap Voids	TON	45.000	45.000

GRUBBING

CATEGORY	STATION	TO	STATION	LOCATION	201.0205 GRUBBING STA	REMARKS
0010	38+15	-	41+15	LT	3	
				TOTAL 0010	3	

NOTE: CLEARING TO BE DONE PRIOR TO CONSTRUCTION BY OTHERS.

LYNN LINE ROAD EARTHWORK SUMMARY

From/To Station	Location	Common Excavation (1) (Item 205.0100) Cut	Unexpanded Fill	Expanded Fill (2) Factor 1.30	Mass Ordinate +/- (3)	Waste	Borrow (Item 208.0100)	Comment:
38+15 to 41+15	LYNN LINE ROAD	243	163	212	31	31	0	
		TOTAL 243					**0**	

1) Common Excavation is the Cut. Item number 205.0100.

2) Expanded Fill. Factor = 1.30; Expanded Fill = Unexpanded Fill * Fill Factor

3) The Mass Ordinate + or - Qty calculated for the Division. Plus quantity indicates an excess of material on the project.

4) All quantities shown in CY.

BASE AGGREGATE

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	624.0100 WATER MGAL	REMARKS
0010	38+15	-	39+74.75	LT/RT	295	3	SOUTH APPROACH
0010	40+25.25	-	41+15	LT/RT	170	2	NORTH APPROACH
				TOTAL 0010	465	5	

EXTRA ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	213.0100.01 FINISHING ROADWAY (PROJECT) (01. 7397-01-70) EACH	619.1000 MOBILIZATION EACH	623.0200 DUST CONTROL SURFACE TREATMENT SY	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	642.5001 FIELD OFFICE TYPE B EACH
0010	38+15.00	-	41+15.00	PROJECT-WIDE	1	1	650	4	4	1
				TOTAL 0010	1	1	650	4	4	1

MAINTENANCE AND REPAIR OF HAUL ROADS

CATEGORY	LOCATION	618.0100.01 MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) (01. 7397-01-70) EACH
0030	LYNN LINE ROAD	1
	TOTAL 0030	1

EROSION CONTROL

CATEGORY	STATION	TO	STATION	LOCATION	625.0100 TOPSOIL SY	627.0200 MULCHING SY	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.2027 EROSION MAT CLASS II TYPE C SY	628.6005 TURBIDITY BARRIERS SY	629.0210 FERTILIZER TYPE B CWT	630.0120 SEEDING MIXTURE NO. 20 LB	630.0200 SEEDING TEMPORARY LB	630.0500 SEED WATER MGAL
0010	38+15	-	40+00	LT	230	330	195	390	20	80	0.3	10	10	8
0010	38+15	-	40+00	RT	150	210	165	330	10		0.2	6	6	5
0010	40+00	-	41+15	LT	110	100	100	200	50	55	0.1	4	4	3
0010	40+00	-	41+15	RT	230	230	145	290	85		0.2	9	9	7
0010			UNDISTRIBUTED		-	220	150	300	40	35	0.2	8	8	6
				TOTAL 0010	720	1,090	755	1,510	205	170	1.0	37	37	29

SIGNS

CATEGORY	STATION	LOCATION	634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH	637.2230 SIGNS TYPE II REFLECTIVE F SF	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS
0010	39+57	LT	1	3	-	-	W5-52L: BRIDGE HASH MARKS
0010	39+57	RT	1	3	-	-	W5-52R: BRIDGE HASH MARKS
0010	39+89	RT	-	-	2	2	W5-56: END OF ROADWAY MARKER
0010	40+30	RT	-	-	1	1	R11-2: ROAD CLOSED
0010	40+42	LT	1	3	-	-	W5-52R: BRIDGE HASH MARKS
0010	40+42	RT	1	3	-	-	W5-52L: BRIDGE HASH MARKS
		TOTAL 0010	4	12	3	3	

TRAFFIC CONTROL

CATEGORY	LOCATION	DURATION DAYS	NO.	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	NO.	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	NO.	643.0900 TRAFFIC CONTROL SIGNS DAY	643.5000 TRAFFIC CONTROL EACH
0010	PER SDD 15C2	70	18	1,260	28	1,960	14	980	-
0010	LYNN LINE ROAD	-	-	-	-	-	-	-	1
	TOTAL 0010			1,260		1,960		980	1

STAKING

CATEGORY	STATION	TO	STATION	LOCATION	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.6501.01 CONSTRUCTION STAKING STRUCTURE LAYOUT (STRUCTURE) (01. B-71-0204) EACH	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 7397-01-70) EACH	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF
0010	38+15	-	41+15	MAINLINE	250	-	-	250
0010	38+15	-	41+15	PROJECT 7397-01-70	-	-	1	-
				TOTAL 0010	250	0	1	250
0020	39+74.75	-	40+25.25	B-71-0204	-	1	-	-
				TOTAL 0020	0	1	0	0
				PROJECT TOTAL	250	1	1	250

MAINTAINING BIRD DETERRENT SYSTEM

CATEGORY	LOCATION	999.2005.S MAINTAINING BIRD DETERRENT SYSTEM EACH
0010	40+00	1
	TOTAL 0010	1

R/W PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS
7397-01-00		
CONSTRUCTION PROJECT NUMBER 7397-01-70	4.01	1

PLAT OF RIGHT OF WAY REQUIRED FOR

TOWN OF ROCK , LYNN LINE ROAD

EAST FORK BLACK RIVER BRIDGE B-71-0204

LOCAL STREET	WOOD COUNTY

CONVENTIONAL SYMBOLS

SECTION LINE
QUARTER LINE
SIXTEENTH LINE
NEW REFERENCE LINE
NEW R/W LINE
EXISTING R/W LINE
PROPERTY LINE
LOT, TIE, AND OTHER MINOR LINES
SLOPE INTERCEPT
CORPORATE LIMITS
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)
FEE ACQUISITION AREA (HATCHING VARIES BY OWNER)
TEMP. LIMITED EASEMENT AREA
EASEMENT AREA (HIGHWAY, PERMANENT LIMITED, OR RESTRICTED DEVELOPMENT)
TRANSMISSION STRUCTURES
BUILDING
BUILDING (TO BE REMOVED)
BRIDGE

PARCEL NUMBER (25)
PRW POINT NUMBER (100)
SECTION CORNER
NOTATION FOR COMBUSTABLE FLUIDS
NOTATION FOR HIGH VOLTAGE TRANSMISSION LINES
UTILITY NUMBER (40)
TLE POINT NUMBER (100)
R/W MONUMENT
NON-MONUMENTED R/W POINT
FOUND IRON PIN
VALVE (GAS, WATER, ETC.)
SIGN
OFF-PREMISE SIGN
ACCESS CONTROLLED BY ACQUISITION
NO ACCESS (BY STATUTORY AUTHORITY)
ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)
NO ACCESS (NEW HIGHWAY)
NATIONAL GEODETIC SURVEY MONUMENT
SIXTEENTH CORNER MONUMENT
PARALLEL OFFSETS

CONVENTIONAL UTILITY SYMBOLS

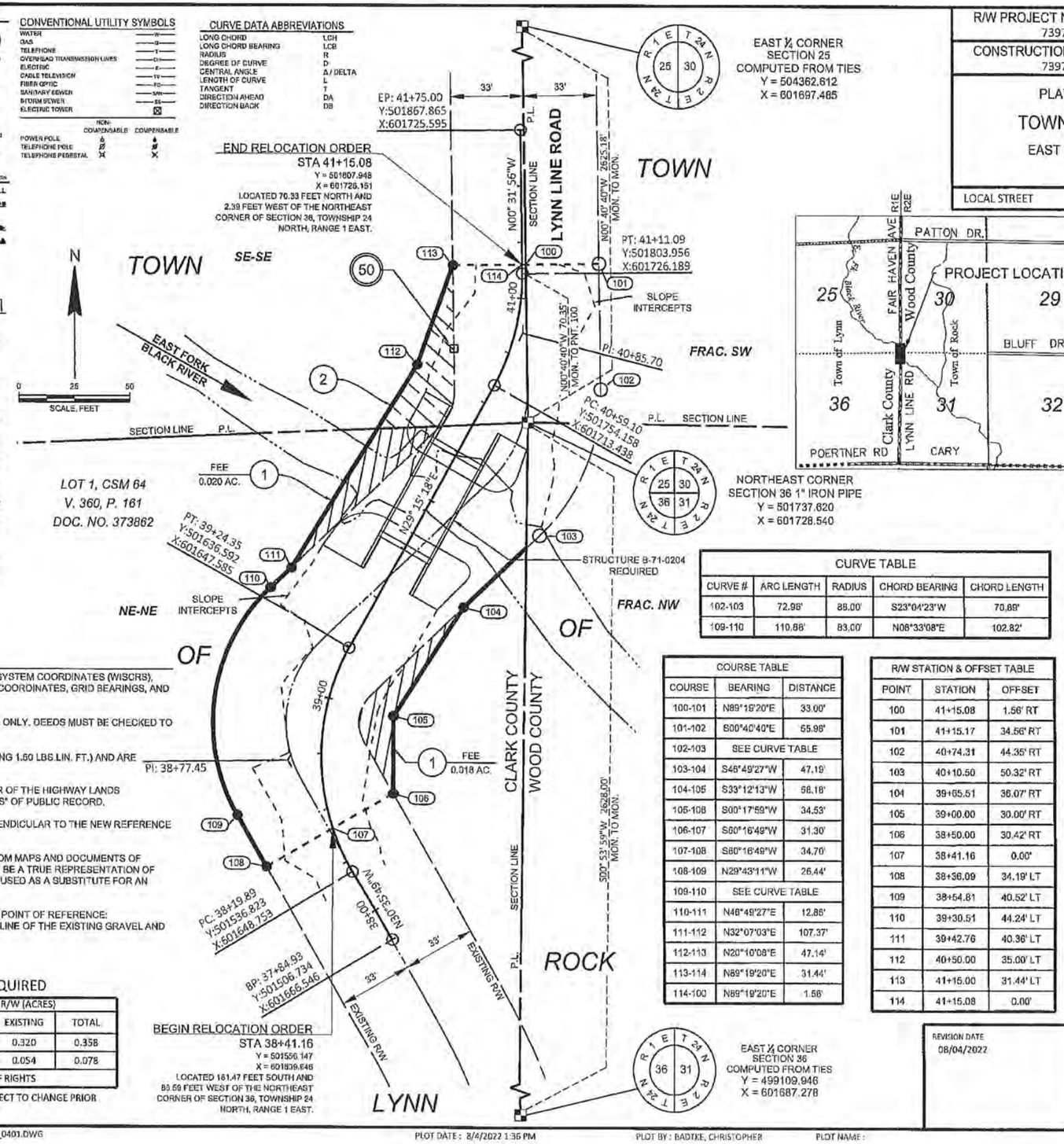
	NON-COMPENSABLE	COMPENSABLE
WATER	W	
GAS	G	
TELEPHONE	T	
OVERHEAD TRANSMISSION LINES	OH	
ELECTRIC	E	
CABLE TELEVISION	TV	
FIBER OPTIC	FO	
SANITARY SEWER	SAN	
STORM SEWER	SS	
ELECTRIC TOWER		
POWER POLE		
TELEPHONE POLE		
TELEPHONE PEDESTAL		

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	OUTLOT	OL
ACRES	AC	PAGE	P
AHEAD	AH	POINT OF TANGENCY	PT
ALUMINUM	ALUM	PROPERTY LINE	PL
AND OTHERS	ET AL	RECORDED AS	(100)
BACK	BK	REEL / IMAGE	R/I
BLOCK	BLK	REFERENCE LINE	R/L
CENTERLINE	CL	PERMANENT LIMITED EASEMENT	PLE
CERTIFIED SURVEY MAP	CSM	POINT OF BEGINNING	POB
CONCRETE	CONC	POINT OF CURVATURE	PC
COUNTY	CO	POINT OF COMPOUND CURVE	PCC
COUNTY TRUNK HIGHWAY	CTH	POINT OF INTERSECTION	PI
DISTANCE	DIST	REMAINING	REM
CORNER	COR	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
DOCUMENT NUMBER	DOC	RIGHT	RT
EASEMENT	EASE	RIGHT OF WAY	R/W
EXISTING	EX	SECTION	SEC
GAS VALVE	GV	SEPTIC VENT	SEPV
GRID NORTH	GN	SQUARE FEET	SF
HIGHWAY EASEMENT	HE	STATE TRUNK HIGHWAY	STH
IDENTIFICATION	ID	STATION	STA
LAND CONTRACT	LC	TELEPHONE PEDESTAL	TP
LEFT	LT	TEMPORARY LIMITED EASEMENT	TLE
MONUMENT	MON	TRANSPORTATION PROJECT PLAT	TPP
NATIONAL GEODETIC SURVEY	NGS	UNITED STATES HIGHWAY	USH
NUMBER	NO	VOLUME	V

CURVE DATA ABBREVIATIONS

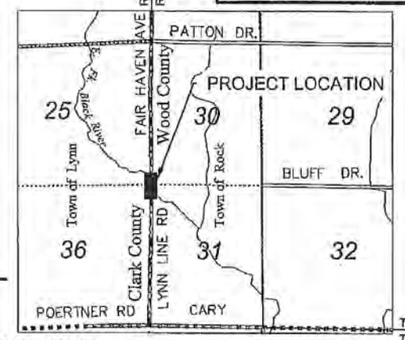
LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ / DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB



EAST ¼ CORNER
SECTION 25
COMPUTED FROM TIES
Y = 504362.612
X = 601697.465

NORTHEAST CORNER
SECTION 36 1" IRON PIPE
Y = 501737.620
X = 601726.540

EAST ¼ CORNER
SECTION 36
COMPUTED FROM TIES
Y = 499109.946
X = 601687.278



ALIGNMENT CURVES

PI STA = 38+77.45	PI STA = 40+85.70
Y = 501586.372	Y = 501777.361
X = 601619.454	X = 601726.436
DELTA = 59°51'07" RT	DELTA = 29°47'14" LT
D = 57°17'45"	D = 57°17'45"
T = 57.56'	T = 26.60'
L = 104.46'	L = 51.99'
R = 100.00'	R = 100.00'
PC STA = 38+19.89	PC STA = 40+59.10
PT STA = 39+24.35	PT STA = 41+11.09

LOCATION SKETCH
SCALE 0 1 MI

TOTAL NET LENGTH OF CENTERLINE = 0.05 MI.

CURVE TABLE

CURVE #	ARC LENGTH	RADIUS	CHORD BEARING	CHORD LENGTH
102-103	72.98'	88.00'	S23°04'23"W	70.88'
109-110	110.86'	83.00'	N08°33'08"E	102.82'

COURSE TABLE

COURSE	BEARING	DISTANCE
100-101	N89°19'20"E	33.00'
101-102	S00°40'40"E	65.98'
102-103	SEE CURVE TABLE	
103-104	S46°49'27"W	47.19'
104-105	S33°12'13"W	56.18'
105-106	S00°17'59"W	34.53'
106-107	S60°16'49"W	31.30'
107-108	S60°16'49"W	34.70'
108-109	N29°43'11"W	26.44'
109-110	SEE CURVE TABLE	
110-111	N46°49'27"E	12.86'
111-112	N32°07'03"E	107.37'
112-113	N20°10'08"E	47.14'
113-114	N89°19'20"E	31.44'
114-100	N89°19'20"E	1.56'

R/W STATION & OFFSET TABLE

POINT	STATION	OFFSET
100	41+15.08	1.56' RT
101	41+15.17	34.56' RT
102	40+74.31	44.35' RT
103	40+10.50	50.32' RT
104	39+65.51	36.07' RT
105	39+00.00	30.00' RT
106	38+50.00	30.42' RT
107	38+41.16	0.00'
108	38+36.09	34.19' LT
109	38+54.81	40.52' LT
110	39+30.51	44.24' LT
111	39+42.76	40.36' LT
112	40+50.00	35.00' LT
113	41+15.00	31.44' LT
114	41+15.08	0.00'

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), WOOD COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

THIS PLAT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSE ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES AND ACCESS RIGHTS

RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (¾"x24" CAPPED IRON REBAR WEIGHING 1.50 LBS./LIN. FT.) AND ARE PLACED PRIOR TO OR AT THE TIME OF LAND TITLE TRANSFER.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

EXISTING ROAD RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINT OF REFERENCE: LYNN LINE ROAD WAS ESTABLISHED FROM STATUTE 82.31 USING THE CENTERLINE OF THE EXISTING GRAVEL AND A PRESUMED 66' WIDTH.

(50) CLARK ELECTRIC COOPERATIVE
V. 181, P. 291, DOC. NO. 264909 - PARCEL 2

SCHEDULE OF LANDS AND INTERESTS REQUIRED

PARCEL NO.	OWNER(S)	INTEREST REQUIRED	R/W (ACRES)		
			NEW	EXISTING	TOTAL
1	JOHN T SOUTHWORTH ET. AL.	FEE	0.038	0.320	0.358
2	DARLENE BARROCAS LOVING TRUST	FEE	0.024	0.054	0.078
50	CLARK ELECTRIC COOPERATIVE	RELEASE OF RIGHTS			

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE COUNTY.

APPROVED FOR TOWN OF ROCK
10-17-22 DATE
TOWN CHAIRMAN

APPROVED FOR TOWN OF LYNN
10-13-2022 DATE
TOWN CHAIRMAN

PLAT PREPARED BY
AYRES

THE SURVEY IS PREPARED AT THE REQUEST OF WOOD COUNTY.

THE FIELD SURVEY WAS PERFORMED IN FEBRUARY 2021.

THIS SURVEY IS ACCURATE TO THE BEST OF MY KNOWLEDGE AND BELIEF.

WISCONSIN
CHRISTOPHER R. BADTKE
S-3150
EAU CLAIRE
WI
LAND SURVEYOR

04/26/2022
CHRISTOPHER R. BADTKE, P.L.S. S-3150 DATE

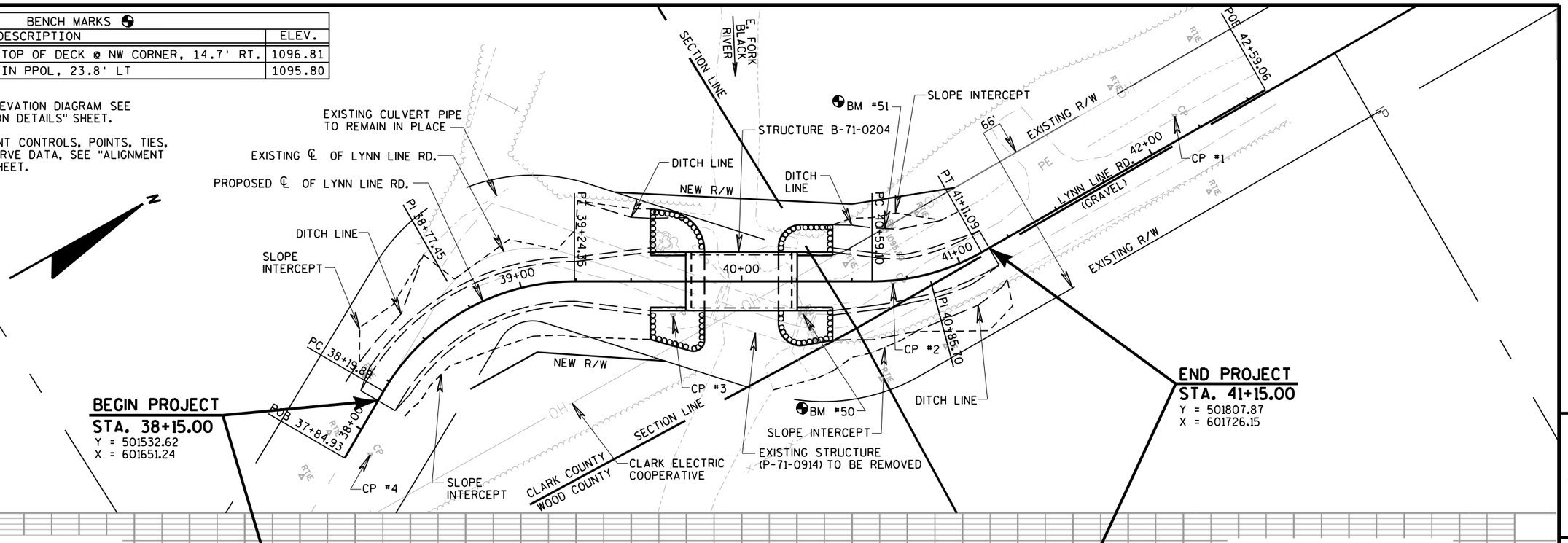
REVISION DATE
08/04/2022

FILE NAME : I:\74\EC\421283.00 WOOD CO, TN ROCK, LYNN LINE RD\CADD\C3D\RW\7397010D_0401.DWG
PLOT DATE : 8/4/2022 1:36 PM
PLOT BY : BADTKE, CHRISTOPHER
PLOT NAME :

BENCH MARKS			
NO.	STA.	DESCRIPTION	ELEV.
50	40+27.8	PK NAIL IN TOP OF DECK @ NW CORNER, 14.7' RT.	1096.81
51	40+66.7	120'D' SPK IN PPOL, 23.8' LT	1095.80

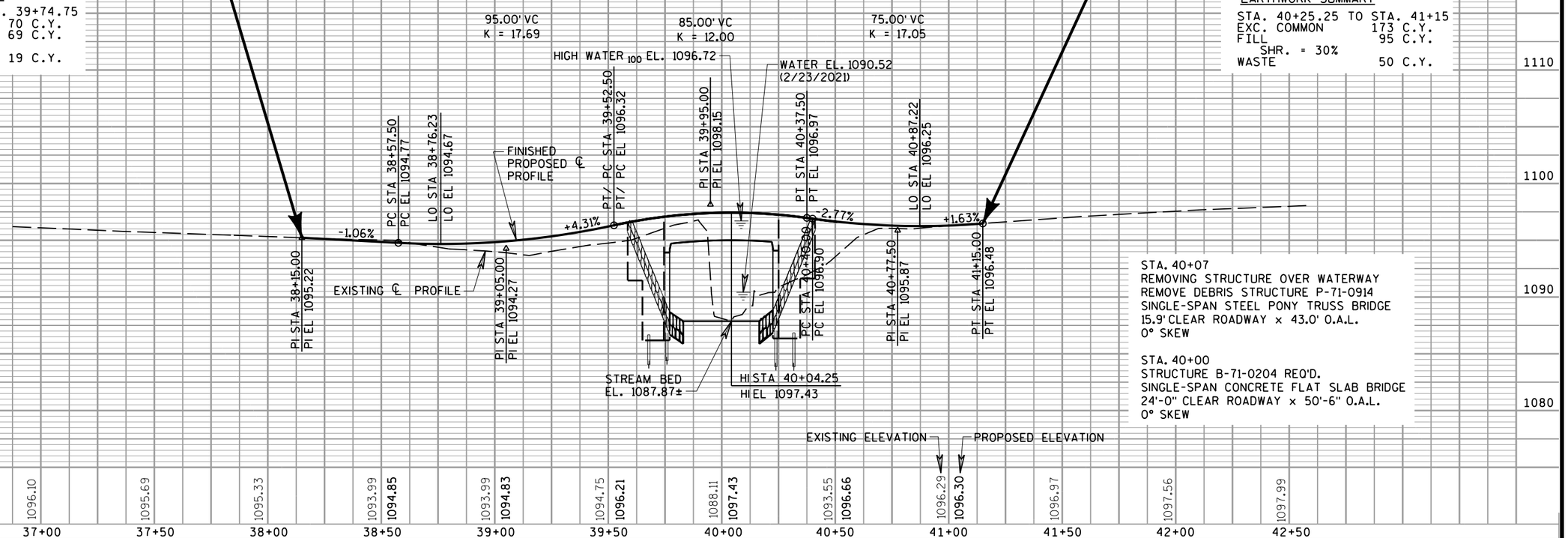
NOTES:
FOR SUPERELEVATION DIAGRAM SEE "CONSTRUCTION DETAILS" SHEET.

FOR ALIGNMENT CONTROLS, POINTS, TIES, BEARINGS, CURVE DATA, SEE "ALIGNMENT CONTROLS" SHEET.



EARTHWORK SUMMARY
STA. 38+15 TO STA. 39+74.75
EXC. COMMON 70 C.Y.
FILL 69 C.Y.
SHR. = 30%
BORROW 19 C.Y.

EARTHWORK SUMMARY
STA. 40+25.25 TO STA. 41+15
EXC. COMMON 173 C.Y.
FILL 95 C.Y.
SHR. = 30%
WASTE 50 C.Y.



STA. 40+07
REMOVING STRUCTURE OVER WATERWAY
REMOVE DEBRIS STRUCTURE P-71-0914
SINGLE-SPAN STEEL PONY TRUSS BRIDGE
15.9' CLEAR ROADWAY x 43.0' O.A.L.
0° SKEW

STA. 40+00
STRUCTURE B-71-0204 REQ'D.
SINGLE-SPAN CONCRETE FLAT SLAB BRIDGE
24'-0" CLEAR ROADWAY x 50'-6" O.A.L.
0° SKEW

EXISTING ELEVATION
PROPOSED ELEVATION

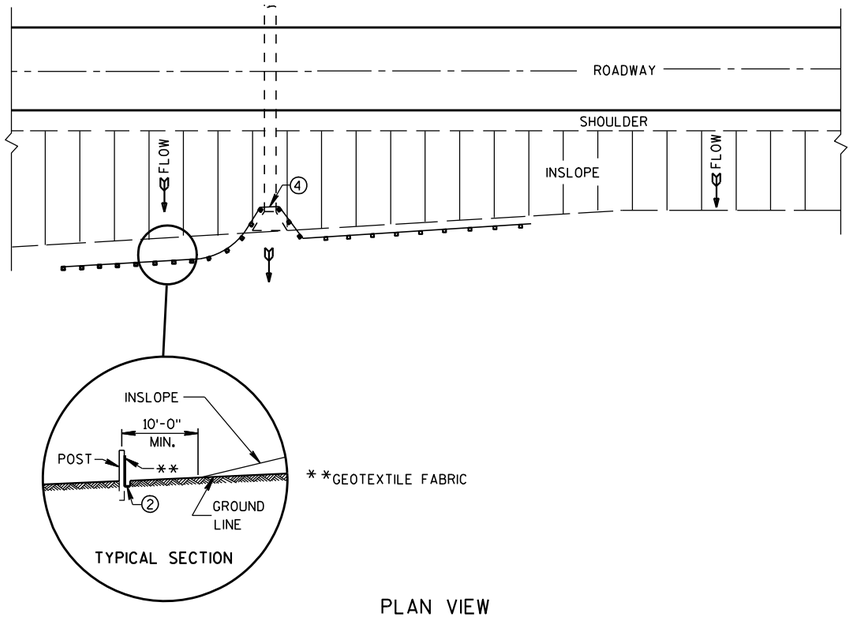
PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD	PLAN AND PROFILE	SCALE, FEET 0 25 50	SHEET 12 E

Standard Detail Drawing List

08E09-06	SILT FENCE
08E11-02	TURBIDITY BARRIER
12A03-10	NAME PLATE (STRUCTURES)
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C06-10	SIGNING & MARKING FOR TWO LANE BRIDGES
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

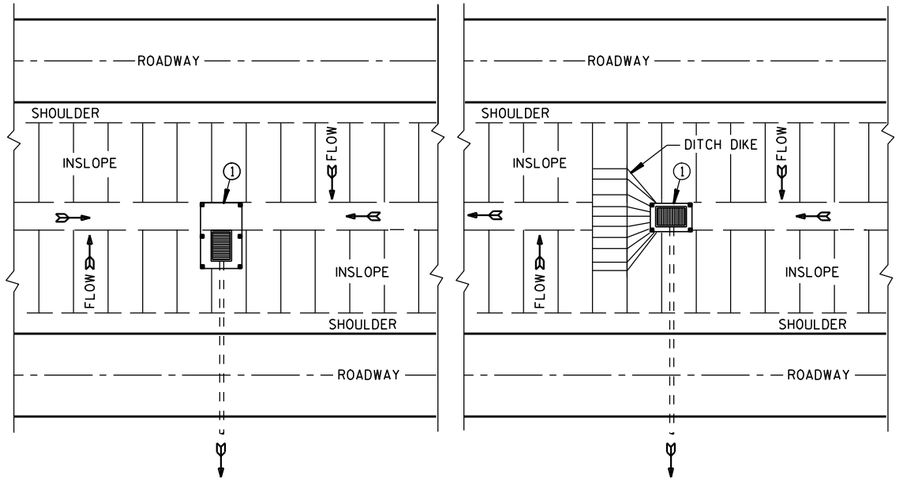
6

6



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



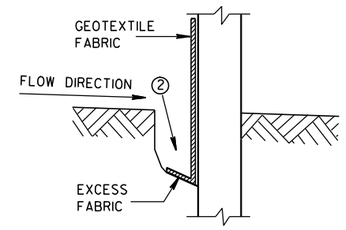
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

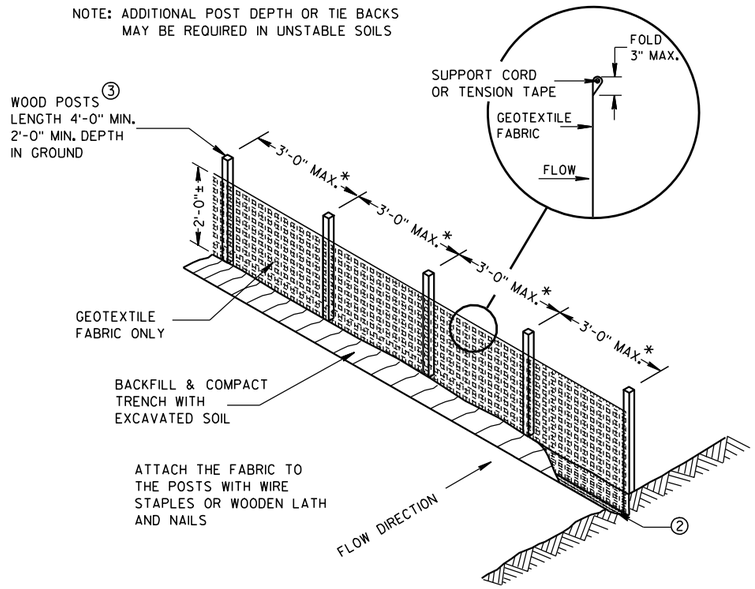
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

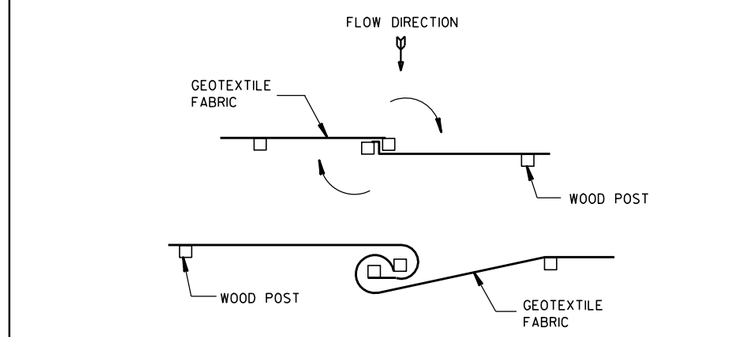


TRENCH DETAIL

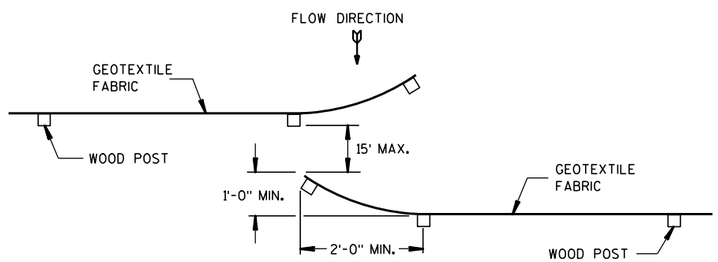


*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

SILT FENCE

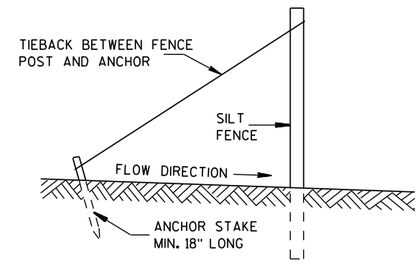


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

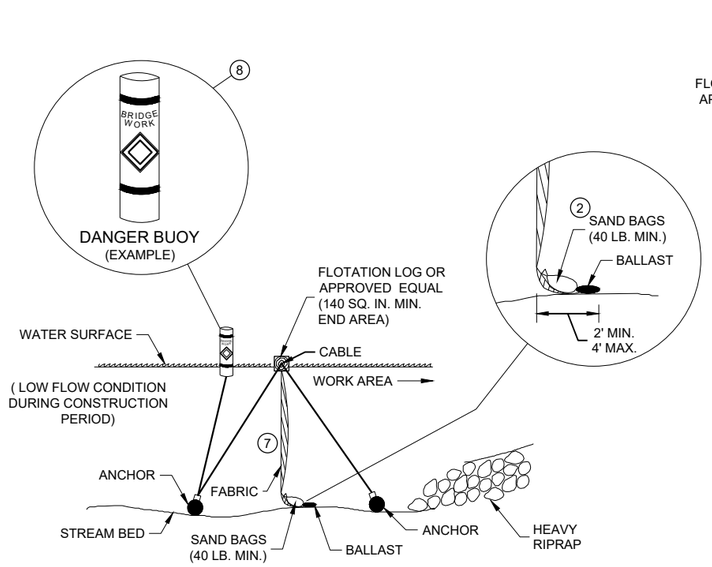
APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

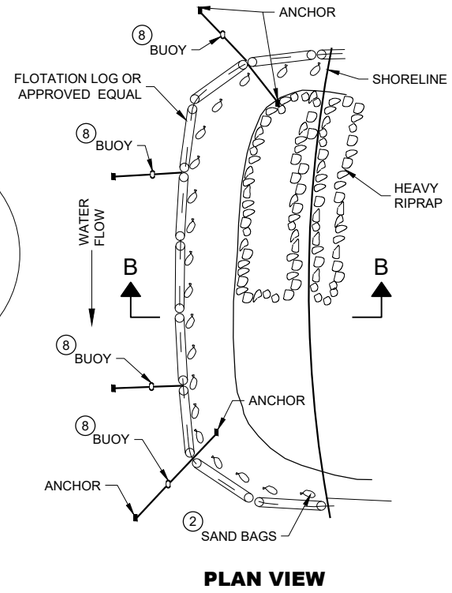
S.D.D. 8 E 9-6

6

S.D.D. 8 E 9-6

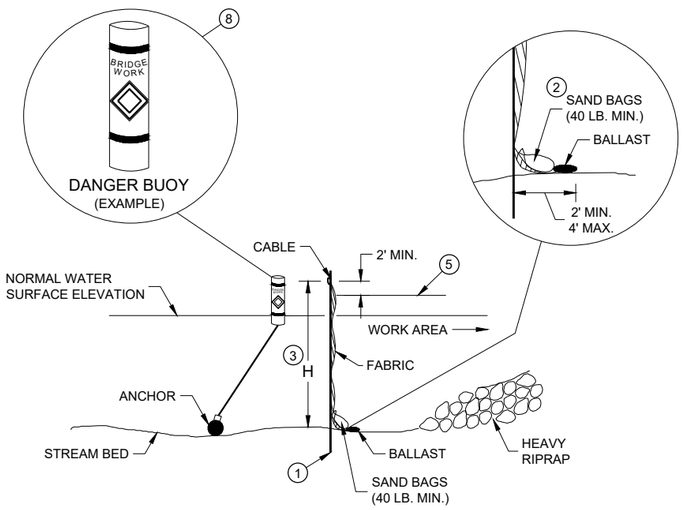


SECTION B - B

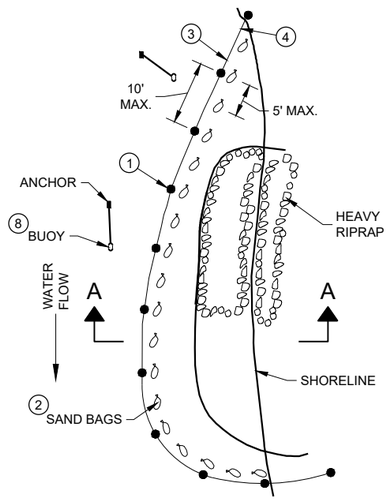


PLAN VIEW

TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6



SECTION A - A



PLAN VIEW

TURBIDITY BARRIER - STANDARD POST INSTALLATION

TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

(1) DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.

(2) SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.

(3) WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.

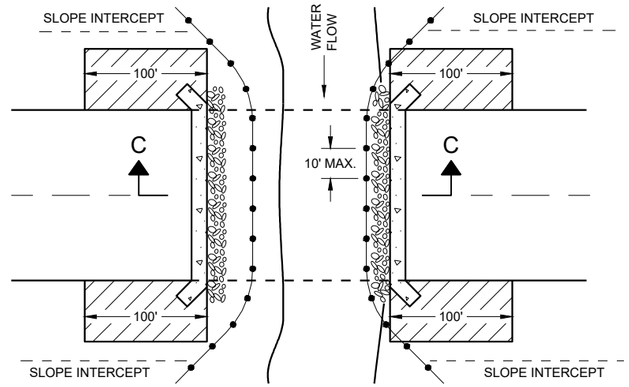
(4) IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.

(5) ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.

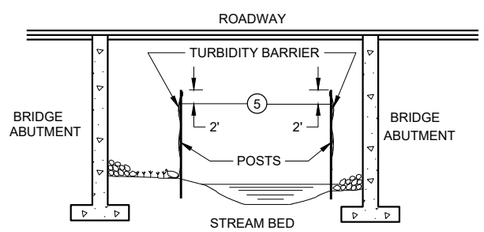
(6) FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.

(7) ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.

(8) USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

TURBIDITY BARRIER DETAIL SHOWING TYPICAL PLACEMENT AT STRUCTURES

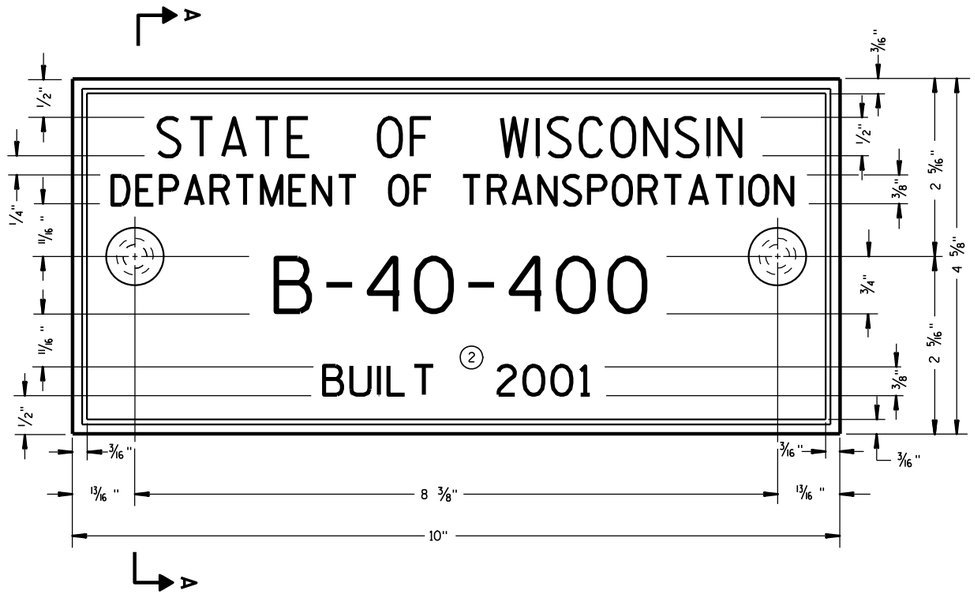
TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

6
SDD 08E11 - 02



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

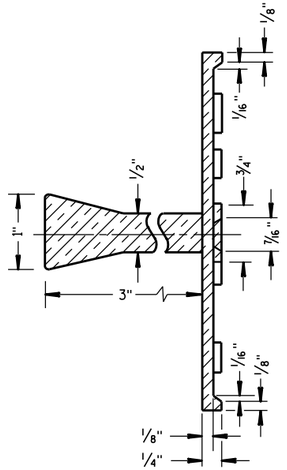
GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

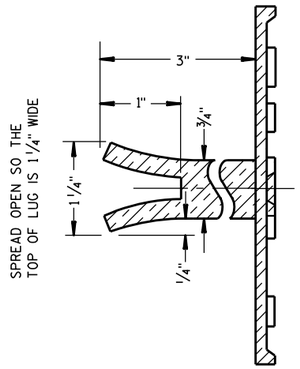
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

(1) EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

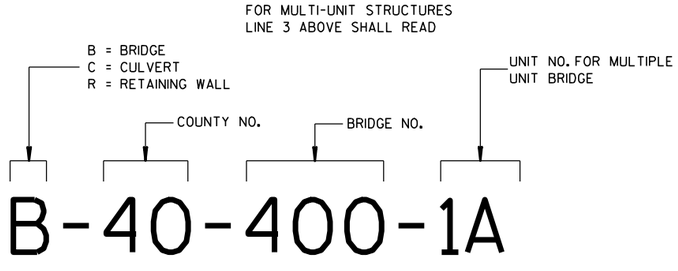
(2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



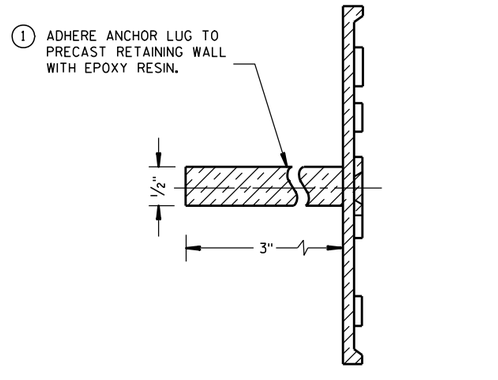
SECTION A-A



ALTERNATE LUG



NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

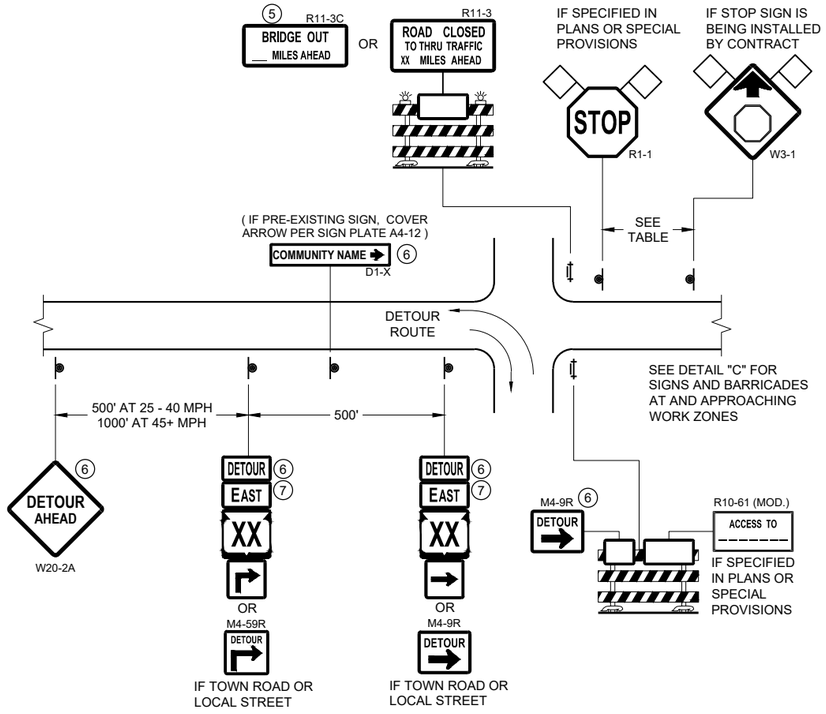
3/26/10 DATE

/S/ Scot Beck-- CHIEF STRUCTURAL DEVELOPMENT ENGINEER

FHWA

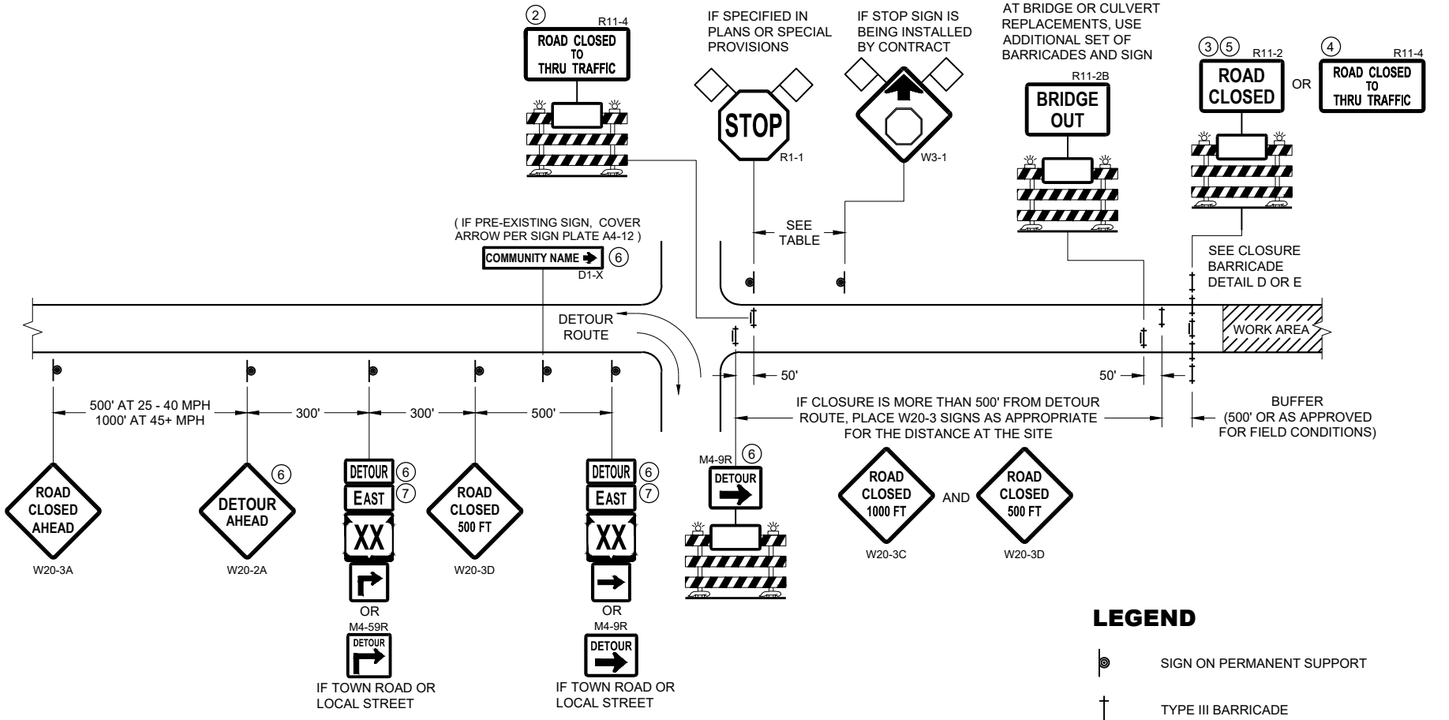
6

S.D.D. 12 A 3-10



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



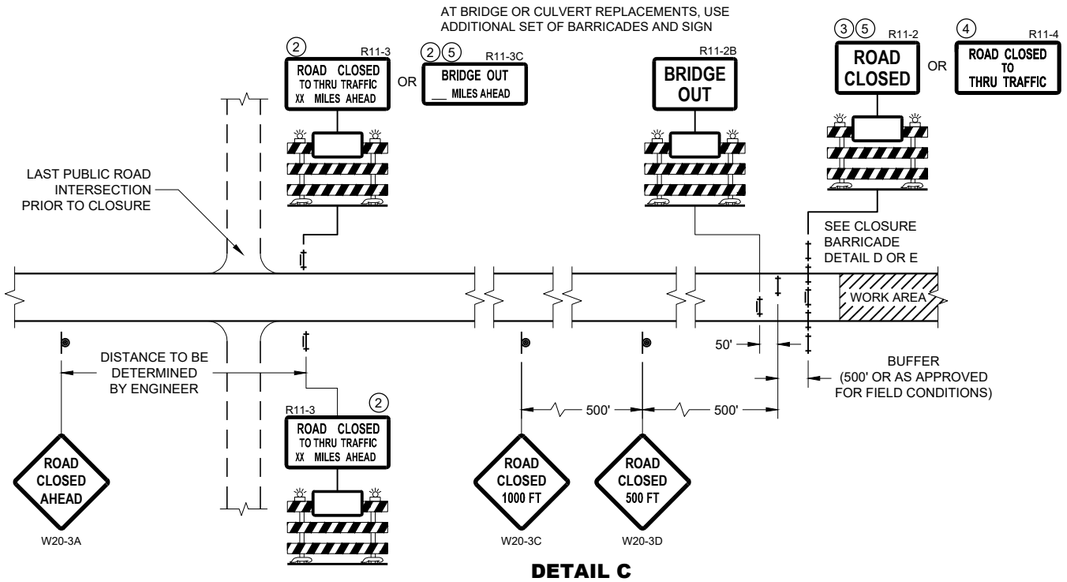
DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

DETOUR M4 - 8
EAST M3 - X
XX M1 - 4 OR XX M1 - 6 OR COUNTY X M1 - 5A
M05 - 1 OR M06 - 1



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

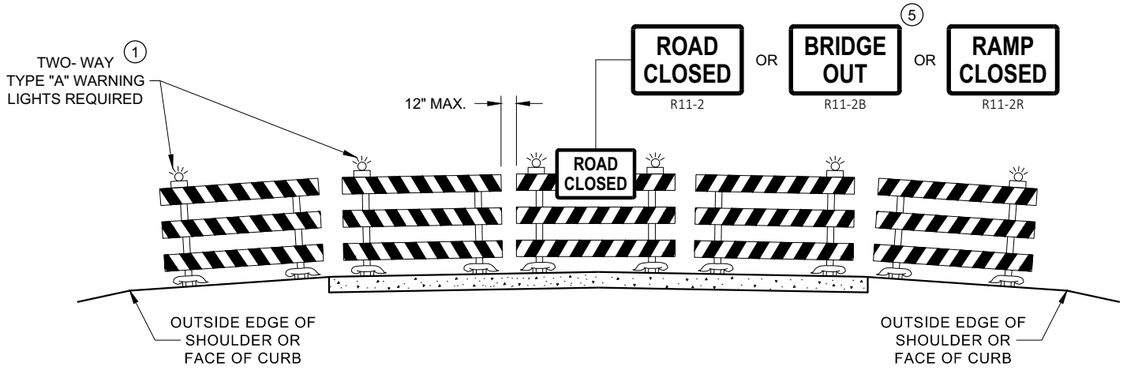
APPROVED
February 2020
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

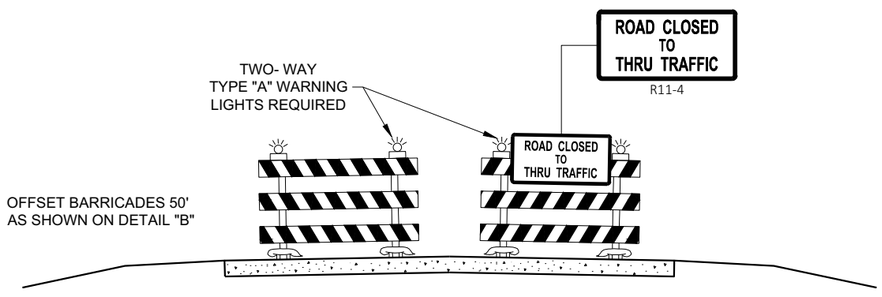
6

SDD 15C02 - 08a

SDD 15C02 - 08a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE
FHWA

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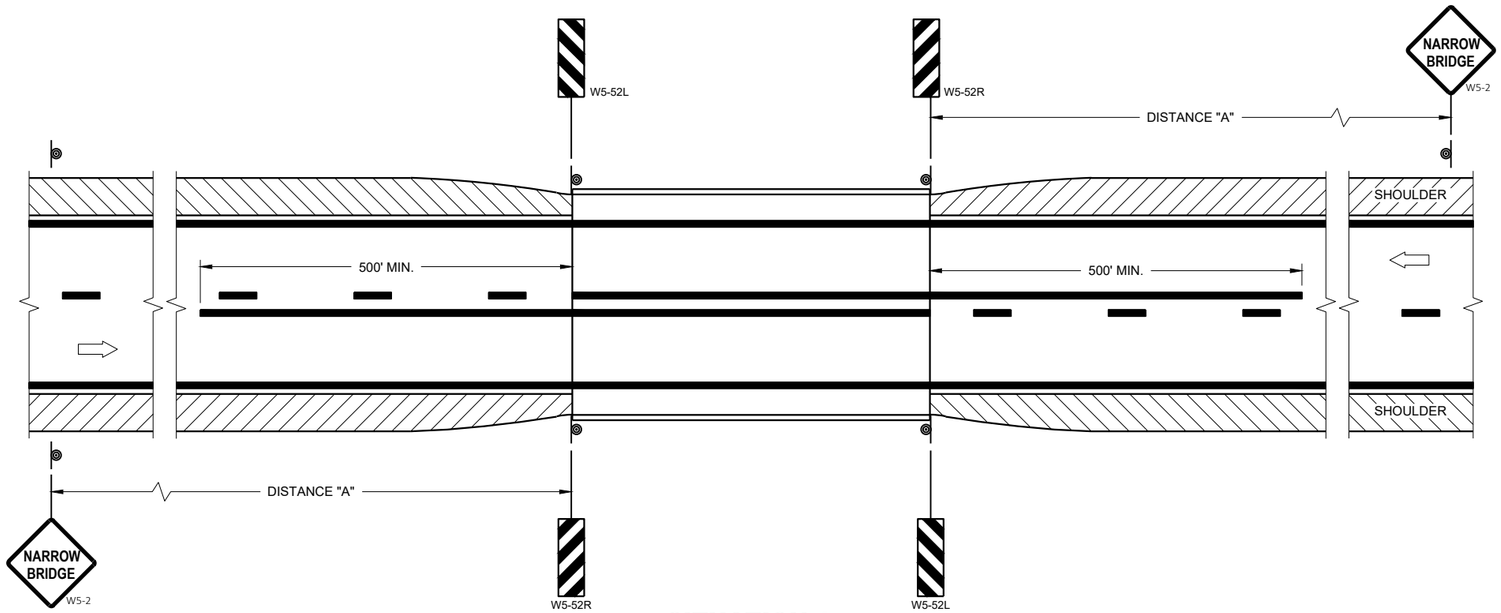
SDD 15C02 - 08b

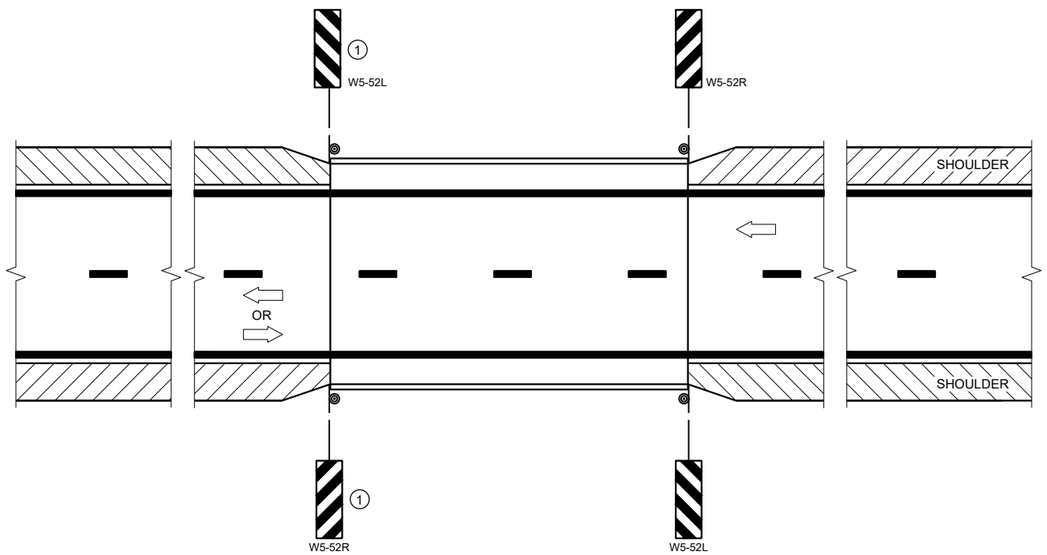
6

SDD 15C02 - 08b



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.



SITUATION 2

WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THE DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

ON BRIDGE ONLY PROJECTS, PLACE 300 FEET OF EDGELINE.

OMIT EDGELINES ON ROADWAYS WITHOUT EXISTING EDGELINES.

(1) OMIT ON ONE-WAY TRAVELED WAYS.

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

DISTANCE TABLE

POSTED OR 85TH PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	700'

SIGNING AND MARKING FOR TWO LANE BRIDGES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Jeannie Silver
STATE SIGNING AND MAF ENGINEER

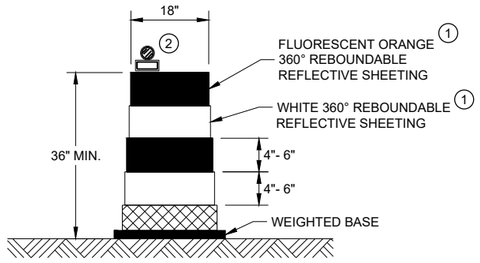
FHWA

6

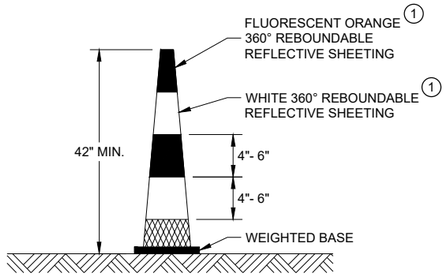
SDD 15C06 - 10

GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.

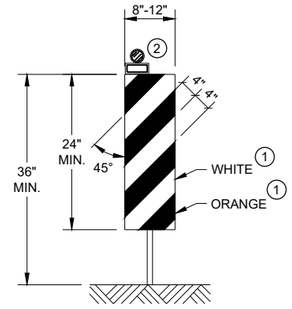


DRUM



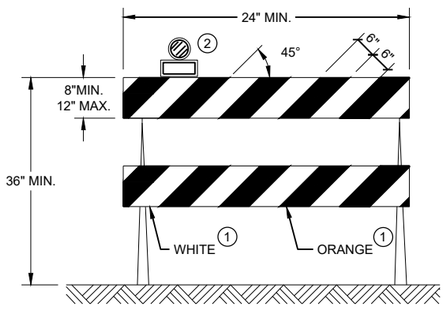
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS



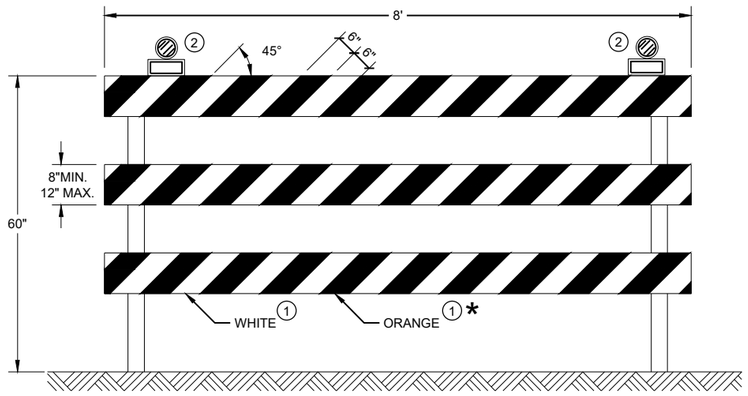
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021 — DATE
/S/ Andrew Heidtke — WORK ZONE ENGINEER
FHWA

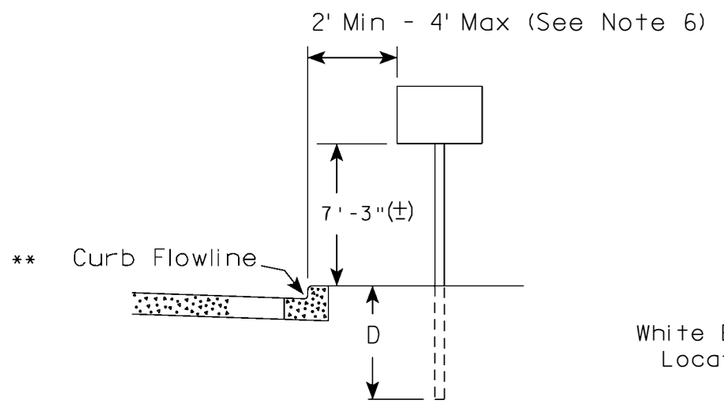
6

SDD 15C11 - 09b

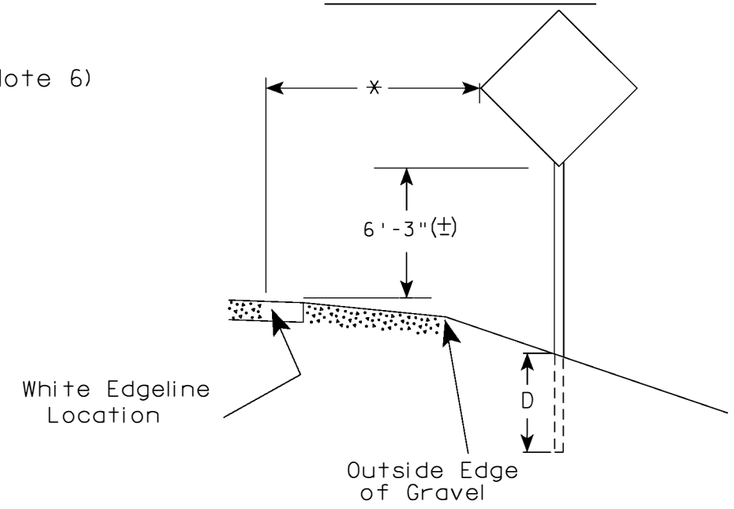
6

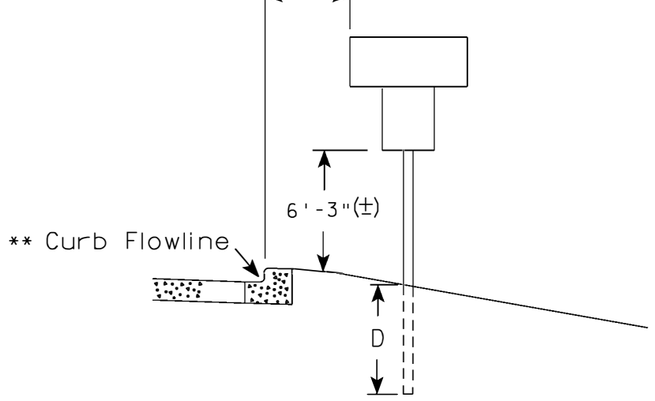
SDD 15C11 - 09b

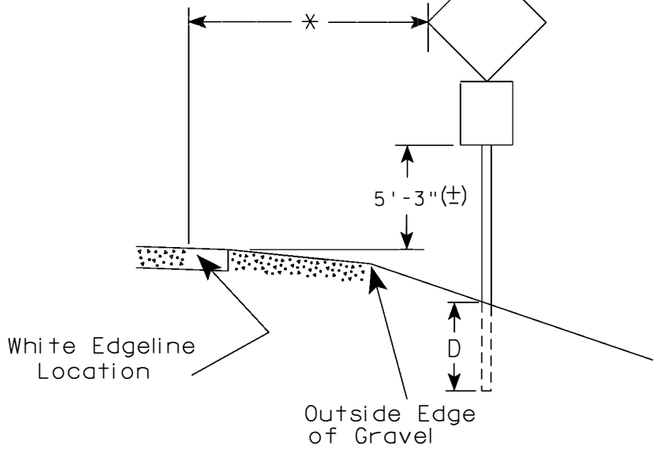
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.

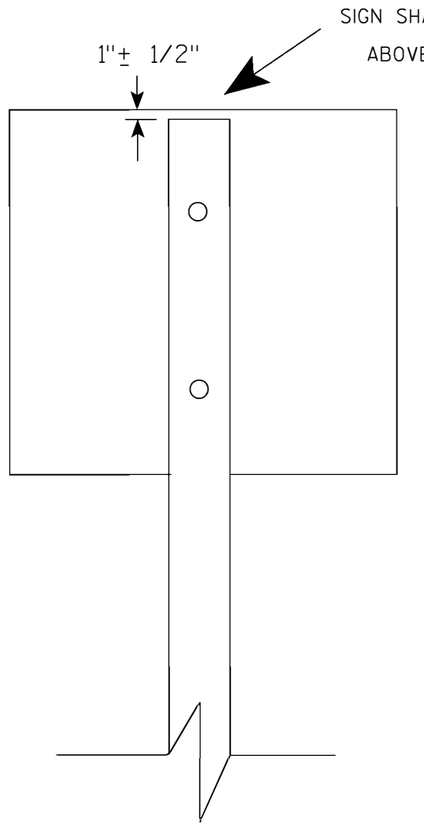
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

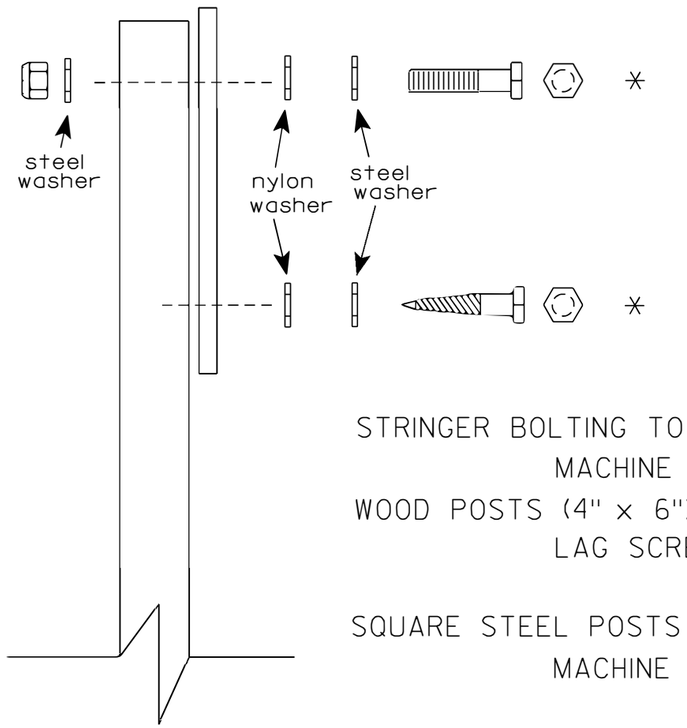
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22

PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD		SHEET NO: 21	E





Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

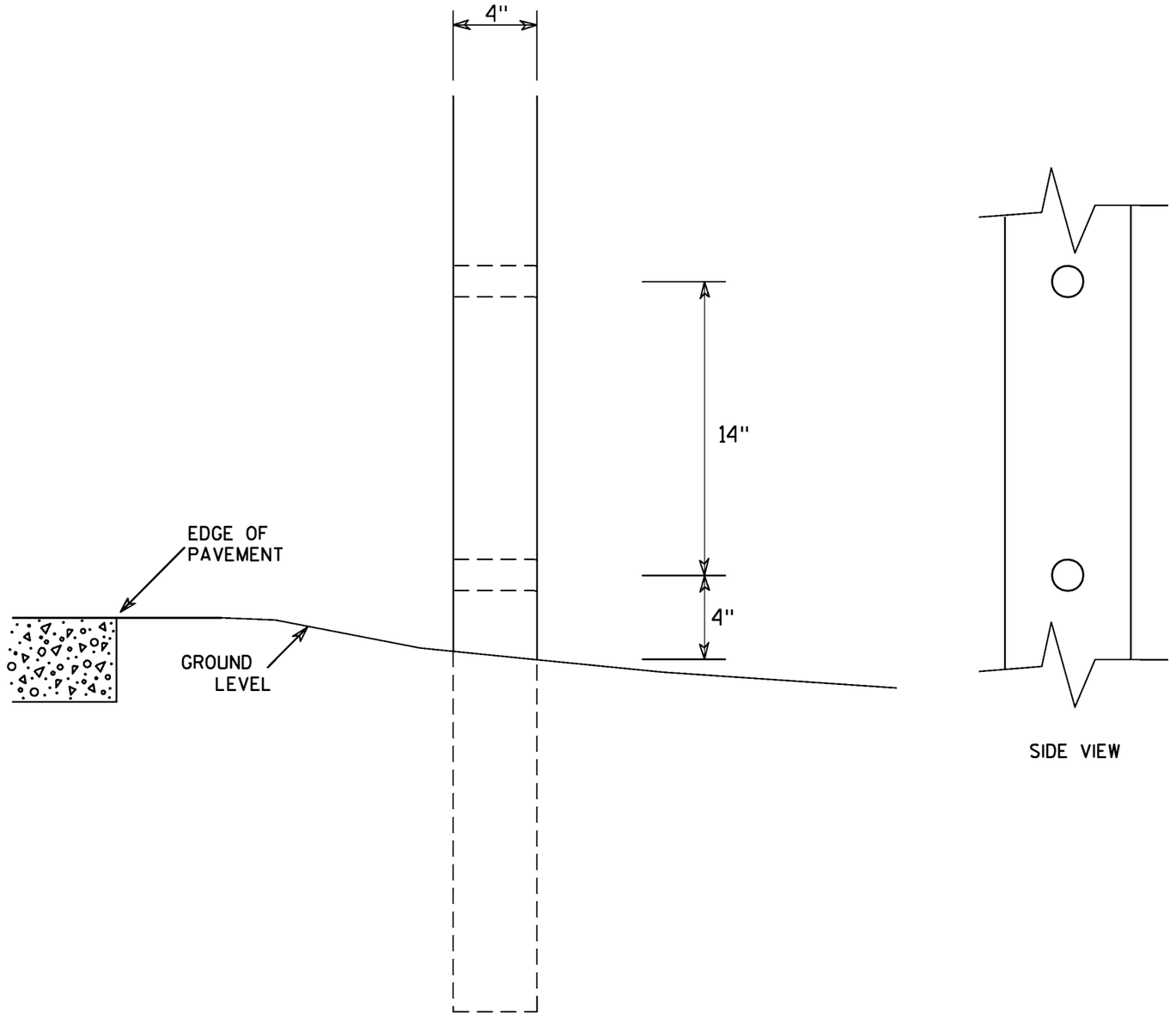
APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 4/1/2020 PLATE NO. A4-8.9

PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD		SHEET NO: 22	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A48.DGN PLOT DATE : 01-APRIL-2020 PLOT BY : dotc4c

WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

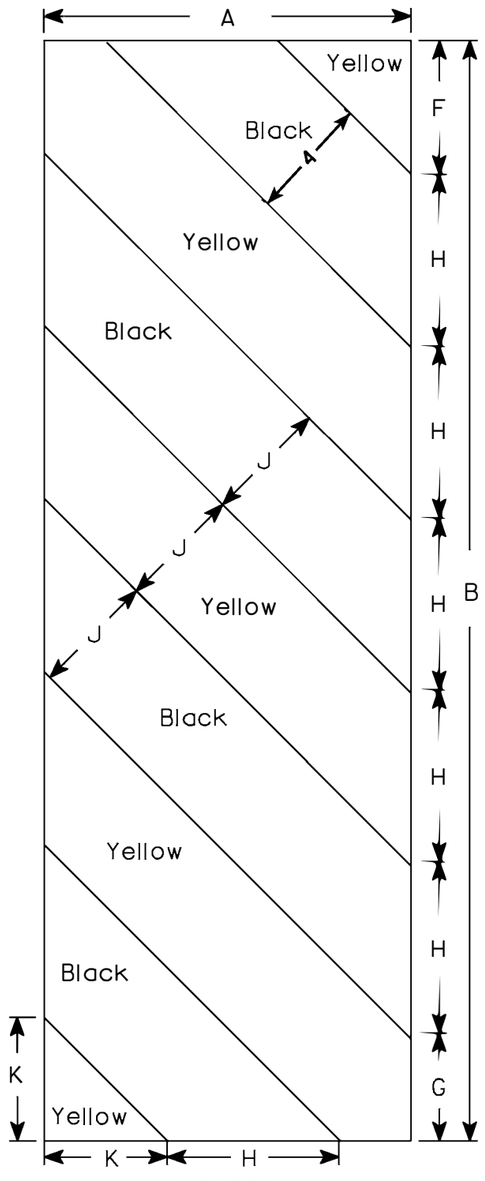
4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

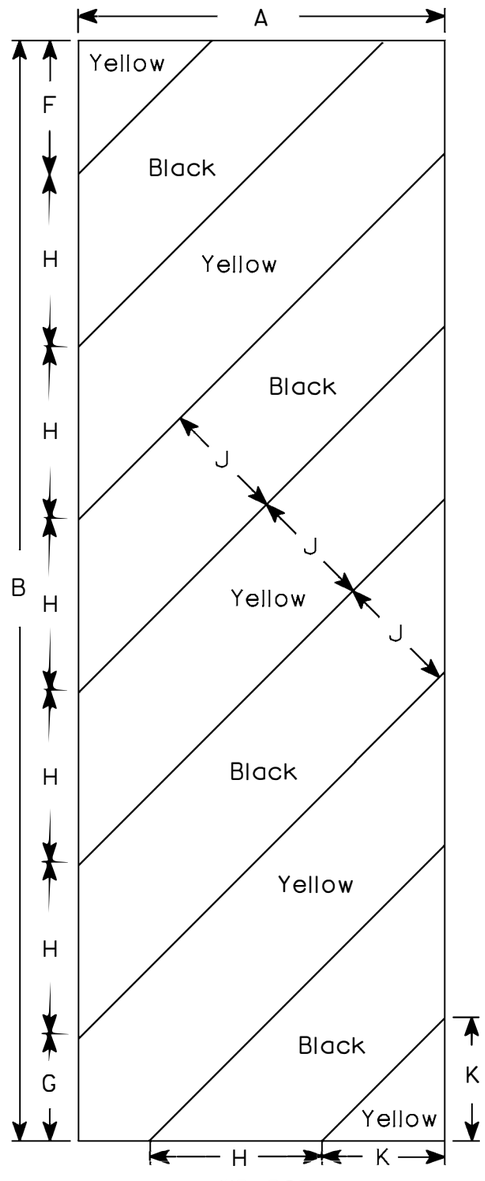
APPROVED Chester J Spang for State Traffic Engineer

DATE 3/27/97 PLATE NO. A4-11.2

PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD		SHEET NO: 23	E



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54				6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

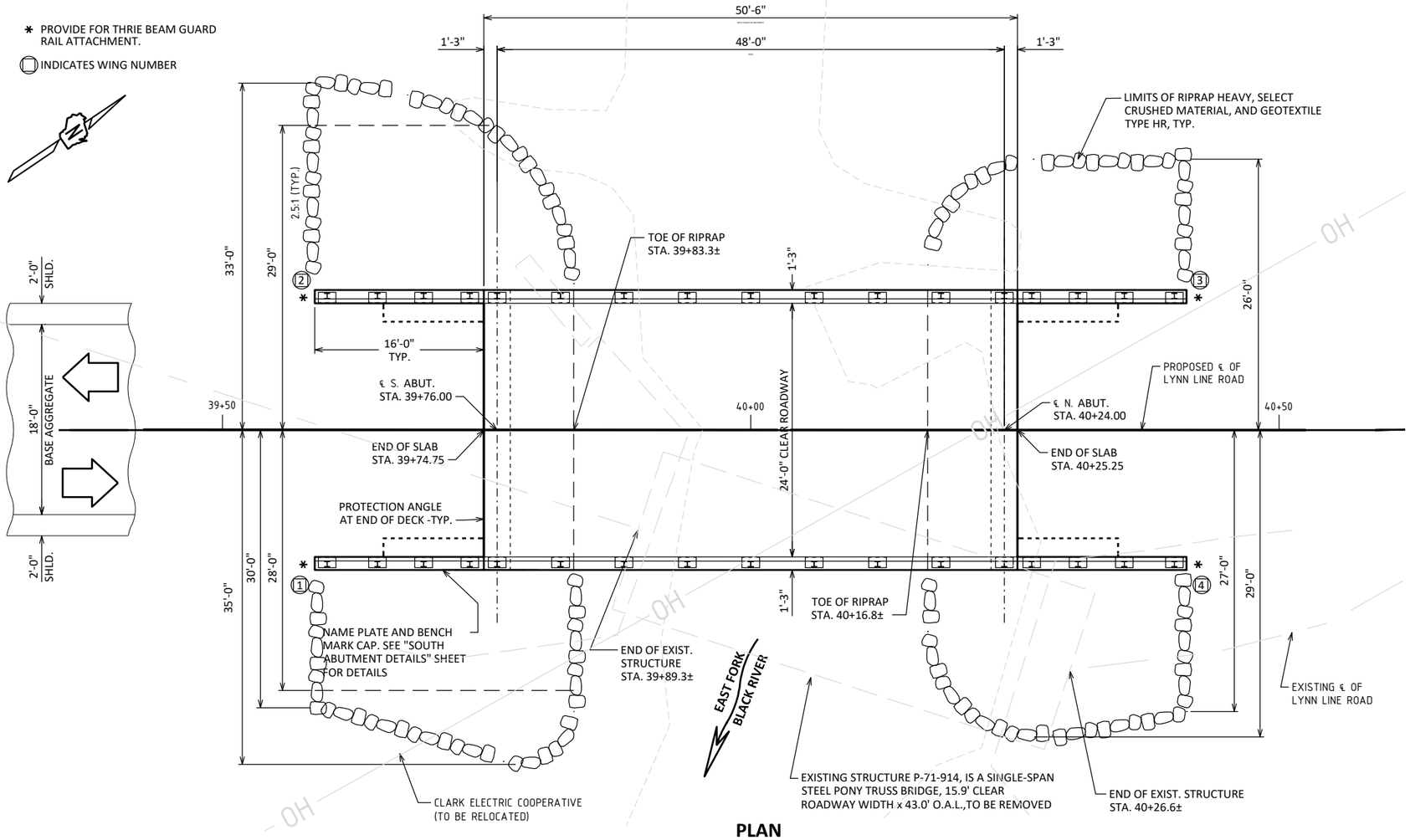
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W5-52.9

PROJECT NO: 7397-01-70 | HWY: LYNN LINE ROAD | COUNTY: WOOD | SHEET NO: 24 | E

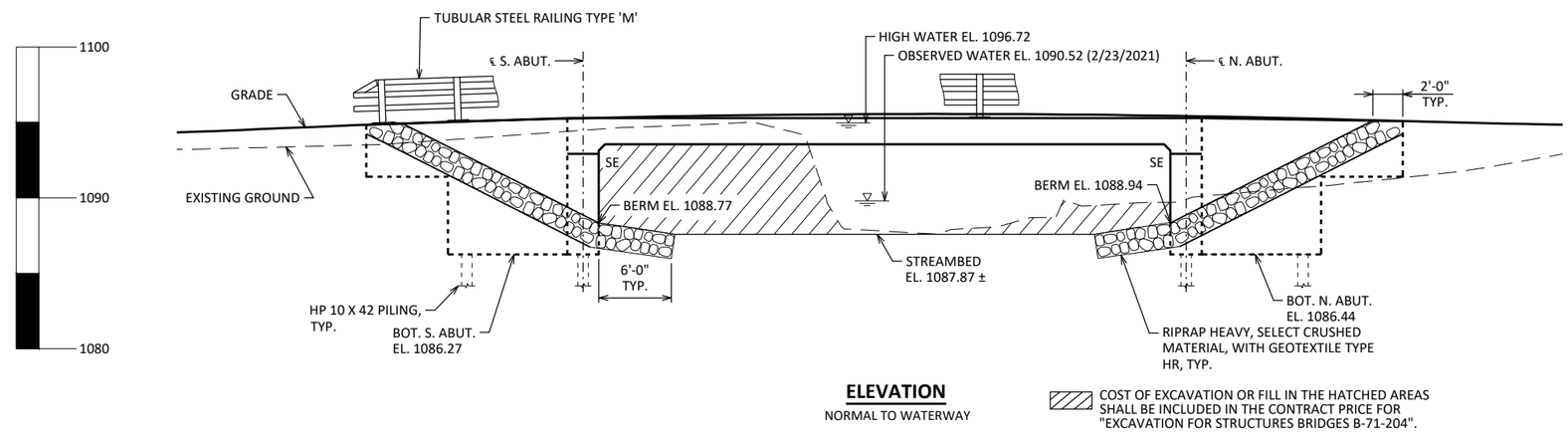
STATE PROJECT NUMBER

7397-01-70



PLAN

SINGLE SPAN FLAT SLAB



ELEVATION

NORMAL TO WATERWAY

COST OF EXCAVATION OR FILL IN THE HATCHED AREAS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR "EXCAVATION FOR STRUCTURES BRIDGES B-71-204".

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
INVENTORY RATING FACTOR: RF = 1.11
OPERATING RATING FACTOR: RF = 1.44
WISCONSIN STANDARD PERMIT VEHICLE (WIS.-SPV): 250 (KIPS)

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:

CONCRETE MASONRY:
SUPERSTRUCTURE — f'c = 4,000 P.S.I.
ALL OTHER — f'c = 3,500 P.S.I.

BAR STEEL REINFORCEMENT:
GRADE 60 — fy = 60,000 P.S.I.

FOUNDATION DATA

SOUTH ABUTMENT TO BE SUPPORTED ON HP 10 x 42 PILING (WITH PILE POINTS) DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 180 TONS †† PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 15'-0" LONG.

†† THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES TO DETERMINE DRIVEN PILE CAPACITY.

NORTH ABUTMENT TO BE SUPPORTED ON HP10x42 PILING SEATED IN PRE-BORED HOLES CORED 3 FEET MINIMUM INTO ROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 180 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5. ESTIMATED 10'-0" LONG.

TRAFFIC VOLUME

Lynn Line Road

ADT = <100 (2043)
R.D.S. = 20 M.P.H.

HYDRAULIC DATA

100 YEAR FREQUENCY

Q_{100} = 2,500 C.F.S. {STRUCTURE 1,438 C.F.S
{OVERFLOW 1,062 C.F.S
$VEL._{100}$ = 5.2 F.P.S.
$HW._{100}$ = EL. 1096.72
WATERWAY AREA = 285 SQ. FT.
DRAINAGE AREA = 8.2 SQ. MI.
SCOUR CRITICAL CODE = 5

2 YEAR FREQUENCY

Q_2 =523 C.F.S.
$VEL._2$ = 2.3 F.P.S.
$HW._2$ = EL. 1093.72

LIST OF DRAWINGS

1. GENERAL PLAN
2. CROSS SECTION & QUANTITIES
3. SUBSURFACE EXPLORATION
4. SOUTH ABUTMENT
5. SOUTH ABUTMENT DETAILS
6. NORTH ABUTMENT
7. NORTH ABUTMENT DETAILS
8. SUPERSTRUCTURE
9. SUPERSTRUCTURE DETAILS
10. TUBULAR STEEL RAILING TYPE 'M'

STRUCTURE DESIGN CONTACTS:

DAN SYDOW 715-834-3161
AARON BONK 608-261-0261

THESE PLANS ARE BASED UPON STANDARD BRIDGE PLANS DEVELOPED AND MAINTAINED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION THROUGH THE USE OF THE WISDOT STANDARD BRIDGE DESIGN TOOL. THE UNDERSIGNED DESIGNER CERTIFIES THE ACCURACY OF THE BRIDGE TYPE, SIZE AND LOCATION, HYDRAULICS AND FOUNDATION SUPPORT, AND INFORMATION IN THE PLANS THAT IS NOT PART OF THE STANDARD PLANS SUPPLIED BY THE DEPARTMENT. THE DESIGNER FURTHER CERTIFIES THAT USE OF THE STANDARD BRIDGE DESIGN TOOL FOR DEVELOPMENT OF THIS PLAN IS CONSISTENT WITH THE GUIDANCE PROVIDED IN THE WISDOT BRIDGE MANUAL.

10/17/2022

NO.	DATE	REVISION	BY

ORIGINAL PLANS PREPARED BY
AYRES 3433 Oakwood Hills Parkway Eau Claire, WI 54701 www.AyresAssociates.com

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORATION

ACCEPTED SDR 11/07/22
CHIEF STRUCTURES DESIGN ENGINEER — DATE

STRUCTURE B-71-204

LYNN LINE ROAD OVER EAST FORK BLACK RIVER

COUNTY	TOWN/~~CITY/VILLAGE~~
WOOD	ROCK

DESIGN SPEC.
AASHTO LRFD BRIDGE DESIGN SPECIFICATION

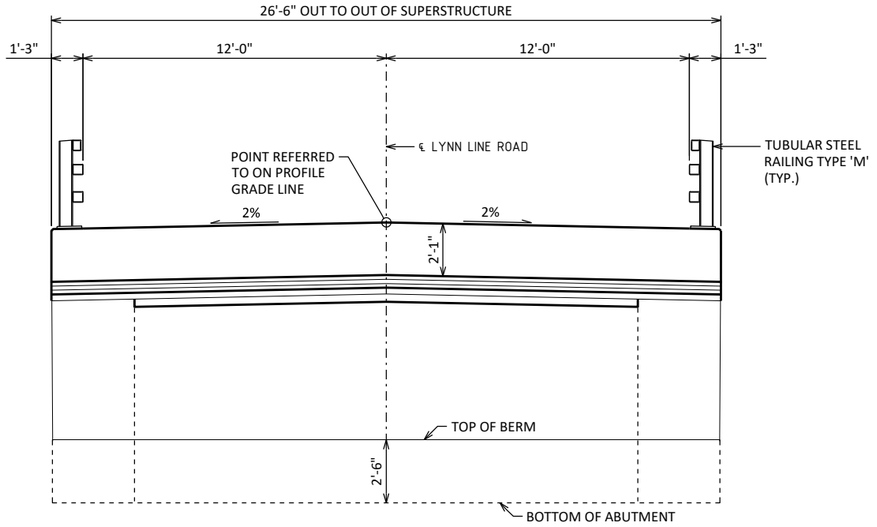
DESIGNED BY	DESIGNED CK'D	DRAWN BY	PLANS CK'D
JLB	ZSS	ZSS	DNS

GENERAL PLAN

SHEET 1 OF 10
25

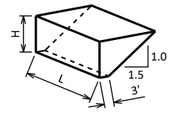
THIS SHEET WAS CREATED BY THE WISDOT BUREAU OF STRUCTURES STANDARD BRIDGE DESIGN TOOL VERSION 0.9

SCALE = 6



CROSS SECTION THRU ROADWAY

LOOKING UPSTATION
(PILING NOT SHOWN FOR CLARITY)



ABUTMENT BACKFILL DIAGRAM

L = OUT TO OUT OF ABUTMENT BODY INCLUDING WINGS (FT)
H = AVERAGE ABUTMENT FILL HEIGHT (FT)
EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H)
V_{CY} = V_{CF}(EF)/27
V_{TON} = V_{CY}(2.0)

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE ¾" UNLESS OTHERWISE NOTED.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES B-71-204" SHALL BE THE EXISTING GROUNDLINE.

AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TYPE A.

EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.

THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.

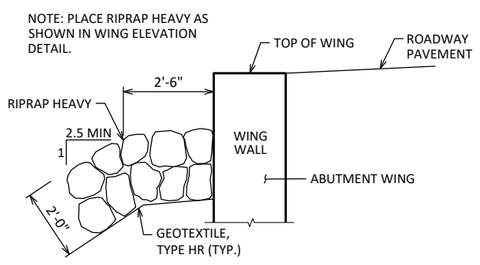
THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH RIPRAP HEAVY, SELECT CRUSHED MATERIAL, AND GEOTEXTILE TYPE HR TO THE EXTENT SHOWN ON SHEET 1 AND THE ABUTMENT DETAILS.

AT ABUTMENTS, CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.

PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO ENTIRE EXPOSED TOP OF SLAB, INCLUDING THE SLAB EDGE AND 1'-0" UNDER THE SLAB, THE TOP AND EXTERIOR EXPOSED FACE OF WINGS AND FRONT FACE OF ABUTMENT TO 1'-0" PAST THE EDGE OF SLAB.

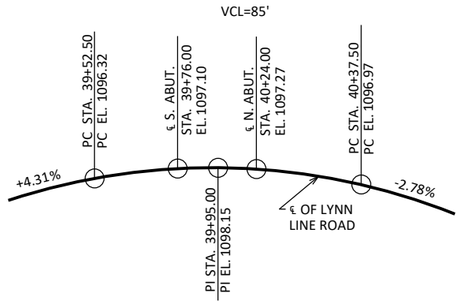
BENCH MARK

NO.	STATION	DESCRIPTION	ELEV.
50	40+27.8	PK NAIL IN TOP OF DECK, NW QUAD, 14.7' RT	1096.81

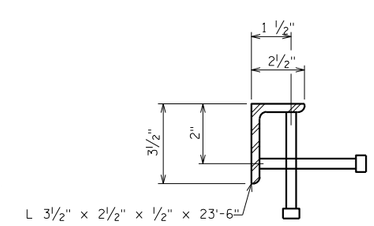


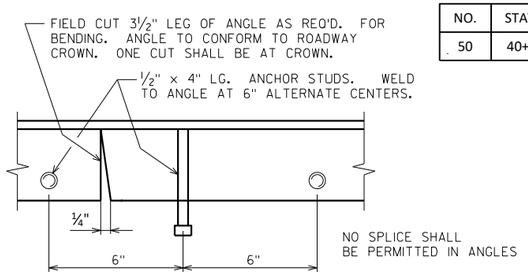
TYPICAL FILL SECTION AT WING

(FILL VOIDS WITH SELECT CRUSHED MATERIAL)



PROFILE GRADE LINE

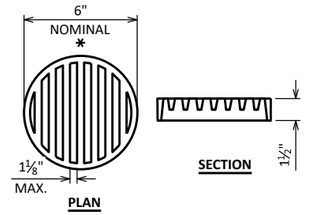




PROTECTION ANGLE DETAIL

(ANGLE AND STUDS TO BE PAID FOR AT THE UNIT PRICE BID FOR "STRUCTURAL STEEL CARBON". (NO PAINT REQ'D.)

SANDBLAST PROTECTION ANGLE AFTER FABRICATION IN ACCORDANCE WITH SSPC SP. #6 "COMMERCIAL BLAST CLEANING". AFTER BLAST CLEANING, THE PROTECTION ANGLE SHALL BE HOT DIPPED GALVANIZED.



RODENT SHIELD DETAIL

* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.

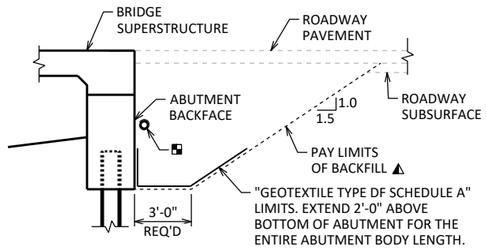
THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAING WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALLY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEM DESCRIPTION	UNIT	SUPER	S. ABUT.	N. ABUT.	TOTALS
203.0250	REMOVING STRUCTURE OVER WATERWAY REMOVE DEBRIS P-71-914	EACH	---	---	---	1
206.1001	EXCAVATION FOR STRUCTURES BRIDGES B-71-204	EACH	---	---	---	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	---	195	195	390
502.0100	CONCRETE MASONRY BRIDGES	CY	107.5	43.6	43.6	195
502.3200	PROTECTIVE SURFACE TREATMENT	SY	185	20	20	225
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	---	1,830	1,830	3,660
505.0600	BAR STEEL REINFORCEMENT COATED STRUCTURES	LB	21,680	2,100	2,100	25,880
506.0105	STRUCTURAL STEEL CARBON	LB	462	---	---	462
513.4061	RAILING TUBULAR TYPE M	LF	101	34.2	34.2	169.4
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	---	8	8	16
550.0020	PRE-BORING ROCK OR CONSOLIDATED MATERIALS	LF	---	---	48	48
550.0500	PILE POINTS	EACH	---	6	---	6
550.1100	PILING STEEL HP 10-INCH X 42 LB	LF	---	90	60	150
606.0300	RIPRAP HEAVY	CY	---	90	65	155
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	---	70	70	140
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	---	45	45	90
645.0120	GEOTEXTILE TYPE HR	SY	---	165	130	295
SPV.0195.01	SELECT CRUSHED MATERIAL FOR RIPRAP VOIDS	TON	---	25	20	45
	NON-BID ITEMS					
	FILLER	SIZE	---	---	---	½", ¾"

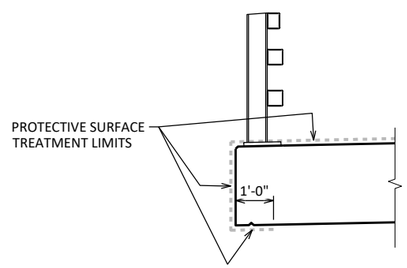
THIS SHEET WAS CREATED BY THE WISDOT BUREAU OF STRUCTURES STANDARD BRIDGE DESIGN TOOL VERSION 0.9



TYPICAL SECTION THRU ABUTMENT

▲ BACKFILL PAY LIMITS. BACKFILL BEYOND PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

▪ PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.



PROTECTIVE SURFACE TREATMENT DETAILS

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-71-204

DRAWN BY ZSS | PLANS CK'D DNS

CROSS SECTION & QUANTITIES

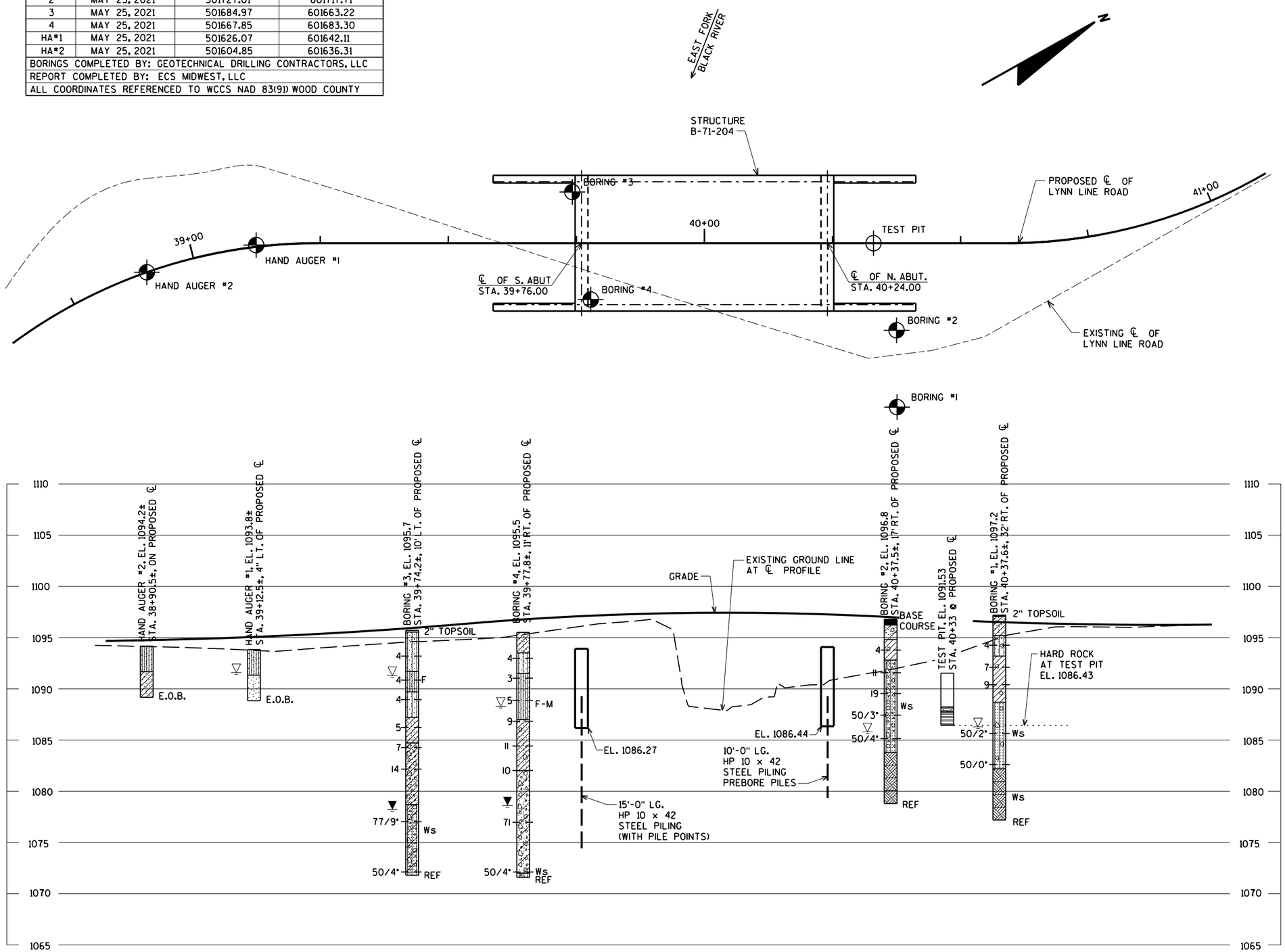
SHEET 2 OF 10
26

8

8

SCALE =

BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	MAY 25, 2021	501719.76	601730.85
2	MAY 25, 2021	501727.01	601717.71
3	MAY 25, 2021	501684.97	601663.22
4	MAY 25, 2021	501667.85	601683.30
HA#1	MAY 25, 2021	501626.07	601642.11
HA#2	MAY 25, 2021	501604.85	601636.31
BORINGS COMPLETED BY: GEOTECHNICAL DRILLING CONTRACTORS, LLC			
REPORT COMPLETED BY: ECS MIDWEST, LLC			
ALL COORDINATES REFERENCED TO WCCS NAD 83(91) WOOD COUNTY			



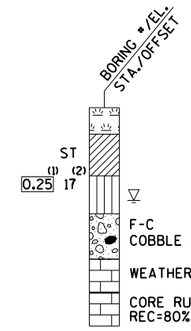
STATE PROJECT NUMBER

7397-01-70

MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/ META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

- ▽ AT TIME OF DRILLING
- ▼ END OF DRILLING
- ▽ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-71-204

DRAWN BY CLP | PLANS CK'D. DNS

SUBSURFACE EXPLORATION

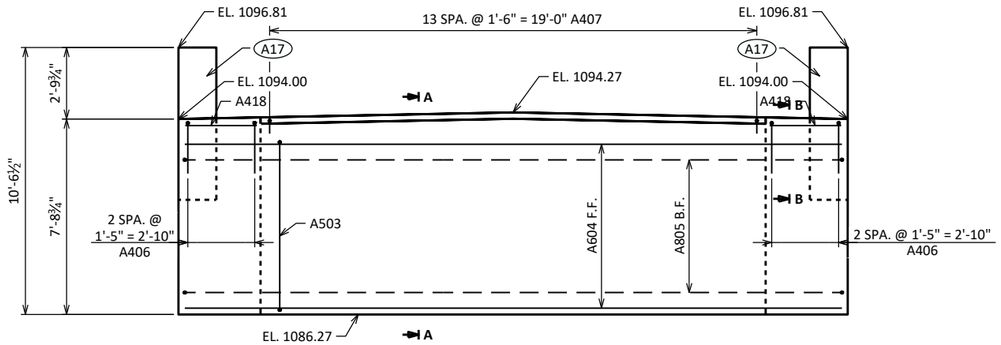
SHEET 3 OF 10
27

8/29/2022
PENTABLE:BReau_shd_util.tbl

I:±42±42-1283.00 - Wood Co. Lynn Line Rd over E Fk Black River±Structures±CADD±Final±421283 soils.dgn

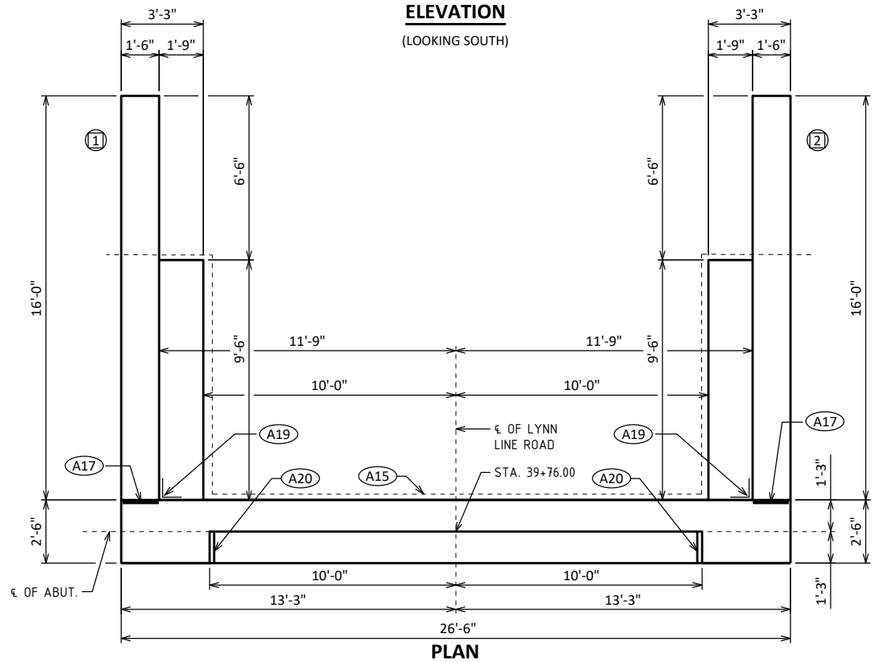
STATE PROJECT NUMBER

7397-01-70

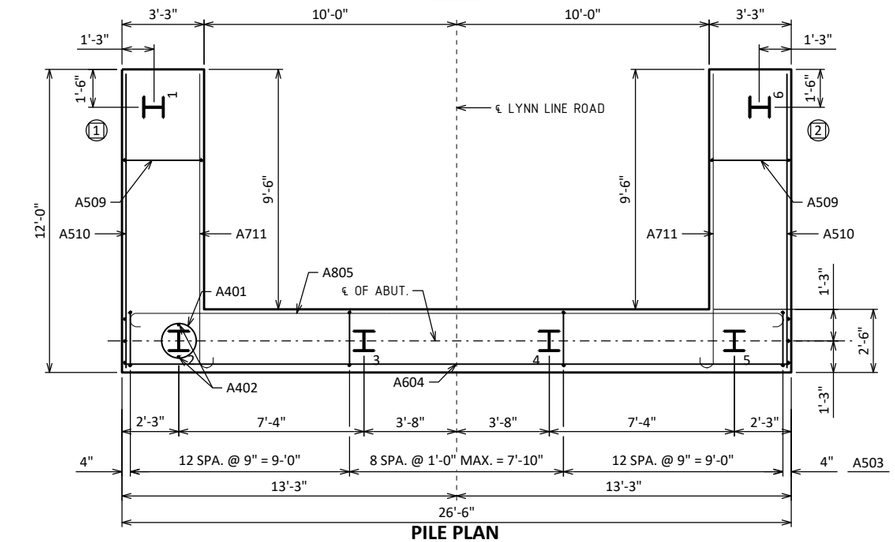


ELEVATION

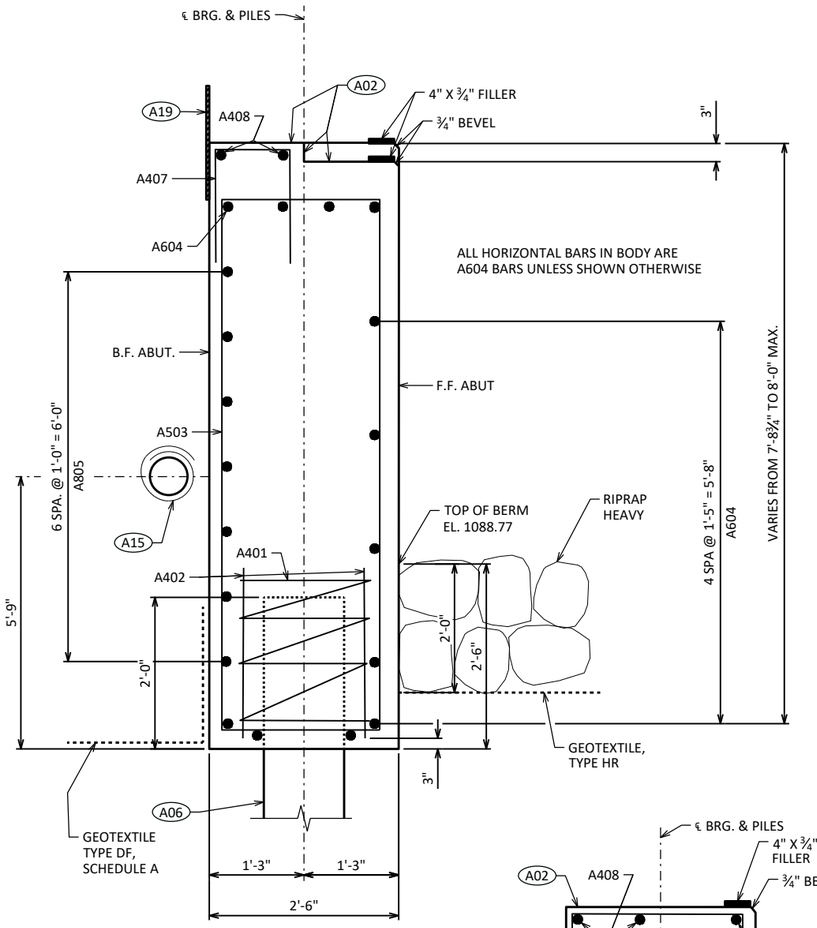
(LOOKING SOUTH)



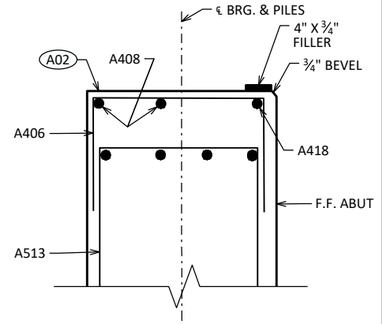
PLAN



PILE PLAN



SECTION A-A



SECTION B-B

- A02 STEEL TROWEL TOP SURFACE OF ABUTMENT. PLACE MULTIPLE LAYERS OF POLYETHYLENE SHEETS OVER ENTIRE ABUTMENT TOP BEFORE PLACING SUPERSTRUCTURE. TOTAL THICKNESS OF ALL SHEETS SHALL BE AT LEAST 0.03"
- A06 SUPPORT ABUTMENT ON HP 10 x 42 PILING, ESTIMATED 15'-0" LONG WITH A REQUIRED DRIVING RESISTANCE OF 180 TONS PER PILE.
- A15 PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- A17 ½" FILLER: SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
- A19 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.
- A20 ¾" CORK FILLER ON VERTICAL FACE
- INDICATES WING NUMBER

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-71-204

DRAWN BY ZSS | PLANS CK'D DNS

SOUTH ABUTMENT

SHEET 4 OF 10
28

SCALE = 3

8

8

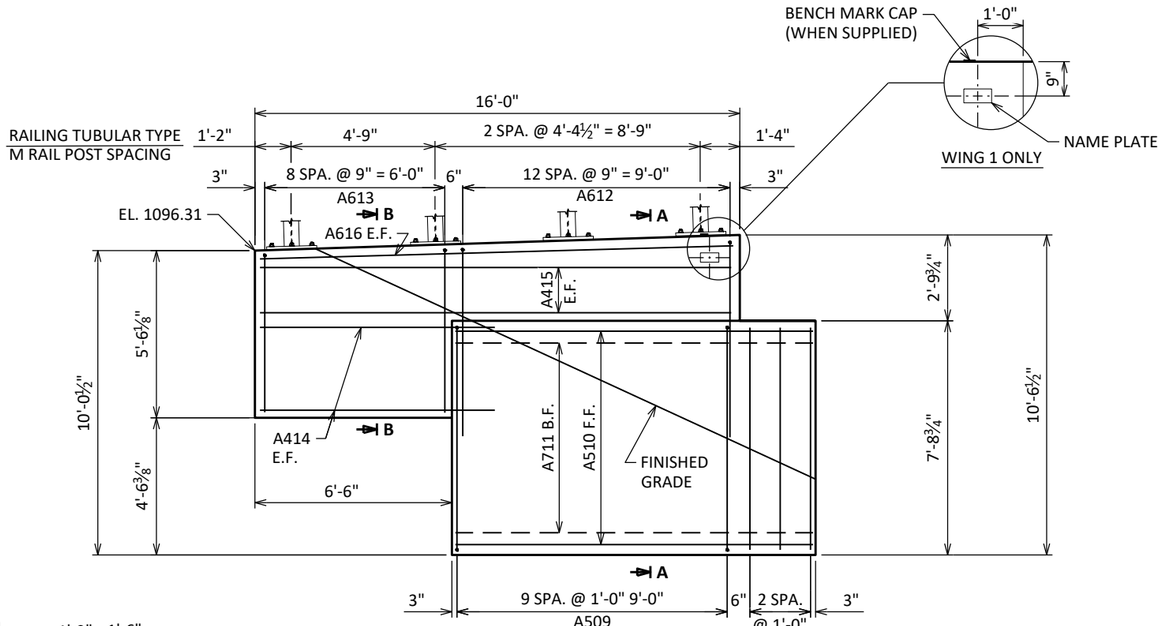
STATE PROJECT NUMBER

7397-01-70

BILL OF BARS

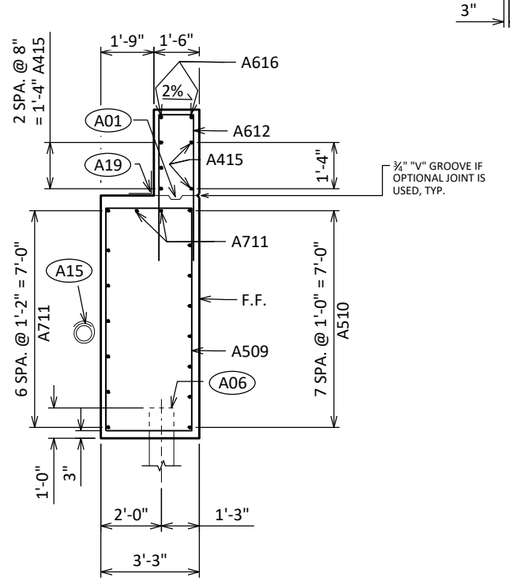
NOTE: THE FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
A401		4	28-0	X		BODY @ PILES
A402		8	2-3			BODY @ PILES
A503		33	18-10	X		BODY VERT.
A604		12	26-2			BODY HORIZ.
A805		7	28-0	X		BODY HORIZ. @ WING B.F.
A406		6	5-4	X		BODY VERT. TOP ENDS
A407		14	4-1	X		BODY VERT. TOP
A408		2	26-2			BODY HORIZ. TOP
A509	X	20	20-10	X		WING VERT.
A510	X	16	11-8			WING HORIZ. F.F.
A711	X	18	12-4	X		WING HORIZ. B. F. & TOP
A612	X	26	10-6	X		WING VERT.
A613	X	18	11-0	X		WING VERT.
A414	X	16	7-9			WING HORIZ. E.F.
A415	X	10	15-7			WING HORIZ. E.F.
A616	X	4	15-7			WING HORIZ. E.F. TOP
A417	X	6	7-3			BODY VERT. END @ WINGS
A418		2	2-11			BODY HORIZ. TOP F.F. ENDS

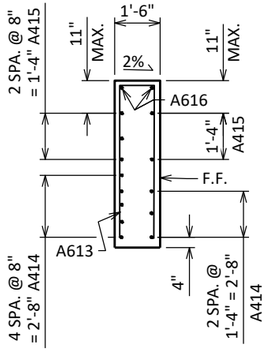


WING ELEVATION

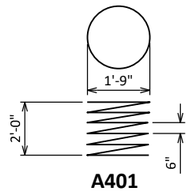
SHOWING F.F. WING 1
(WING 2 SIMILAR)



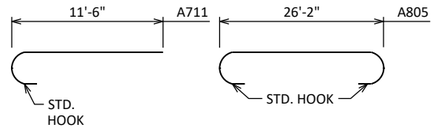
SECTION A-A

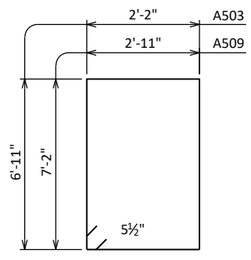


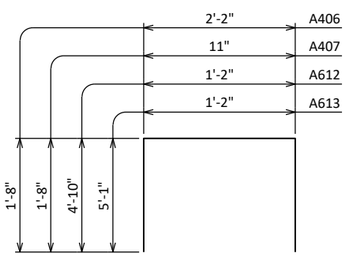
SECTION B-B



A401







A01 OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6. PROVIDE ¾" "V" GROOVE ON F.F. OF WINGWALL IF JOINT IS USED.

A06 SUPPORT ABUTMENT ON HP 10 x 42 PILING, ESTIMATED 15'-0" LONG WITH A REQUIRED DRIVING RESISTANCE OF 180TONS PER PILE.

A15 PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.

A19 18" RUBBERIZED MEMBRANE WATERPROOFING, ONLY IF OPTIONAL CONSTRUCTION JOINT IS USED.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-71-204

DRAWN BY ZSS | PLANS CK'D DNS

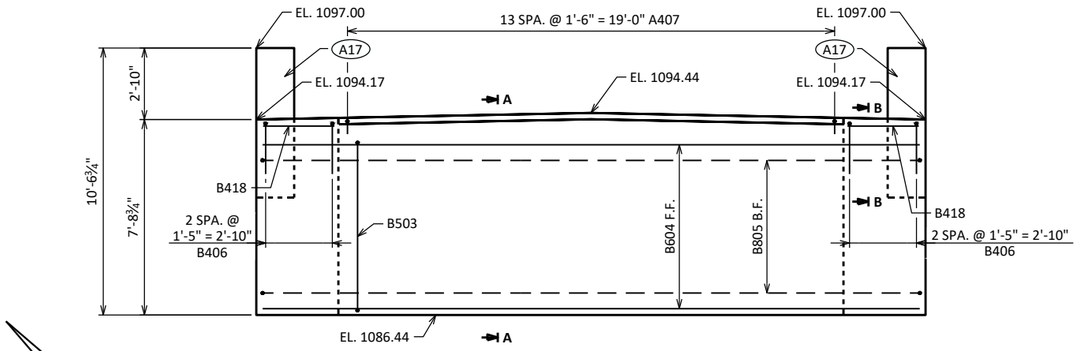
SOUTH ABUTMENT DETAILS

SHEET 5 OF 10
29

SCALE = 2

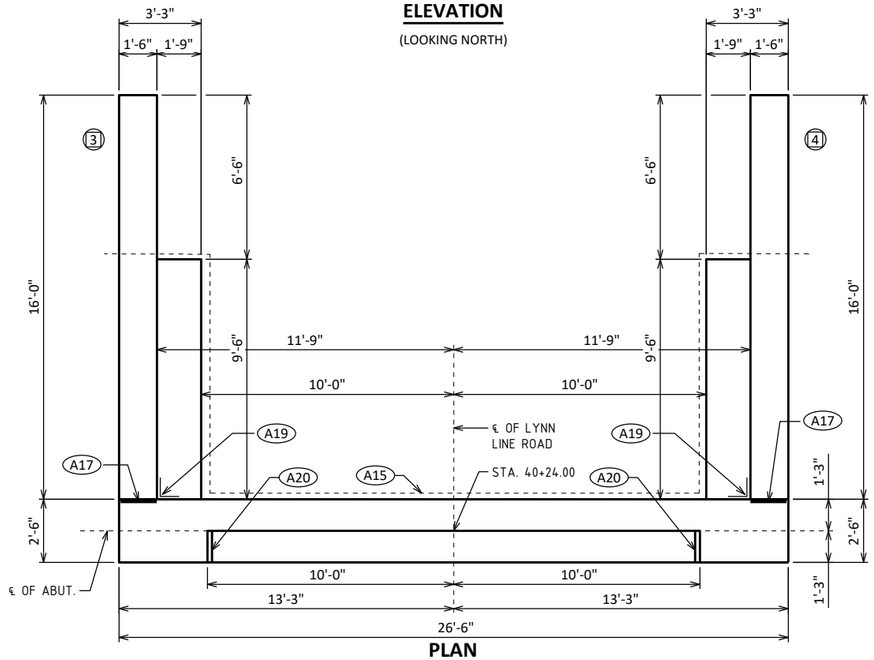
STATE PROJECT NUMBER

7397-01-70

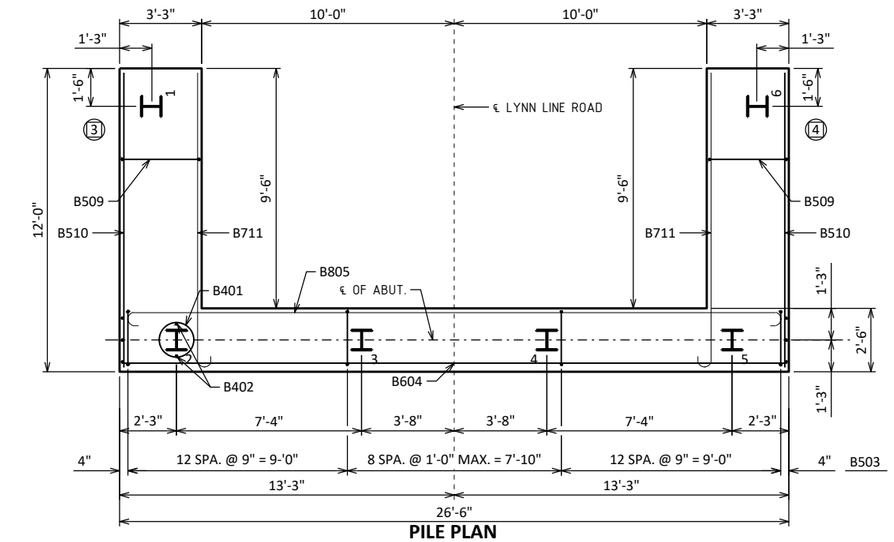


ELEVATION

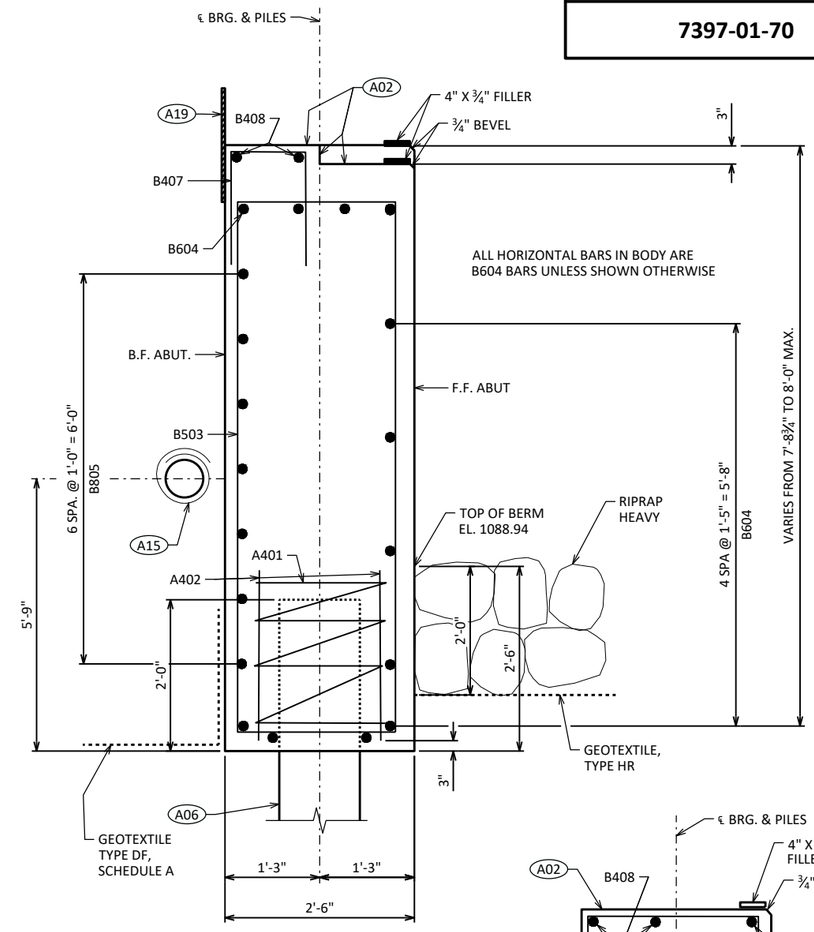
(LOOKING NORTH)



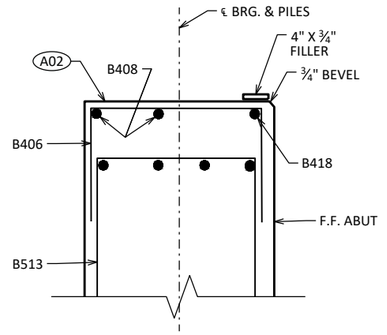
PLAN



PILE PLAN



SECTION A-A



SECTION B-B

(A02) STEEL TROWEL TOP SURFACE OF ABUTMENT. PLACE MULTIPLE LAYERS OF POLYETHYLENE SHEETS OVER ENTIRE ABUTMENT TOP BEFORE PLACING SUPERSTRUCTURE. TOTAL THICKNESS OF ALL SHEETS SHALL BE AT LEAST 0.03"

(A06) SUPPORT ABUTMENT ON HP 10 x 42 PILING, ESTIMATED 10'-0" LONG WITH A FACTORED AXIAL RESISTANCE OF 180 TONS PER PILE. PRE-BORE A MINIMUM OF 3-FEET INTO ROCK.

(A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.

(A17) ½" FILLER: SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.

(A19) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.

(A20) ¾" CORK FILLER ON VERTICAL FACE

(3) INDICATES WING NUMBER

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-71-204

DRAWN BY ZSS | PLANS CK'D DNS

NORTH ABUTMENT

SHEET 6 OF 10
30

SCALE = 3

8

8

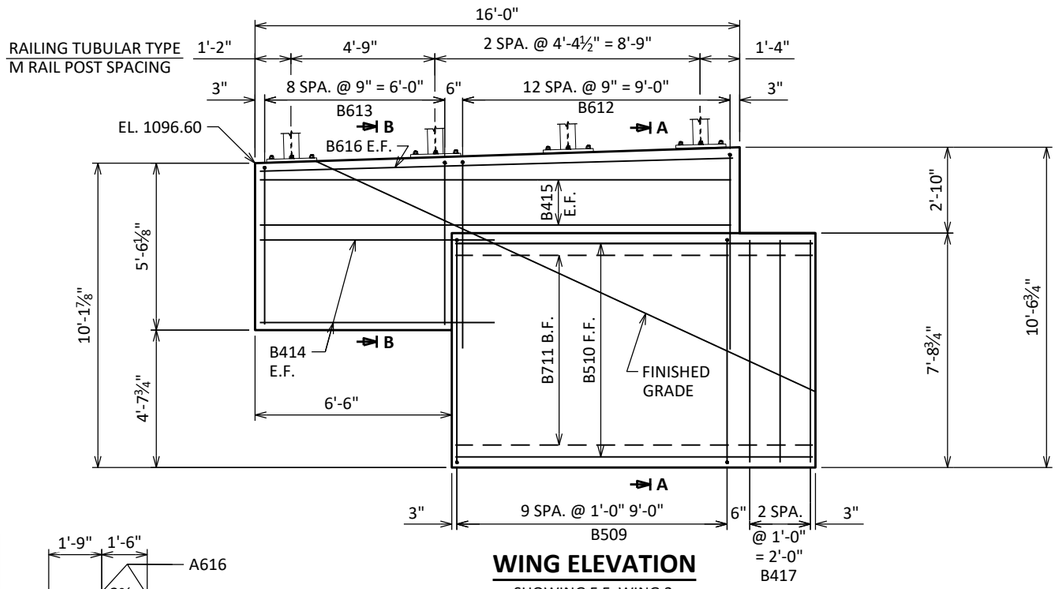
STATE PROJECT NUMBER

7397-01-70

BILL OF BARS

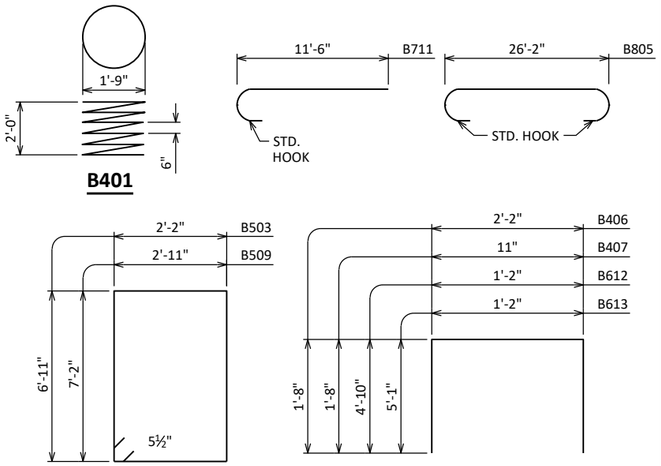
NOTE: THE FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

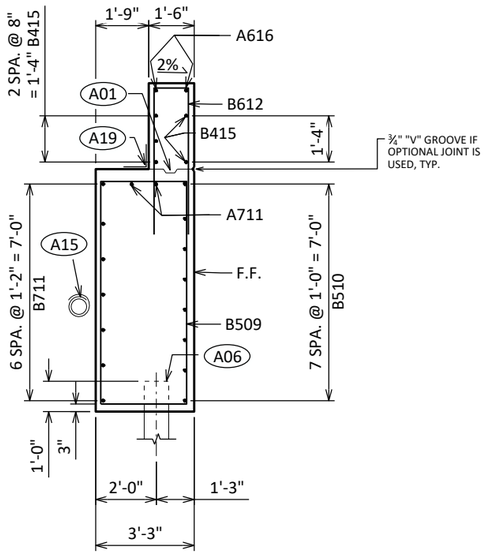
BAR MARK	COAT	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
B401		4	28-0	X		BODY @ PILES
B402		8	2-3			BODY @ PILES
B503		33	18-10	X		BODY VERT.
B604		12	26-2			BODY HORIZ.
B805		7	28-0	X		BODY HORIZ. @ WING B.F.
B406		6	5-4	X		BODY VERT. TOP ENDS
B407		14	4-1	X		BODY VERT. TOP
B408		2	26-2			BODY HORIZ. TOP
B509	X	20	20-10	X		WING VERT.
B510	X	16	11-8			WING HORIZ. F.F.
B711	X	18	12-4	X		WING HORIZ. B. F. & TOP
B612	X	26	10-6	X		WING VERT.
B613	X	18	11-0	X		WING VERT.
B414	X	16	7-9			WING HORIZ. E.F.
B415	X	10	15-7			WING HORIZ. E.F.
B616	X	4	15-7			WING HORIZ. E.F. TOP
B417	X	6	7-3			BODY VERT. END @ WINGS
B418		2	2-11			BODY HORIZ. TOP F.F. ENDS



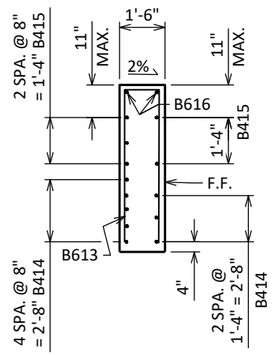
WING ELEVATION

SHOWING F.F. WING 3
(WING 4 SIMILAR)





SECTION A-A



SECTION B-B

A01 OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6. PROVIDE ¾" "V" GROOVE ON F.F. OF WINGWALL IF JOINT IS USED.

A06 SUPPORT ABUTMENT ON HP 10 x 42 PILING, ESTIMATED 10'-0" LONG WITH A FACTORED AXIAL RESISTANCE OF 180 TONS PER PILE.

A15 PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.

A19 18" RUBBERIZED MEMBRANE WATERPROOFING, ONLY IF OPTIONAL CONSTRUCTION JOINT IS USED.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-71-204

DRAWN BY ZSS | PLANS CK'D DNS

NORTH ABUTMENT DETAILS

SHEET 7 OF 10
31

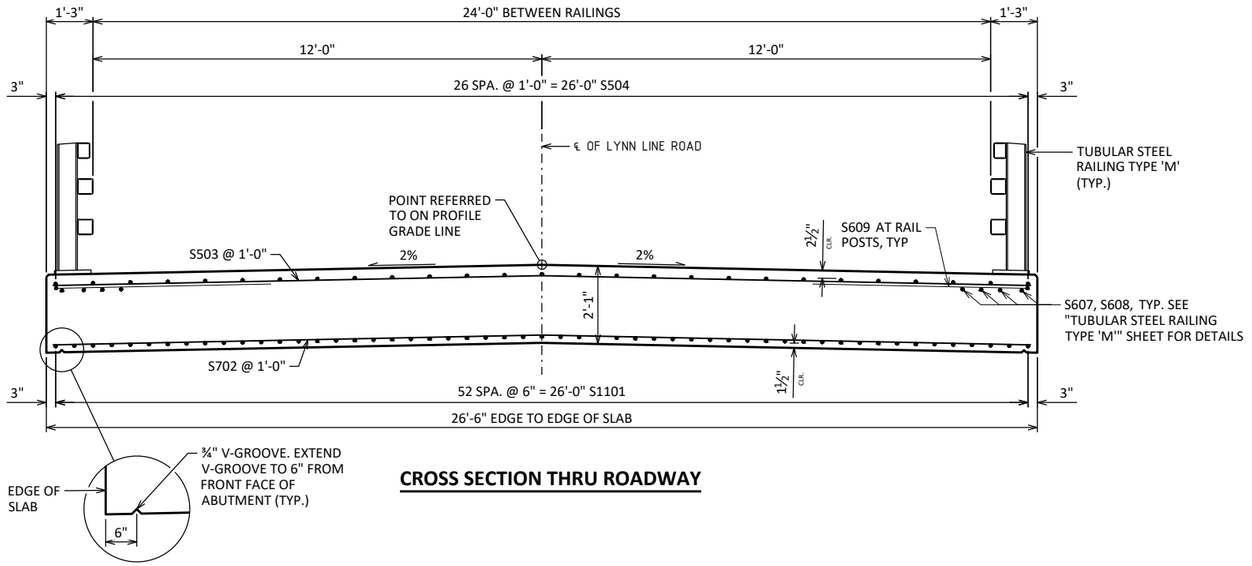
SCALE = 2

8

8

STATE PROJECT NUMBER

7397-01-70

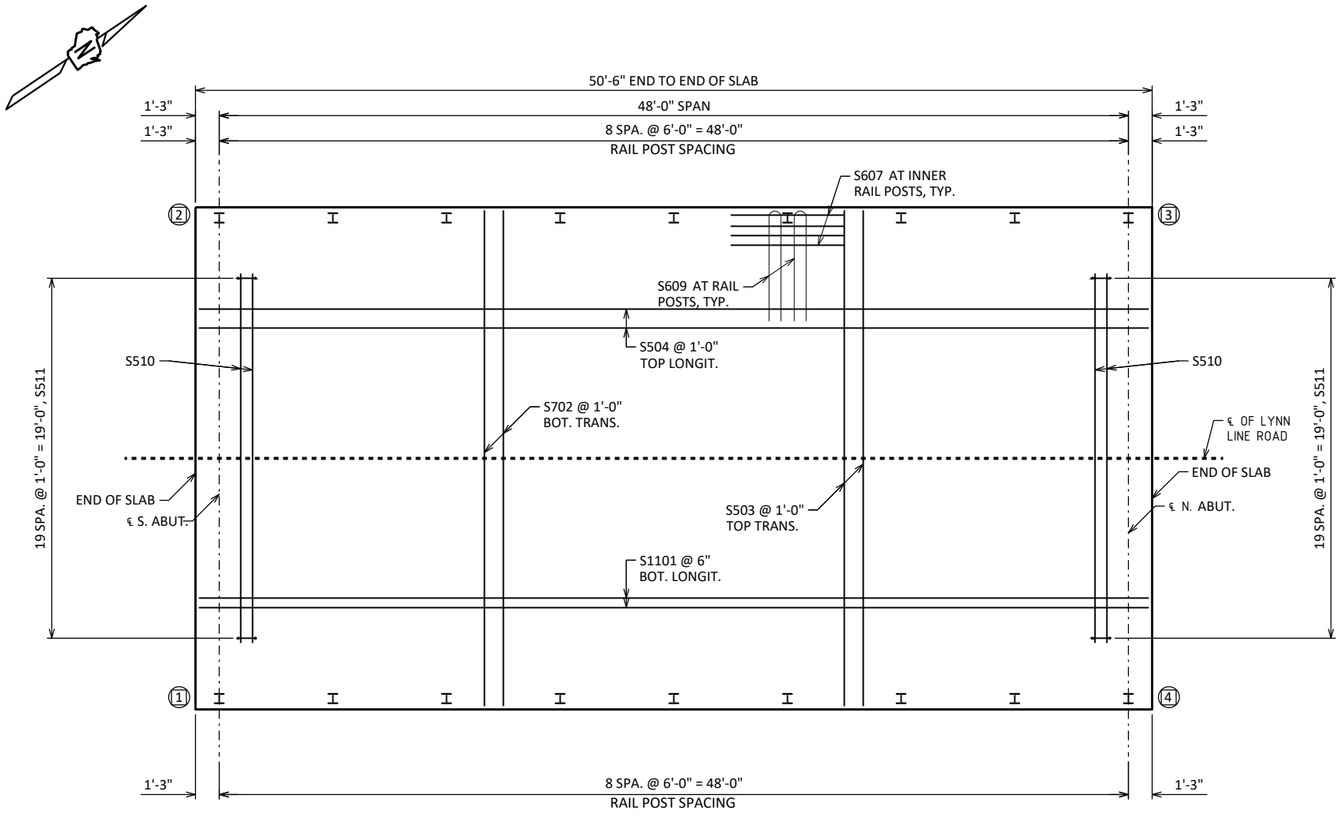


CROSS SECTION THRU ROADWAY

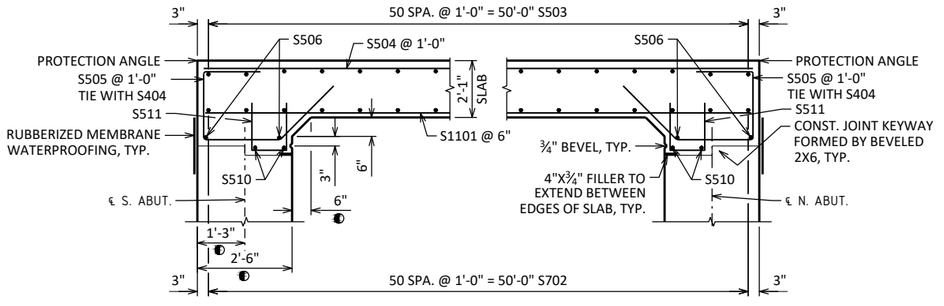
NOTES

TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).



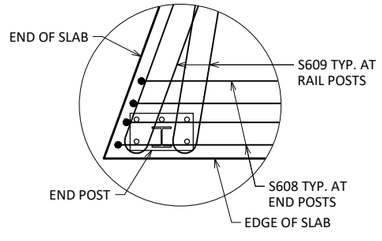
PLAN



LONGITUDINAL SECTION

DIMENSIONS ARE GIVEN PARALLEL TO ℄ ROADWAY UNLESS OTHERWISE NOTED.

◐ MEASURED NORMAL TO THE ℄ OF ABUTMENT. DIMENSIONS ARE TYPICAL FOR BOTH ABUTMENTS.



END POST DETAILS

◯ INDICATES WING NUMBER

THIS SHEET WAS CREATED BY THE WISDOT BUREAU OF STRUCTURES STANDARD BRIDGE DESIGN TOOL VERSION 0.9

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-71-204

DRAWN BY ZSS | PLANS CK'D DNS

SUPERSTRUCTURE

SHEET 8 OF 10
32

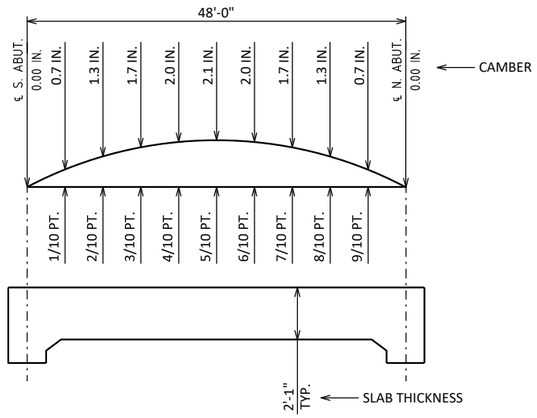
SCALE = 4

8

8

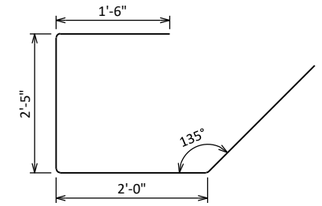
STATE PROJECT NUMBER

7397-01-70



CAMBER AND SLAB THICKNESS DIAGRAM

CAMBER SHOWN IS BASED ON 3 TIMES DEAD LOAD DEFLECTIONS. CAMBER SPANS AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT.



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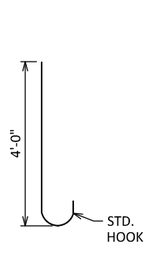

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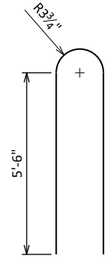

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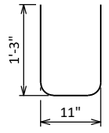


S511

BILL OF BARS

BAR MARK	COAT	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
S1101	X	53	50'-2"			SLAB BOTTOM LONGITUDINAL
S702	X	51	26'-2"			SLAB BOTTOM TRANSVERSE
S503	X	51	26'-2"			SLAB TOP TRANSVERSE
S504	X	27	50'-2"			SLAB TOP LONGITUDINAL
S505	X	54	7'-8"	X		ABUTMENT DIAPHRAGM STIRRUPS
S506	X	4	26'-2"			ABUTMENT DIAPHRAGM LONGITUDINAL
S607	X	56	6'-0"			SLAB TOP LONGIT. UNDER RAIL POSTS
S608	X	16	4'-8"	X		SLAB TOP LONGIT. UNDER RAIL END POSTS
S609	X	36	12'-0"	X		SLAB TOP HOOKS UNDER RAIL POSTS
S510	X	4	19'-6"			SLAB BOTTOM TRANS. AT ABUTS.
S511	X	40	3'-2"	X		SLAB VERT. AT ABUTS.

BENDING DIMENSIONS ARE OUT TO OUT OF BARS.

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

	TOP OF SLAB ELEVATION AT FINAL GRADE
LESS	SLAB THICKNESS
PLUS	CAMBER
PLUS	FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
EQUALS	TOP OF SLAB FALSEWORK ELEVATION

TOP OF SLAB ELEVATIONS

	℄ BRG. S. ABUT.	1/10	2/10	3/10	4/10	5/10	6/10	7/10	8/10	9/10	℄ BRG. N. ABUT.
W. EDGE OF DECK	1096.84	1096.94	1097.02	1097.09	1097.13	1097.16	1097.17	1097.16	1097.12	1097.07	1097.00
℄ OF LYNN LINE ROAD	1097.10	1097.20	1097.29	1097.35	1097.40	1097.43	1097.43	1097.42	1097.39	1097.34	1097.27
E. EDGE OF DECK	1096.84	1096.94	1097.02	1097.09	1097.13	1097.16	1097.17	1097.16	1097.12	1097.07	1097.00

SURVEY TOP OF SLAB ELEVATIONS

	ABUTMENT	5/10 PT.	ABUTMENT
W. EDGE OF SLAB			
℄OF LYNN LINE ROAD			
E. EDGE OF SLAB			

PRIOR TO RELEASING SLAB FORMWORK, TAKE TOP OF DECK ELEVATIONS AT THE ℄ OF ABUTMENTS, ℄ OF PIERS AND AT 5/10 PTS. TO VERIFY CAMBER. TAKE ELEVATIONS ALONG EDGE OF SLAB AND ℄. RECORD ELEVATIONS IN THE TABLE ABOVE FOR THE "AS BUILT" PLANS.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-71-204

DRAWN BY ZSS | PLANS CK'D DNS

SUPERSTRUCTURE DETAILS

SHEET 9 OF 10

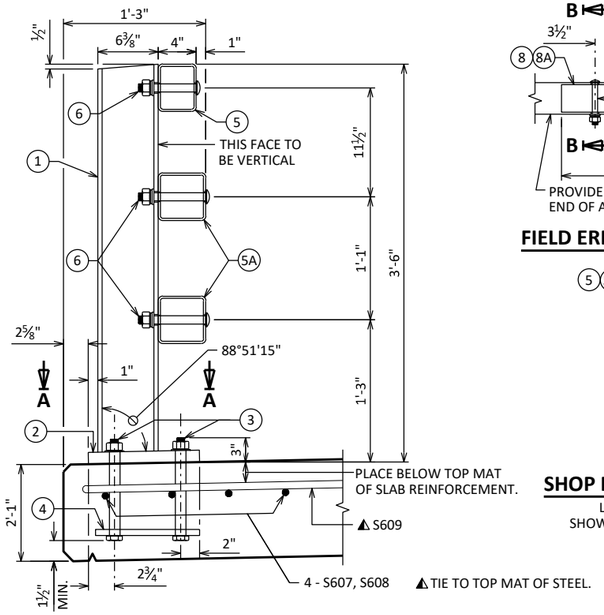
33

THIS SHEET WAS CREATED BY THE WISDOT BUREAU OF STRUCTURES STANDARD BRIDGE DESIGN TOOL VERSION 0.9

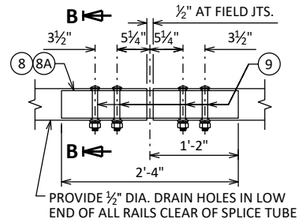
SCALE =

STATE PROJECT NUMBER

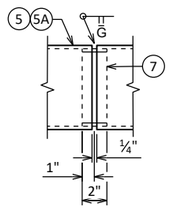
7397-01-70



SECTION THRU RAILING ON SLAB

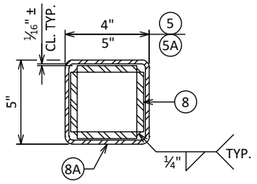


FIELD ERECTION JOINT DETAIL

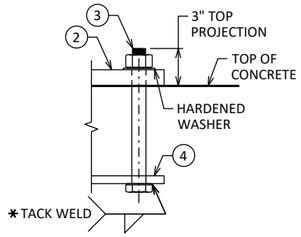


SHOP RAIL SPLICE DETAIL

LOCATION MUST BE SHOWN ON SHOP DRAWINGS

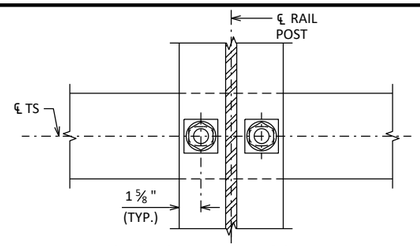


SECTION B-B

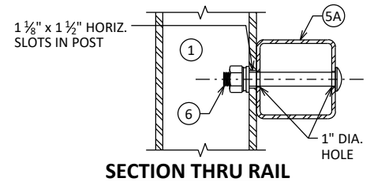


ANCHOR BOLTS

∗ ANCHOR BOLT ASSEMBLY MAY BE TACK WELDED, EITHER IN THE SHOP, OR IN THE FIELD AFTER THE ANCHOR PLATE IS PLACED.



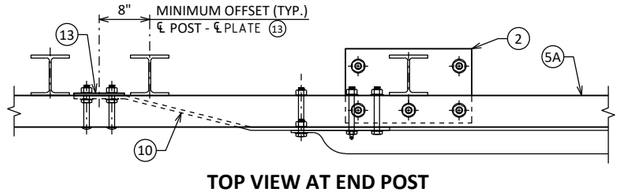
SECTION THRU POST WEB



SECTION THRU RAIL

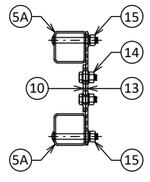
NOTE: CONNECTIONS AT LOWER RAILS SHOWN. CONNECTIONS AT TOP RAIL SIMILAR.

TYPICAL RAIL TO POST CONNECTIONS



TOP VIEW AT END POST

THRIE BEAM RAIL ATTACHMENT



SECTION C-C

SECTION D-D

LEGEND

(1) W6 x 25 WITH 1⅛ " X 1½ " HORIZ. SLOTS ON EACH SIDE OF POST FOR BOLT NO. 6. CUT BOTTOM OF POST TO MATCH CROSS SLOPE OF ROADWAY. PLACE POST VERTICAL. PLACE POSTS NORMAL TO GRADE LINE.

(2) PLATE 1 ¼" x 11 ¾" x 1'-8" WITH 1 7/16" OVERSIZED HOLES FOR ANCHOR BOLTS NO. 3. WELD TO NO. 1 AS SHOWN.

(3) ASTM A449 - 1 ⅛" DIA. ANCHOR BOLTS WITH NUT AND HARDENED WASHER (ALL GALVANIZED). 5 REQ'D. PER POST. THREAD 3" AND PLACE NORMAL TO PLATE NO. 2. CHAMFER TOP OF BOLTS BEFORE THREADING. USE 1'-9" LONG IN ABUTMENT WINGS. AT POSTS ON CONCRETE SLAB SUPERSTRUCTURES WHERE THE SLAB THICKNESS IS > 16" USE 1'-3" LONG. USE 10 ¾" LONG AT ALL OTHER LOCATIONS. (AN EQUIVALENT THREADED ROD WITH NUTS AND HARDENED WASHERS MAY BE SUBSTITUTED FOR ANCHOR BOLTS IN WINGS IF REQ'D. FOR CONSTRUCTIBILITY.)

(4) ⅝" x 11" x 1'-8" ANCHOR PLATE (GALVANIZED) WITH 1 3/16" DIA. HOLES FOR ANCHOR BOLTS NO. 3

(5) TS 5 x 4 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.

(5A) TS 5 x 5 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.

(6) ⅞" DIA. A325 SLOTTED ROUND HEAD BOLT WITH NUT, 3/16" X 1 ⅝" X 1 ⅝" MIN. WASHER, AND LOCK WASHER (2 REQ'D. AT EACH RAIL TO POST LOCATION.)

(7) SPLICE SLEEVE FABRICATED FROM ¼" PLATE. PROVIDE "SLIDING FIT".

(8) ⅜" X 3 ⅝" X 2'-4" PLATE. 2 PER RAIL. USED IN NO. 5 & 5A.

(8A) ⅜" X 2 ⅝" X 2'-4" PLATE USED IN NO. 5, ⅜" X 3 ⅝" X 2'-4" PLATE USED IN NO. 5A. 2 PER RAIL.

(9) ⅞" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER, AND LOCK WASHER. USE 15/16" x 1 ¼" LONGIT. SLOTTED HOLES IN PLATE NO. 10A AT FIELD JOINTS AND 15/16" X 2 ¼" MIN. LONGIT. SLOTTED HOLES AT EXP. JOINTS IN PLATE NO. 8A. PROVIDE 15/16" DIA. ROUND HOLES IN TUBES NO. 5 AND NO. 5A.

(10) ½" THK. BACK-UP PLATE WITH 2 - ⅞" X 1 ½" THREADED SHOP WELDED STUDS (NO. 12). BOLT TO RAIL AS SHOWN IN DETAIL. REQUIRED AT THRIE BEAM GUARD RAIL ATTACHMENTS ONLY. PLACE SYMMETRICALLY ABOUT TUBES NO. 5A.

(11) 1"DIA. HOLES IN PLATE NO. 10 & TUBES NO. 5A FOR ⅞" DIA. A325 BOLTS WITH HEX NUTS AND WASHERS. 6 HOLES IN TUBES AND PLATE NO. 10.

(12) ⅞" DIA. X 1½" LONG THREADED SHOP WELDED STUDS (2 REQ'D).

(13) ⅜" X 8" X 1'-6" PLATE. BOLT TO RAIL AS SHOWN IN DETAIL. REQ'D. AT THRIE BEAM GUARD RAIL ATTACHMENTS ONLY. PLACE SYM. ABOUT TUBES NO. 5A.

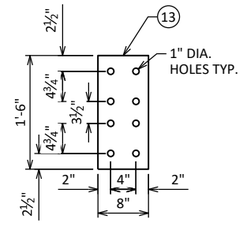
(14) ⅞" DIA. X 2" LONG A325 HEX BOLT WITH NUT AND WASHER (5 REQ'D.).

(15) 1" DIA. HOLES IN TUBES NO. 5A FOR ⅞" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER AND LOCK WASHER (4 REQ'D.). 4 HOLES IN TUBES.

GENERAL NOTES

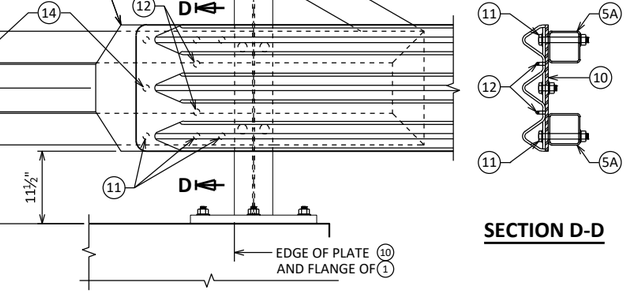
1. BID ITEM SHALL BE "RAILING TUBULAR TYPE M" WHICH INCLUDES ALL ITEMS SHOWN.
2. RAIL POST AND BASE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 50. HOLLOW RAILING STRUCTURAL TUBING SHALL CONFORM TO THE REQUIREMENTS OF ASTM A500 GRADE B OR C WITH A CERTIFIED FY = 50 KSI. ANCHOR PLATES, AND SPLICE TUBE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 36.
3. THE NUT SECURING THE POST BASE PLATE TO THE CONCRETE SHALL BE TIGHTENED TO A SNUG FIT AND GIVEN AN ADDITIONAL ⅛ TURN.
4. RAILS SHALL BE CONTINUOUS OVER A MINIMUM OF THREE (3) POSTS WITHOUT SPLICES WHERE POSSIBLE. RAILS SHALL BE SPLICED IN A PANEL OVER EXPANSION JOINTS.
5. ENDS OF TUBE SECTIONS SHALL BE SAWED. GRIND SMOOTH EXPOSED EDGES. ALL CUT ENDS SHALL BE TRUE AND SMOOTH.
6. WELD IS THE SAME ON BOTH FLANGES. FLANGE WELD DOES NOT REQUIRE MAGNETIC PARTICLE TESTING.
7. FILL BOLT SLOT OPENINGS IN POST SHIMS AND PLATE NO. 2 AND CAULK AROUND PERIMETER OF PLATE NO. 2 WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. STEEL POST SHIMS MAY BE USED UNDER POSTS WHERE REQ'D. FOR ALIGNMENT.
8. POST BASE PLATES SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUT.
9. ALL MATERIAL SHALL BE GALVANIZED AFTER FABRICATION. PRIOR TO GALVANIZING, ALL STEEL RAILING POSTS & STEEL TUBING SHALL BE GIVEN A NO. 6 BLAST CLEANING BY SSPC SPECIFICATIONS.

SECTION A-A



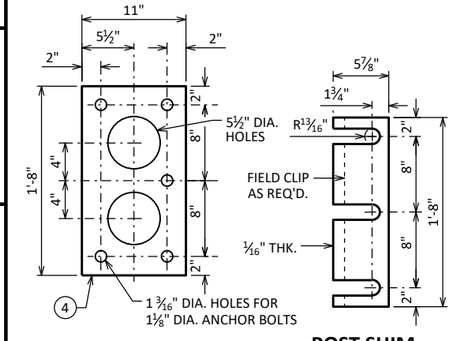
ANCHOR PLATE

AT BEAM GUARD ATTACHMENT



DETAIL AT END POST

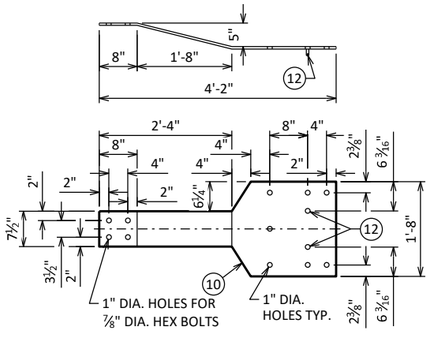
THRIE BEAM RAIL ATTACHMENT



ANCHOR PLATE

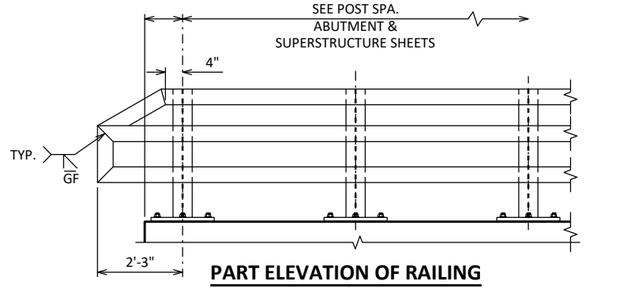
AT RAIL TO DECK CONNECTION

POST SHIM DETAIL



BACK-UP PLATE DETAIL

AT BEAM GUARD ATTACHMENT



PART ELEVATION OF RAILING

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-71-204

DRAWN BY ZSS | PLANS CK'D DNS

TUBULAR STEEL RAILING TYPE 'M'

SHEET 10 OF 10

34

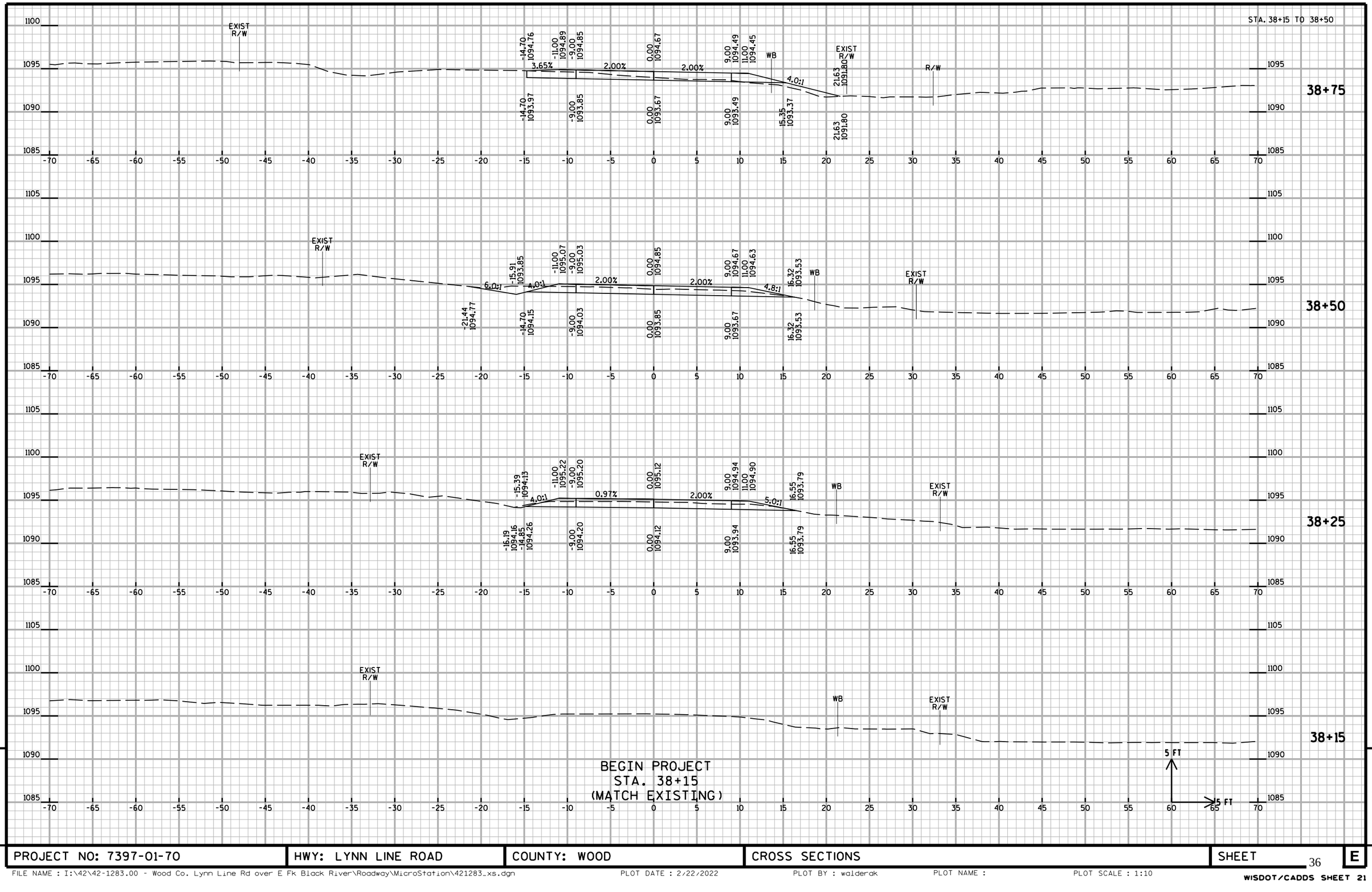
THIS SHEET WAS CREATED BY THE WISDOT BUREAU OF STRUCTURES STANDARD BRIDGE DESIGN TOOL VERSION 0.9

SCALE =

COMPUTER EARTHWORK

Station	Distance	Area (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		Mass Ordinate
		Cut	Fill	Cut	Fill	Cut	Expanded Fill	
						1.00	1.30	
				Note 1	Note 2	Note 1		Note 3
38+15.00		27.8	0.0					
38+25.00	10	18.6	0.0	9	0	9	0	9
38+50.00	25	21.5	0.0	19	0	27	0	27
38+75.00	25	11.1	3.7	15	2	42	2	40
39+00.00	25	6.9	16.9	8	10	51	15	36
39+25.00	25	2.3	31.6	4	22	55	44	11
39+50.00	25	12.1	25.8	7	27	62	78	-17
39+58.75	9	9.1	5.7	3	5	65	85	-20
39+74.75	16	9.1	5.7	5	3	70	89	-19
BRIDGE	--	--	--	--	--	--	--	--
40+25.25	--	78.6	76.6	--	--	--	--	--
40+41.25	16	78.6	76.6	47	45	117	148	-31
40+50.00	9	62.4	44.3	23	20	140	174	-34
40+75.00	25	45.8	8.6	50	24	190	206	-16
41+00.00	25	34.4	1.7	37	5	227	212	15
41+15.00	15	22.6	0.0	16	0	243	212	31
				243	163			

STA. 38+15 TO 38+50

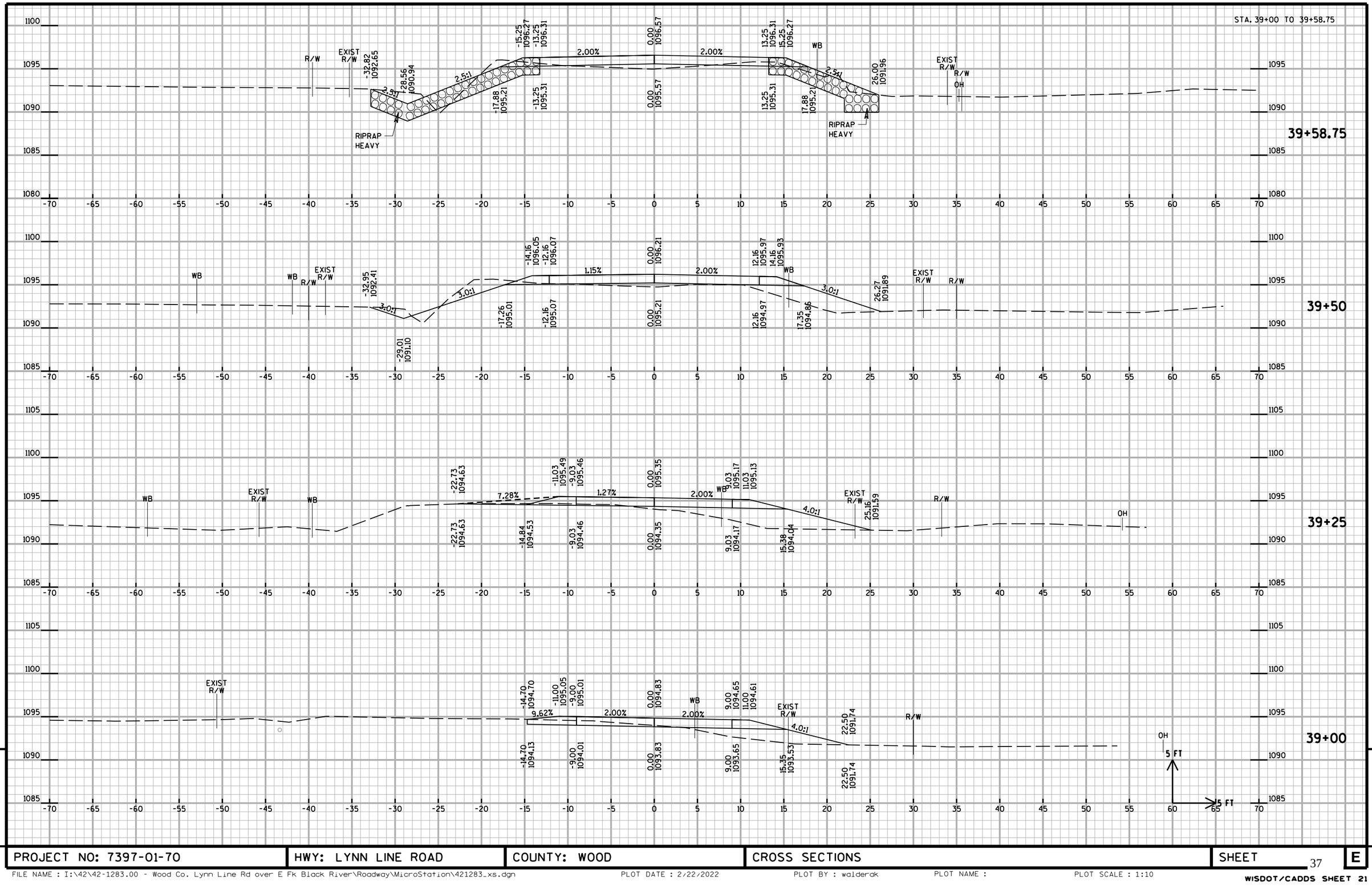


PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD	CROSS SECTIONS	SHEET	E

FILE NAME : I:\42\42-1283.00 - Wood Co. Lynn Line Rd over E Fk Black River\Roadway\MicroStation\421283_xs.dgn PLOT DATE : 2/22/2022 PLOT BY : walderak PLOT NAME : PLOT SCALE : 1:10

WISDOT/CADDS SHEET 21

STA. 39+00 TO 39+58.75

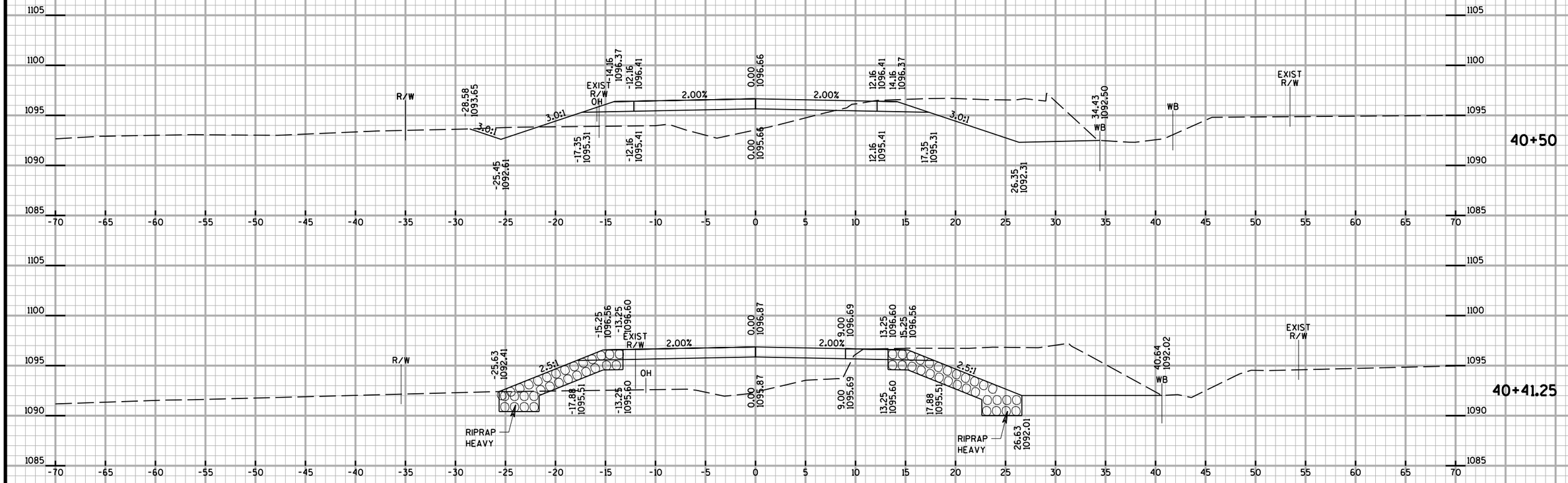


PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD	CROSS SECTIONS	SHEET	E

FILE NAME : I:\42\42-1283.00 - Wood Co. Lynn Line Rd over E Fk Black River\Roadway\MicroStation\421283_xs.dgn PLOT DATE : 2/22/2022 PLOT BY : walderak PLOT NAME : PLOT SCALE : 1:10

WISDOT/CADDS SHEET 21

STA. 40+41.25 TO 40+50



STRUCTURE B-71-0204

5 FT
5 FT

9 9

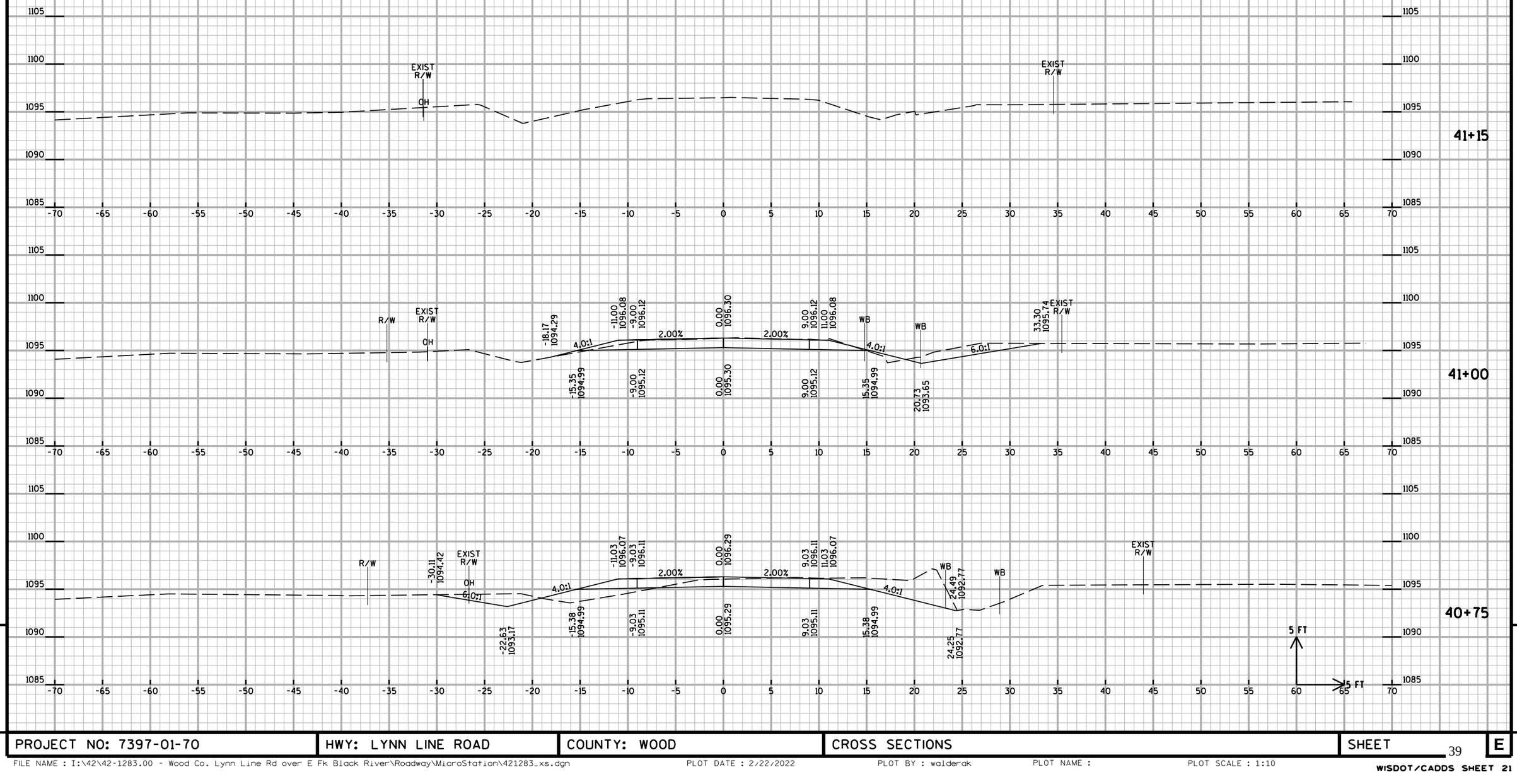
PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD	CROSS SECTIONS	SHEET 38	E

FILE NAME : I:\42\42-1283.00 - Wood Co. Lynn Line Rd over E Fk Black River\Roadway\MicroStation\421283_xs.dgn PLOT DATE : 2/22/2022 PLOT BY : walderak PLOT NAME : PLOT SCALE : 1:10

WISDOT/CADDS SHEET 21

STA. 40+75 TO 41+15

END PROJECT
STA. 41+15
(MATCH EXISTING)



PROJECT NO: 7397-01-70	HWY: LYNN LINE ROAD	COUNTY: WOOD	CROSS SECTIONS	SHEET	E

FILE NAME : I:\42\42-1283.00 - Wood Co. Lynn Line Rd over E Fk Black River\Roadway\MicroStation\421283_xs.dgn PLOT DATE : 2/22/2022 PLOT BY : walderak PLOT NAME : PLOT SCALE : 1:10

WISDOT/CADDS SHEET 21

Wisconsin Department of Transportation

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