

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1016-01-63	WISC 2024019	1

# STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

## PLAN OF PROPOSED IMPROVEMENT

# TOMAH - PORTAGE

MIRROR LAKE BRIDGE TO STH 33

IH-90

SAUK AND COLUMBIA COUNTIES

STATE PROJECT NUMBER  
**1016-01-63**

ORDER OF SHEETS

Section No.	Title
1	Title
2	Typical Sections and Details
3	Estimate of Quantities
3	Miscellaneous Quantities
<del>4</del>	<del>Right of Way Plat</del>
5	Plan and Profile
6	Standard Detail Drawings
<del>7</del>	<del>Sign Plates</del>
<del>8</del>	<del>Structure Plans</del>
<del>9</del>	<del>Computer Earthwork Data</del>
<del>9</del>	<del>Cross Sections</del>

TOTAL SHEETS = 56



**01**

DESIGN DESIGNATION

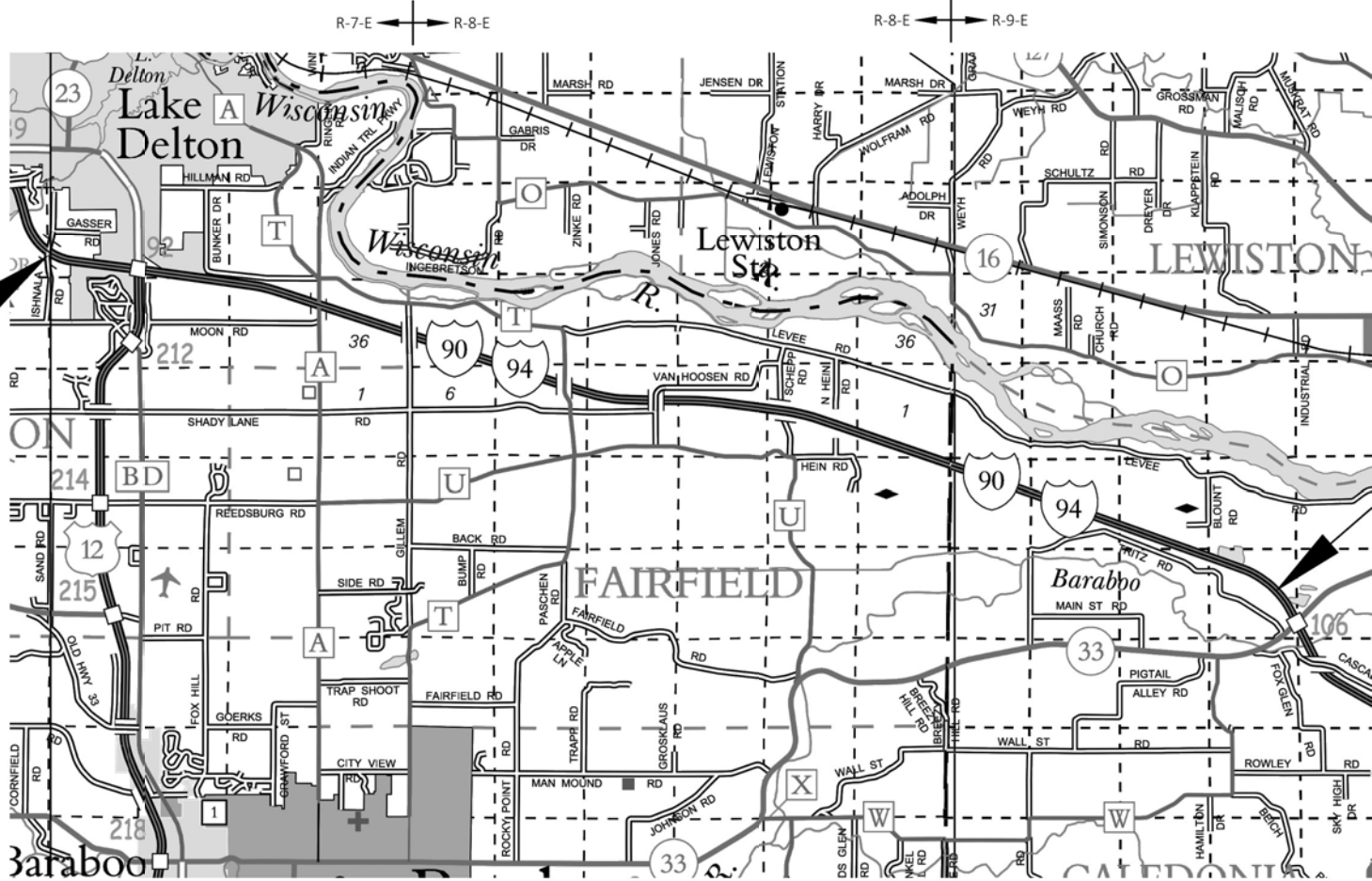
A.A.D.T.	2016	=	40,200
A.A.D.T.	2043	=	48,900
D.H.V.		=	17.1
D.D.		=	58/42
T.		=	28.6
DESIGN SPEED		=	70 M.P.H.
ESALS		=	61,000,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

**BEGIN PROJECT**  
STA 302+85  
X= 636000.01  
Y= 274633.71

**END PROJECT**  
STA 1044+23



LAYOUT  
SCALE 0 2 MI

TOTAL NET LENGTH OF CENTERLINE = 14.060

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), SAUK COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY  
 Surveyor \_\_\_\_\_ WISDOT  
 Designer \_\_\_\_\_ BRIAN DAHL  
 Project Manager \_\_\_\_\_ CORY MIKSHOWSKY  
 Regional Examiner \_\_\_\_\_ SW REGION  
 Regional Supervisor \_\_\_\_\_ JAMES SAVOLDELLI

APPROVED FOR THE DEPARTMENT  
 DATE: 7/31/23 \_\_\_\_\_  
 (Signature)

**E**

**GENERAL NOTES**

- THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.
- HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.
- PLACE THE 3.5" HMA PAVEMENT IN A SINGLE LAYER
- STATIONING OF CONCRETE PAVEMENT REPAIR AND REPLACEMENT LOCATIONS IN THE PLAN ARE APPROXIMATE. VERIFY LOCATIONS WITH ENGINEER
- SAWING CONCRETE WILL BE MEASURED AND PAID FOR ONLY ONCE WHEN REPAIRS ARE ADJACENT ALONG THE CENTERLINE OF THE LANES
- OVER-SAWING INTO PAVEMENT THAT IS TO REMAIN TO FACILITATE REMOVAL OF REPAIR AREAS SHALL BE SEALED WITH AN APPROVED EPOXY. THE OVER-SAWING AND EPOXY ARE INCIDENTAL TO BID ITEMS 416.1715, 416.1725.
- WITHIN CONCRETE PAVEMENT REPLACEMENT SECTIONS, THE NEW CONTRACTION JOINTS MAY NOT MATCH ABUTTING EXISTING JOINTS BECAUSE OF CHANGES TO STANDARDS AND PRIOR REPAIRS. ADJUST NEW JOINTS TO MATCH EXISTING JOINTS WHERE PRACTICAL. DO NOT EXCEED THE MAXIMUM CONTRACTION JOINT SPACING OF 15'.
- THE NEW TINING SHOULD MATCH THE ADJACENT PAVEMENT PATTERN.

**STANDARD ABBREVIATIONS**

AC	ACRE	LC.	LONG CHORD
AGG	AGGREGATE	LS	LUMP SUM
<	ANGLE	M.P.	MARKER POST
AE, AEW	APRON ENDWALL	MGAL	1000 GALLONS
ASPH.	ASPHALTIC	N.C.	NORMAL CROWN
A.D.T.	AVERAGE DAILY TRAFFIC	N	NORTH
A.A.D.T.	ANNUAL AVERAGE DAILY TRAFFIC	NB	NORTHBOUND
B.F.	BACK FACE	NOR	NORMAL
BM	BENCHMARK	NO.	NUMBER
BTWN	BETWEEN	PAV'T	PAVEMENT
CTR.	CENTER	P.L.E.	PERMANENT LIMITED EASEMENT
C/L	CENTER LINE	P.C.	POINT OF CURVATURE
Δ	CENTRAL ANGLE OR DELTA	P.I.	POINT OF INTERSECTION
C.E.	COMMERCIAL ENTRANCE	P.T.	POINT OF TANGENCY
CONST.	CONSTRUCTION	PCC	PORTLAND CEMENT CONCRETE
CMCP	CORRUGATED METAL CULVERT PIPE	P.E.	PRIVATE ENTRANCE
CMP	CORRUGATED METAL PIPE	PGL	PROFILE GRADE LINE
CO.	COUNTY	P.L.	PROPERTY LINE
CTH	COUNTY TRUNK HIGHWAY	R	RADIUS OR RANGE
CR.	CREEK	R/L	REFERENCE LINE
CABC	CRUSHED AGGREGATE BASE COURSE	R.C.C.P.	REINFORCED CONCRETE CULVERT PIPE
CY	CUBIC YARD	REQ'D	REQUIRED
CP	CONTROL POINT OR CULVERT PIPE	RT	RIGHT
C&G	CURB AND GUTTER	R.H.F.	RIGHT HAND FORWARD
D	DEGREE OF CURVE	R/W	RIGHT OF WAY
D.H.V.	DESIGN HOURLY VOLUME	RD.	ROAD
DIA.	DIAMETER	SHLD.	SHOULDER(S)
D.D.	DIRECTIONAL DISTRIBUTION	SHR.	SHRINKAGE
DISCH.	DISCHARGE	S	SOUTH
DMS	DYNAMIC MESSAGE SIGN	SB	SOUTHBOUND
EA	EACH	S.F.	SQUARE FOOT (FEET)
E	EAST	SDD	STANDARD DETAIL DRAWING(S)
EB	EASTBOUND	STH	STATE TRUNK HIGHWAY
ELEC.	ELECTRIC(AL), ELEC. CABLE	STA.	STATION
EL., ELEV.	ELEVATION	S.E.	SUPERELEVATION
ESALS	EQUIVALENT SINGLE AXLE LOADS	S/L	SURVEY LINE
EXC.	EXCAVATION	SYM	SYMMETRICAL
EXIST	EXISTING	T.	PERCENT TRUCKS
F.F.	FACE TO FACE	TEL.	TELEPHONE
FERT.	FERTILIZER	TEMP.	TEMPORARY
F.E.	FIELD ENTRANCE	T.L.E.	TEMPORARY LIMITED EASEMENT
F/L, F.L.	FLOW LINE	T.O.C.	TOP OF CURB
GALV.	GALVANIZE	TYP	TYPICAL
H.S.	HIGH STRENGTH	UNCL.	UNCLASSIFIED
CWT	HUNDRED WEIGHT	U.G.	UNDERGROUND (CABLE)
INL	INLET	VAR	VARIABLE
INTER.	INTERSECTION	V.C.	VERTICAL CURVE
IH	INTERSTATE HIGHWAY	V.P.C.	VERTICAL POINT OF CURVATURE
JT.	JOINT	V.P.I.	VERTICAL POINT OF INTERSECTION
LT	LEFT	V.P.T.	VERTICAL POINT OF TANGENCY
L.H.F.	LEFT HAND FORWARD	WT.	WEIGHT
L.	LENGTH OF CURVE	W	WEST
L.F.	LINEAR FOOT(FEET)	WB	WESTBOUND

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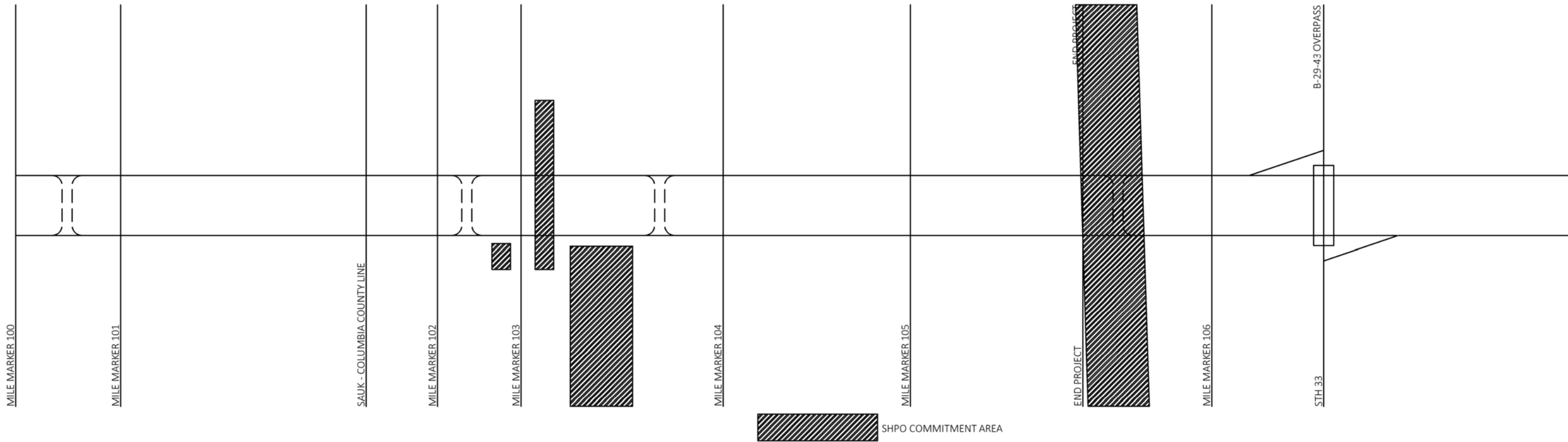
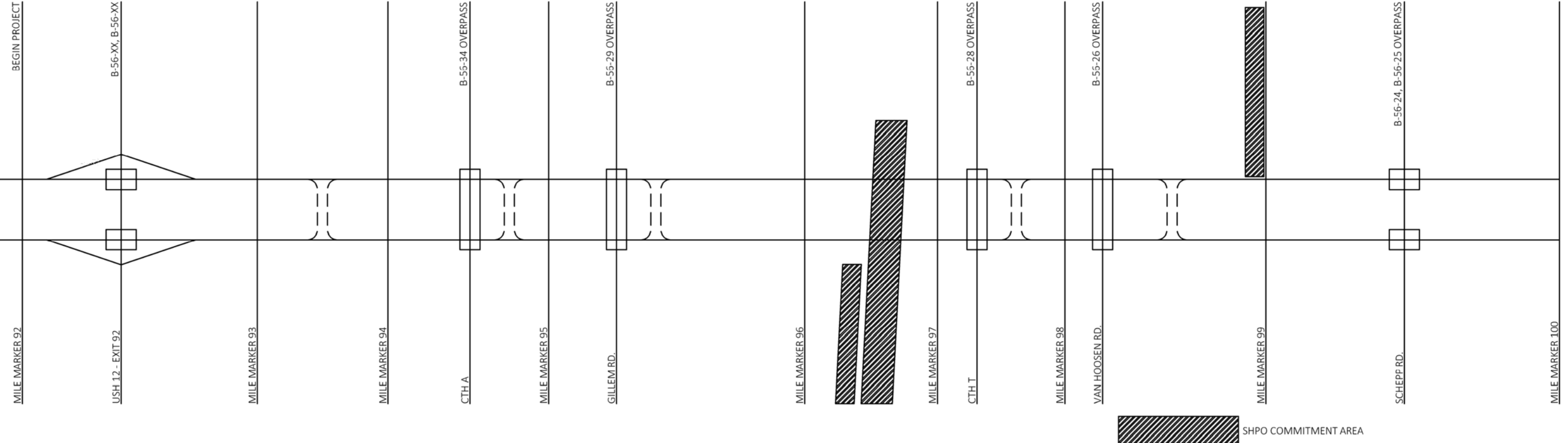
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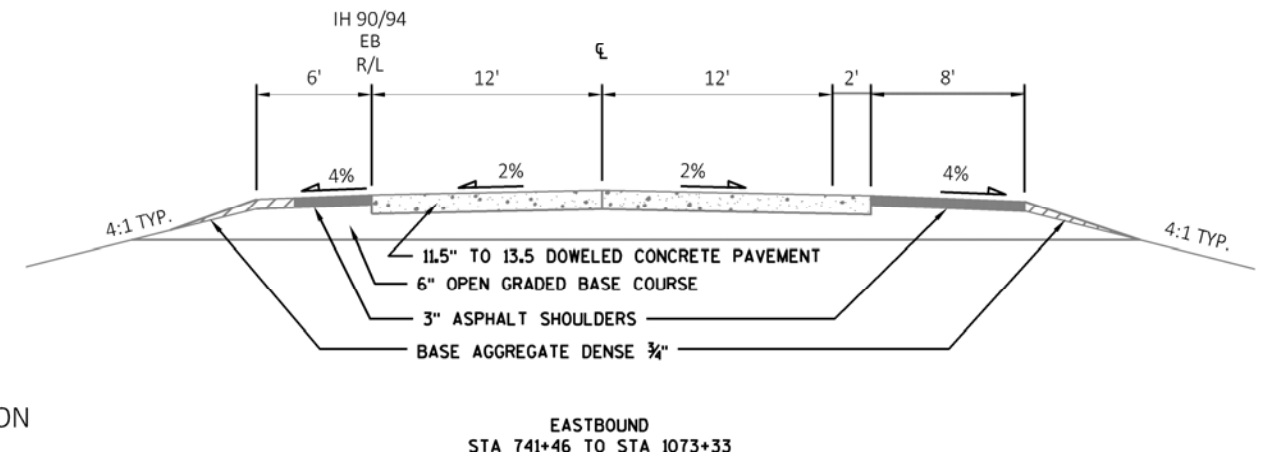
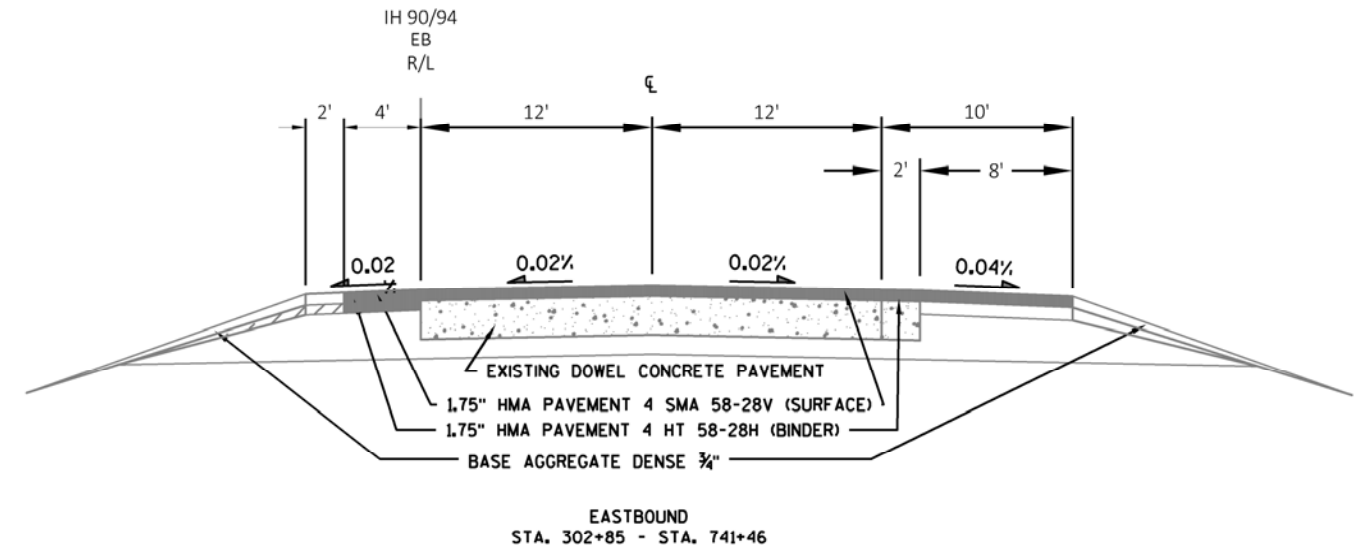
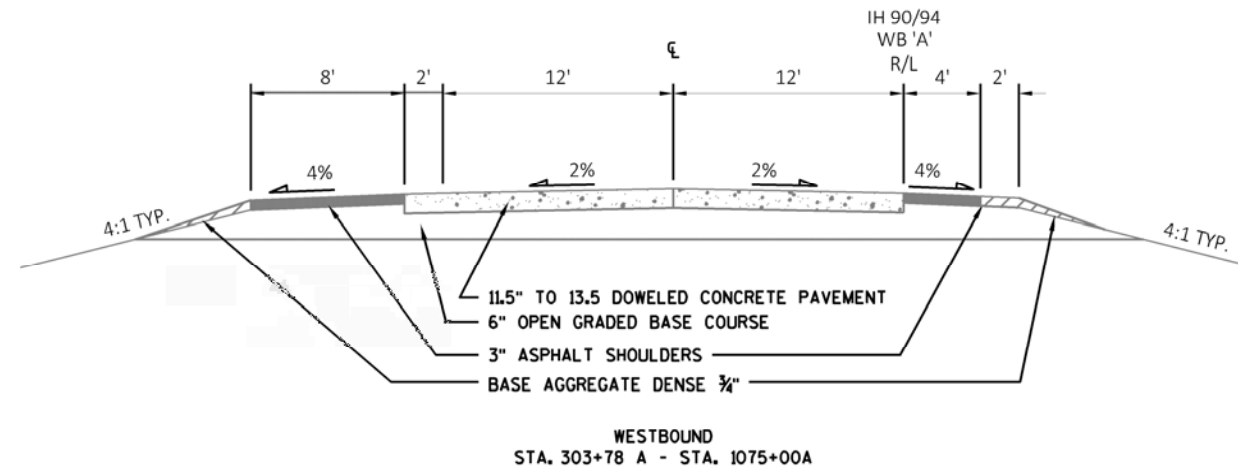
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**DNR LIAISON**

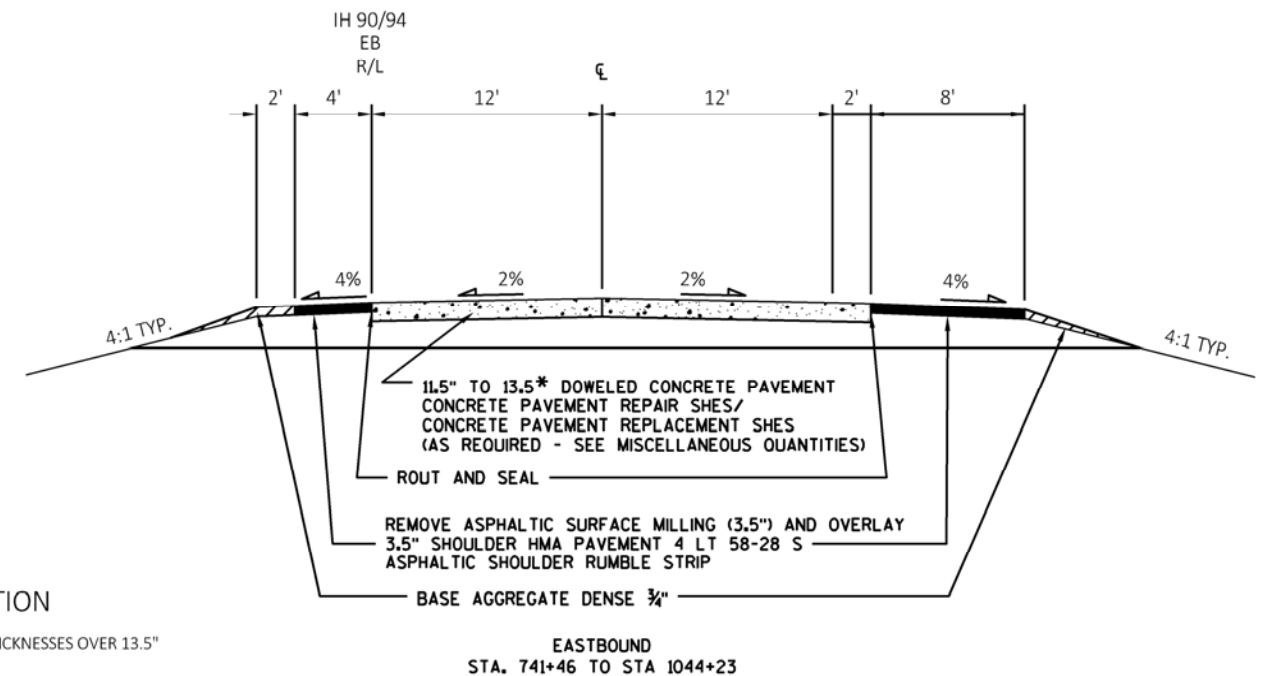
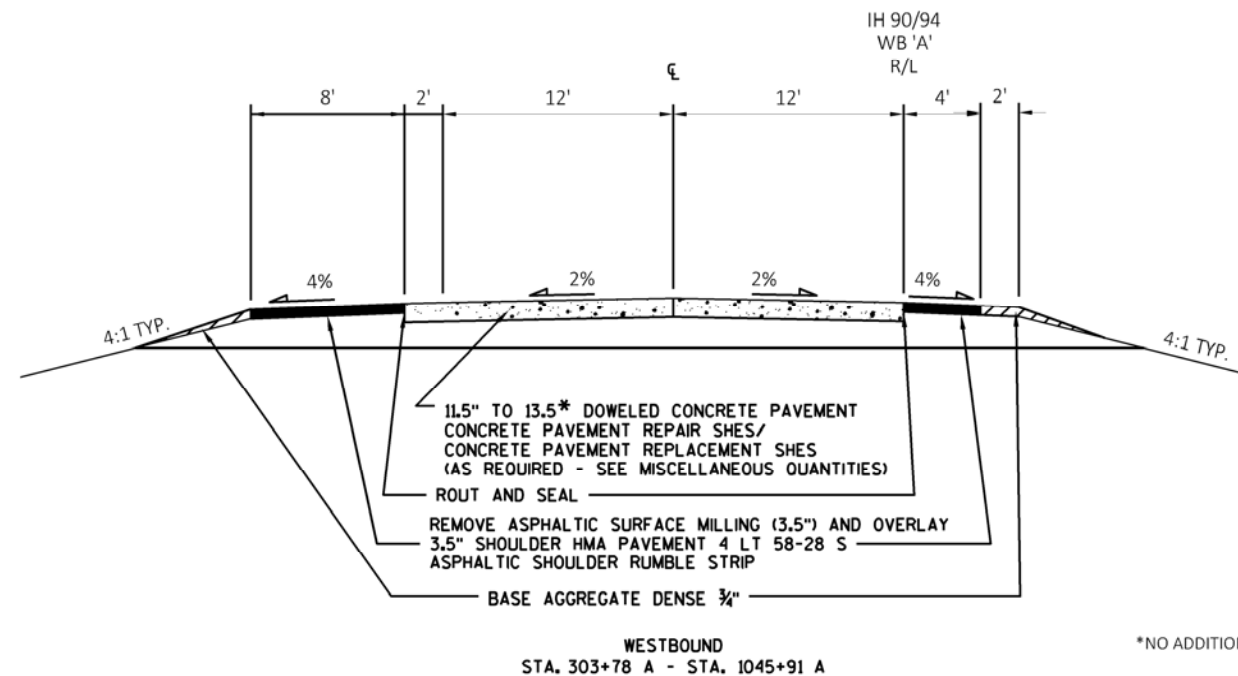
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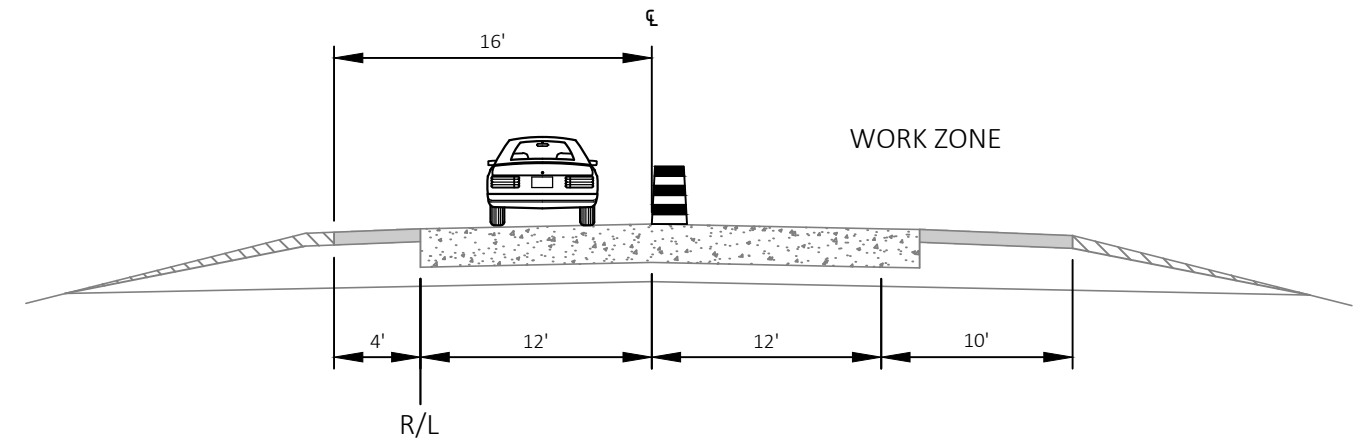
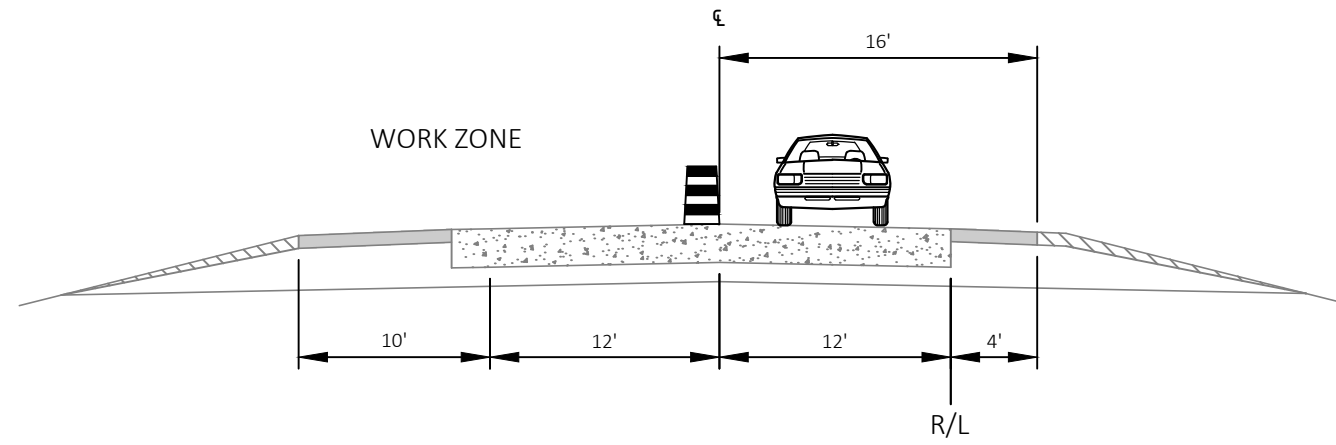


TYPICAL EXISTING SECTION

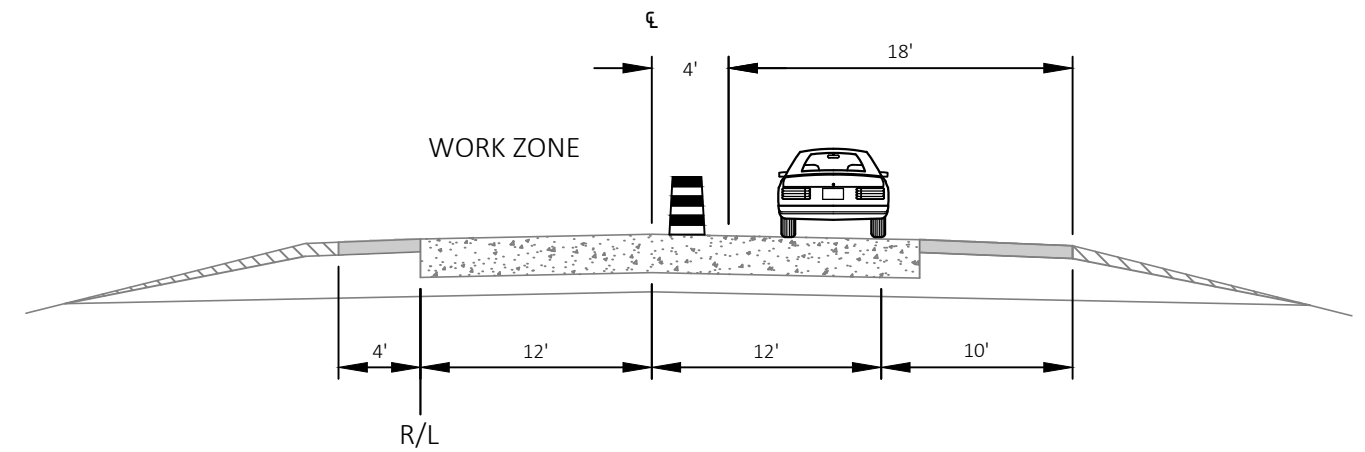
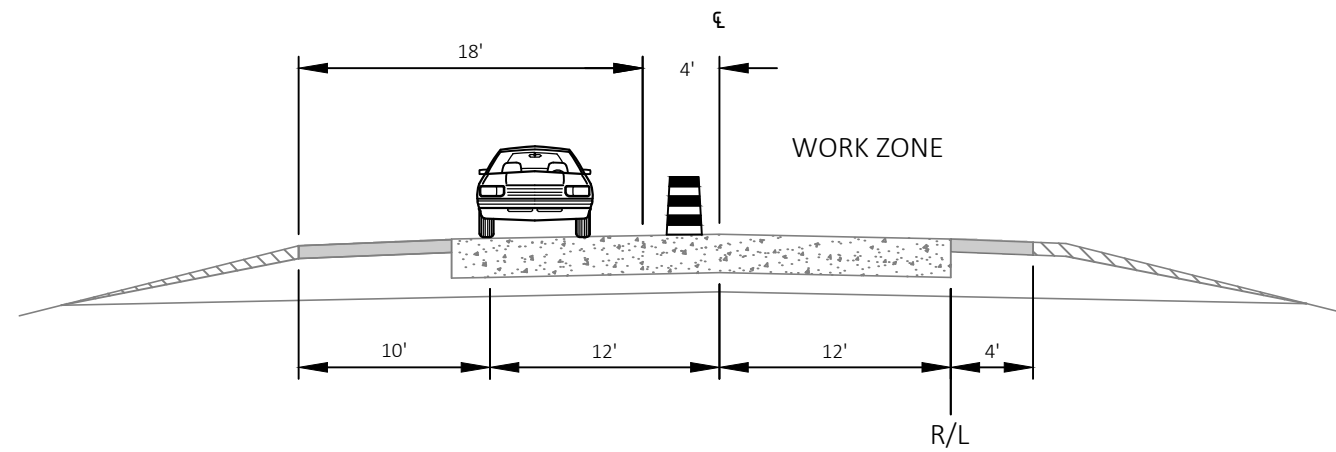


TYPICAL PROPOSED SECTION

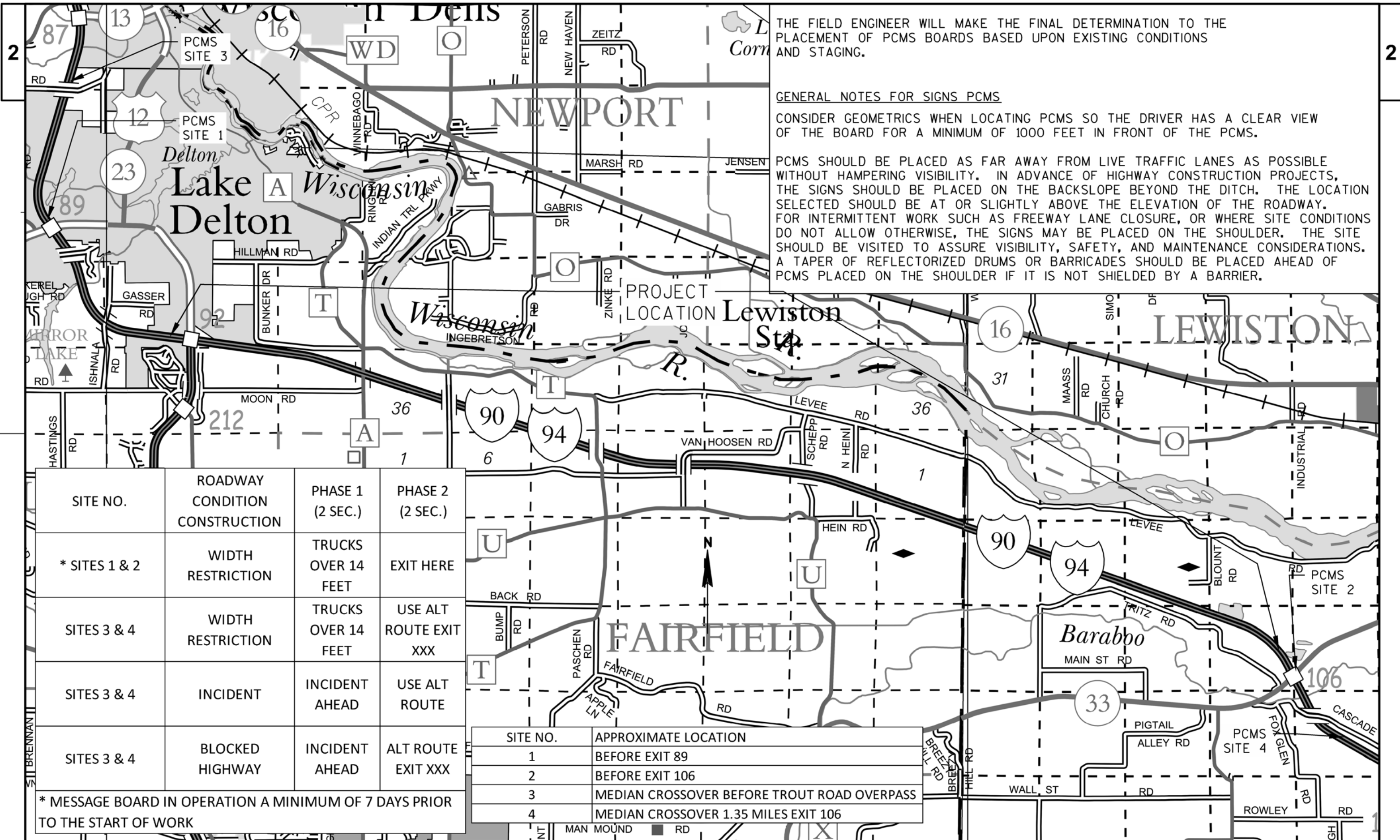
\*NO ADDITIONAL COMPENSATION SHALL BE PAID FOR THICKNESSES OVER 13.5"



TYPICAL SECTION: OUTSIDE LANE CLOSED



TYPICAL SECTION: MEDIAN LANE CLOSED



THE FIELD ENGINEER WILL MAKE THE FINAL DETERMINATION TO THE PLACEMENT OF PCMS BOARDS BASED UPON EXISTING CONDITIONS AND STAGING.

GENERAL NOTES FOR SIGNS PCMS

CONSIDER GEOMETRICS WHEN LOCATING PCMS SO THE DRIVER HAS A CLEAR VIEW OF THE BOARD FOR A MINIMUM OF 1000 FEET IN FRONT OF THE PCMS.

PCMS SHOULD BE PLACED AS FAR AWAY FROM LIVE TRAFFIC LANES AS POSSIBLE WITHOUT HAMPERING VISIBILITY. IN ADVANCE OF HIGHWAY CONSTRUCTION PROJECTS, THE SIGNS SHOULD BE PLACED ON THE BACKSLOPE BEYOND THE DITCH. THE LOCATION SELECTED SHOULD BE AT OR SLIGHTLY ABOVE THE ELEVATION OF THE ROADWAY. FOR INTERMITTENT WORK SUCH AS FREEWAY LANE CLOSURE, OR WHERE SITE CONDITIONS DO NOT ALLOW OTHERWISE, THE SIGNS MAY BE PLACED ON THE SHOULDER. THE SITE SHOULD BE VISITED TO ASSURE VISIBILITY, SAFETY, AND MAINTENANCE CONSIDERATIONS. A TAPER OF REFLECTORIZED DRUMS OR BARRICADES SHOULD BE PLACED AHEAD OF PCMS PLACED ON THE SHOULDER IF IT IS NOT SHIELDED BY A BARRIER.

SITE NO.	ROADWAY CONDITION CONSTRUCTION	PHASE 1 (2 SEC.)	PHASE 2 (2 SEC.)
* SITES 1 & 2	WIDTH RESTRICTION	TRUCKS OVER 14 FEET	EXIT HERE
SITES 3 & 4	WIDTH RESTRICTION	TRUCKS OVER 14 FEET	USE ALT ROUTE EXIT XXX
SITES 3 & 4	INCIDENT	INCIDENT AHEAD	USE ALT ROUTE
SITES 3 & 4	BLOCKED HIGHWAY	INCIDENT AHEAD	ALT ROUTE EXIT XXX

\* MESSAGE BOARD IN OPERATION A MINIMUM OF 7 DAYS PRIOR TO THE START OF WORK

SITE NO.	APPROXIMATE LOCATION
1	BEFORE EXIT 89
2	BEFORE EXIT 106
3	MEDIAN CROSSOVER BEFORE TROUT ROAD OVERPASS
4	MEDIAN CROSSOVER 1.35 MILES EXIT 106

Estimate Of Quantities

1016-01-63

Line	Item	Item Description	Unit	Total	Qty
0002	204.0120	Removing Asphaltic Surface Milling	SY	184,900.000	184,900.000
0004	204.0170	Removing Fence	LF	138,900.000	138,900.000
0006	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	2,082.000	2,082.000
0008	213.0100	Finishing Roadway (project) 01. 1016-01-63	EACH	1.000	1.000
0010	305.0110	Base Aggregate Dense 3/4-Inch	TON	4,800.000	4,800.000
0012	415.6000.S	Rout and Seal	LF	208,074.000	208,074.000
0014	416.0610	Drilled Tie Bars	EACH	560.000	560.000
0016	416.0620	Drilled Dowel Bars	EACH	18,560.000	18,560.000
0018	416.1715	Concrete Pavement Repair SHES	SY	10,500.000	10,500.000
0020	416.1725	Concrete Pavement Replacement SHES	SY	800.000	800.000
0022	460.2000	Incentive Density HMA Pavement	DOL	17,600.000	17,600.000
0024	460.5224	HMA Pavement 4 LT 58-28 S	TON	27,500.000	27,500.000
0026	465.0510	Asphaltic Rumble Strips, Shoulder Divided Roadway	LF	208,074.000	208,074.000
0028	616.0100	Fence Woven Wire (height) 01. 4-FT	LF	138,900.000	138,900.000
0030	618.0100	Maintenance And Repair of Haul Roads (project) 01. 1016-01-63	EACH	1.000	1.000
0032	619.1000	Mobilization	EACH	1.000	1.000
0034	624.0100	Water	MGAL	96.000	96.000
0036	642.5201	Field Office Type C	EACH	1.000	1.000
0038	643.0300	Traffic Control Drums	DAY	40,989.000	40,989.000
0040	643.0420	Traffic Control Barricades Type III	DAY	3,111.000	3,111.000
0042	643.0705	Traffic Control Warning Lights Type A	DAY	6,222.000	6,222.000
0044	643.0715	Traffic Control Warning Lights Type C	DAY	300.000	300.000
0046	643.0800	Traffic Control Arrow Boards	DAY	150.000	150.000
0048	643.0900	Traffic Control Signs	DAY	1,200.000	1,200.000
0050	643.1051	Traffic Control Signs PCMS with Cellular Communications	DAY	150.000	150.000
0052	643.1205.S	Basic Traffic Queue Warning System	DAY	75.000	75.000
0054	643.4100	Traffic Control Interim Lane Closure	EACH	75.000	75.000
0056	643.5000	Traffic Control	EACH	1.000	1.000
0058	646.2020	Marking Line Epoxy 6-Inch	LF	341,600.000	341,600.000
0060	646.4020	Marking Line Epoxy 10-Inch	LF	1,500.000	1,500.000
0062	650.8000	Construction Staking Resurfacing Reference	LF	104,037.000	104,037.000
0064	650.9911	Construction Staking Supplemental Control (project) 01. 1016-01-63	EACH	1.000	1.000
0066	690.0250	Sawing Concrete	LF	37,330.000	37,330.000
0068	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	2,400.000	2,400.000
0070	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	3,200.000	3,200.000



CONCRETE REPAIR SUMMARY

CONCRETE REPAIR SUMMARY (CONTINUED)

EB MILE MARKER	PLUS	EASTBOUND LOCATION		416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
		SIDE	LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
99.6	145	LEFT	6	-	16	8.00	-	30	
99.6	280	LEFT	6	-	16	8.00	-	30	
99.6	360	LEFT	6	-	16	8.00	-	30	
99.6	450	RIGHT	6	-	16	9.33	-	34	
99.6	500	LEFT	6	-	16	8.00	-	30	
99.6	530	LEFT	6	-	16	8.00	-	30	
99.6	550	LEFT	6	-	16	8.00	-	30	
99.6	670	RIGHT	6	-	16	9.33	-	34	
99.6	700	RIGHT	30	12	16	-	46.67	58	
99.6	800	LEFT	6	-	16	8.00	-	30	
99.6	900	LEFT	6	-	16	8.00	-	30	
99.6	1100	LEFT	6	-	16	8.00	-	30	
99.6	1200	LEFT	8	4	16	10.67	-	32	
99.6	1320	LEFT	6	-	16	8.00	-	30	
99.6	1330	LEFT	6	-	16	8.00	-	30	
99.6	1440	BOTH	6	-	32	17.33	-	58	
99.6	1540	LEFT	6	-	16	8.00	-	30	
99.6	1575	RIGHT	6	-	16	9.33	-	34	
99.6	1630	LEFT	6	-	16	8.00	-	30	
99.6	1660	LEFT	6	-	16	8.00	-	30	
99.6	1745	RIGHT	6	-	16	9.33	-	34	
99.6	1810	RIGHT	6	-	16	9.33	-	34	
99.6	1910	RIGHT	6	-	16	9.33	-	34	
99.6	2030	RIGHT	6	-	16	9.33	-	34	
100	200	RIGHT	6	-	16	9.33	-	34	
100	300	RIGHT	6	-	16	9.33	-	34	
100	470	RIGHT	6	-	16	9.33	-	34	
100	520	RIGHT	6	-	16	9.33	-	34	
100	610	RIGHT	6	-	16	9.33	-	34	
100	670	LEFT	6	-	16	8.00	-	30	
100	700	RIGHT	10	4	16	15.56	-	38	
100	720	LEFT	8	4	16	10.67	-	32	
100	740	BOTH	6	-	32	17.33	-	58	
100	755	LEFT	6	-	16	8.00	-	30	
100	770	LEFT	6	-	16	8.00	-	30	
100	815	LEFT	6	-	16	8.00	-	30	
100	945	BOTH	6	-	32	17.33	-	58	
100	985	BOTH	6	-	32	17.33	-	58	
100	1020	RIGHT	6	-	16	9.33	-	34	
100	1090	LEFT	8	4	16	10.67	-	32	
100	1240	LEFT	6	-	16	8.00	-	30	
100	1290	RIGHT	6	-	16	9.33	-	34	
100	1320	LEFT	6	-	16	8.00	-	30	
100	1340	RIGHT	6	-	16	9.33	-	34	
100	1360	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 1 =				28	784	425	47	1,564	

EB MILE MARKER	PLUS	EASTBOUND LOCATION		416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
		SIDE	LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
100	1385	LEFT	6	-	16	8.00	-	30	
100	1445	RIGHT	6	-	16	9.33	-	34	
100	1465	RIGHT	6	-	16	9.33	-	34	
100	1480	LEFT	6	-	16	8.00	-	30	
100	1505	LEFT	6	-	16	8.00	-	30	
100	1625	BOTH	6	-	32	17.33	-	58	
100	2000	RIGHT	6	-	16	9.33	-	34	
100	2290	RIGHT	6	-	16	9.33	-	34	
100	2315	RIGHT	6	-	16	9.33	-	34	
100	2375	RIGHT	6	-	16	9.33	-	34	
100	2440	BOTH	6	-	32	17.33	-	58	
100	2565	RIGHT	6	-	16	9.33	-	34	
100	2600	RIGHT	6	-	16	9.33	-	34	
100	2705	RIGHT	6	-	16	9.33	-	34	
100	2815	LEFT	6	-	16	8.00	-	30	
100	2905	RIGHT	6	-	16	9.33	-	34	
100	2940	RIGHT	6	-	16	9.33	-	34	
100	3040	RIGHT	30	12	16	-	46.67	58	
100	3555	LEFT	6	-	16	8.00	-	30	
100	3595	RIGHT	6	-	16	9.33	-	34	
100	3710	RIGHT	6	-	16	9.33	-	34	
100	3735	RIGHT	6	-	16	9.33	-	34	
100	3765	RIGHT	6	-	16	9.33	-	34	
100	3780	LEFT	6	-	16	8.00	-	30	
100	3875	LEFT	6	-	16	8.00	-	30	
100	3900	RIGHT	6	-	16	9.33	-	34	
100	3955	RIGHT	6	-	16	9.33	-	34	
100	3995	RIGHT	6	-	16	9.33	-	34	
100	4030	RIGHT	8	4	16	12.44	-	36	
100	4050	RIGHT	6	-	16	9.33	-	34	
100	4115	RIGHT	6	-	16	9.33	-	34	
100	4150	RIGHT	6	-	16	9.33	-	34	
100	4200	RIGHT	6	-	16	9.33	-	34	
100	4230	BOTH	6	-	32	17.33	-	58	
100	4240	RIGHT	6	-	16	9.33	-	34	
100	4315	RIGHT	15	6	16	-	23.33	43	
100	4490	RIGHT	6	-	16	9.33	-	34	
100	4660	RIGHT	6	-	16	9.33	-	34	
100	4850	RIGHT	6	-	16	9.33	-	34	
100	4880	RIGHT	6	-	16	9.33	-	34	
100	4920	RIGHT	6	-	16	9.33	-	34	
100	4985	RIGHT	6	-	16	9.33	-	34	
100	5050	RIGHT	6	-	16	9.33	-	34	
100	5115	RIGHT	6	-	16	9.33	-	34	
100	5170	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 2 =				22	768	418	70	1,605	

CONCRETE REPAIR SUMMARY (CONTINUED)

CONCRETE REPAIR SUMMARY (CONTINUED)

EB MILE MARKER	PLUS	EASTBOUND LOCATION SIDE	LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
				DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
101	30	LEFT	6	-	16	8.00	-	30	
101	90	RIGHT	6	-	16	9.33	-	34	
101	140	BOTH	6	-	32	17.33	-	58	
101	240	LEFT	6	-	16	8.00	-	30	
101	335	RIGHT	6	-	16	9.33	-	34	
101	505	RIGHT	6	-	16	9.33	-	34	
101	525	RIGHT	6	-	16	9.33	-	34	
101	580	LEFT	6	-	16	8.00	-	30	
101	705	RIGHT	6	-	16	9.33	-	34	
101	735	RIGHT	6	-	16	9.33	-	34	
101	825	LEFT	6	-	16	8.00	-	30	
101	850	LEFT	6	-	16	8.00	-	30	
101	1100	RIGHT	6	-	16	9.33	-	34	
101	1170	RIGHT	6	-	16	9.33	-	34	
101	1565	LEFT	6	-	16	8.00	-	30	
101	1680	RIGHT	6	-	16	9.33	-	34	
101	1730	RIGHT	6	-	16	9.33	-	34	
101	1815	RIGHT	6	-	16	9.33	-	34	
101	1850	RIGHT	6	-	16	9.33	-	34	
COLUMBIA COUNTY LINE									
101	2115	LEFT	6	-	16	8.00	-	30	
101	2325	LEFT	6	-	16	8.00	-	30	
101	2675	LEFT	6	-	16	8.00	-	30	
101	2800	RIGHT	10	4	16	15.56	-	38	
101	2840	RIGHT	6	-	16	9.33	-	34	
101	2940	RIGHT	6	-	16	9.33	-	34	
101	3000	RIGHT	6	-	16	9.33	-	34	
101	3050	LEFT	6	-	16	8.00	-	30	
101	3120	RIGHT	6	-	16	9.33	-	34	
101	3140	RIGHT	6	-	16	9.33	-	34	
101	3345	LEFT	6	-	16	8.00	-	30	
101	3390	LEFT	6	-	16	8.00	-	30	
101	3590	LEFT	6	-	16	8.00	-	30	
101	3660	RIGHT	6	-	16	9.33	-	34	
101	3740	RIGHT	6	-	16	9.33	-	34	
101	3785	LEFT	6	-	16	8.00	-	30	
101	3800	LEFT	8	4	16	10.67	-	32	
101	4040	RIGHT	6	-	16	9.33	-	34	
101	4240	LEFT	6	-	16	8.00	-	30	
101	4390	RIGHT	6	-	16	9.33	-	34	
101	4525	RIGHT	6	-	16	9.33	-	34	
101	4605	LEFT	6	-	16	8.00	-	30	
101	4810	LEFT	6	-	16	8.00	-	30	
101	5145	RIGHT	6	-	16	9.33	-	34	
101	5200	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 3 =				8	720	402	0	1,450	

EB MILE MARKER	PLUS	EASTBOUND LOCATION SIDE	LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
				DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
101	5240	RIGHT	8	4	16	12.44	-	36	REPATCH
101	5380	LEFT	6	-	16	8.00	-	30	
101	5610	RIGHT	6	-	16	9.33	-	34	
101	5850	RIGHT	6	-	16	9.33	-	34	
101	5920	RIGHT	6	-	16	9.33	-	34	
102.2	250	RIGHT	6	-	16	9.33	-	34	
102.2	350	RIGHT	6	-	16	9.33	-	34	
102.2	415	RIGHT	6	-	16	9.33	-	34	
102.2	500	RIGHT	6	-	16	9.33	-	34	
102.2	540	RIGHT	6	-	16	9.33	-	34	
102.2	1030	RIGHT	6	-	16	9.33	-	34	
102.2	1175	RIGHT	6	-	16	9.33	-	34	
102.2	1255	RIGHT	6	-	16	9.33	-	34	
102.2	1305	RIGHT	6	-	16	9.33	-	34	
102.2	1320	RIGHT	6	-	16	9.33	-	34	
102.2	1405	LEFT	6	-	16	8.00	-	30	
102.2	1430	RIGHT	6	-	16	9.33	-	34	
102.2	1520	RIGHT	6	-	16	9.33	-	34	
102.2	1700	RIGHT	30	12	16	-	46.67	58	
102.2	1890	LEFT	6	-	16	8.00	-	30	
102.2	1920	LEFT	6	-	16	8.00	-	30	
102.2	2015	RIGHT	6	-	16	9.33	-	34	
102.2	2075	RIGHT	6	-	16	9.33	-	34	
102.2	2140	LEFT	6	-	16	8.00	-	30	
102.2	2380	RIGHT	6	-	16	9.33	-	34	
102.2	2630	LEFT	6	-	16	8.00	-	30	
102.2	2670	RIGHT	6	-	16	9.33	-	34	
102.2	2730	LEFT	6	-	16	8.00	-	30	
102.2	2770	LEFT	6	-	16	8.00	-	30	
102.2	2940	RIGHT	6	-	16	9.33	-	34	
102.2	3045	BOTH	6	-	32	17.33	-	58	
102.2	3100	RIGHT	6	-	16	9.33	-	34	
102.2	3200	LEFT	6	-	16	8.00	-	30	
102.2	3280	RIGHT	6	-	16	9.33	-	34	
102.2	3340	RIGHT	6	-	16	9.33	-	34	
102.2	3420	LEFT	6	-	16	8.00	-	30	
102.2	3440	LEFT	6	-	16	8.00	-	30	
102.2	3465	LEFT	6	-	16	8.00	-	30	
102.2	3585	LEFT	8	4	16	10.67	-	32	
102.2	3610	LEFT	6	-	16	8.00	-	30	
102.2	3680	LEFT	6	-	16	8.00	-	30	
102.2	3750	RIGHT	6	-	16	9.33	-	34	
102.2	3765	BOTH	6	-	32	17.33	-	58	
102.2	3985	RIGHT	6	-	16	9.33	-	34	
102.2	4020	RIGHT	6	-	16	9.33	-	34	REPATCH
SUBTOTAL 4 =				20	752	412	47	1,546	

CONCRETE REPAIR SUMMARY (CONTINUED)

CONCRETE REPAIR SUMMARY (CONTINUED)

EB MILE MARKER	PLUS	EASTBOUND LOCATION		DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE REPLACEMENT SHES SY	SAWING CONCRETE LF	REMARKS
		SIDE	LENGTH						
102.2	4093	RIGHT	8	4	16	12.44	-	36	REPATCH
102.2	4100	LEFT	6	-	16	8.00	-	30	
102.2	4120	BOTH	6	-	32	17.33	-	58	
102.2	4160	LEFT	6	-	16	8.00	-	30	
102.2	4250	LEFT	6	-	16	8.00	-	30	
103	65	LEFT	6	-	16	8.00	-	30	
103	90	LEFT	6	-	16	8.00	-	30	
103	105	RIGHT	6	-	16	9.33	-	34	
103	135	LEFT	6	-	16	8.00	-	30	
103	150	BOTH	6	-	32	17.33	-	58	
103	170	BOTH	6	-	32	17.33	-	58	
103	250	LEFT	6	-	16	8.00	-	30	
103	280	LEFT	6	-	16	8.00	-	30	
103	300	LEFT	6	-	16	8.00	-	30	
103	445	RIGHT	8	4	16	12.44	-	36	REPATCH
103	495	RIGHT	8	4	16	12.44	-	36	REPATCH
103	560	RIGHT	10	4	16	15.56	-	38	
103	720	LEFT	6	-	16	8.00	-	30	
103	845	RIGHT	6	-	16	9.33	-	34	
103	1020	RIGHT	6	-	16	9.33	-	34	
103	1087	RIGHT	6	-	16	9.33	-	34	
103	1185	LEFT	6	-	16	8.00	-	30	
103	1240	RIGHT	6	-	16	9.33	-	34	
103	1305	RIGHT	6	-	16	9.33	-	34	
103	1520	RIGHT	6	-	16	9.33	-	34	
103	1535	LEFT	6	-	16	8.00	-	30	
103	1585	LEFT	6	-	16	8.00	-	30	
103	1590	LEFT	6	-	16	8.00	-	30	
103	1625	RIGHT	6	-	16	9.33	-	34	
103	1650	RIGHT	6	-	16	9.33	-	34	
103	1720	RIGHT	8	4	16	12.44	-	36	REPATCH
103	1735	LEFT	6	-	16	8.00	-	30	
103	1800	RIGHT	6	-	16	9.33	-	34	
103	1935	LEFT	6	-	16	8.00	-	30	
103	1980	LEFT	6	-	16	8.00	-	30	
103	1990	RIGHT	8	4	16	12.44	-	36	REPATCH
103	2000	RIGHT	8	4	16	12.44	-	36	REPATCH
103	2050	LEFT	6	-	16	8.00	-	30	
103	2100	RIGHT	30	12	16	-	46.67	58	
103	2320	RIGHT	6	-	16	9.33	-	34	
103	2455	RIGHT	15	6	16	-	23.33	43	
103	2610	RIGHT	8	4	16	12.44	-	36	REPATCH
103	2735	LEFT	6	-	16	8.00	-	30	
103	2770	LEFT	6	-	16	8.00	-	30	
103	2790	LEFT	8	4	16	10.67	-	32	
SUBTOTAL 5 =				54	768	428	70	1,571	

EB MILE MARKER	PLUS	EASTBOUND LOCATION		DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE REPLACEMENT SHES SY	SAWING CONCRETE LF	REMARKS
		SIDE	LENGTH						
103	2950	RIGHT	15	6	16	-	23.33	43	
103	2970	LEFT	6	-	16	8.00	-	30	
103	2990	LEFT	6	-	16	8.00	-	30	
103	2990	RIGHT	8	4	16	12.44	-	36	REPATCH
103	3005	RIGHT	6	-	16	9.33	-	34	
103	3020	RIGHT	15	6	16	-	23.33	43	
103	3020	LEFT	8	4	16	10.67	-	32	
103	3060	BOTH	6	-	32	17.33	-	58	
103	3080	LEFT	6	-	16	8.00	-	30	
103	3090	LEFT	8	4	16	10.67	-	32	
103	3110	BOTH	6	-	32	17.33	-	58	
103	3125	BOTH	6	-	32	17.33	-	58	
103	3145	LEFT	6	-	16	8.00	-	30	
103	3155	LEFT	6	-	16	8.00	-	30	
103	3200	LEFT	6	-	16	8.00	-	30	
103	3320	LEFT	6	-	16	8.00	-	30	
103	3335	RIGHT	6	-	16	9.33	-	34	
103	3350	BOTH	6	-	32	17.33	-	58	
103	3390	LEFT	6	-	16	8.00	-	30	
103	3410	LEFT	8	4	16	10.67	-	32	
103	3430	LEFT	6	-	16	8.00	-	30	
103	3510	RIGHT	8	4	16	12.44	-	36	
103	3560	LEFT	6	-	16	8.00	-	30	
103	3640	RIGHT	6	-	16	9.33	-	34	
103	3680	RIGHT	6	-	16	9.33	-	34	
103	3700	RIGHT	6	-	16	9.33	-	34	
103	3865	LEFT	6	-	16	8.00	-	30	
103	3995	RIGHT	8	4	16	12.44	-	36	REPATCH
103	4015	RIGHT	8	4	16	12.44	-	36	REPATCH
103	4020	LEFT	6	-	16	8.00	-	30	
103	4050	LEFT	6	-	16	8.00	-	30	
103	4150	RIGHT	6	-	16	9.33	-	34	
103	4150	RIGHT	6	-	16	9.33	-	34	
103	4200	LEFT	6	-	16	8.00	-	30	
103	4390	RIGHT	6	-	16	9.33	-	34	
103	4430	RIGHT	6	-	16	9.33	-	34	
103	4460	LEFT	6	-	16	8.00	-	30	
103	4500	BOTH	6	-	32	17.33	-	58	
103	4520	LEFT	6	-	16	8.00	-	30	
103	4740	RIGHT	6	-	16	9.33	-	34	
103	4830	LEFT	6	-	16	8.00	-	30	
103	4855	LEFT	6	-	16	8.00	-	30	
103	5160	LEFT	6	-	16	8.00	-	30	
104	0	BOTH	6	-	32	17.33	-	58	
104	50	RIGHT	8	4	16	12.44	-	36	
SUBTOTAL 6 =				44	816	444	47	1,620	

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PROJECT NO: 1016-01-63

HWY: IH - 90

COUNTY: SAUK, COLUMBIA

MISCELLANEOUS QUANTITIES

SHEET:

E

CONCRETE REPAIR SUMMARY (CONTINUED)

EB MILE MARKER	PLUS	EASTBOUND LOCATION SIDE LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS	
			DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF		
104		75 LEFT	6	-	16	8.00	-	30	
104		130 LEFT	6	-	16	8.00	-	30	
104		200 LEFT	6	-	16	8.00	-	30	
104		340 RIGHT	6	-	16	9.33	-	34	
104		410 LEFT	6	-	16	8.00	-	30	
104		445 LEFT	6	-	16	8.00	-	30	
104		560 LEFT	6	-	16	8.00	-	30	
104		600 LEFT	6	-	16	8.00	-	30	
104		650 BOTH	6	-	32	17.33	-	58	
104		755 LEFT	6	-	16	8.00	-	30	
104		825 RIGHT	6	-	16	9.33	-	34	
104		850 RIGHT	6	-	16	9.33	-	34	
104		875 LEFT	6	-	16	8.00	-	30	
104		900 LEFT	6	-	16	8.00	-	30	
104		945 LEFT	6	-	16	8.00	-	30	
104		985 LEFT	6	-	16	8.00	-	30	
104		1005 LEFT	6	-	16	8.00	-	30	
104		1050 LEFT	6	-	16	8.00	-	30	
104		1095 LEFT	6	-	16	8.00	-	30	
104		1110 LEFT	6	-	16	8.00	-	30	
104		1147 LEFT	8	4	16	10.67	-	32	
104		1180 LEFT	6	-	16	8.00	-	30	
104		1240 LEFT	6	-	16	8.00	-	30	
104		1250 RIGHT	6	-	16	9.33	-	34	
104		1275 LEFT	6	-	16	8.00	-	30	
104		1285 LEFT	6	-	16	8.00	-	30	
104		1295 LEFT	6	-	16	8.00	-	30	
104		1300 BOTH	6	-	32	17.33	-	58	
104		1340 BOTH	6	-	32	17.33	-	58	
104		1325 RIGHT	6	-	16	9.33	-	34	
104		1360 BOTH	6	-	32	17.33	-	58	
104		1400 LEFT	6	-	16	8.00	-	30	
104		1440 LEFT	6	-	16	8.00	-	30	
104		1475 LEFT	6	-	16	8.00	-	30	
104		1500 BOTH	6	-	32	17.33	-	58	
104		1535 BOTH	6	-	32	17.33	-	58	
104		1560 LEFT	8	4	16	10.67	-	32	
104		1585 LEFT	6	-	16	8.00	-	30	
104		1600 LEFT	6	-	16	8.00	-	30	
104		1635 LEFT	6	-	16	8.00	-	30	
104		1650 BOTH	6	-	32	17.33	-	58	
104		1685 LEFT	6	-	16	8.00	-	30	
104		1710 LEFT	6	-	16	8.00	-	30	
104		1725 BOTH	6	-	32	17.33	-	58	
104		1745 LEFT	6	-	16	8.00	-	30	
SUBTOTAL 7 =			8	848	447	0	1,598		

CONCRETE REPAIR SUMMARY (CONTINUED)

EB MILE MARKER	PLUS	EASTBOUND LOCATION SIDE LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS	
			DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF		
104		1770 LEFT	6	-	16	8.00	-	30	
104		1785 LEFT	6	-	16	8.00	-	30	
104		1830 LEFT	6	-	16	8.00	-	30	
104		1850 LEFT	6	-	16	8.00	-	30	
104		1875 LEFT	8	4	16	10.67	-	32	
104		1895 LEFT	6	-	16	8.00	-	30	
104		1915 LEFT	6	-	16	8.00	-	30	
104		1935 LEFT	6	-	16	8.00	-	30	
104		1960 LEFT	6	-	16	8.00	-	30	
104		1985 RIGHT	6	-	16	9.33	-	34	
104		2000 LEFT	8	4	16	10.67	-	32	
104		2030 LEFT	6	-	16	8.00	-	30	
104		2050 LEFT	8	4	16	10.67	-	32	
104		2070 LEFT	6	-	16	8.00	-	30	
104		2090 LEFT	6	-	16	8.00	-	30	
104		2110 LEFT	6	-	16	8.00	-	30	
104		2140 LEFT	6	-	16	8.00	-	30	
104		2160 LEFT	6	-	16	8.00	-	30	
104		2175 LEFT	6	-	16	8.00	-	30	
104		2190 LEFT	6	-	16	8.00	-	30	
104		2210 BOTH	6	-	32	17.33	-	58	
104		2225 LEFT	6	-	16	8.00	-	30	
104		2240 LEFT	6	-	16	8.00	-	30	
104		2255 LEFT	8	4	16	10.67	-	32	
104		2280 LEFT	6	-	16	8.00	-	30	
104		2350 LEFT	8	4	16	10.67	-	32	
104		2485 LEFT	6	-	16	8.00	-	30	
104		2510 RIGHT	6	-	16	9.33	-	34	
104		2540 RIGHT	6	-	16	9.33	-	34	
104		2575 LEFT	6	-	16	8.00	-	30	
104		2635 LEFT	6	-	16	8.00	-	30	
104		2675 LEFT	6	-	16	8.00	-	30	
104		2780 LEFT	6	-	16	8.00	-	30	
104		2800 LEFT	6	-	16	8.00	-	30	
104		2815 LEFT	6	-	16	8.00	-	30	
104		2840 BOTH	6	-	32	17.33	-	58	
104		2860 LEFT	6	-	16	8.00	-	30	
104		2935 RIGHT	6	-	16	9.33	-	34	
104		2950 RIGHT	6	-	16	9.33	-	34	
104		2965 LEFT	6	-	16	8.00	-	30	
104		3025 LEFT	6	-	16	8.00	-	30	
104		3050 LEFT	8	4	16	10.67	-	32	
104		3100 LEFT	6	-	16	8.00	-	30	
104		3140 LEFT	6	-	16	8.00	-	30	
104		3185 LEFT	6	-	16	8.00	-	30	
SUBTOTAL 8 =			24	752	401	0	1,438		

CONCRETE REPAIR SUMMARY (CONTINUED)

EB MILE MARKER	PLUS	EASTBOUND LOCATION SIDE	LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
				DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
104	3230	BOTH	6	-	32	17.33	-	58	
104	3250	LEFT	6	-	16	8.00	-	30	
104	3265	LEFT	6	-	16	8.00	-	30	
104	3345	LEFT	6	-	16	8.00	-	30	
104	3390	LEFT	6	-	16	8.00	-	30	
104	3410	RIGHT	6	-	16	9.33	-	34	
104	3430	BOTH	6	-	32	17.33	-	58	
104	3450	LEFT	6	-	16	8.00	-	30	
104	3495	LEFT	8	4	16	10.67	-	32	
104	3550	LEFT	6	-	16	8.00	-	30	
104	3610	LEFT	6	-	16	8.00	-	30	
104	3635	LEFT	6	-	16	8.00	-	30	
104	3660	LEFT	6	-	16	8.00	-	30	
104	3685	LEFT	10	4	16	13.33	-	34	REPATCH
104	3760	BOTH	6	-	32	17.33	-	58	
104	3815	RIGHT	8	4	16	12.44	-	36	REPATCH
104	3875	LEFT	6	-	16	8.00	-	30	
104	3900	LEFT	6	-	16	8.00	-	30	
104	3915	LEFT	6	-	16	8.00	-	30	
104	3920	LEFT	6	-	16	8.00	-	30	
104	3940	LEFT	6	-	16	8.00	-	30	
104	3980	RIGHT	6	-	16	9.33	-	34	
104	4020	LEFT	6	-	16	8.00	-	30	
104	4040	LEFT	6	-	16	8.00	-	30	
104	4065	LEFT	6	-	16	8.00	-	30	
104	4095	LEFT	6	-	16	8.00	-	30	
104	4205	LEFT	6	-	16	8.00	-	30	
104	4235	BOTH	6	-	32	17.33	-	58	
104	4250	LEFT	6	-	16	8.00	-	30	
104	4295	RIGHT	6	-	16	9.33	-	34	
104	4360	RIGHT	6	-	16	9.33	-	34	
104	4395	LEFT	6	-	16	8.00	-	30	
104	4420	LEFT	6	-	16	8.00	-	30	
104	4450	BOTH	6	-	32	17.33	-	58	
104	4490	LEFT	6	-	16	8.00	-	30	
104	4500	LEFT	6	-	16	8.00	-	30	
104	4520	LEFT	6	-	16	8.00	-	30	
104	4540	LEFT	6	-	16	8.00	-	30	
104	4575	LEFT	6	-	16	8.00	-	30	
104	4595	BOTH	6	-	32	17.33	-	58	
104	4625	BOTH	6	-	32	17.33	-	58	
104	4650	LEFT	6	-	16	8.00	-	30	
104	4670	LEFT	6	-	16	8.00	-	30	
104	4700	LEFT	6	-	16	8.00	-	30	
104	4725	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 9 =				12	832	443	0	1,574	

CONCRETE REPAIR SUMMARY (CONTINUED)

EB MILE MARKER	PLUS	EASTBOUND LOCATION SIDE	LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
				DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
104	4755	LEFT	6	-	16	8.00	-	30	
104	4800	RIGHT	6	-	16	9.33	-	34	
104	4890	RIGHT	6	-	16	9.33	-	34	
104	4925	BOTH	6	-	32	17.33	-	58	
104	4980	BOTH	6	-	32	17.33	-	58	
104	5040	RIGHT	10	4	16	15.56	-	38	REPATCH
105	25	RIGHT	6	-	16	9.33	-	34	
105	55	RIGHT	6	-	16	9.33	-	34	
105	125	RIGHT	6	-	16	9.33	-	34	
105	240	RIGHT	6	-	16	9.33	-	34	
SUBTOTAL 10 =				4	192	114	0	388	

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CONCRETE REPAIR SUMMARY (CONTINUED)

CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	MINUS	WESTBOUND LOCATION		416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
		SIDE	LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
105	95	BOTH	6	-	32	17.33	-	58	
105	185	LEFT	6	-	16	8.00	-	30	
105	200	LEFT	6	-	16	8.00	-	30	
105	250	LEFT	6	-	16	8.00	-	30	
105	320	LEFT	6	-	16	8.00	-	30	
105	400	LEFT	6	-	16	8.00	-	30	
105	450	LEFT	6	-	16	8.00	-	30	
105	465	RIGHT	8	4	16	12.44	-	36	REPATCH
105	465	LEFT	6	-	16	8.00	-	30	
105	505	LEFT	6	-	16	8.00	-	30	
105	535	RIGHT	6	-	16	9.33	-	34	
105	550	LEFT	6	-	16	8.00	-	30	
105	695	LEFT	6	-	16	8.00	-	30	
105	710	RIGHT	8	4	16	12.44	-	36	REPATCH
105	750	RIGHT	8	4	16	12.44	-	36	REPATCH
105	765	LEFT	6	-	16	8.00	-	30	
105	805	LEFT	6	-	16	8.00	-	30	
105	830	BOTH	6	-	32	17.33	-	58	
105	840	BOTH	6	-	32	17.33	-	58	
105	865	BOTH	6	-	32	17.33	-	58	
105	890	LEFT	6	-	16	8.00	-	30	
105	910	LEFT	6	-	16	8.00	-	30	
105	925	LEFT	6	-	16	8.00	-	30	
105	950	BOTH	6	-	32	17.33	-	58	
105	980	LEFT	6	-	16	8.00	-	30	
105	1010	RIGHT	8	4	16	12.44	-	36	REPATCH
105	1025	RIGHT	8	4	16	12.44	-	36	REPATCH
105	1025	LEFT	6	-	16	8.00	-	30	
105	1060	RIGHT	6	-	16	9.33	-	34	
105	1140	LEFT	6	-	16	8.00	-	30	
105	1180	LEFT	6	-	16	8.00	-	30	
105	1286	LEFT	6	-	16	8.00	-	30	
105	1310	LEFT	6	-	16	8.00	-	30	
105	1330	RIGHT	6	-	16	9.33	-	34	
105	1345	RIGHT	6	-	16	9.33	-	34	
105	1360	LEFT	6	-	16	8.00	-	30	
105	1380	RIGHT	6	-	16	9.33	-	34	
105	1515	LEFT	6	-	16	8.00	-	30	
105	1760	LEFT	6	-	16	8.00	-	30	
105	1800	LEFT	6	-	16	8.00	-	30	
105	1855	RIGHT	6	-	16	9.33	-	34	
105	1880	LEFT	6	-	16	8.00	-	30	
105	1900	LEFT	6	-	16	8.00	-	30	
105	1940	LEFT	6	-	16	8.00	-	30	
105	1900	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 11 =				20	800	437	0	1,544	

WB MILE MARKER	MINUS	WESTBOUND LOCATION		416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
		SIDE	LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
105	2110	RIGHT	6	-	16	9.33	-	34	
105	2155	LEFT	6	-	16	8.00	-	30	
105	2175	LEFT	6	-	16	8.00	-	30	
105	2220	LEFT	6	-	16	8.00	-	30	
105	2475	LEFT	6	-	16	8.00	-	30	
105	2595	LEFT	6	-	16	8.00	-	30	
105	2630	RIGHT	6	-	16	9.33	-	34	
105	2655	BOTH	6	-	32	17.33	-	58	
105	2680	LEFT	6	-	16	8.00	-	30	
105	2710	RIGHT	8	4	16	12.44	-	36	REPATCH
105	2725	RIGHT	6	-	16	9.33	-	34	
105	2745	RIGHT	8	4	16	12.44	-	36	REPATCH
105	2820	LEFT	6	-	16	8.00	-	30	
105	2840	BOTH	6	-	32	17.33	-	58	
105	2860	LEFT	6	-	16	8.00	-	30	
105	2915	RIGHT	6	-	16	9.33	-	34	
105	2950	BOTH	6	-	32	17.33	-	58	
105	2990	LEFT	6	-	16	8.00	-	30	
105	3000	RIGHT	6	-	16	9.33	-	34	
105	3030	LEFT	6	-	16	8.00	-	30	
105	3090	LEFT	6	-	16	8.00	-	30	
105	3120	RIGHT	6	-	16	9.33	-	34	
105	3130	LEFT	6	-	16	8.00	-	30	
105	3150	LEFT	6	-	16	8.00	-	30	
105	3185	LEFT	6	-	16	8.00	-	30	
105	3210	LEFT	6	-	16	8.00	-	30	
105	3250	BOTH	6	-	32	17.33	-	58	
105	3275	RIGHT	15	6	16	-	23.33	43	
105	3310	LEFT	6	-	16	8.00	-	30	
105	3315	BOTH	6	-	32	17.33	-	58	
105	3345	BOTH	6	-	32	17.33	-	58	
105	3370	LEFT	6	-	16	8.00	-	30	
105	3410	RIGHT	15	6	16	-	23.33	43	
105	3460	RIGHT	6	-	16	9.33	-	34	
105	3540	LEFT	6	-	16	8.00	-	30	
105	3570	LEFT	6	-	16	8.00	-	30	
105	3585	LEFT	8	4	16	10.67	-	32	
105	3595	LEFT	6	-	16	8.00	-	30	
105	3615	RIGHT	6	-	16	9.33	-	34	
105	3630	RIGHT	6	-	16	9.33	-	34	
105	3690	LEFT	6	-	16	8.00	-	30	
105	3730	BOTH	6	-	32	17.33	-	58	
105	3745	RIGHT	6	-	16	9.33	-	34	
105	3770	LEFT	6	-	16	8.00	-	30	
105	3800	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 12 =				24	832	434	47	1,626	

CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	MINUS	WESTBOUND LOCATION SIDE LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
			DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
105		3820 RIGHT	6	-	16	9.33	-	34
105		3835 BOTH	6	-	32	17.33	-	58
105		3870 RIGHT	6	-	16	9.33	-	34
105		3900 RIGHT	6	-	16	9.33	-	34
105		4020 RIGHT	6	-	16	9.33	-	34
105		4070 LEFT	6	-	16	8.00	-	30
105		4135 RIGHT	6	-	16	9.33	-	34
105		4165 RIGHT	6	-	16	9.33	-	34
105		4205 LEFT	6	-	16	8.00	-	30
105		4230 RIGHT	6	-	16	9.33	-	34
105		4265 RIGHT	6	-	16	9.33	-	34
105		4280 LEFT	6	-	16	8.00	-	30
105		4350 LEFT	6	-	16	8.00	-	30
105		4370 RIGHT	10	4	16	15.56	-	38
105		4400 LEFT	8	4	16	10.67	-	32
105		4410 LEFT	6	-	16	8.00	-	30
105		4420 LEFT	8	4	16	10.67	-	32
105		4530 RIGHT	6	-	16	9.33	-	34
105		4620 RIGHT	6	-	16	9.33	-	34
105		4680 LEFT	6	-	16	8.00	-	30
105		4700 RIGHT	6	-	16	9.33	-	34
105		4740 LEFT	6	-	16	8.00	-	30
105		4840 RIGHT	6	-	16	9.33	-	34
105		4865 LEFT	6	-	16	8.00	-	30
105		5050 RIGHT	10	4	16	15.56	-	38
105		5140 BOTH	6	-	32	17.33	-	58
104		35 RIGHT	6	-	16	9.33	-	34
104		50 RIGHT	6	-	16	9.33	-	34
104		65 RIGHT	6	-	16	9.33	-	34
104		125 RIGHT	6	-	16	9.33	-	34
104		155 RIGHT	6	-	16	9.33	-	34
104		185 RIGHT	8	4	16	12.44	-	36
104		305 RIGHT	6	-	16	9.33	-	34
104		340 LEFT	6	-	16	8.00	-	30
104		355 RIGHT	6	-	16	9.33	-	34
104		420 LEFT	6	-	16	8.00	-	30
104		450 RIGHT	6	-	16	9.33	-	34
104		545 LEFT	6	-	16	8.00	-	30
104		570 LEFT	6	-	16	8.00	-	30
104		670 BOTH	6	-	32	17.33	-	58
104		680 RIGHT	6	-	16	9.33	-	34
104		720 RIGHT	6	-	16	9.33	-	34
104		735 RIGHT	6	-	16	9.33	-	34
104		805 RIGHT	6	-	16	9.33	-	34
104		825 LEFT	6	-	16	8.00	-	30
SUBTOTAL 13 =			20	768	445	0	1,556	

CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	MINUS	WESTBOUND LOCATION SIDE LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
			DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
104		855 RIGHT	8	4	16	12.44	-	36
104		1000 LEFT	6	-	16	8.00	-	30
104		1020 BOTH	6	-	32	17.33	-	58
104		1060 BOTH	6	-	32	17.33	-	58
104		1075 RIGHT	6	-	16	9.33	-	34
104		1105 RIGHT	6	-	16	9.33	-	34
104		1125 LEFT	6	-	16	8.00	-	30
104		1150 RIGHT	6	-	16	9.33	-	34
104		1185 BOTH	6	-	32	17.33	-	58
104		1225 LEFT	6	-	16	8.00	-	30
104		1260 LEFT	8	4	16	10.67	-	32
104		1290 LEFT	6	-	16	8.00	-	30
104		1360 RIGHT	6	-	16	9.33	-	34
104		1460 RIGHT	6	-	16	9.33	-	34
104		1530 RIGHT	6	-	16	9.33	-	34
104		1560 RIGHT	6	-	16	9.33	-	34
104		1620 LEFT	6	-	16	8.00	-	30
104		1650 RIGHT	6	-	16	9.33	-	34
104		1705 RIGHT	6	-	16	9.33	-	34
104		1900 RIGHT	6	-	16	9.33	-	34
104		2140 RIGHT	6	-	16	9.33	-	34
104		2165 RIGHT	6	-	16	9.33	-	34
104		2310 RIGHT	15	6	16	-	23.33	43
104		2470 RIGHT	6	-	16	9.33	-	34
104		2550 RIGHT	6	-	16	9.33	-	34
104		2685 RIGHT	6	-	16	9.33	-	34
104		2905 LEFT	6	-	16	8.00	-	30
104		2930 RIGHT	6	-	16	9.33	-	34
104		2980 RIGHT	6	-	16	9.33	-	34
104		3000 RIGHT	6	-	16	9.33	-	34
104		3120 RIGHT	10	4	16	15.56	-	38
104		3140 LEFT	6	-	16	8.00	-	30
104		3195 LEFT	6	-	16	8.00	-	30
104		3270 LEFT	6	-	16	8.00	-	30
104		3310 LEFT	6	-	16	8.00	-	30
104		3380 BOTH	6	-	32	17.33	-	58
104		3415 LEFT	6	-	16	8.00	-	30
104		3435 RIGHT	6	-	16	9.33	-	34
104		3610 RIGHT	6	-	16	9.33	-	34
104		3710 LEFT	6	-	16	8.00	-	30
104		3750 RIGHT	6	-	16	9.33	-	34
104		3790 BOTH	6	-	32	17.33	-	58
104		3955 LEFT	6	-	16	8.00	-	30
104		4020 RIGHT	6	-	16	9.33	-	34
104		4065 RIGHT	6	-	16	9.33	-	34
SUBTOTAL 14 =			18	800	444	23	1,611	

CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	MINUS	WESTBOUND LOCATION SIDE	LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
				DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
104		4100 RIGHT	6	-	16	9.33	-	34	
104		4130 LEFT	8	4	16	10.67	-	32	
104		4150 LEFT	6	-	16	8.00	-	30	
104		4170 LEFT	6	-	16	8.00	-	30	
104		4185 LEFT	6	-	16	8.00	-	30	
104		4225 RIGHT	6	-	16	9.33	-	34	
104		4245 RIGHT	6	-	16	9.33	-	34	
104		4295 LEFT	6	-	16	8.00	-	30	
104		4510 LEFT	6	-	16	8.00	-	30	
104		4705 LEFT	6	-	16	8.00	-	30	
104		4770 LEFT	6	-	16	8.00	-	30	
104		4940 RIGHT	6	-	16	9.33	-	34	
104		4970 LEFT	6	-	16	8.00	-	30	
104		5005 LEFT	6	-	16	8.00	-	30	
104		5095 LEFT	6	-	16	8.00	-	30	
103		70 RIGHT	6	-	16	9.33	-	34	
103		150 RIGHT	6	-	16	9.33	-	34	
103		255 RIGHT	6	-	16	9.33	-	34	
103		285 BOTH	6	-	32	17.33	-	58	
103		310 LEFT	6	-	16	8.00	-	30	
103		365 RIGHT	6	-	16	9.33	-	34	
103		450 RIGHT	6	-	16	9.33	-	34	
103		480 RIGHT	6	-	16	9.33	-	34	
103		515 LEFT	6	-	16	8.00	-	30	
103		740 RIGHT	6	-	16	9.33	-	34	
103		765 RIGHT	6	-	16	9.33	-	34	
103		800 BOTH	6	-	32	17.33	-	58	
103		865 RIGHT	6	-	16	9.33	-	34	
103		900 RIGHT	6	-	16	9.33	-	34	
103		920 RIGHT	6	-	16	9.33	-	34	
103		950 RIGHT	6	-	16	9.33	-	34	
103		1050 RIGHT	6	-	16	9.33	-	34	
103		1090 RIGHT	6	-	16	9.33	-	34	
103		1120 LEFT	6	-	16	8.00	-	30	
103		1550 RIGHT	10	4	16	15.56	-	38	
103		1750 RIGHT	6	-	16	9.33	-	34	
103		1780 LEFT	6	-	16	8.00	-	30	
103		1840 BOTH	6	-	32	17.33	-	58	
103		1870 LEFT	6	-	16	8.00	-	30	
103		1895 LEFT	6	-	16	8.00	-	30	
103		1930 RIGHT	6	-	16	9.33	-	34	
103		1965 RIGHT	6	-	16	9.33	-	34	
103		1990 RIGHT	6	-	16	9.33	-	34	
103		2030 RIGHT	6	-	16	9.33	-	34	
103		2060 RIGHT	6	-	16	9.33	-	34	
SUBTOTAL 15 =				8	768	430	0	1,540	

CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	MINUS	WESTBOUND LOCATION SIDE	LENGTH	416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
				DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
103		2080 LEFT	6	-	16	8.00	-	30	
103		2110 LEFT	6	-	16	8.00	-	30	
103		2130 RIGHT	6	-	16	9.33	-	34	
103		2150 RIGHT	6	-	16	9.33	-	34	
103		2180 RIGHT	6	-	16	9.33	-	34	
103		2205 LEFT	6	-	16	8.00	-	30	
103		2255 LEFT	6	-	16	8.00	-	30	
103		2380 LEFT	6	-	16	8.00	-	30	
103		2410 LEFT	6	-	16	8.00	-	30	
103		2440 LEFT	6	-	16	8.00	-	30	
103		2470 LEFT	6	-	16	8.00	-	30	
103		2500 LEFT	6	-	16	8.00	-	30	
103		2540 LEFT	8	4	16	10.67	-	32	
103		2640 LEFT	6	-	16	8.00	-	30	
103		2845 LEFT	6	-	16	8.00	-	30	
103		2880 BOTH	6	-	32	17.33	-	58	
103		2950 RIGHT	6	-	16	9.33	-	34	
103		2975 RIGHT	6	-	16	9.33	-	34	
103		3030 LEFT	6	-	16	8.00	-	30	
103		3050 LEFT	6	-	16	8.00	-	30	
103		3095 RIGHT	6	-	16	9.33	-	34	
103		3280 LEFT	6	-	16	8.00	-	30	
103		3430 RIGHT	6	-	16	9.33	-	34	
103		3455 RIGHT	6	-	16	9.33	-	34	
103		3625 RIGHT	6	-	16	9.33	-	34	
103		3660 RIGHT	6	-	16	9.33	-	34	
103		3830 RIGHT	6	-	16	9.33	-	34	
103		4085 RIGHT	6	-	16	9.33	-	34	
103		4175 RIGHT	6	-	16	9.33	-	34	
103		4210 RIGHT	6	-	16	9.33	-	34	
103		4230 LEFT	6	-	16	8.00	-	30	
103		4275 RIGHT	6	-	16	9.33	-	34	
103		4425 RIGHT	6	-	16	9.33	-	34	
103		4480 RIGHT	6	-	16	9.33	-	34	
103		4620 RIGHT	6	-	16	9.33	-	34	
103		4645 RIGHT	6	-	16	9.33	-	34	
103		4670 RIGHT	10	4	16	15.56	-	38	
103		4690 RIGHT	6	-	16	9.33	-	34	
103		4730 BOTH	6	-	32	17.33	-	58	
103		4820 RIGHT	6	-	16	9.33	-	34	
103		4860 RIGHT	6	-	16	9.33	-	34	
103		4910 RIGHT	6	-	16	9.33	-	34	
103		4930 RIGHT	6	-	16	9.33	-	34	
103		5070 LEFT	6	-	16	8.00	-	30	
103		5090 LEFT	6	-	16	8.00	-	30	
SUBTOTAL 16 =				8	752	421	0	1,512	

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CONCRETE REPAIR SUMMARY (CONTINUED)

CONCRETE REPAIR SUMMARY (CONTINUED)

Table with columns: WB MILE MARKER, MINUS, WESTBOUND LOCATION (SIDE, LENGTH), DRILLED TIE BARS EACH, DRILLED DOWEL BARS EACH, CONCRETE PAVEMENT REPAIR SHES SY, CONCRETE PAVEMENT REPLACEMENT SHES SY, SAWING CONCRETE LF, REMARKS. Includes rows 102-1920 and a subtotal.

Table with columns: WB MILE MARKER, MINUS, WESTBOUND LOCATION (SIDE, LENGTH), DRILLED TIE BARS EACH, DRILLED DOWEL BARS EACH, CONCRETE PAVEMENT REPAIR SHES SY, CONCRETE PAVEMENT REPLACEMENT SHES SY, SAWING CONCRETE LF, REMARKS. Includes rows 102-4850 and a subtotal.

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CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	WESTBOUND LOCATION		416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS	
	MINUS	SIDE LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF		
102	5010	RIGHT	6	-	16	9.33	-	34	
101	230	RIGHT	6	-	16	9.33	-	34	
101	260	RIGHT	6	-	16	9.33	-	34	
101	560	RIGHT	6	-	16	9.33	-	34	
101	770	RIGHT	6	-	16	9.33	-	34	
101	800	RIGHT	6	-	16	9.33	-	34	
101	825	LEFT	6	-	16	8.00	-	30	
101	840	LEFT	6	-	16	8.00	-	30	
101	860	RIGHT	6	-	16	9.33	-	34	
101	970	LEFT	6	-	16	8.00	-	30	
101	1000	LEFT	6	-	16	8.00	-	30	
101	1020	BOTH	6	-	32	17.33	-	58	
101	1090	BOTH	6	-	32	17.33	-	58	
101	1110	RIGHT	6	-	16	9.33	-	34	
101	1150	BOTH	6	-	32	17.33	-	58	
101	1240	RIGHT	6	-	16	9.33	-	34	
101	1280	RIGHT	6	-	16	9.33	-	34	
101	1320	RIGHT	6	-	16	9.33	-	34	
101	1370	LEFT	6	-	16	8.00	-	30	
101	1390	LEFT	6	-	16	8.00	-	30	
101	1850	LEFT	6	-	16	8.00	-	30	
101	1960	LEFT	6	-	16	8.00	-	30	
101	2000	LEFT	6	-	16	8.00	-	30	
101	2050	LEFT	6	-	16	8.00	-	30	
101	2085	RIGHT	6	-	16	9.33	-	34	
101	2150	RIGHT	6	-	16	9.33	-	34	
101	2290	RIGHT	6	-	16	9.33	-	34	
101	2450	LEFT	6	-	16	8.00	-	30	
101	2540	LEFT	6	-	16	8.00	-	30	
101	2595	LEFT	6	-	16	8.00	-	30	
101	2615	LEFT	6	-	16	8.00	-	30	
101	2680	LEFT	6	-	16	8.00	-	30	
101	2790	LEFT	6	-	16	8.00	-	30	
101	2810	RIGHT	6	-	16	9.33	-	34	
101	2860	RIGHT	6	-	16	9.33	-	34	
101	2880	RIGHT	6	-	16	9.33	-	34	
101	2925	LEFT	6	-	16	8.00	-	30	
101	3160	RIGHT	6	-	16	9.33	-	34	
101	3280	RIGHT	6	-	16	9.33	-	34	
101	3590	RIGHT	10	4	16	15.56	-	38	
101	3630	RIGHT	6	-	16	9.33	-	34	
101	3660	RIGHT	6	-	16	9.33	-	34	
101	4180	LEFT	6	-	16	8.00	-	30	
101	4470	LEFT	6	-	16	8.00	-	30	
101	4570	RIGHT	6	-	16	9.33	-	34	
SUBTOTAL 19 =			4	768	425	0	1,530		

CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	WESTBOUND LOCATION		416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS	
	MINUS	SIDE LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF		
101	4770	RIGHT	6	-	16	9.33	-	34	
101	4840	RIGHT	6	-	16	9.33	-	34	
101	4880	RIGHT	6	-	16	9.33	-	34	
101	5030	RIGHT	6	-	16	9.33	-	34	
101	5205	LEFT	6	-	16	8.00	-	30	
100	10	LEFT	6	-	16	8.00	-	30	
100	150	RIGHT	8	4	16	12.44	-	36	REPATCH
100	305	RIGHT	6	-	16	9.33	-	34	
100	330	BOTH	6	-	32	17.33	-	58	
100	350	LEFT	6	-	16	8.00	-	30	
100	370	RIGHT	6	-	16	9.33	-	34	
100	460	RIGHT	6	-	16	9.33	-	34	
100	500	LEFT	6	-	16	8.00	-	30	
100	530	BOTH	6	-	32	17.33	-	58	
100	610	RIGHT	6	-	16	9.33	-	34	
100	700	LEFT	6	-	16	8.00	-	30	
100	730	LEFT	6	-	16	8.00	-	30	
100	760	LEFT	6	-	16	8.00	-	30	
100	780	LEFT	6	-	16	8.00	-	30	
100	790	RIGHT	6	-	16	9.33	-	34	
100	860	LEFT	6	-	16	8.00	-	30	
100	885	LEFT	6	-	16	8.00	-	30	
100	940	LEFT	6	-	16	8.00	-	30	
100	970	RIGHT	6	-	16	9.33	-	34	
100	1130	LEFT	6	-	16	8.00	-	30	
100	1250	LEFT	6	-	16	8.00	-	30	
100	1440	LEFT	6	-	16	8.00	-	30	
100	1630	LEFT	6	-	16	8.00	-	30	
100	1720	LEFT	6	-	16	8.00	-	30	
100	1860	LEFT	6	-	16	8.00	-	30	
100	1980	RIGHT	6	-	16	9.33	-	34	
100	3280	RIGHT	6	-	16	9.33	-	34	
100	3610	RIGHT	6	-	16	9.33	-	34	
100	3480	RIGHT	6	-	16	9.33	-	34	
100	3900	LEFT	6	-	16	8.00	-	30	
100	4000	LEFT	6	-	16	8.00	-	30	
100	4100	LEFT	6	-	16	8.00	-	30	
100	4200	LEFT	6	-	16	8.00	-	30	
100	4300	RIGHT	10	4	16	15.56	-	38	
100	4575	RIGHT	6	-	16	9.33	-	34	
100	4590	RIGHT	6	-	16	9.33	-	34	
100	4610	RIGHT	6	-	16	9.33	-	34	
100	5000	RIGHT	6	-	16	9.33	-	34	
99	100	RIGHT	6	-	16	9.33	-	34	
99	250	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 20 =			8	752	416	0	1,496		

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CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	MINUS	WESTBOUND LOCATION		416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
		SIDE	LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
99	500	RIGHT	6	-	16	9.33	-	34	
99	770	LEFT	6	-	16	8.00	-	30	
99	1000	LEFT	10	4	16	13.33	-	34	
99	1190	RIGHT	6	-	16	9.33	-	34	
99	1210	RIGHT	8	4	16	12.44	-	36	REPATCH
99		RIGHT	6	-	16	9.33	-	34	
99	1510	RIGHT	6	-	16	9.33	-	34	
99	1690	RIGHT	6	-	16	9.33	-	34	
99	1710	RIGHT	6	-	16	9.33	-	34	
99	1850	RIGHT	8	4	16	12.44	-	36	
99	2040	RIGHT	6	-	16	9.33	-	34	
99	2120	RIGHT	6	-	16	9.33	-	34	
99	2150	LEFT	6	-	16	8.00	-	30	
99	2250	RIGHT	6	-	16	9.33	-	34	
99	2390	RIGHT	6	-	16	9.33	-	34	
99	2420	LEFT	6	-	16	8.00	-	30	
99	3180	LEFT	6	-	16	8.00	-	30	
99	3220	RIGHT	6	-	16	9.33	-	34	
99	3240	LEFT	6	-	16	8.00	-	30	
99	3275	LEFT	6	-	16	8.00	-	30	
99	3530	LEFT	6	-	16	8.00	-	30	
99	3570	LEFT	6	-	16	8.00	-	30	
99	3640	RIGHT	6	-	16	9.33	-	34	
99	3685	LEFT	6	-	16	8.00	-	30	
99	3740	RIGHT	8	4	16	12.44	-	36	REPATCH
99	3765	RIGHT	6	-	16	9.33	-	34	
99	3960	RIGHT	6	-	16	9.33	-	34	
99	4090	LEFT	6	-	16	8.00	-	30	
99	4290	LEFT	6	-	16	8.00	-	30	
99	4475	BOTH	6	-	32	17.33	-	58	
99	5270	RIGHT	6	-	16	9.33	-	34	
98	485	RIGHT	30	12	16	-	46.67	58	
98	540	BOTH	6	-	32	17.33	-	58	
98	605	RIGHT	6	-	16	9.33	-	34	
98	735	RIGHT	6	-	16	9.33	-	34	
98	1080	RIGHT	6	-	16	9.33	-	34	
98	2410	RIGHT	8	4	16	12.44	-	36	
98	2920	LEFT	6	-	16	8.00	-	30	
98	2980	BOTH	6	-	32	17.33	-	58	
98	3390	RIGHT	6	-	16	9.33	-	34	
98	3420	LEFT	6	-	16	8.00	-	30	
98	3550	LEFT	6	-	16	8.00	-	30	
98	3590	RIGHT	6	-	16	9.33	-	34	
98	3680	LEFT	6	-	16	8.00	-	30	
98	3695	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 21 =				32	768	430	47	1,570	

CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	MINUS	WESTBOUND LOCATION		416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
		SIDE	LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
98	3840	LEFT	6	-	16	8.00	-	30	
98	3910	LEFT	6	-	16	8.00	-	30	
98	3940	LEFT	6	-	16	8.00	-	30	
98	4040	RIGHT	6	-	16	9.33	-	34	
98	4060	RIGHT	6	-	16	9.33	-	34	
98	4150	RIGHT	8	4	16	12.44	-	36	
98	4310	RIGHT	10	4	16	15.56	-	38	
98	4420	LEFT	6	-	16	8.00	-	30	
98	4490	LEFT	6	-	16	8.00	-	30	
98	4505	LEFT	6	-	16	8.00	-	30	
98	4545	RIGHT	10	4	16	15.56	-	38	
98	4750	LEFT	6	-	16	8.00	-	30	
98	4850	BOTH	6	-	32	17.33	-	58	
98	4910	LEFT	6	-	16	8.00	-	30	
97	10	LEFT	6	-	16	8.00	-	30	
97	120	RIGHT	6	-	16	9.33	-	34	
97	150	LEFT	6	-	16	8.00	-	30	
97	210	LEFT	6	-	16	8.00	-	30	
97	560	LEFT	6	-	16	8.00	-	30	
97	685	RIGHT	6	-	16	9.33	-	34	
97	880	RIGHT	10	4	16	15.56	-	38	
97	1110	RIGHT	6	-	16	9.33	-	34	
97	1180	BOTH	6	-	32	17.33	-	58	
97	1255	LEFT	6	-	16	8.00	-	30	
97	1335	LEFT	6	-	16	8.00	-	30	
97	1440	LEFT	6	-	16	8.00	-	30	
97	1620	RIGHT	10	4	16	15.56	-	38	
97	1750	RIGHT	6	-	16	9.33	-	34	
97	2150	RIGHT	6	-	16	9.33	-	34	
97	2200	RIGHT	6	-	16	9.33	-	34	
97	2605	RIGHT	10	4	16	15.56	-	38	
97	3020	RIGHT	6	-	16	9.33	-	34	
97	3240	RIGHT	6	-	16	9.33	-	34	
97	3320	LEFT	6	-	16	8.00	-	30	
97	3380	LEFT	6	-	16	8.00	-	30	
97	3610	RIGHT	6	-	16	9.33	-	34	
97	3940	RIGHT	6	-	16	9.33	-	34	
97	4625	RIGHT	6	-	16	9.33	-	34	
97	4980	LEFT	6	-	16	8.00	-	30	
97	5080	LEFT	6	-	16	8.00	-	30	
96	80	RIGHT	6	-	16	9.33	-	34	
96	510	RIGHT	6	-	16	9.33	-	34	
96	615	LEFT	6	-	16	8.00	-	30	
96	790	RIGHT	10	4	16	15.56	-	38	
96	870	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 22 =				28	752	448	0	1,520	

PROJECT NO: 1016-01-63

HWY: IH - 90

COUNTY: SAUK, COLUMBIA

MISCELLANEOUS QUANTITIES

SHEET:

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CONCRETE REPAIR SUMMARY (CONTINUED)

CONCRETE REPAIR SUMMARY (CONTINUED)

WB MILE MARKER	WESTBOUND LOCATION			416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
	MINUS	SIDE	LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
96	1715	LEFT	6	-	16	8.00	-	30	
96	2040	LEFT	6	-	16	8.00	-	30	
96	3100	RIGHT	6	-	16	9.33	-	34	
96	3920	RIGHT	6	-	16	9.33	-	34	
96	4400	RIGHT	6	-	16	9.33	-	34	
96	4560	RIGHT	6	-	16	9.33	-	34	
96	5130	LEFT	6	-	16	8.00	-	30	
96	5180	RIGHT	8	4	16	12.44	-	36	
95	20	RIGHT	6	-	16	9.33	-	34	
95	580	LEFT	6	-	16	8.00	-	30	
95	640	LEFT	6	-	16	8.00	-	30	
95	810	LEFT	6	-	16	8.00	-	30	
95	835	RIGHT	6	-	16	9.33	-	34	
95	880	RIGHT	6	-	16	9.33	-	34	
95	1160	RIGHT	10	4	16	15.56	-	38	
95	2900	RIGHT	6	-	16	9.33	-	34	
95	2930	LEFT	6	-	16	8.00	-	30	
95	3130	LEFT	6	-	16	8.00	-	30	
95	3370	RIGHT	6	-	16	9.33	-	34	
95	3640	LEFT	6	-	16	8.00	-	30	
95	3820	RIGHT	6	-	16	9.33	-	34	
95	3840	RIGHT	6	-	16	9.33	-	34	
95	3920	RIGHT	8	4	16	12.44	-	36	REPATCH
95	4005	LEFT	6	-	16	8.00	-	30	
95	4040	RIGHT	6	-	16	9.33	-	34	
95	4425	RIGHT	6	-	16	9.33	-	34	
95	4610	RIGHT	8	4	16	12.44	-	36	
95	4630	LEFT	6	-	16	8.00	-	30	
94	570	RIGHT	6	-	16	9.33	-	34	
94	720	RIGHT	6	-	16	9.33	-	34	
94	862	RIGHT	6	-	16	9.33	-	34	
94	940	LEFT	6	-	16	8.00	-	30	
94	1020	LEFT	6	-	16	8.00	-	30	
94	1110	LEFT	6	-	16	8.00	-	30	
94	1187	RIGHT	6	-	16	9.33	-	34	
94	1190	LEFT	6	-	16	8.00	-	30	
94	1290	RIGHT	6	-	16	9.33	-	34	
94	1590	RIGHT	6	-	16	9.33	-	34	
94	1740	LEFT	6	-	16	8.00	-	30	
94	1830	RIGHT	6	-	16	9.33	-	34	
94	1850	RIGHT	6	-	16	9.33	-	34	
94	1920	LEFT	6	-	16	8.00	-	30	
94	2080	LEFT	8	4	16	10.67	-	32	REPATCH
94	2150	RIGHT	6	-	16	9.33	-	34	
SUBTOTAL 23 =				20	704	405	0	1,436	

WB MILE MARKER	WESTBOUND LOCATION			416.0610	416.0620	416.1715	416.1725	690.0250	REMARKS
	MINUS	SIDE	LENGTH	DRILLED TIE BARS EACH	DRILLED DOWEL BARS EACH	CONCRETE PAVEMENT REPAIR SHES SY	CONCRETE PAVEMENT REPLACEMENT SHES SY	SAWING CONCRETE LF	
94	2200	BOTH	6	-	32	17.33	-	58	
94	2350	BOTH	6	-	32	17.33	-	58	
94	2550	LEFT	6	-	16	8.00	-	30	
94	2575	LEFT	6	-	16	8.00	-	30	
94	2605	LEFT	6	-	16	8.00	-	30	
94	2680	LEFT	6	-	16	8.00	-	30	
94	2860	LEFT	6	-	16	8.00	-	30	
94	2905	LEFT	6	-	16	8.00	-	30	
94	2950	LEFT	6	-	16	8.00	-	30	
94	2970	LEFT	6	-	16	8.00	-	30	
94	3125	LEFT	6	-	16	8.00	-	30	
94	3275	LEFT	6	-	16	8.00	-	30	
94	3340	LEFT	6	-	16	8.00	-	30	
94	3440	RIGHT	6	-	16	9.33	-	34	
94	3495	RIGHT	6	-	16	9.33	-	34	
94	4025	RIGHT	6	-	16	9.33	-	34	
94	4050	RIGHT	6	-	16	9.33	-	34	
94	4110	LEFT	6	-	16	8.00	-	30	
94	4150	RIGHT	6	-	16	9.33	-	34	
94	4190	LEFT	6	-	16	8.00	-	30	
94	4270	LEFT	6	-	16	8.00	-	30	
94	4300	RIGHT	6	-	16	9.33	-	34	
94	4400	LEFT	6	-	16	8.00	-	30	
94	4500	LEFT	6	-	16	8.00	-	30	
94	4600	RIGHT	6	-	16	9.33	-	34	
94	4700	BOTH	6	-	32	17.33	-	58	
94	4740	LEFT	6	-	16	8.00	-	30	
94	4750	LEFT	6	-	16	8.00	-	30	
94	4800	RIGHT	6	-	16	9.33	-	34	
94	4900	RIGHT	6	-	16	9.33	-	34	
94	5000	LEFT	6	-	16	8.00	-	30	
94	5100	LEFT	6	-	16	8.00	-	30	
94	5120	RIGHT	6	-	16	9.33	-	34	
94	5150	LEFT	6	-	16	8.00	-	30	
94	5200	LEFT	6	-	16	8.00	-	30	
94	5250	RIGHT	6	-	16	9.33	-	34	
93	70	LEFT	6	-	16	8.00	-	30	
93	150	LEFT	6	-	16	8.00	-	30	
93	180	RIGHT	6	-	16	9.33	-	34	
93	190	LEFT	8	4	16	10.67	-	32	REPATCH
93	215	LEFT	6	-	16	8.00	-	30	REPATCH
93	310	LEFT	6	-	16	8.00	-	30	
93	350	RIGHT	6	-	16	9.33	-	34	
93	375	LEFT	6	-	16	8.00	-	30	
93	415	LEFT	6	-	16	8.00	-	30	
SUBTOTAL 24 =				4	768	408	0	1,488	



FENCE SUMMARY (EASTBOUND)

FENCE SUMMARY (EASTBOUND - CONTINUED)

PAVEMENT MARKING SUMMARY

STATION TO	STATION	LOCATION	204.0170 REMOVING FENCE LF	616.0100 FENCE WOVEN WIRE (HEIGHT) (01. 4-FT) LF
SAUK COUNTY				
302+00 -	326+00		2,400	2,400
329+00 -	336+00		700	700
338+00 -	345+00		700	700
359+00 -	396+00		3,700	3,700
397+00 -	405+00		800	800
407+00 -	414+00		700	700
416+00 -	423+00		700	700
424+00 -	433+00		900	900
435+00 -	440+00		500	500
441+00 -	446+00		500	500
447+00 -	455+00		800	800
457+00 -	465+00		800	800
466+00 -	474+00		800	800
475+00 -	483+00		800	800
485+00 -	492+00		700	700
494+00 -	501+00		700	700
503+00 -	510+00		700	700
511+00 -	519+00		800	800
520+00 -	527+00		700	700
529+00 -	536+00		700	700
538+00 -	545+00		700	700
547+00 -	552+00		500	500
554+00 -	561+00		700	700
562+00 -	570+00		800	800
571+00 -	579+00		800	800
580+00 -	588+00		800	800
590+00 -	597+00		700	700
599+00 -	607+00		800	800
608+00 -	616+00		800	800
618+00 -	625+00		700	700
627+00 -	634+00		700	700
636+00 -	643+00		700	700
645+00 -	652+00		700	700
654+00 -	662+00		800	800
664+00 -	672+00		800	800
674+00 -	682+00		800	800
683+00 -	701+00		1,800	1,800
703+00 -	711+00		800	800
713+00 -	722+00		900	900
723+00 -	732+00		900	900
734+00 -	742+00		800	800
745+00 -	751+00		600	600
752+00 -	759+00		700	700
760+00 -	767+00		700	700
769+00 -	776+00		700	700

STATION TO	STATION	LOCATION	204.0170 REMOVING FENCE LF	616.0100 FENCE WOVEN WIRE (HEIGHT) (01. 4-FT) LF
777+00 -	786+00		900	900
788+00 -	796+00		800	800
807+00 -	815+00		800	800
816+00 -	825+00		900	900
826+00 -	834+00		800	800
836+00 -	843+00		700	700
845+00 -	853+00		800	800
855+00 -	863+00		800	800
COLUMBIA COUNTY				
865+00 -	900+00		3,700	3,700
901+00 -	907+00		600	600
909+00 -	1073+33		16,500	16,500
SUBTOTAL EASTBOUND =			66,100	66,100

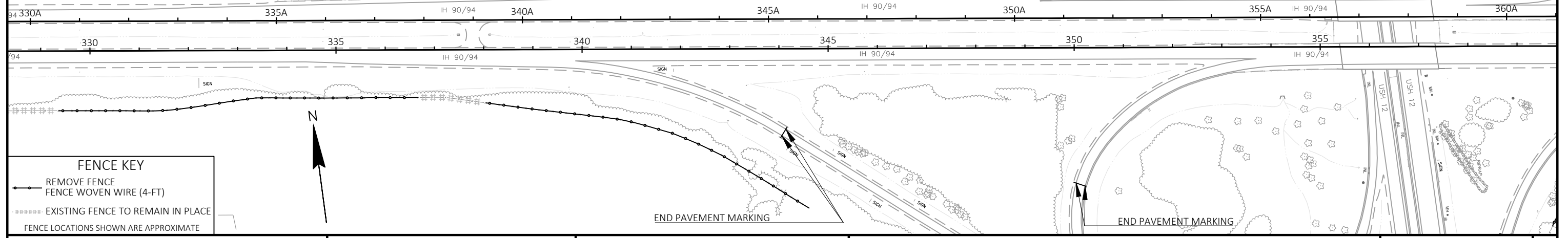
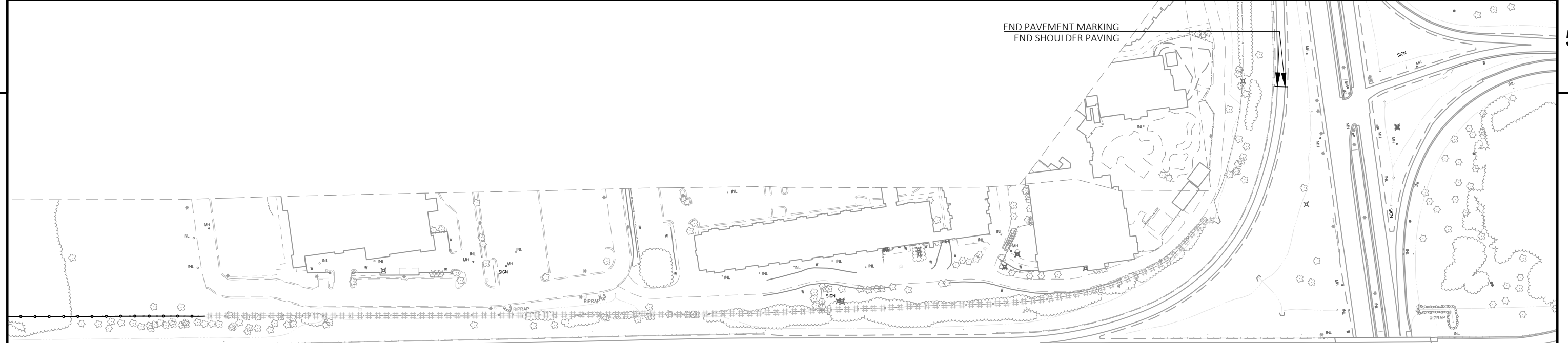
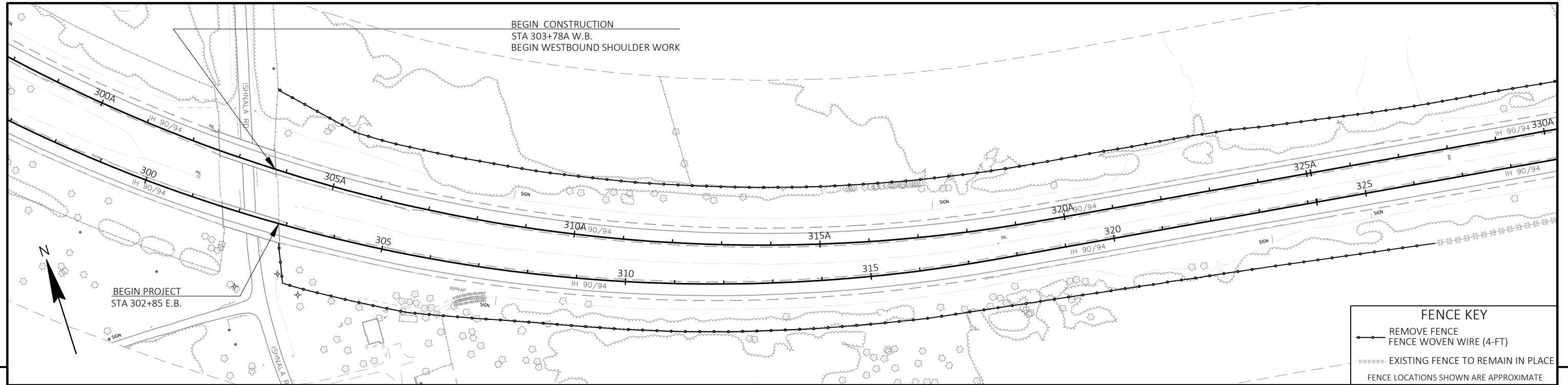
FENCE SUMMARY (WESTBOUND)

STATION TO	STATION	LOCATION	204.0170 REMOVING FENCE LF	616.0100 FENCE WOVEN WIRE (HEIGHT) (01. 4-FT) LF
SAUK COUNTY				
304+00A -	334+00A		3,000	3,000
375+00A -	466+00A		9,100	9,100
466+00A -	521+00A		5,500	5,500
521+00A -	616+00A		9,500	9,500
616+00A -	671+00A		5,500	5,500
671+00A -	745+00A		7,400	7,400
746+00A -	887+00A	COLUMBIA COUNTY	14,100	14,100
889+00A -	895+00A		600	600
897+00A -	1071+00A		18,100	18,100
SUBTOTAL WESTBOUND =			72,800	72,800
SUBTOTAL EASTBOUND =			66,100	66,100
TOTAL 0010			138,900	138,900

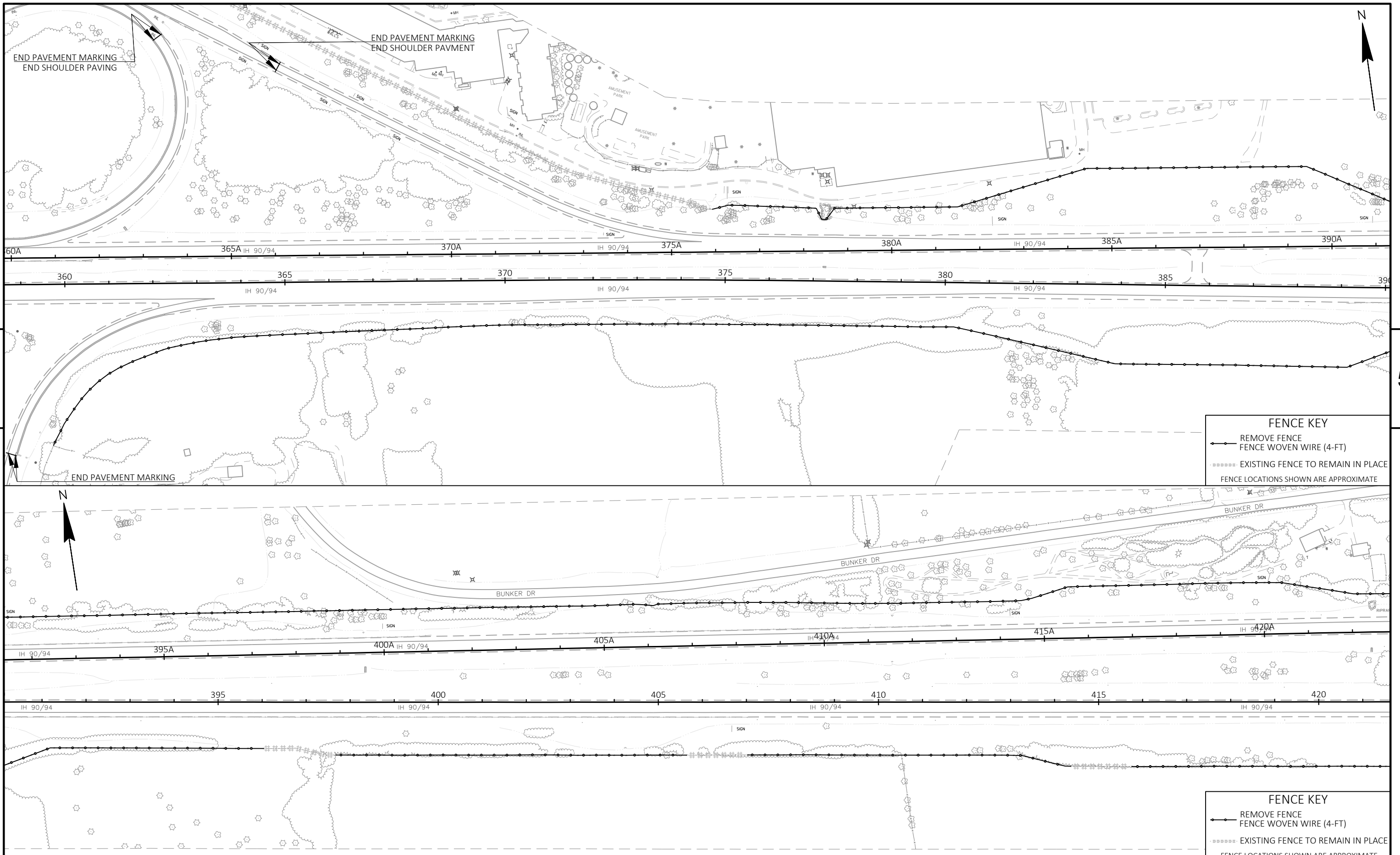
STATION TO	STATION	LOCATION	646.2020 MARKING LINE EPOXY 6-INCH LF	646.4020 MARKING LINE EPOXY 10-INCH LF	REMARKS
302+85 -	1044+23	EB LT EDGELINE	74,138	-	YELLOW
302+85 -	1044+23	EB RT EDGELINE	74,138	-	WHITE
302+85 -	1044+23	EB CL SKIPS	18,535	-	WHITE
303+78A -	1045+19A	WB LT EDGELINE	74,141	-	YELLOW
303+78A -	1045+19A	WB RT EDGELINE	74,141	-	WHITE
303+78A -	1045+19A	WB CL SKIPS	18,535	-	WHITE
<b>DELLS EXIT 92</b>					
			130	-	3' WHITE SKIPS
			-	215	WHITE
			233	-	YELLOW
			438	-	WHITE
			539	-	3' WHITE SKIPS
			462	-	WHITE
			-	164	WHITE
			325	-	YELLOW
			929	-	WHITE
			-	168	WHITE
			452	-	YELLOW
			72	-	12' WHITE SKIPS
			365	-	WHITE
			152	-	3' WHITE SKIPS
			-	221	WHITE
			152	-	YELLOW
			815	-	WHITE
			554	-	YELLOW
			-	304	WHITE
			509	-	3' WHITE SKIPS
			950	-	WHITE
			633	-	YELLOW
			-	337	WHITE
			240	-	3' WHITE SKIPS
			341,600	1,500	
TOTAL 0010:					

CONSTRUCTION STAKING

650.8000	650.9911
CONSTRUCTION	CONSTRUCTION
STAKING	STAKING
CONSTRUCTION	SUPPLEMENTAL
STAKING	CONTROL
RESURFACING	(PROJECT) (01.
REFERENCE	1016-01-63)
LF	EACH
104,037	1



PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: SAUK	PLAN	SHEET	<b>E</b>
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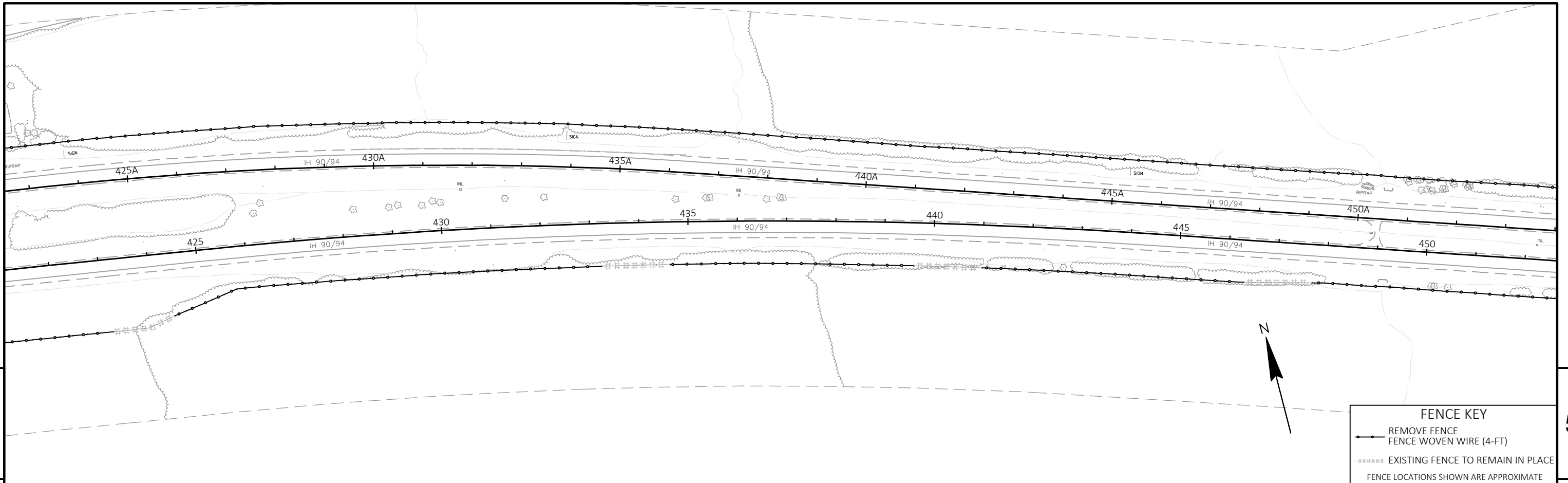


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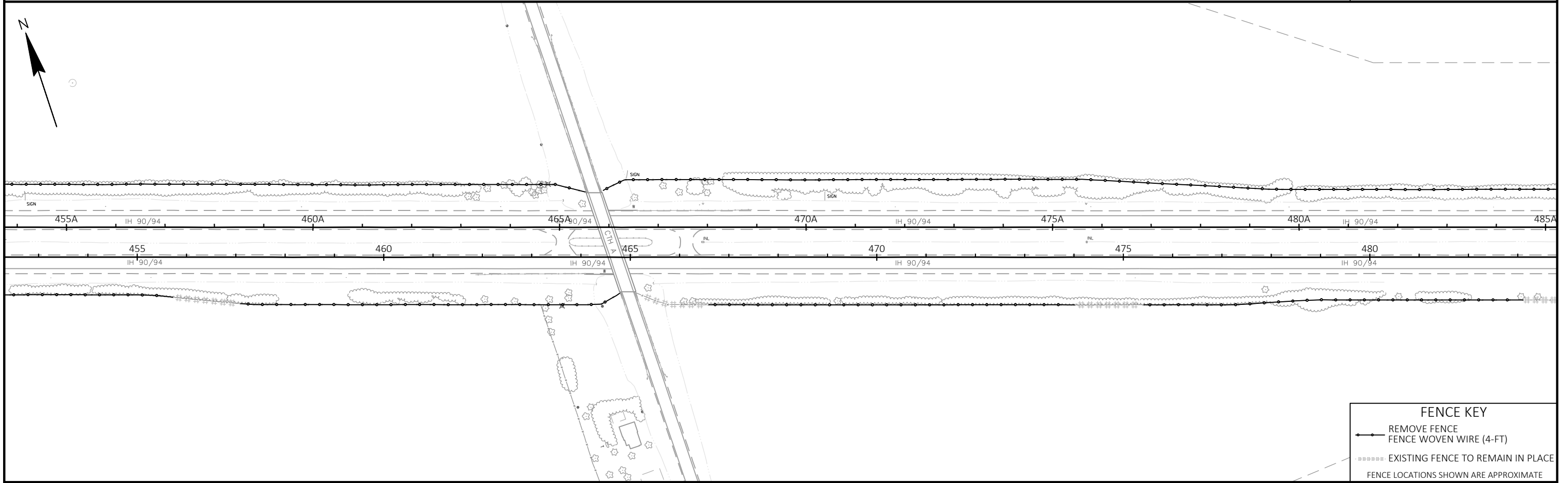
PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: SAUK	PLAN	SHEET	E
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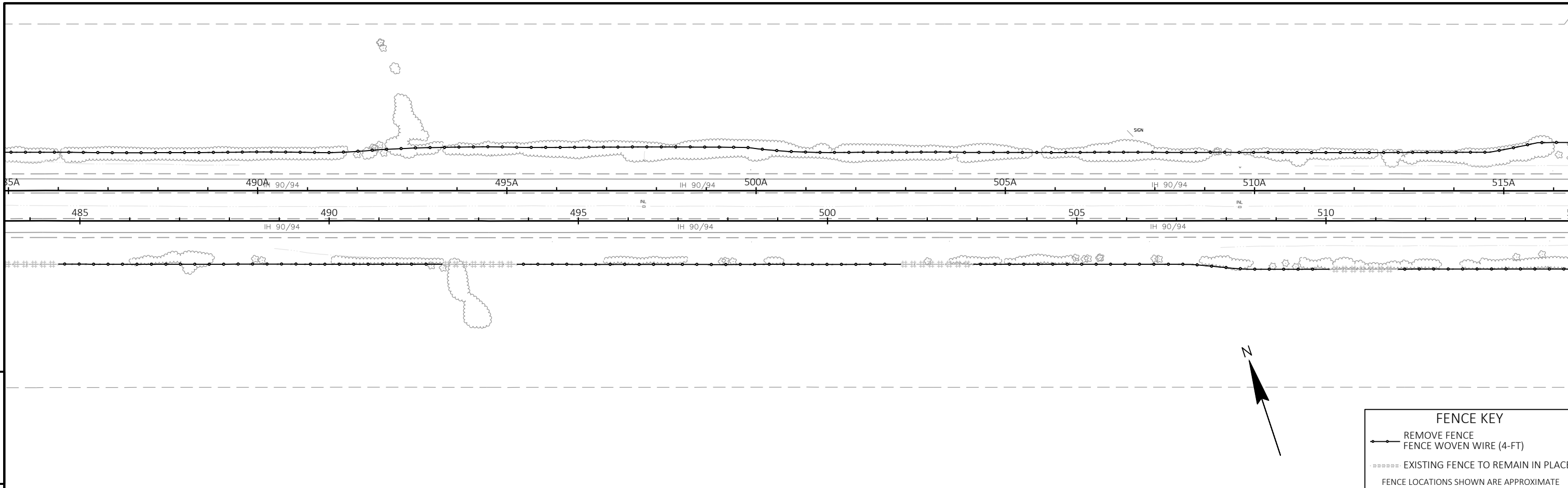


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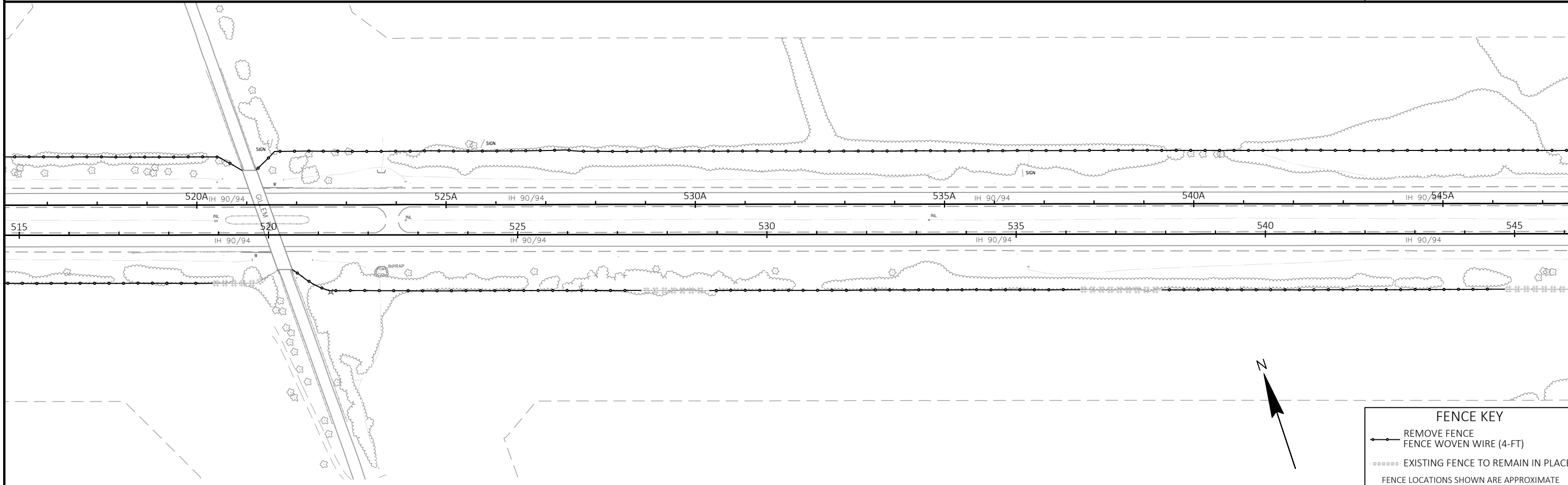


PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: SAUK	PLAN	SHEET	E
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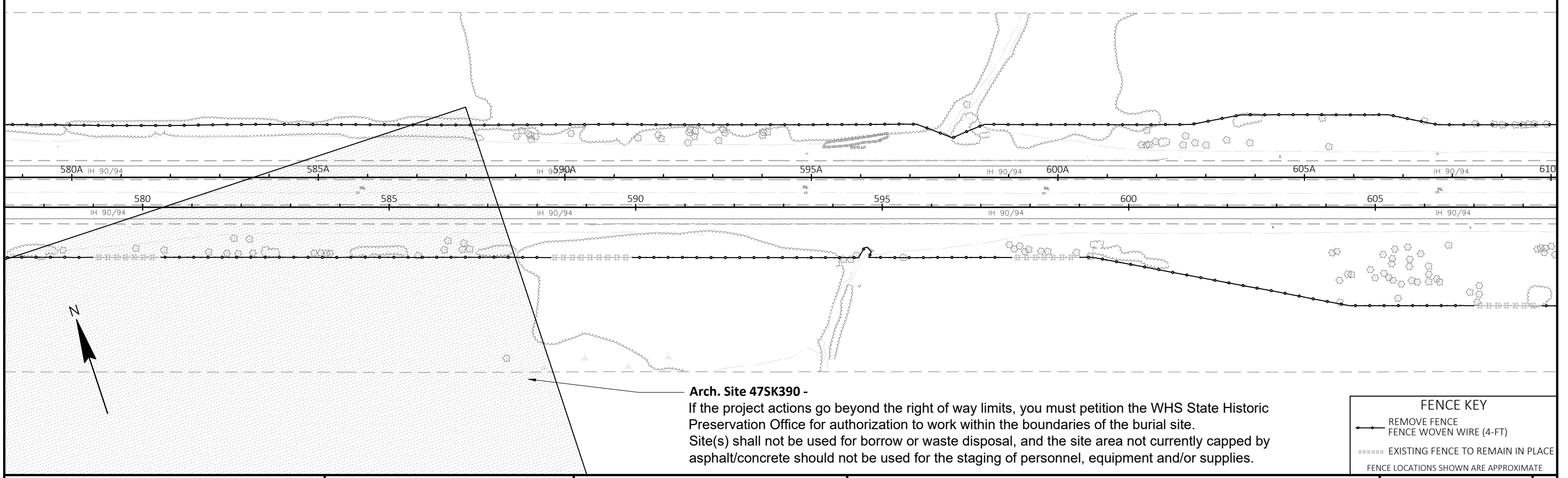
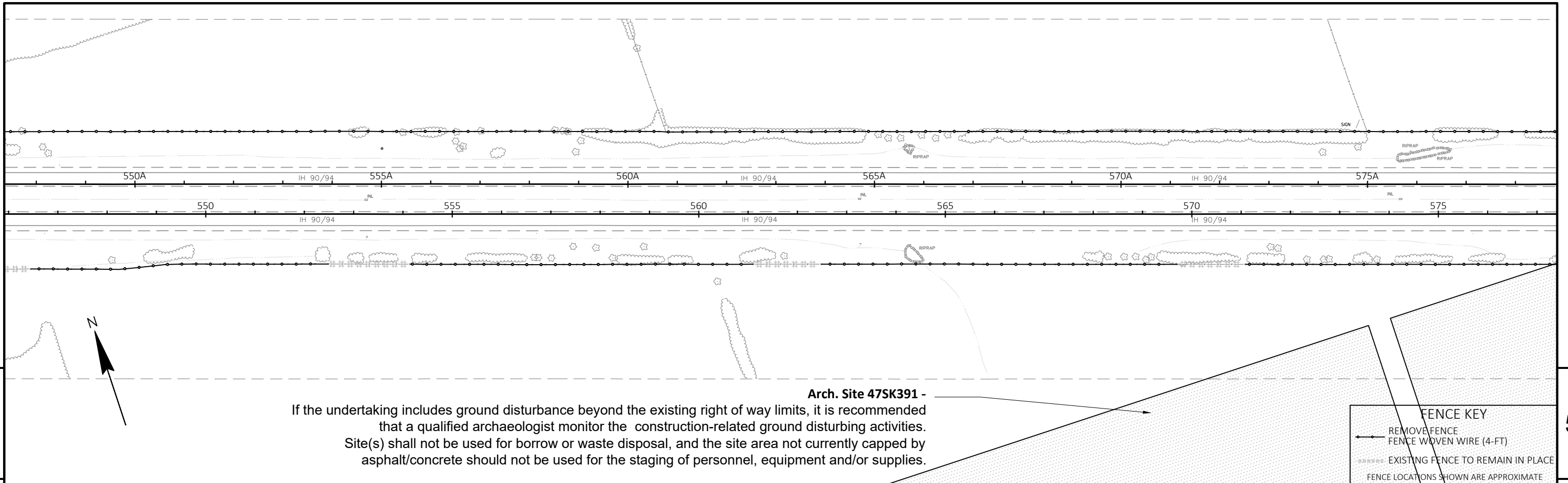


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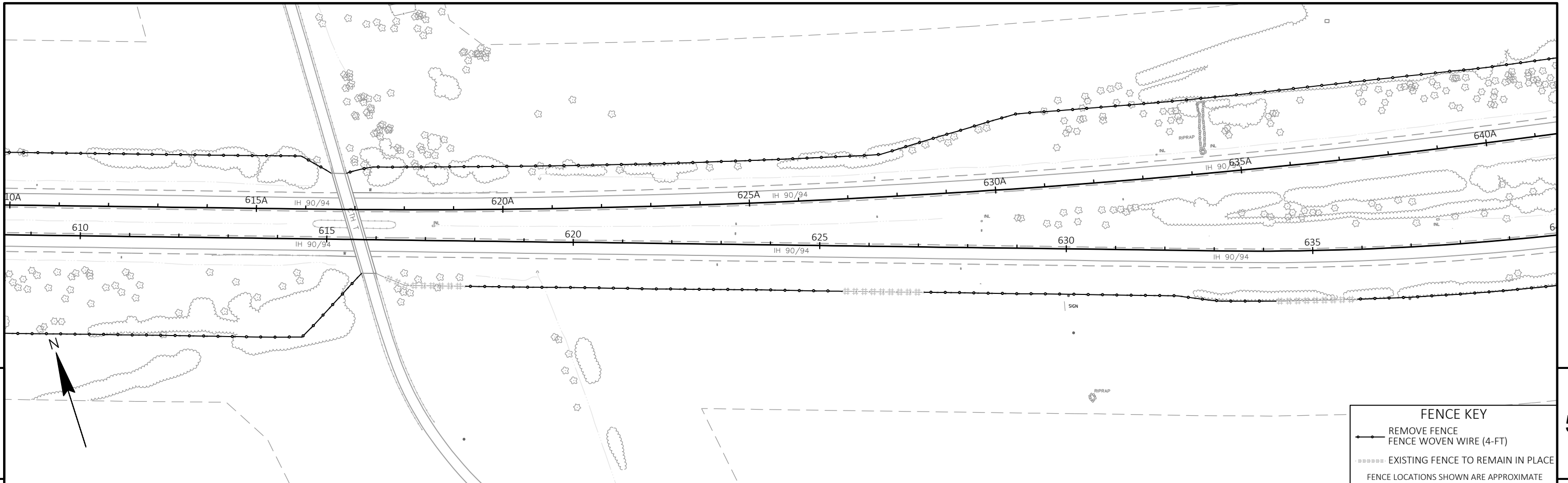
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PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: SAUK	PLAN	SHEET	E
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PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: SAUK	PLAN	SHEET	<b>E</b>
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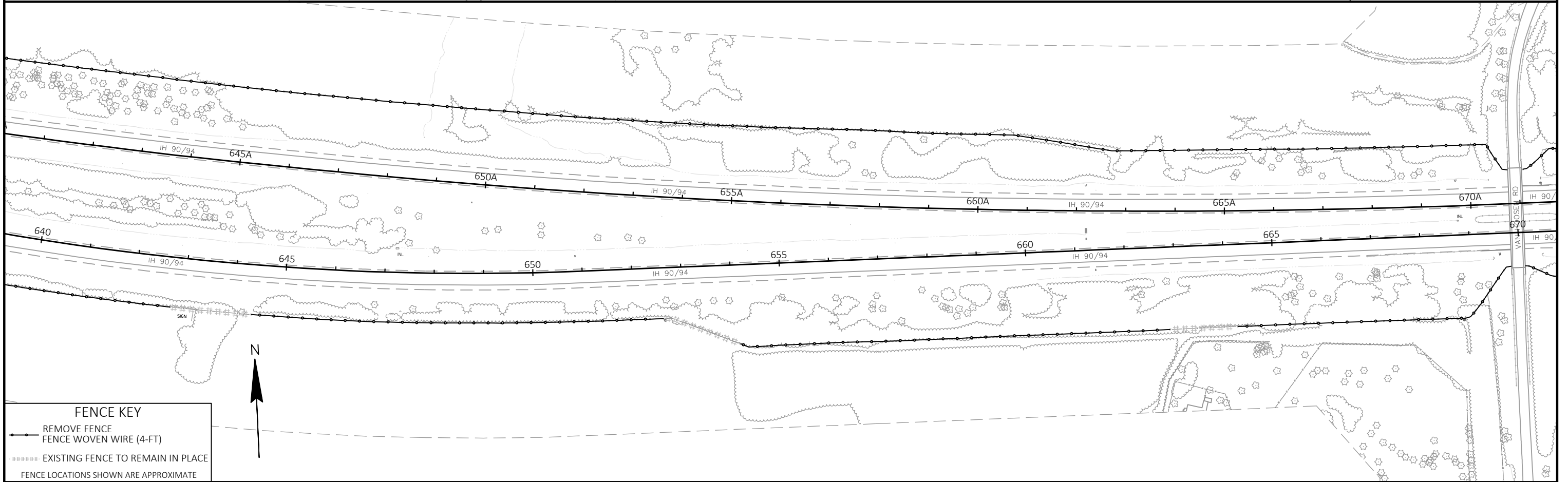
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**FENCE KEY**

- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- EXISTING FENCE TO REMAIN IN PLACE

FENCE LOCATIONS SHOWN ARE APPROXIMATE

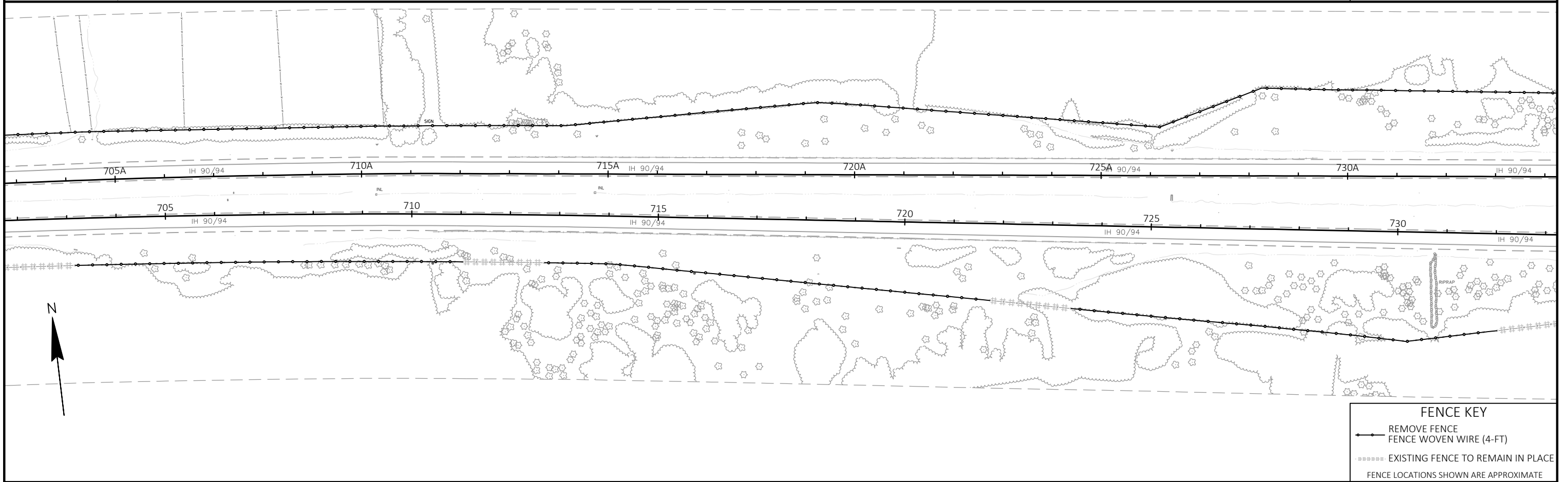
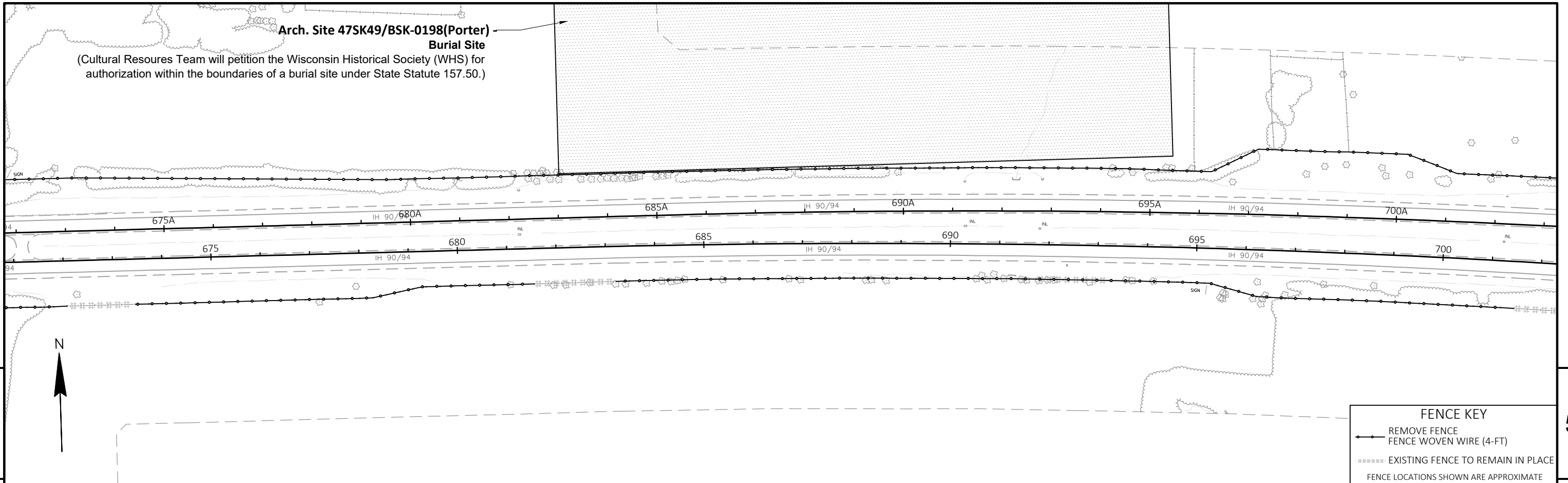


**FENCE KEY**

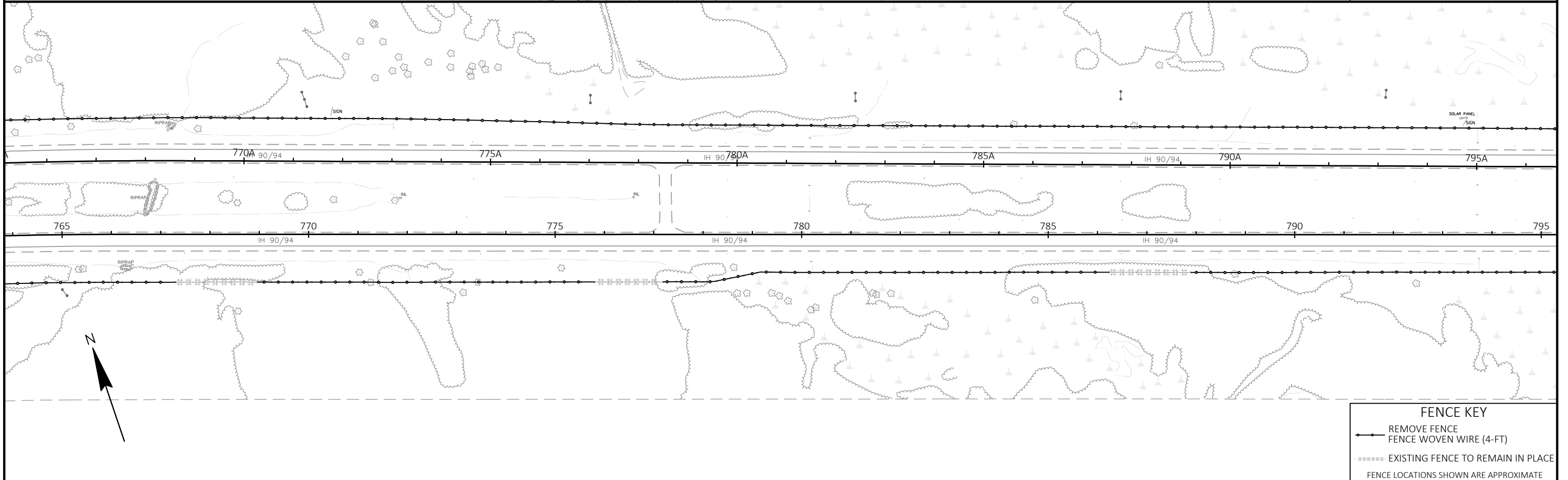
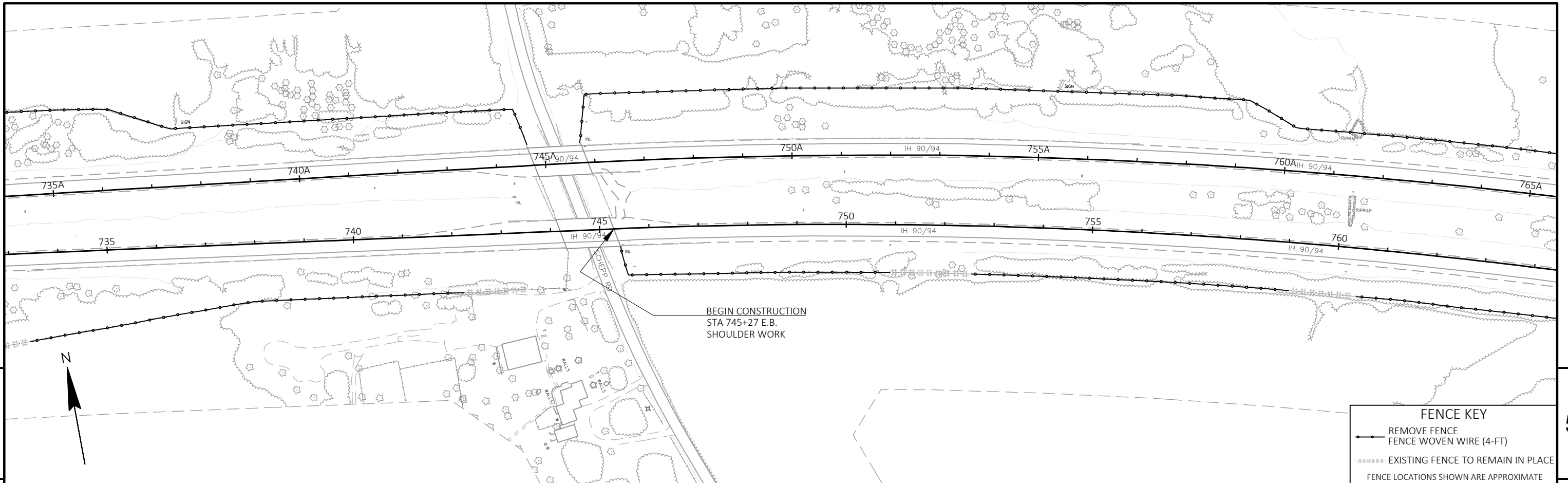
- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- EXISTING FENCE TO REMAIN IN PLACE

FENCE LOCATIONS SHOWN ARE APPROXIMATE

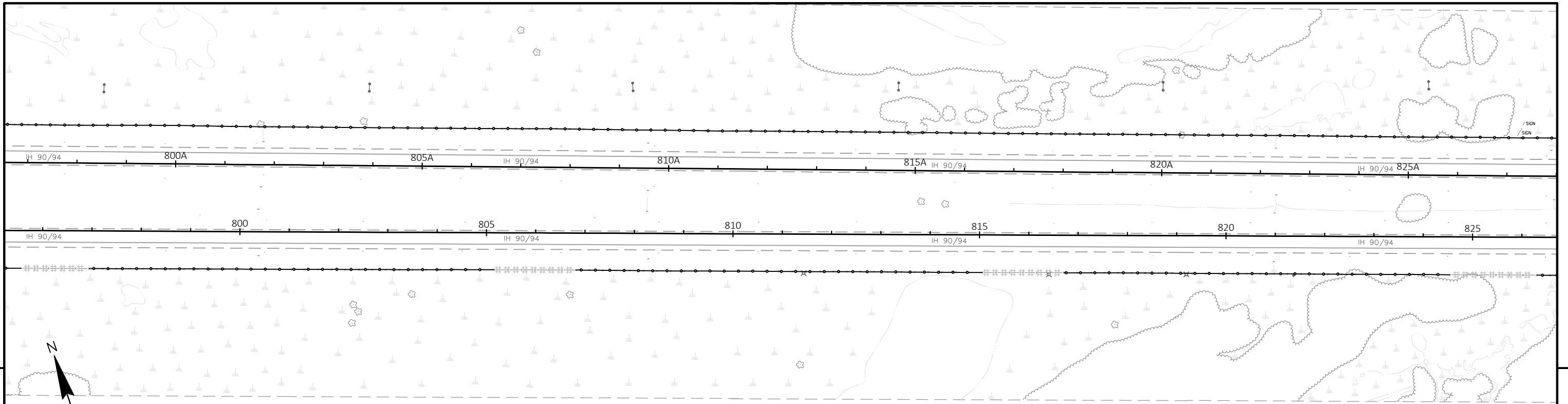
PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: SAUK	PLAN	SHEET	<b>E</b>
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PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: SAUK	PLAN	SHEET	<b>E</b>
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PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: SAUK	PLAN	SHEET	<b>E</b>
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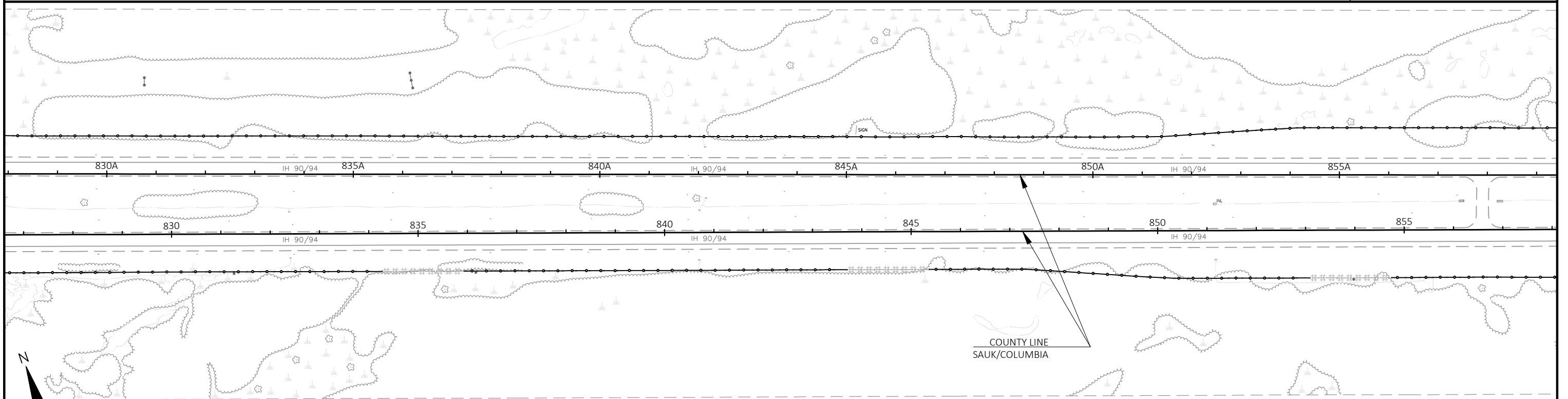
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**FENCE KEY**

- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- - - - - EXISTING FENCE TO REMAIN IN PLACE

FENCE LOCATIONS SHOWN ARE APPROXIMATE



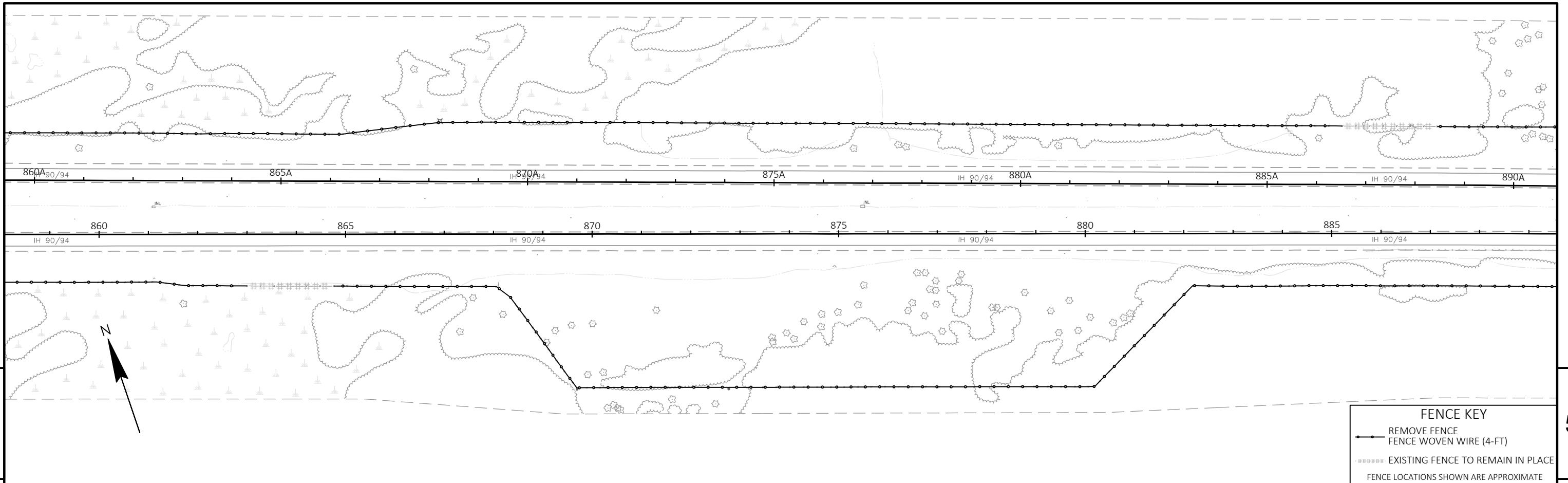
COUNTY LINE  
SAUK/COLUMBIA

**FENCE KEY**

- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- - - - - EXISTING FENCE TO REMAIN IN PLACE

FENCE LOCATIONS SHOWN ARE APPROXIMATE

PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: SAUK AND COLUMBIA	PLAN	SHEET	<b>E</b>
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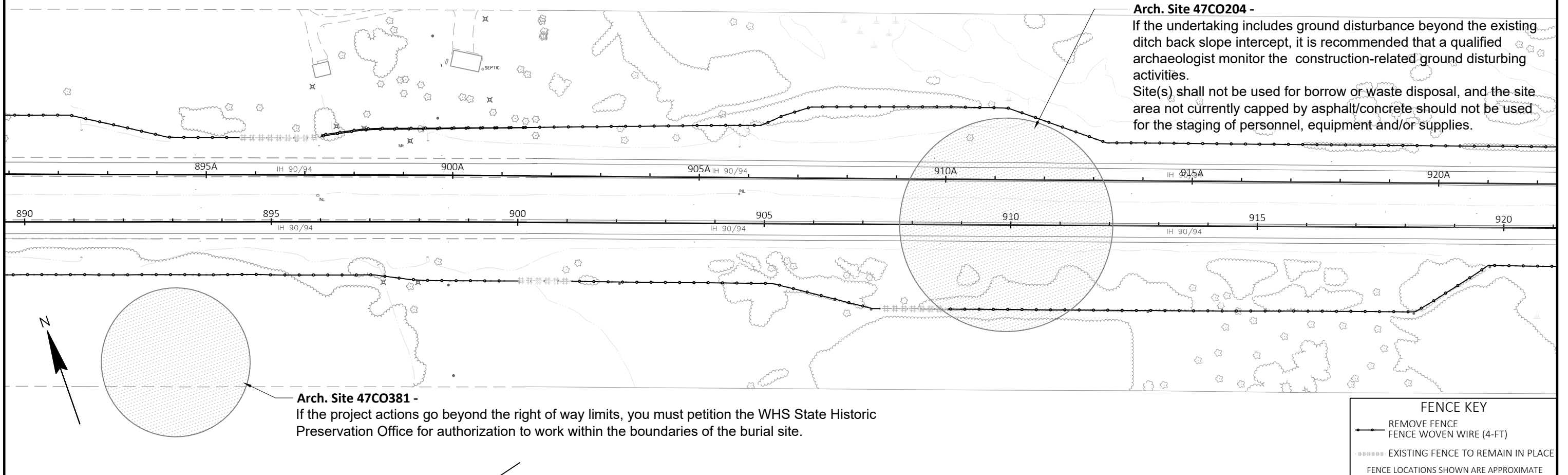
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**FENCE KEY**

- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- - - - - EXISTING FENCE TO REMAIN IN PLACE

FENCE LOCATIONS SHOWN ARE APPROXIMATE



**Arch. Site 47C0204 -**  
 If the undertaking includes ground disturbance beyond the existing ditch back slope intercept, it is recommended that a qualified archaeologist monitor the construction-related ground disturbing activities.  
 Site(s) shall not be used for borrow or waste disposal, and the site area not currently capped by asphalt/concrete should not be used for the staging of personnel, equipment and/or supplies.

**Arch. Site 47C0381 -**  
 If the project actions go beyond the right of way limits, you must petition the WHS State Historic Preservation Office for authorization to work within the boundaries of the burial site.

**FENCE KEY**

- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- - - - - EXISTING FENCE TO REMAIN IN PLACE

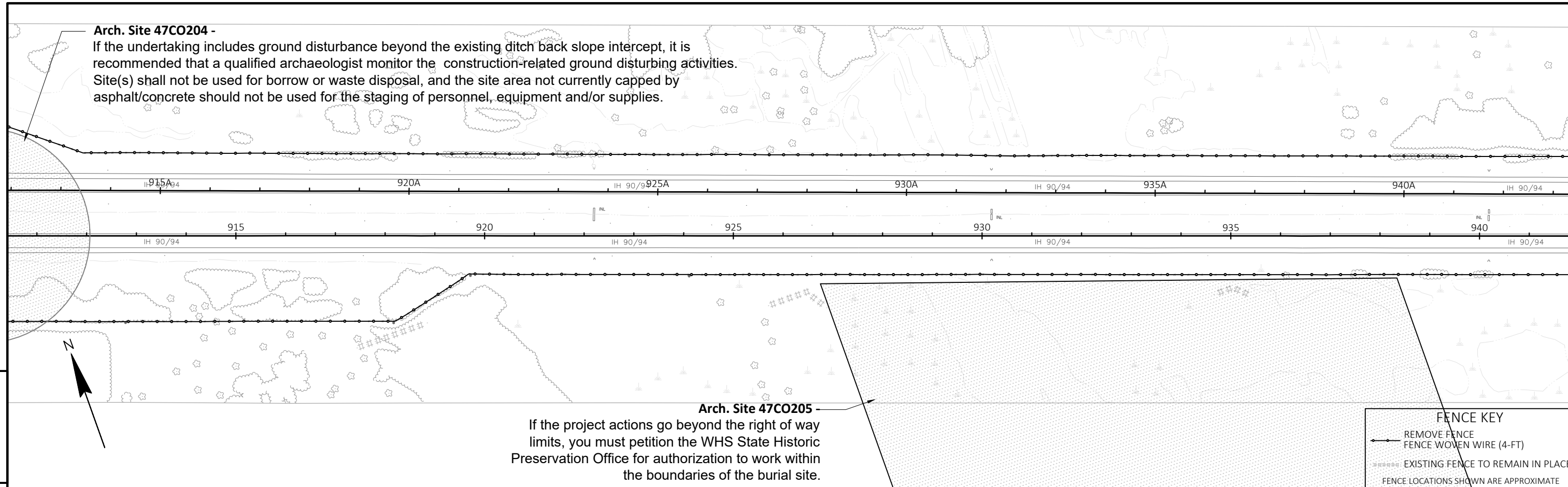
FENCE LOCATIONS SHOWN ARE APPROXIMATE

PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: COLUMBIA	PLAN	SHEET	<b>E</b>
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**Arch. Site 47CO204 -**

If the undertaking includes ground disturbance beyond the existing ditch back slope intercept, it is recommended that a qualified archaeologist monitor the construction-related ground disturbing activities. Site(s) shall not be used for borrow or waste disposal, and the site area not currently capped by asphalt/concrete should not be used for the staging of personnel, equipment and/or supplies.



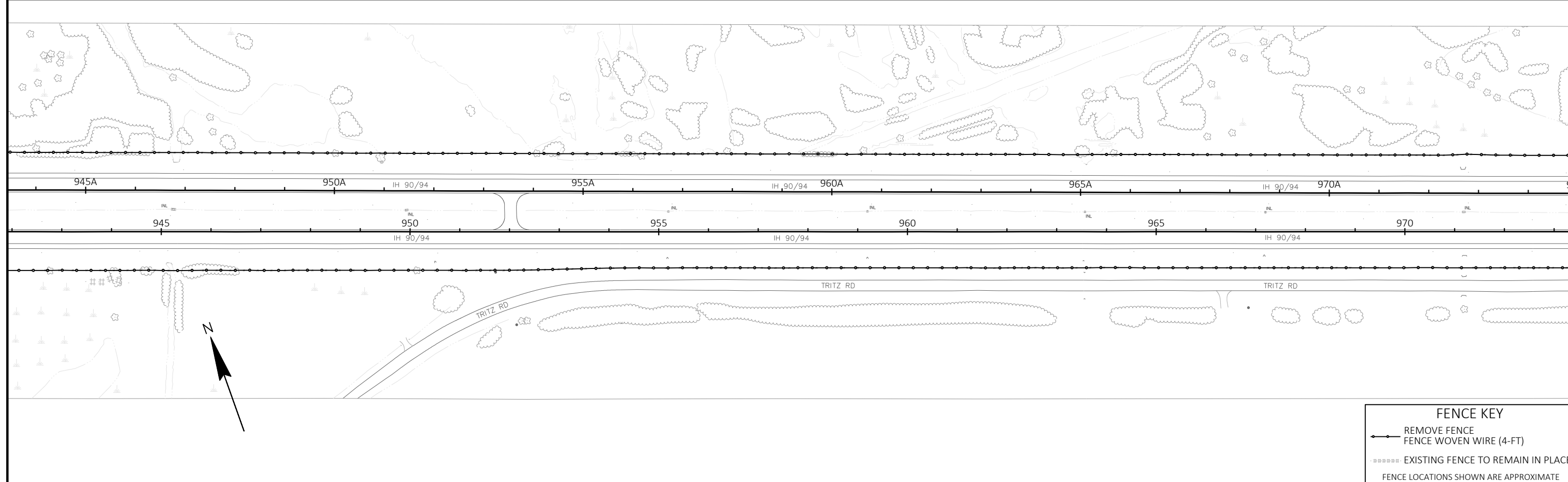
**Arch. Site 47CO205 -**

If the project actions go beyond the right of way limits, you must petition the WHS State Historic Preservation Office for authorization to work within the boundaries of the burial site.

**FENCE KEY**

- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- EXISTING FENCE TO REMAIN IN PLACE

FENCE LOCATIONS SHOWN ARE APPROXIMATE



**FENCE KEY**

- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- EXISTING FENCE TO REMAIN IN PLACE

FENCE LOCATIONS SHOWN ARE APPROXIMATE

PROJECT NO: 1016-01-63

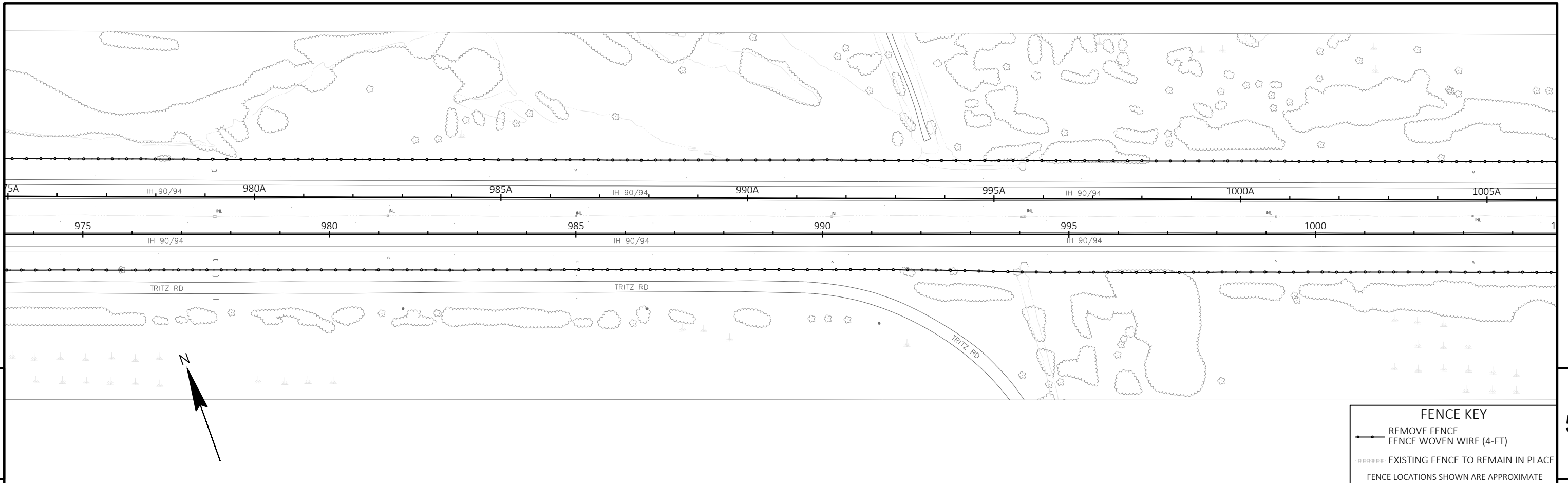
HWY: IH - 90

COUNTY: COLUMBIA

PLAN

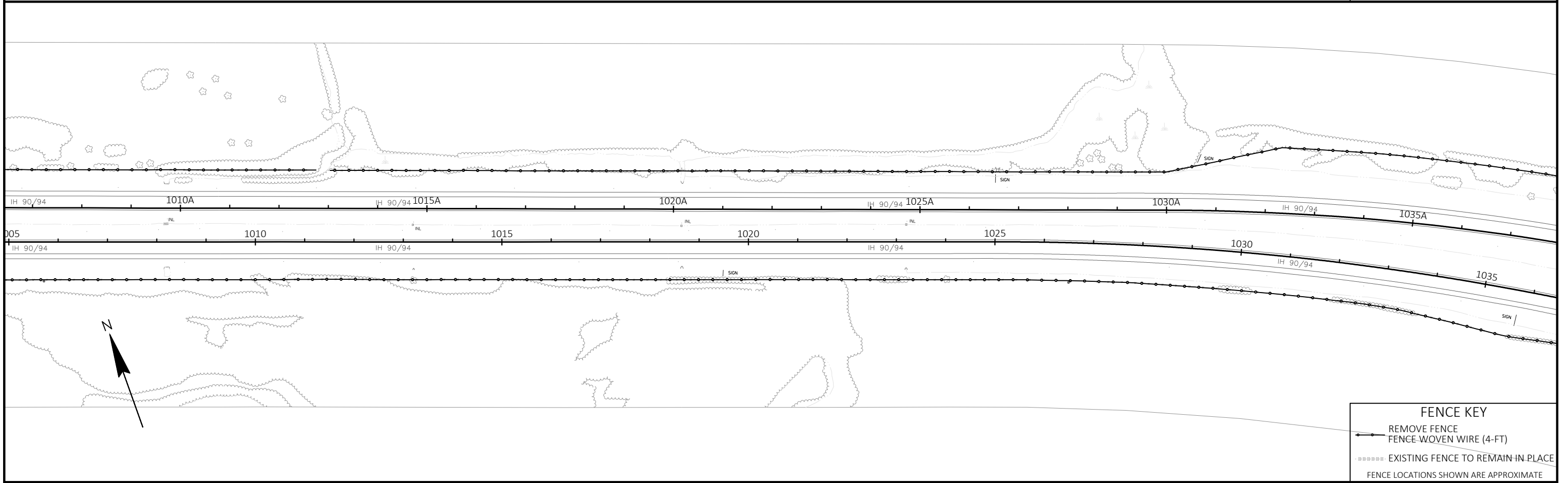
SHEET

**E**

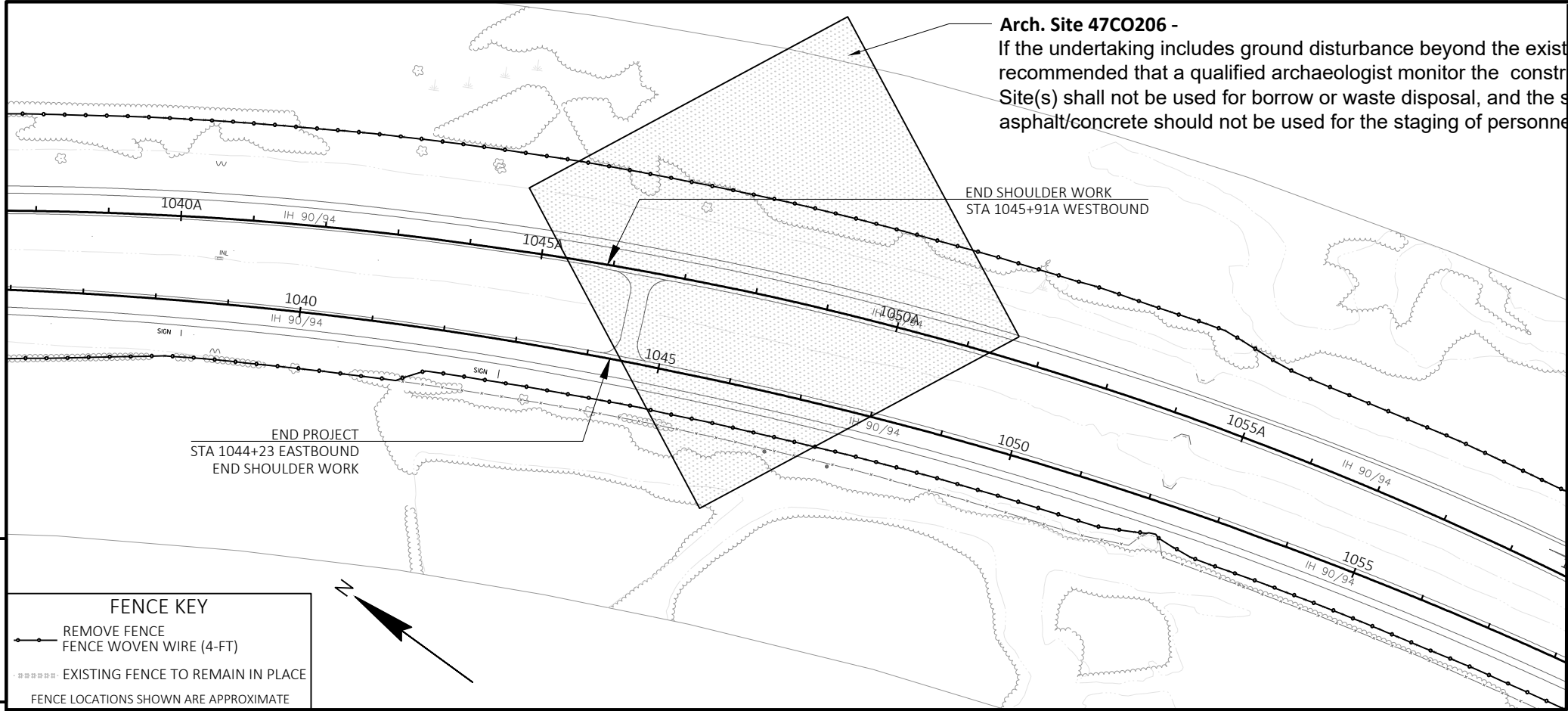


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PROJECT NO: 1016-01-63	HWY: IH - 90	COUNTY: COLUMBIA	PLAN	SHEET	<b>E</b>
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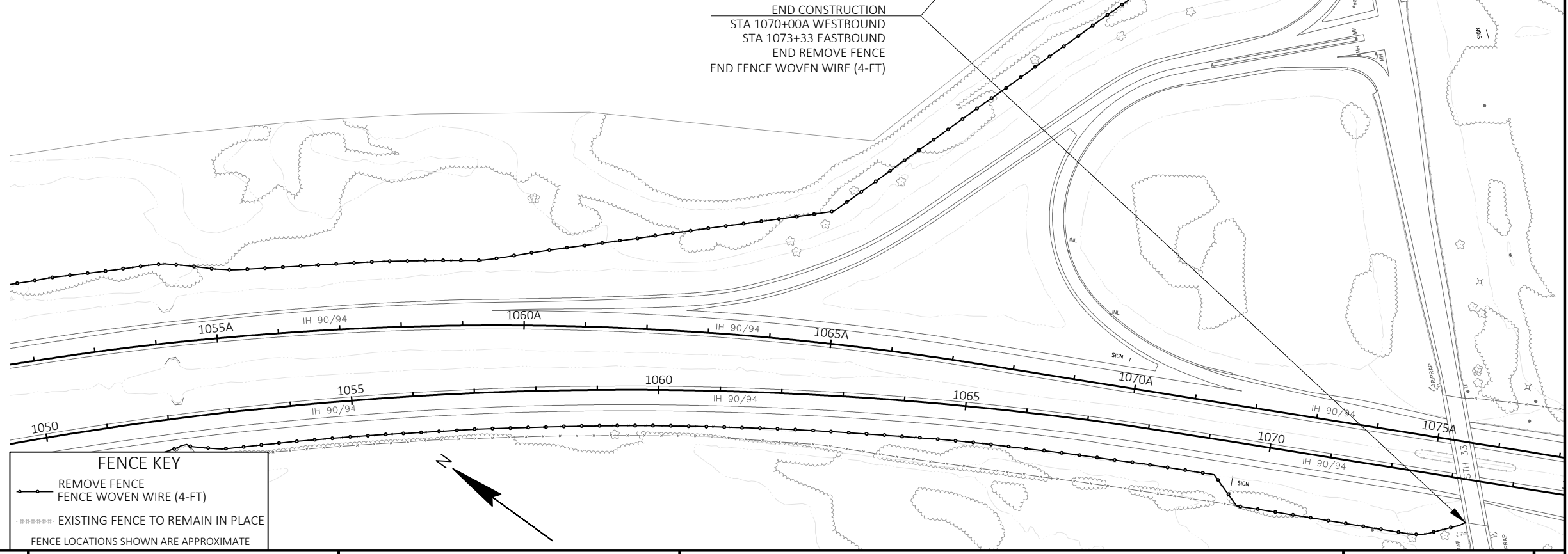
**Arch. Site 47CO206 -**  
 If the undertaking includes ground disturbance beyond the existing ditch back slope intercept, it is recommended that a qualified archaeologist monitor the construction-related ground disturbing activities. Site(s) shall not be used for borrow or waste disposal, and the site area not currently capped by asphalt/concrete should not be used for the staging of personnel, equipment and/or supplies.

**5**

**FENCE KEY**

- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- EXISTING FENCE TO REMAIN IN PLACE

FENCE LOCATIONS SHOWN ARE APPROXIMATE



**5**

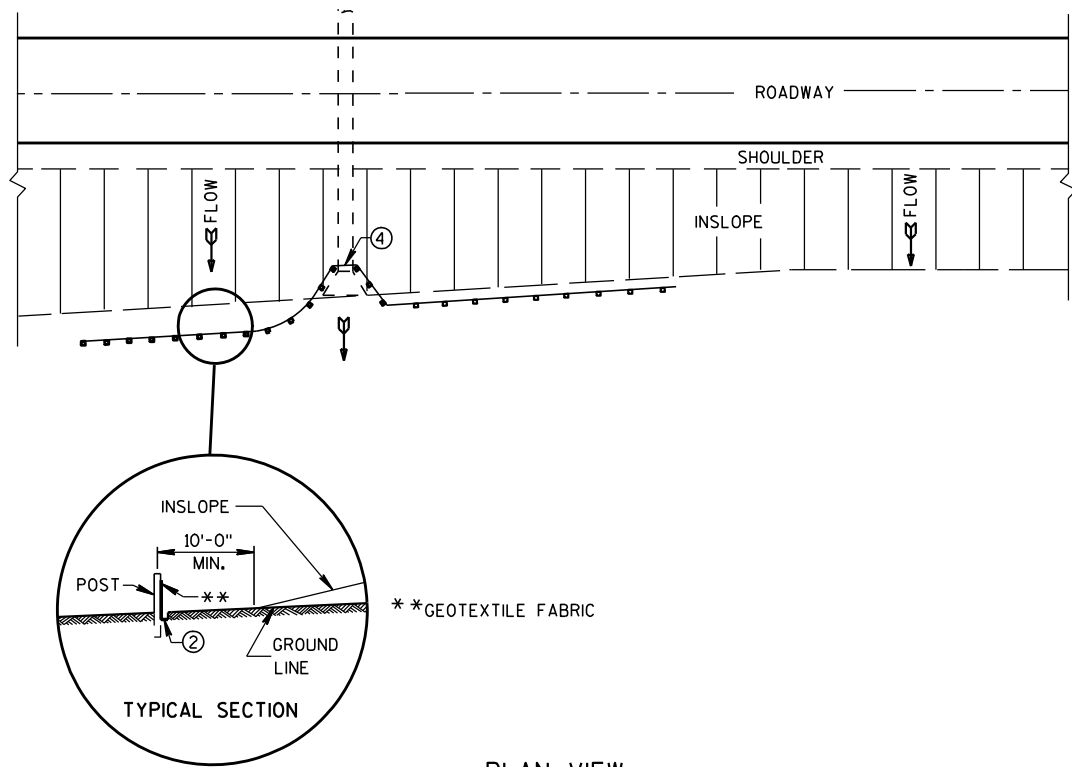
**FENCE KEY**

- REMOVE FENCE
- FENCE WOVEN WIRE (4-FT)
- EXISTING FENCE TO REMAIN IN PLACE

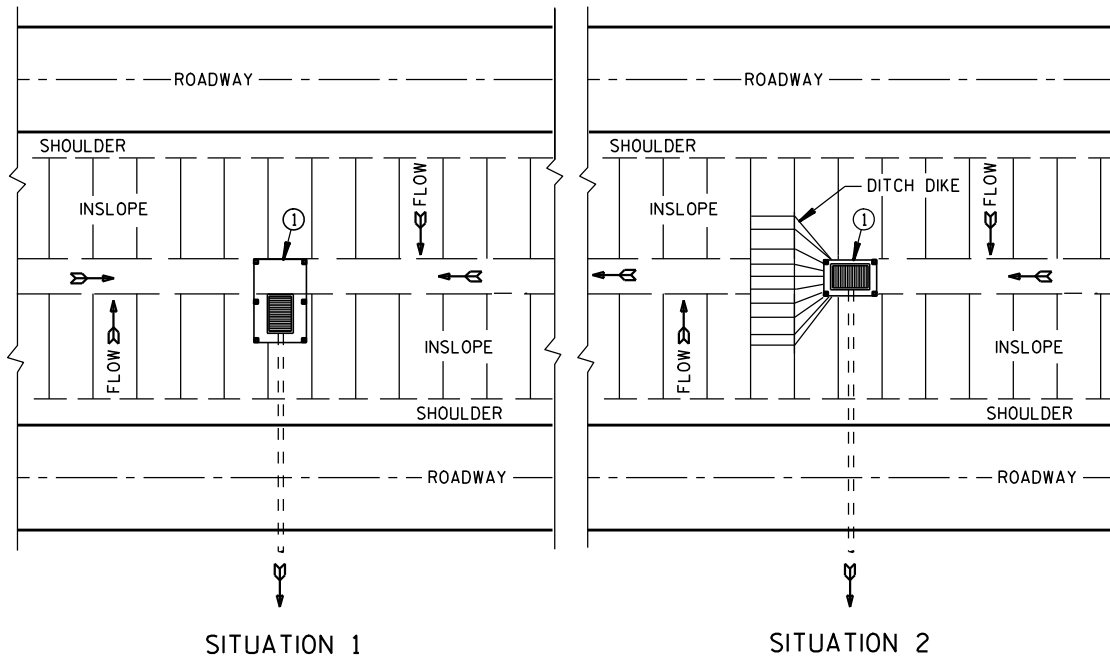
FENCE LOCATIONS SHOWN ARE APPROXIMATE

## Standard Detail Drawing List

08E09-06	SILT FENCE
13A05-06A	SHOULDER RUMBLE STRIPS, DIVIDED ROADWAY
13A05-06B	SHOULDER RUMBLE STRIPS, DIVIDED ROADWAY
13C09-17A	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C09-17B	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C09-17C	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
15B01-08A	FENCE WOVEN WIRE
15B01-08B	FENCE WOVEN WIRE
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C19-08C	MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY
15C31-05A	PAVEMENT MARKING EXIT RAMP AND PARALLEL EXIT RAMP
15C31-05C	PAVEMENT MARKING ENTRANCE RAMP AND PARALLEL ENTRANCE RAMP
15D12-11A	TRAFFIC CONTROL, LANE CLOSURE
15D12-11B	TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION
15D12-11D	TRAFFIC CONTROL, LANE CLOSURE, BASIC TRAFFIC QUEUE WARNING SYSTEM
15D15-07A	TRAFFIC CONTROL, PARALLEL ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-07E	TRAFFIC CONTROL, PARALLEL EXIT RAMP WITHIN LANE CLOSURE
15D27-03	TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH



PLAN VIEW  
TYPICAL APPLICATION OF SILT FENCE

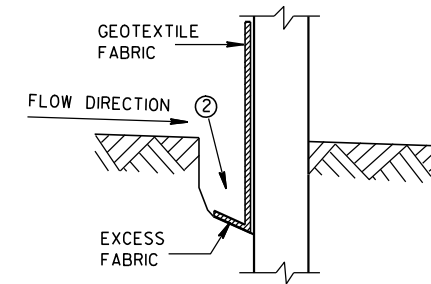


SITUATION 1 SITUATION 2  
PLAN VIEW  
SILT FENCE AT MEDIAN SURFACE DRAINS

**GENERAL NOTES**

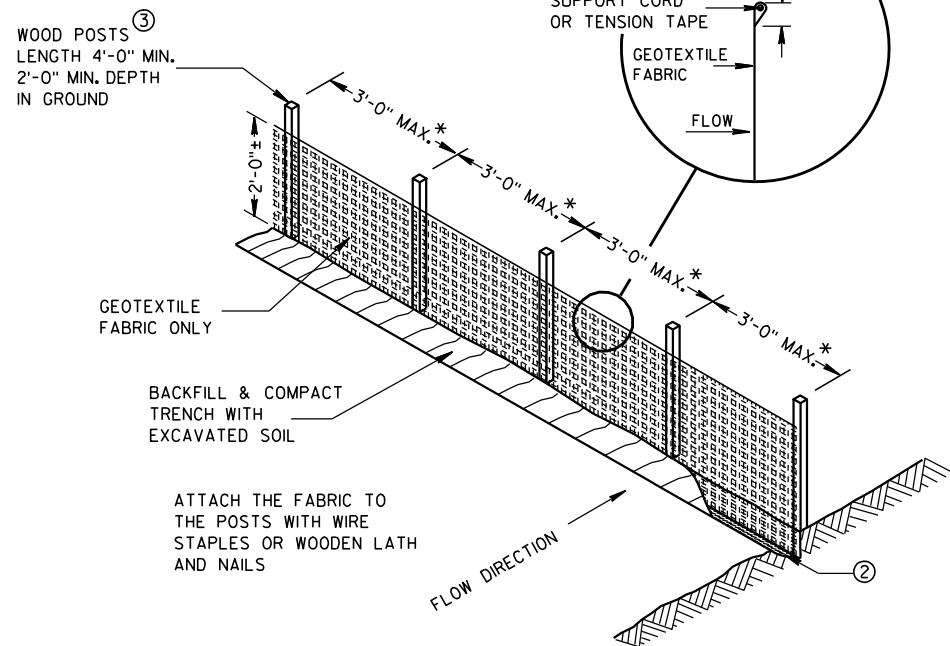
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

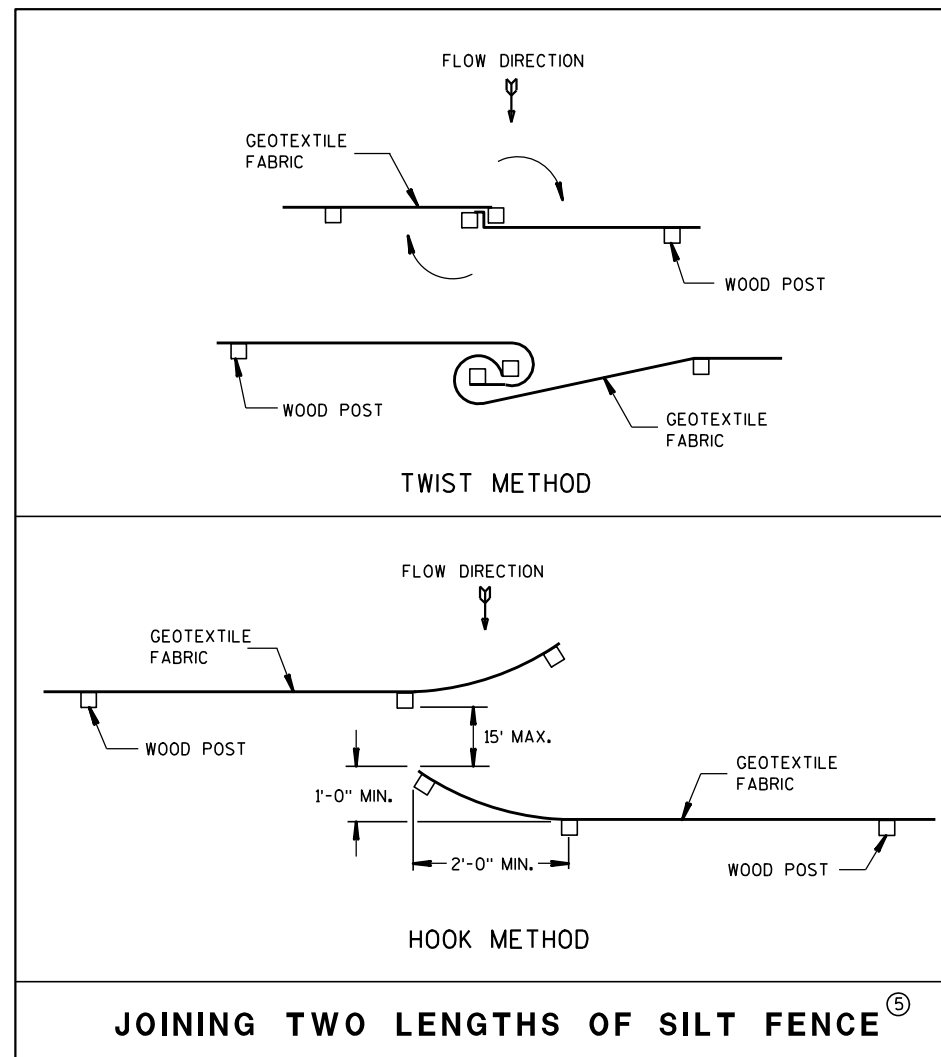


TRENCH DETAIL

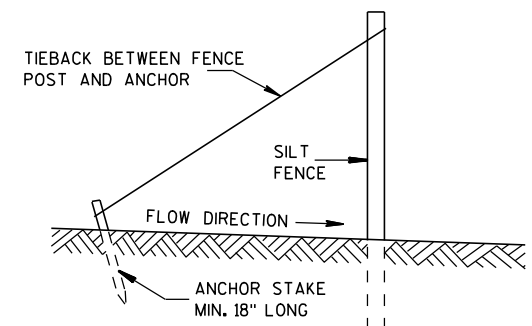
NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS



SILT FENCE



JOINING TWO LENGTHS OF SILT FENCE ⑤

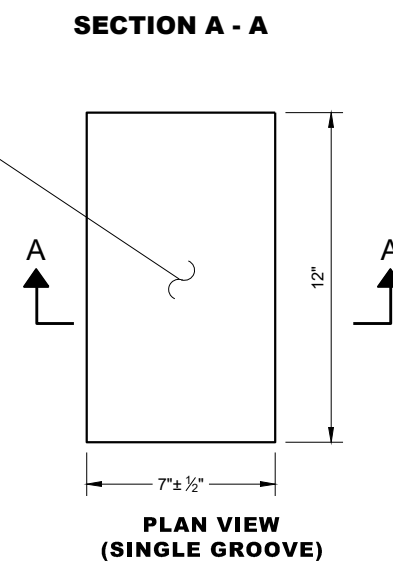
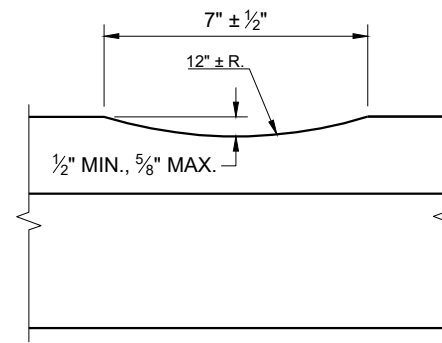
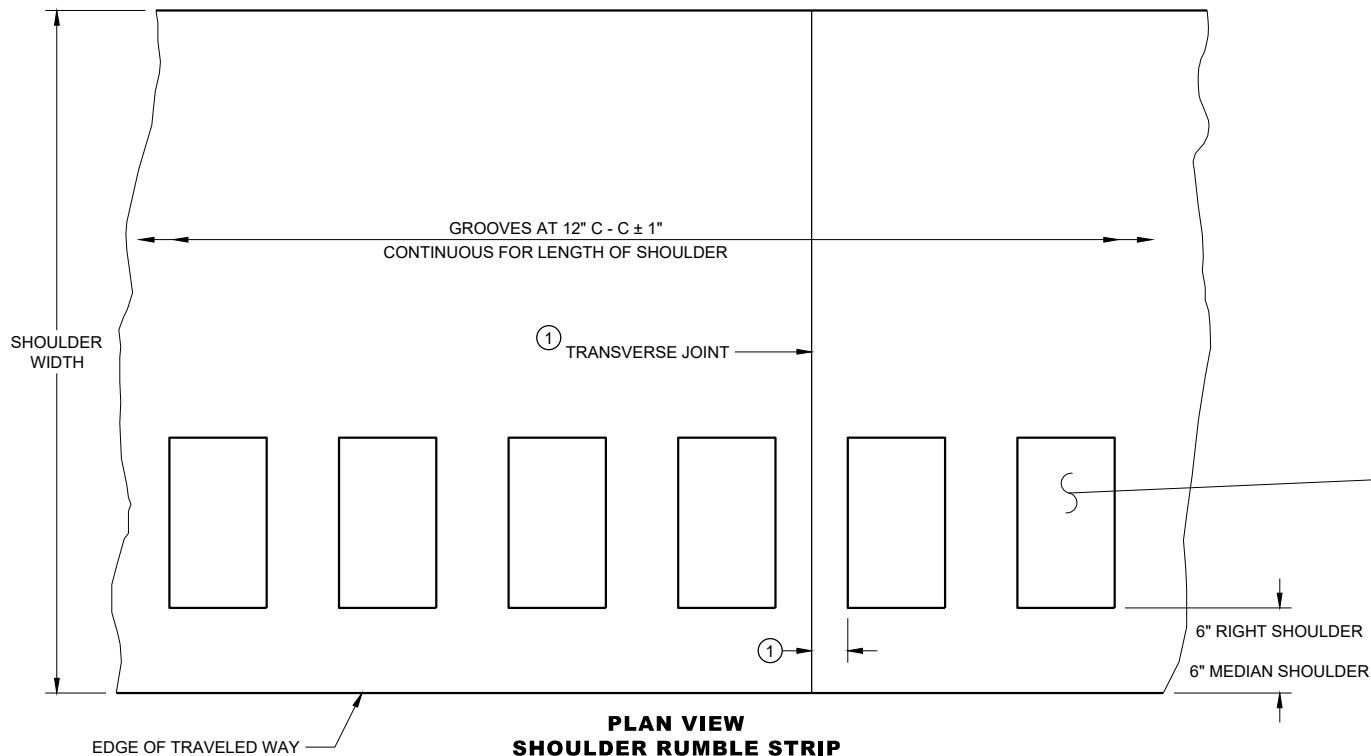


SILT FENCE TIE BACK  
(WHEN REQUIRED BY THE ENGINEER)

**SILT FENCE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
4-29-05 /S/ Beth Cannestra  
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER  
FHWA

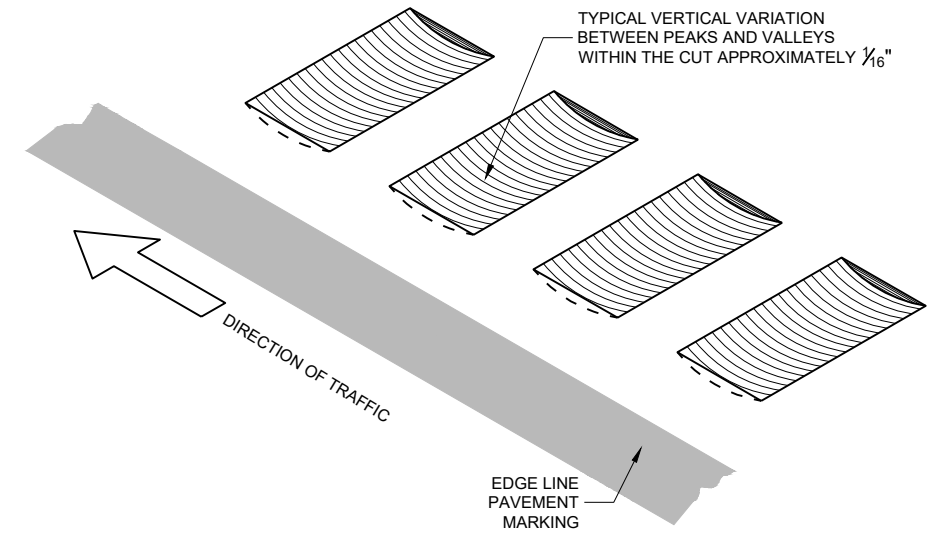


**GENERAL NOTES**

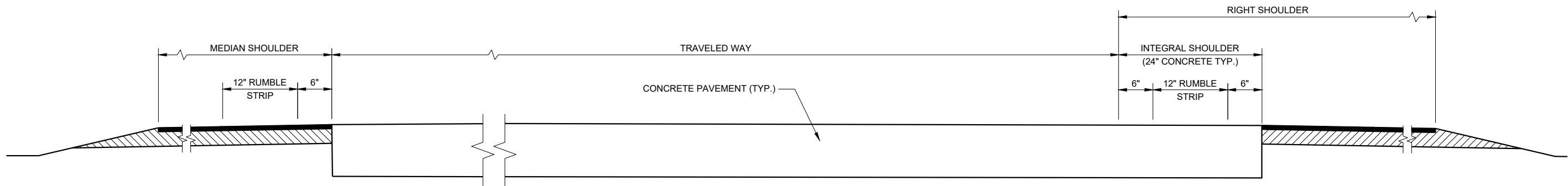
SDD 13A5, SHEET "b" SHOWS THE LOCATION OF THE RUMBLE STRIPS AT RAMP AND GORE LOCATIONS.

**RUMBLE STRIPS ON EXPRESSWAYS:**  
DO NOT INSTALL SHOULDER RUMBLE STRIPS ACROSS SIDE ROAD INTERSECTIONS, COMMERCIAL AND PRIVATE DRIVEWAYS, ADJACENT TO RIGHT TURN LANES, LEFT TURN LANES, TURN LANE TAPERS, 25' IN ADVANCE OF BRIDGE DECKS, 25' IN ADVANCE OF BRIDGE APPROACHES, OR 100 FEET IN ADVANCE OF RAILROAD CROSSINGS.

- ① CONCRETE PAVEMENT - RUMBLE STRIPS SHALL BE A MINIMUM OF 6 INCHES AWAY FROM TRANSVERSE JOINTS.



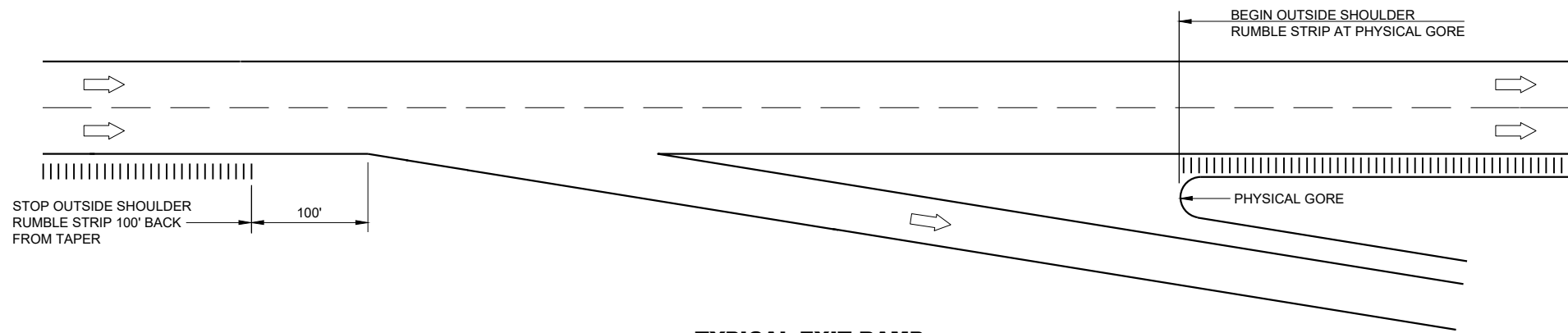
**PLACEMENT DETAIL FOR RUMBLE STRIP**



**TYPICAL SHOULDER RUMBLE STRIPS  
(ONE ROADWAY IS SHOWN)**

**SHOULDER RUMBLE STRIPS,  
DIVIDED ROADWAY**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**TYPICAL EXIT RAMP**

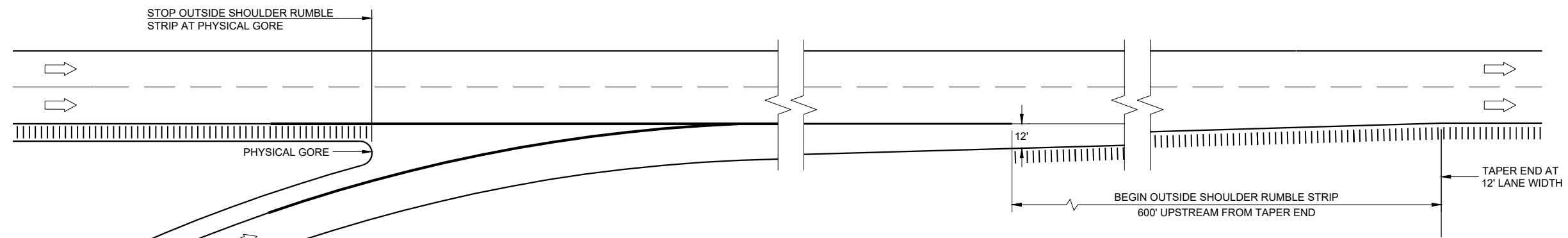
**GENERAL NOTES**

NO RUMBLE STRIP ON EXIT, DIRECTIONAL OR ENTRANCE RAMP, EXCEPT NEAR THE ENTRANCE TAPER END AND ALONG THE PARALLEL RAMP AREA AS SHOWN.

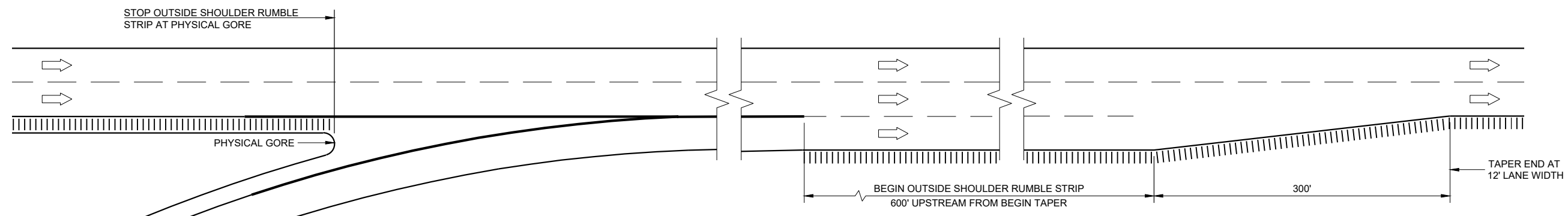
RUMBLE STRIPS ON EXPRESSWAYS:  
DO NOT INSTALL SHOULDER RUMBLE STRIPS ACROSS SIDE ROAD INTERSECTIONS, COMMERCIAL AND PRIVATE DRIVEWAYS, ADJACENT TO RIGHT TURN LANES, LEFT TURN LANES, TURN LANE TAPERS, 25' IN ADVANCE OF BRIDGE DECKS, 25' IN ADVANCE OF BRIDGE APPROACHES, OR 100 FEET IN ADVANCE OF RAILROAD CROSSINGS.

**LEGEND**

➡ DIRECTION OF TRAFFIC



**TYPICAL TAPERED ENTRANCE RAMP  
RAMP AND GORE SHOULDER RUMBLE STRIP LOCATIONS**



**TYPICAL PARALLEL ENTRANCE RAMP  
RAMP AND GORE SHOULDER RUMBLE STRIP LOCATIONS**

6

6

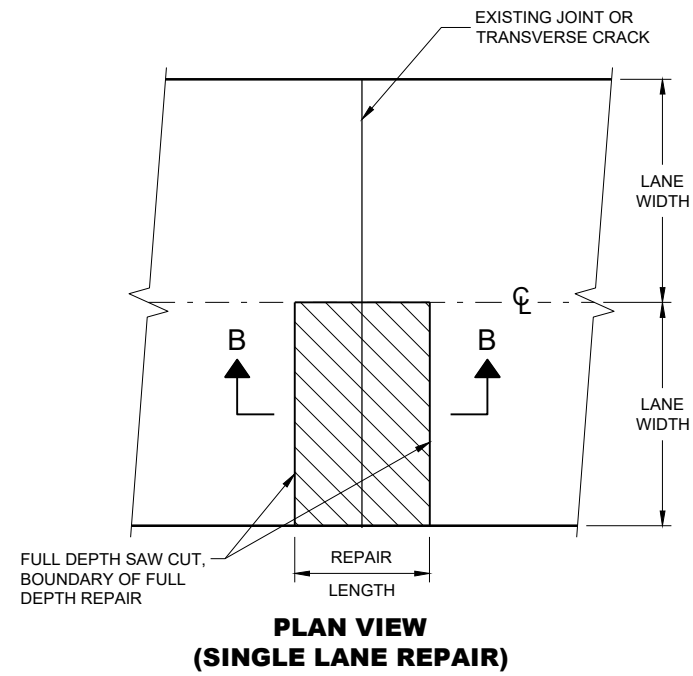
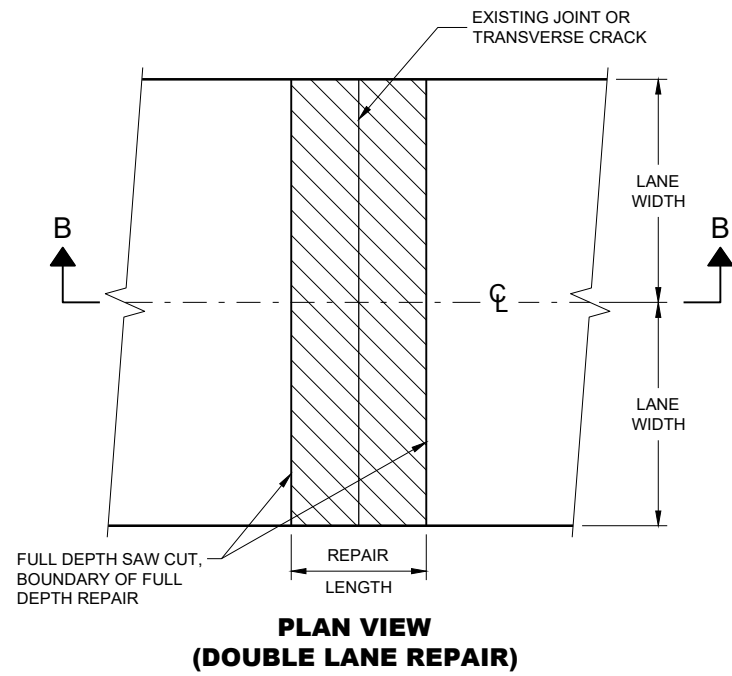
SDD 13A05-06b

SDD 13A05-06b

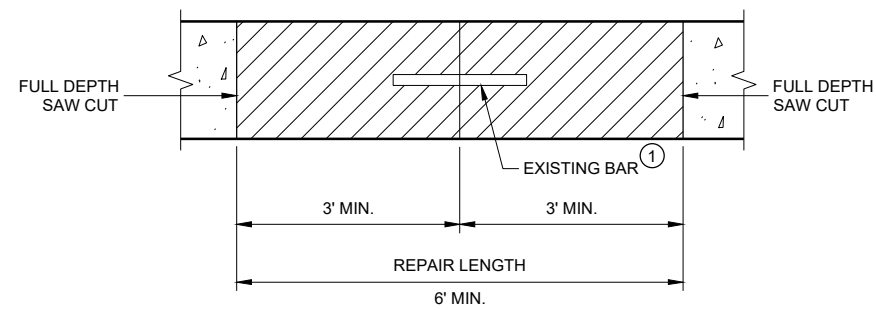
**SHOULDER RUMBLE STRIPS,  
DIVIDED ROADWAY**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2023 /S/ Rodney Taylor  
ROADWAY DESIGN STANDARDS  
UNIT SUPERVISOR  
FHWA



**FULL DEPTH CONCRETE PAVEMENT REMOVAL**



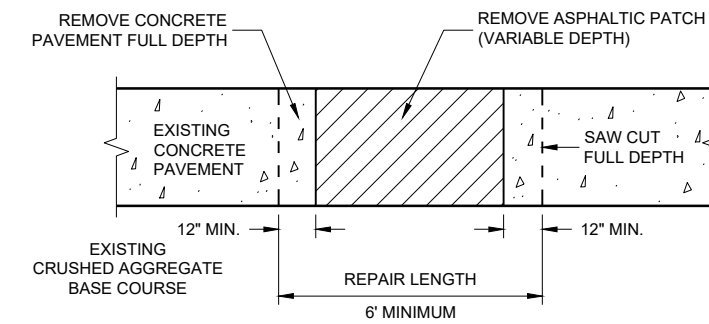
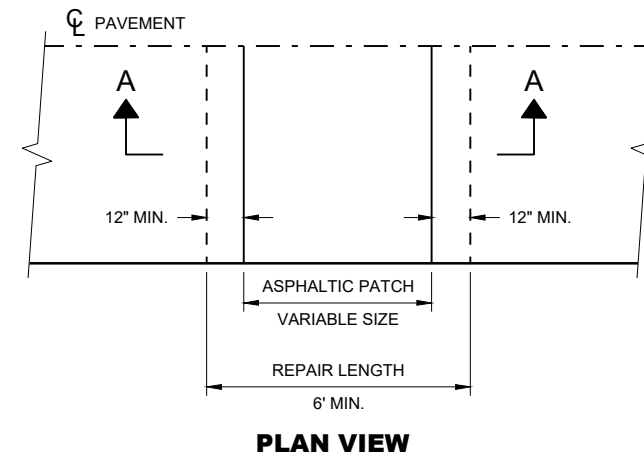
**GENERAL NOTES**

SAW CUT, DRILL, AND LIFT OUT EXISTING CONCRETE PAVEMENT WITHIN THE BOUNDARIES OF CONCRETE REPAIR AREAS. THE CONTRACTOR MAY MAKE ADDITIONAL SAW CUTS INSIDE THE REPAIR LIMITS TO REDUCE WEIGHT AND SIZE OF CONCRETE PIECES.

PROVIDE A 6 FOOT MINIMUM DISTANCE FROM BOUNDARIES OF CONCRETE REPAIR AREA TO ADJACENT TRANSVERSE JOINT OR CRACK.

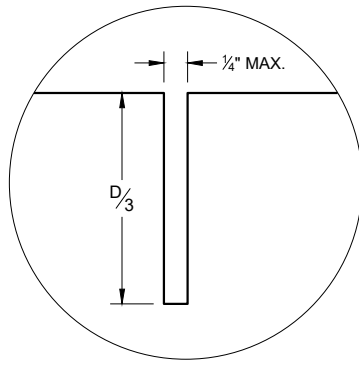
THE LENGTH OF THE REPAIRS MAY VARY FROM THE DIMENSIONS SHOWN IF THE EXISTING CONCRETE PAVEMENT IS NON-DOWELED AND THE PAVEMENT IS TO BE OVERLAID AFTER REPAIRING.

① DOWEL BARS MAY NOT BE PRESENT.

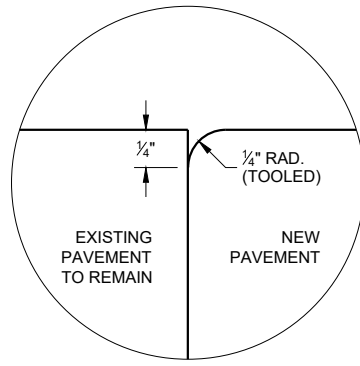


<b>CONCRETE PAVEMENT REPAIR AND REPLACEMENT</b>
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



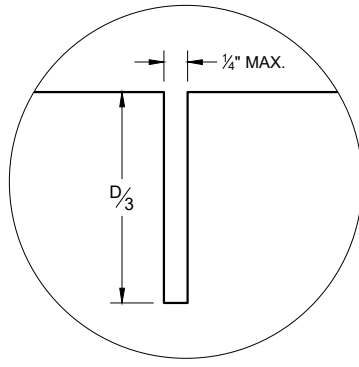


C1

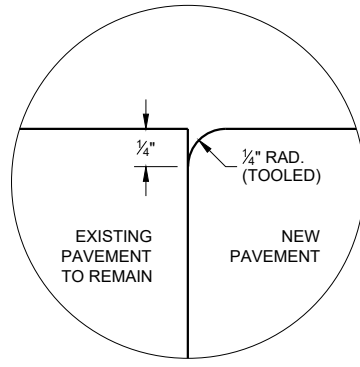


C2

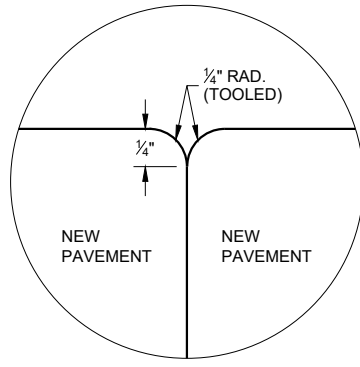
**TRANSVERSE JOINTS**



L1

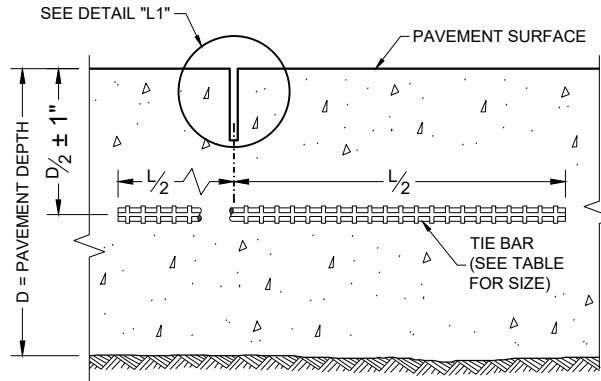


L2

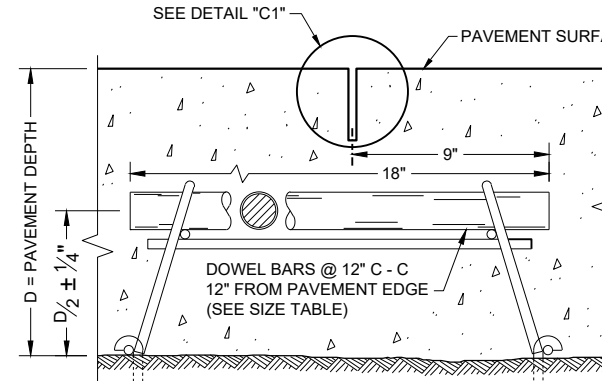


L3

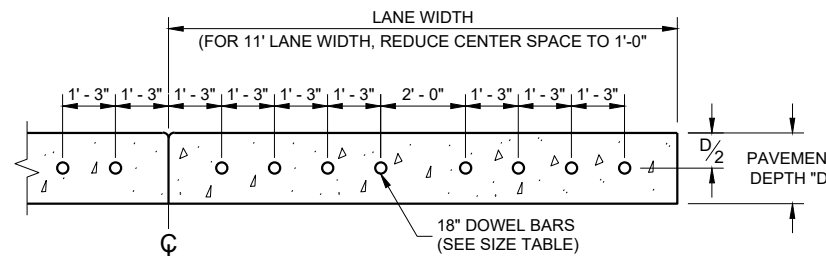
**LONGITUDINAL JOINTS**



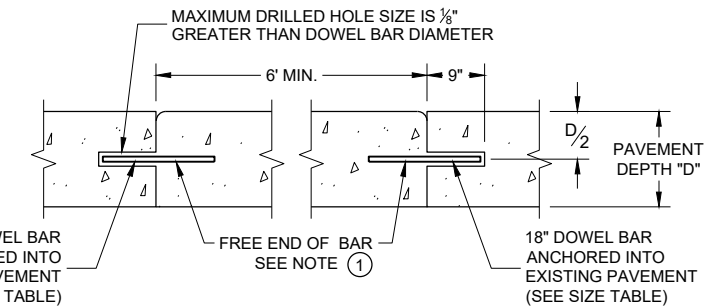
**SECTION C - C  
SAWED LONGITUDINAL JOINT**



**SECTION F - F  
DOWELED CONTRACTION JOINT**



**SECTION E - E  
DRILLED DOWEL BAR CONSTRUCTION JOINT**



**SECTION D - D**

**GENERAL NOTES**

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

CONCRETE PAVEMENT REPAIRS OF EXISTING NON-DOWELED CONCRETE PAVEMENTS DO NOT NEED TO BE DOWELED.

ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

FOR MULTI-LANE CONCRETE PAVEMENT REPLACEMENTS, PROVIDE A MINIMUM DISTANCE OF 15 INCHES FROM ALL TRANSVERSE JOINTS OR EDGES OF REPLACEMENT TO THE CENTER OF THE TIE BAR NEAREST THAT JOINT OR EDGE.

- ① APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.

**TIE BAR TABLE**

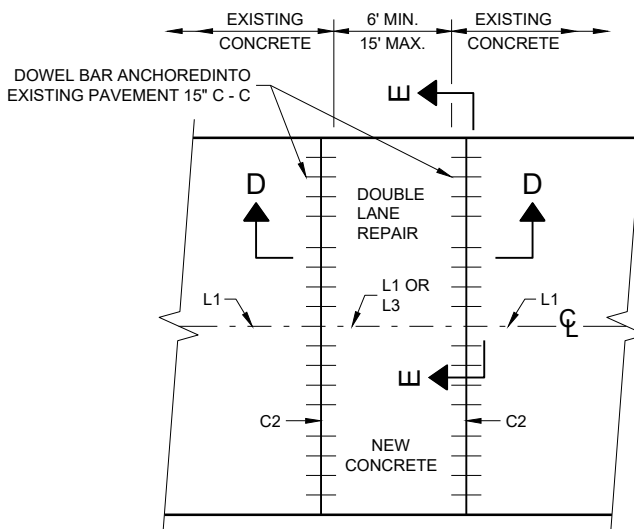
PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
	NO. 4*	30"	24" **

\* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

\*\* CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

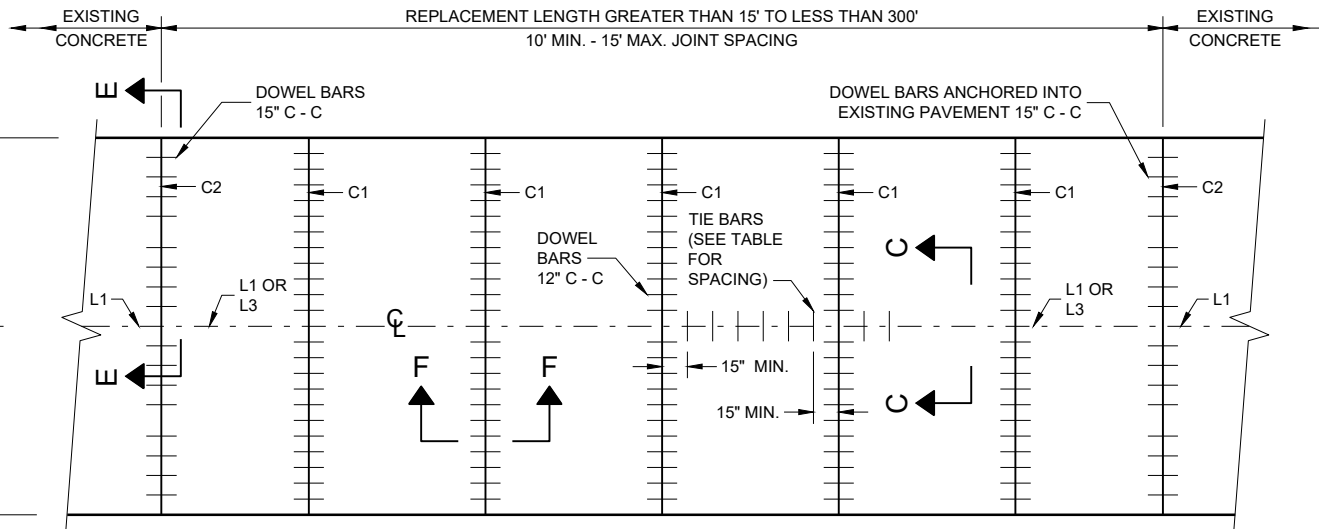
**PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE**

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	DRILLED DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
6", 6 1/2"	NONE	NONE	12'
7", 7 1/2"	1"	1"	14'
8" & ABOVE	1 1/4"	1 1/4"	15'



**PLAN VIEW**

**MULTILANE CONCRETE PAVEMENT REPAIR**

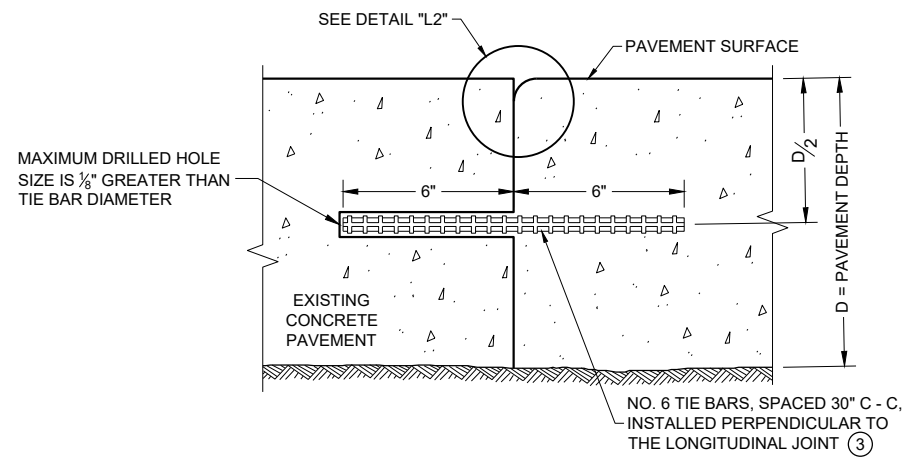


**PLAN VIEW**

**MULTILANE CONCRETE PAVEMENT REPLACEMENT**

**CONCRETE PAVEMENT REPAIR AND REPLACEMENT**

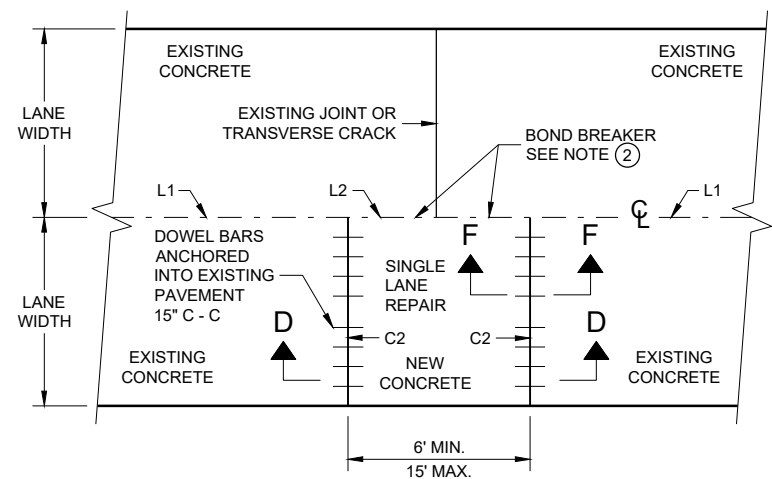
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



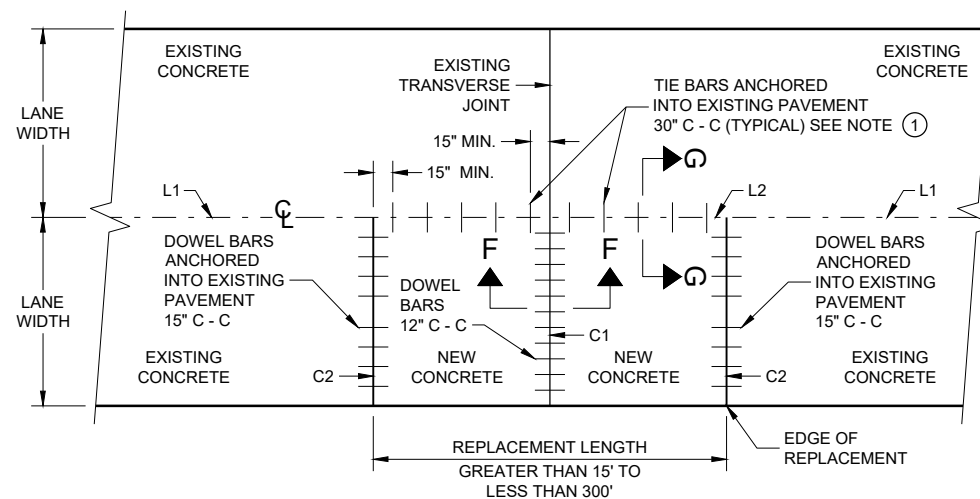
**SECTION G - G**  
**TIE BARS ANCHORED INTO EXISTING PAVEMENT**

**GENERAL NOTES**

- ① WITH THE APPROVAL OF THE ENGINEER, FOR SINGLE LANE PAVEMENT REPLACEMENTS LESS THAN 30 FEET IN LENGTH, THE CONTRACTOR MAY INSTALL DRILLED TIE BARS ON 6:1 SKEW HORIZONTALLY, DIRECTION OF SKEW ALTERNATING WITH EACH SUCCESSIVE BAR. DRIVE SKEWED TIE BARS TO A DEPTH OF 6 INCHES IN A HOLE OF SUCH A DIAMETER AS TO PROVIDE A TIGHT DRIVEN FIT.
- ② USE AN ENGINEER APPROVED BOND BREAKER (E.G. RELEASE AGENT, CURING COMPOUND) FOR SINGLE LANE REPAIRS UP TO 15 FEET IN LENGTH.
- ③ ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.



**PLAN VIEW**  
**SINGLE LANE CONCRETE PAVEMENT REPAIR**



**PLAN VIEW**  
**SINGLE LANE CONCRETE PAVEMENT REPLACEMENT**

**CONCRETE REPAIR AND REPLACEMENT**

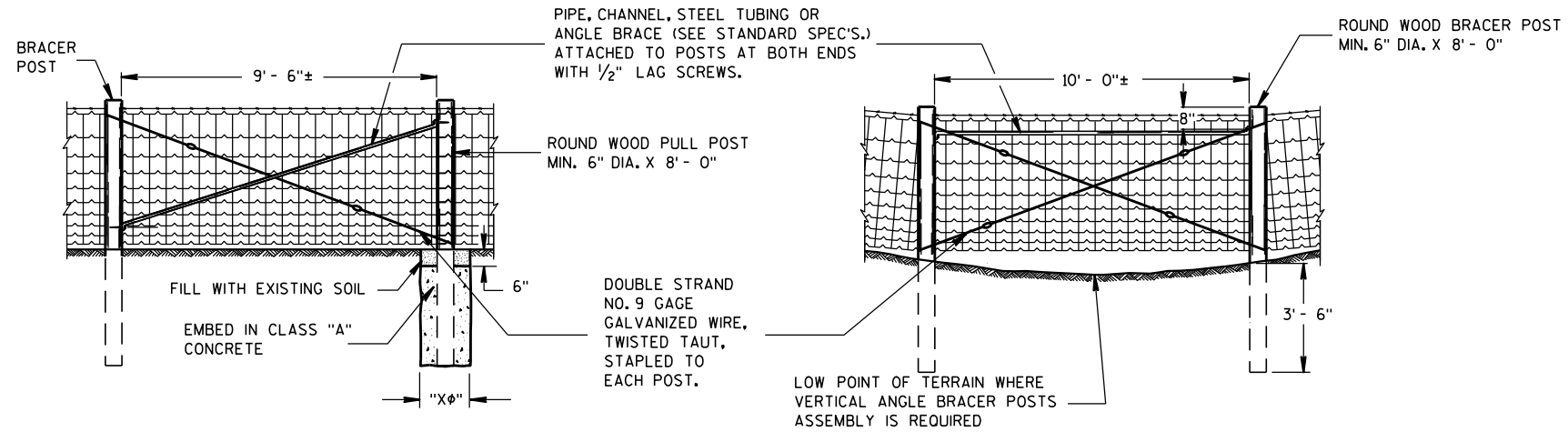
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2022 /S/ Peter Kemp P.E.  
DATE PAVEMENT SUPERVISOR

FHWA

NOTE: PULL OR STRETCHER POST ASSEMBLIES SHALL BE PLACED MIDWAY BETWEEN END POSTS AND CORNER POSTS WHERE A RUN OF FENCE EXCEEDS 660' BUT IS LESS THAN 1,320'. FOR RUNS OF FENCE IN EXCESS OF 1,320' MAXIMUM SPACING OF PULL OR STRETCHER POST ASSEMBLIES SHALL BE 660'± C-C.

ILLUSTRATION SHOWS POSITION OF STANDARD STEEL BRACE, DOUBLE STRAND GALVANIZED WIRE, AND THE POST TO BE EMBEDDED IN CONCRETE WHEN WIRE FENCE IS INSTALLED FROM LEFT TO RIGHT. THE BRACES SHALL BE POSITIONED ON THE OPPOSITE DIAGONALS AND THE OPPOSITE POST SHALL BE EMBEDDED IN CONCRETE WHEN WIRE FENCE IS INSTALLED FROM RIGHT TO LEFT.



**PULL OR STRETCHER POSTS ASSEMBLY**

**VERTICAL ANGLE BRACER POSTS ASSEMBLY**

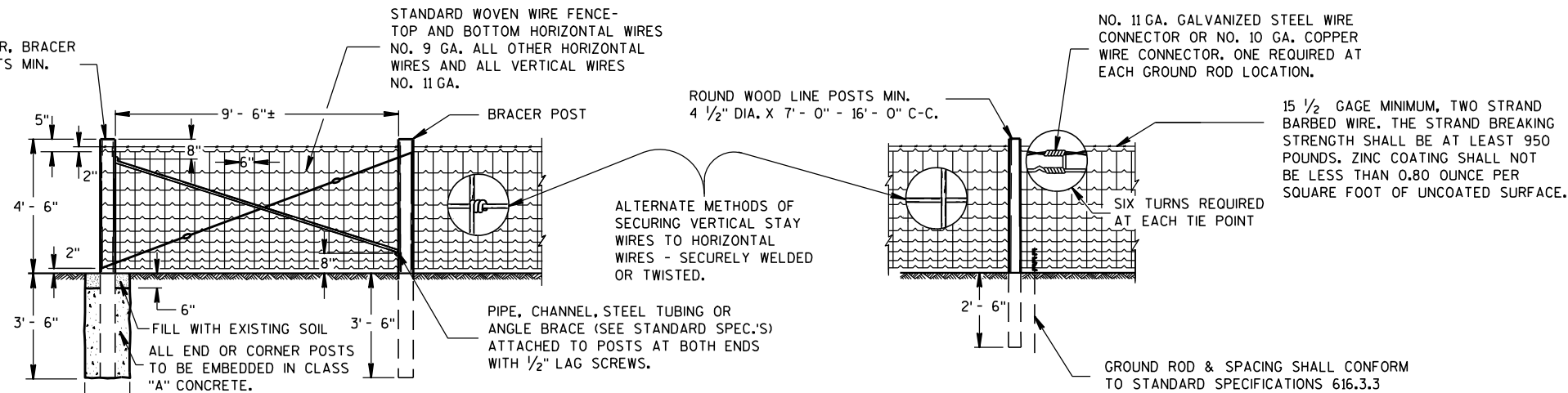
**GENERAL NOTES**

"Xφ" = DIAMETER OF THE POST PLUS 12".  
 FENCE STAPLES SHOULD NEVER BE DRIVEN VERTICALLY INTO WOOD POSTS (WITH BOTH LEGS PARALLEL WITH THE WOOD GRAIN). DOING SO CAN SEPARATE THE GRAIN AND SIGNIFICANTLY REDUCE THE HOLDING POWER. ROTATING THE STAPLES SLIGHTLY OFF VERTICAL STRADDLES THE GRAIN AND PROVIDES MORE RESISTANCE TO PULL-OUT.

DO NOT STAPLE WIRE TIGHT TO THE LINE POSTS. ALLOW MOVEMENT OF WIRE FOR EXPANSION AND CONTRACTION. STAPLE ARRANGEMENT SHALL BE THE SAME FOR ALL OTHER POSTS EXCEPT THAT THEY SHALL BE DRIVEN TIGHT TO POSTS. ALL STAPLES SHALL BE 2" X 9 GAGE AND SHALL BE MANUFACTURED FROM GALVANIZED WIRE OR HOT DIP GALVANIZED AFTER FORMING. STAPLES SHALL HAVE SLASH-CUT POINTS.

FENCE SHALL BE LOCATED 3'-0" INSIDE THE RIGHT OF WAY LINE UNLESS OTHERWISE INDICATED ON THE PLANS.

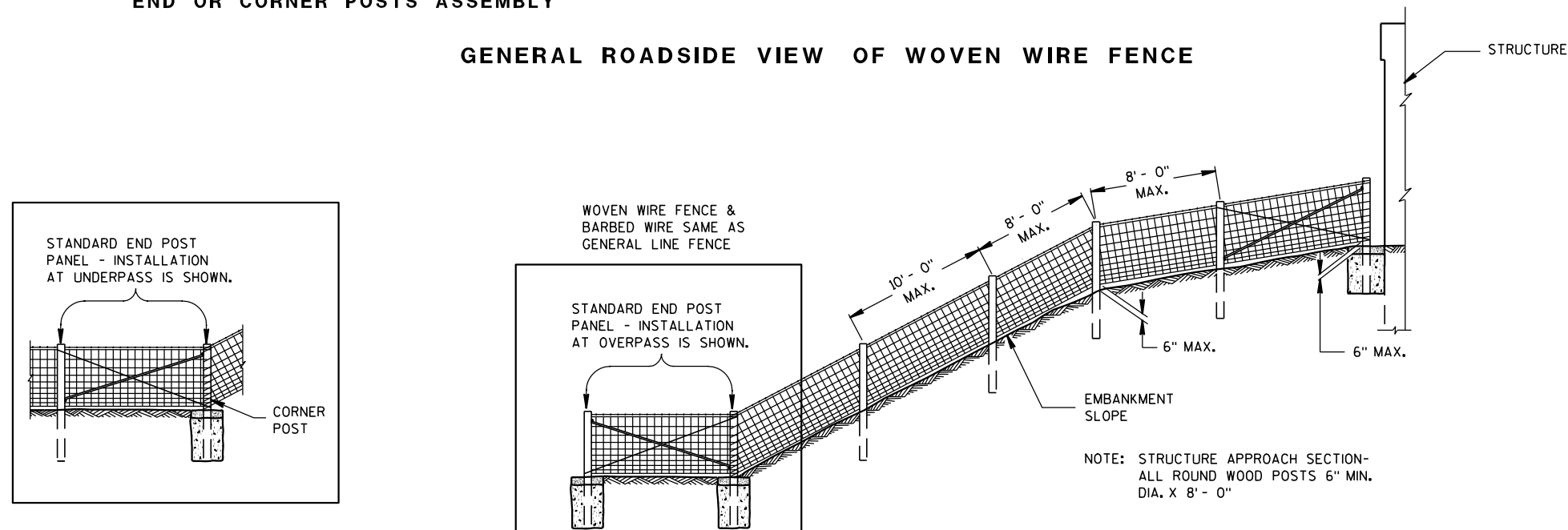
ROUND WOOD END, CORNER, BRACER OR VERTICAL ANGLE POSTS MIN. 6" DIA. X 8' - 0"



**END OR CORNER POSTS ASSEMBLY**

**LINE FENCE CONSTRUCTION**

**GENERAL ROADSIDE VIEW OF WOVEN WIRE FENCE**



**ALTERNATE FENCE DESIGN AT STRUCTURE**

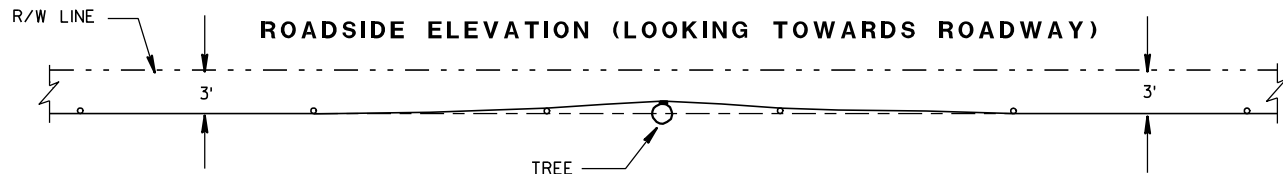
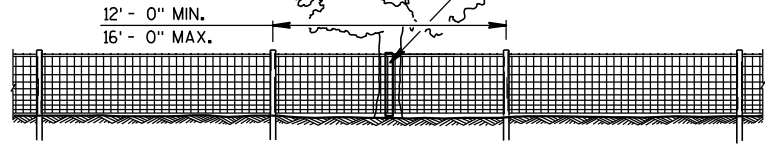
**FENCE DESIGN AT STRUCTURE APPROACH**

**FENCE WOVEN WIRE**

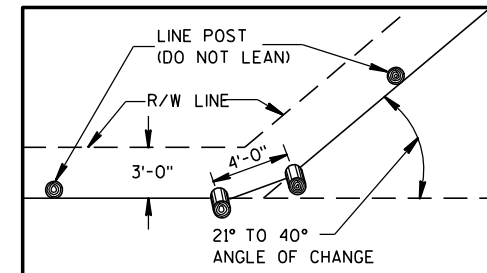
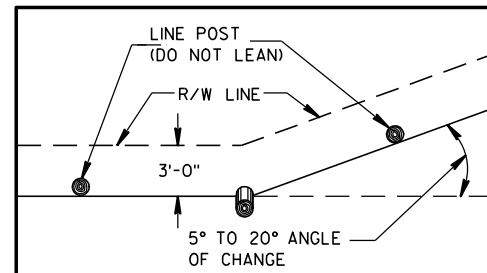
STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION

NOTE: TREE IN NORMAL FENCE LINE SPECIFICALLY ORDERED BY ENGINEER TO REMAIN IN PLACE.

2" X 6" DOUGLAS FIR OR SO. YELLOW PINE PLACED BETWEEN TREE AND WOVEN WIRE FENCE. WOVEN WIRE FENCE AND BARBED WIRE TO BE STAPLED TO 2" X 6" LIKE AS TO LINE POST. 2" X 6" NOT FASTENED TO TREE.



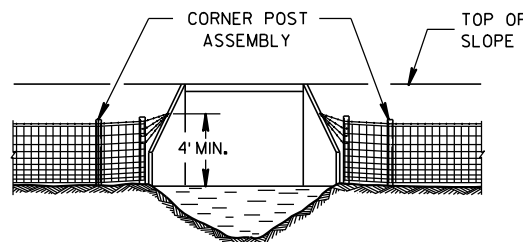
**PLAN VIEW  
FENCE DESIGN AT TREES REMAINING  
IN NORMAL FENCE LINE**



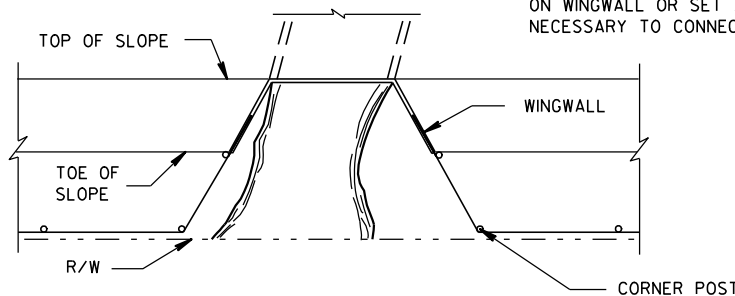
**PLAN VIEW  
SINGLE POST CORNER  
PLAN VIEW  
DOUBLE POST CORNER  
RIGHT OF WAY LINE CHANGE 40° AND LESS**

NOTE: SINGLE AND DOUBLE POSTS SHALL BE A MIN. 6" DIA. X 8'-0" WITH A LEAN OF 4" TOWARD THE OUTSIDE OF THE CURVE.

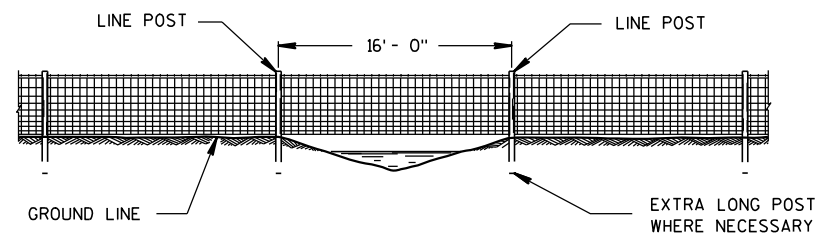
WHEN THE RIGHT OF WAY LINE CHANGE IS MORE THAN 40° USE THE CORNER OR STRETCHER POSTS ASSEMBLY.



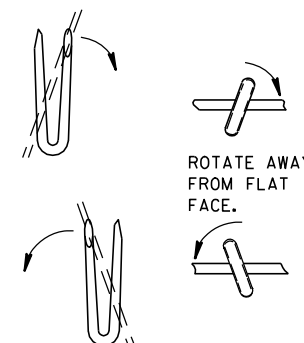
NOTE: PLACE A MINIMUM OF 4 STRANDS OF BARBED WIRE, 6" MAXIMUM CENTERS IN FAN SHAPE CONNECTED TO AN EYE BOLT ON WINGWALL OR SET A LONE POST WHEN NECESSARY TO CONNECT BARBED WIRE.



**FENCE INSTALLATION TO WINGWALLS**



**FENCE CONSTRUCTION OVER STREAM  
COURSES OF 15 FT. OR LESS IN WIDTH**



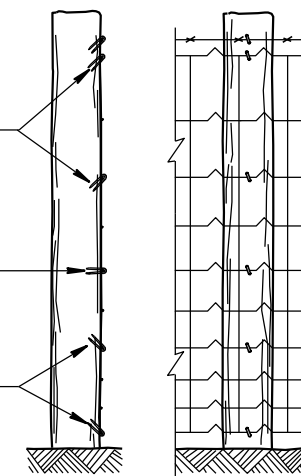
**LINE POST**

NOTE: WHEN POSTS ARE DRIVEN THE SMALL END SHALL BE DOWN.

STAPLES SLOPED DOWNWARD FOR SUSTAINED GRADES AND OVER KNOLLS.

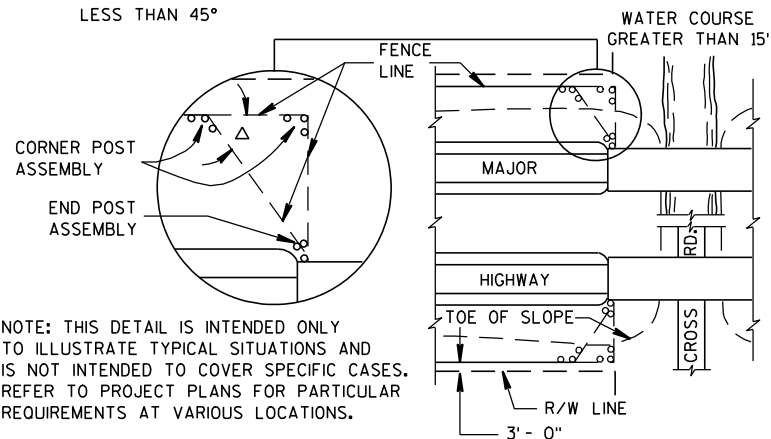
STAPLES LEVEL FOR LEVEL GROUND.

SLOPE UPWARDS WHEN FENCE TENDS TO LIFT.

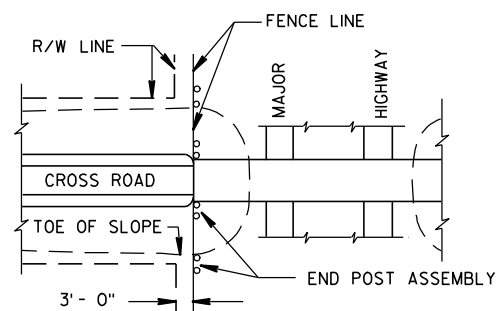


**END ELEVATION  
FARM SIDE ELEVATION  
FENCE MOUNTING DETAIL**

NOTE: Δ NOT LESS THAN 45°



**PLAN VIEW  
MAJOR HIGHWAY OVERPASS OR STREAM COURSE  
CROSSING OF GREATER THAN 15 FT. IN WIDTH**



**PLAN VIEW  
MAJOR HIGHWAY UNDERPASS**

**FENCE LOCATION AT STRUCTURES**

**FENCE WOVEN WIRE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
4/4/2008  
DATE

/s/ Jerry H. Zogg  
ROADWAY STANDARDS DEVELOPMENT  
ENGINEER




FHWA

**GENERAL NOTES**

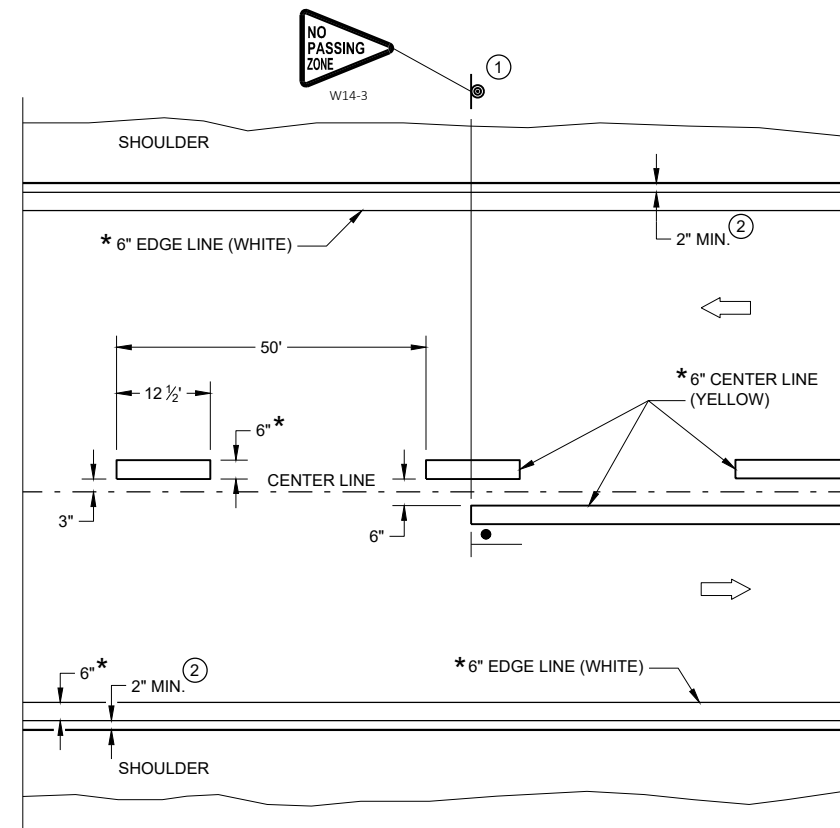
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

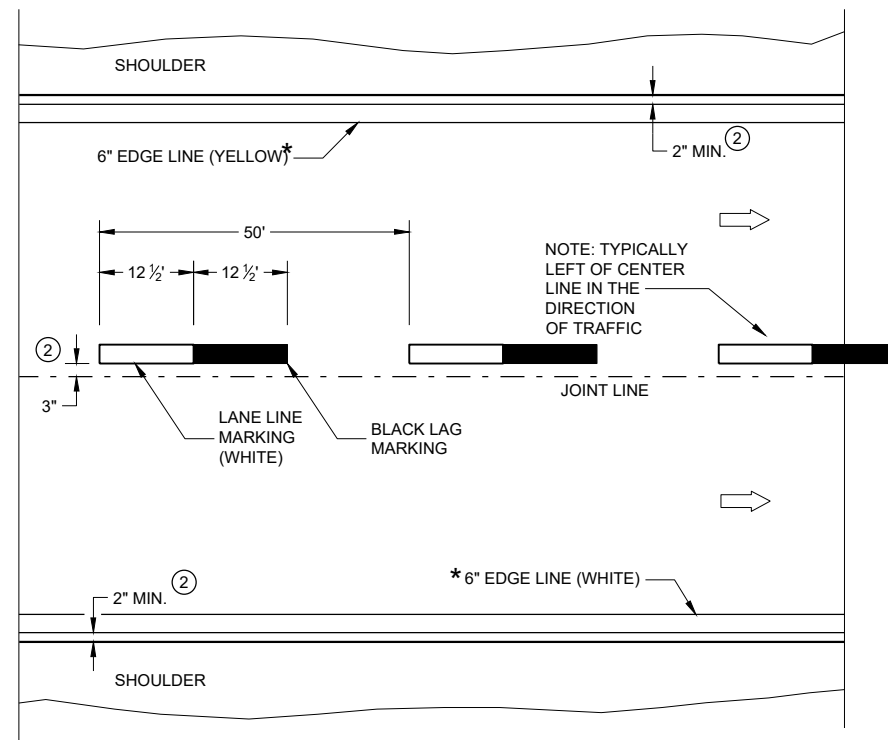
**LEGEND**

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

\* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



**TWO WAY TRAFFIC**



**ONE WAY TRAFFIC**






**PERMANENT PAVEMENT MARKING**

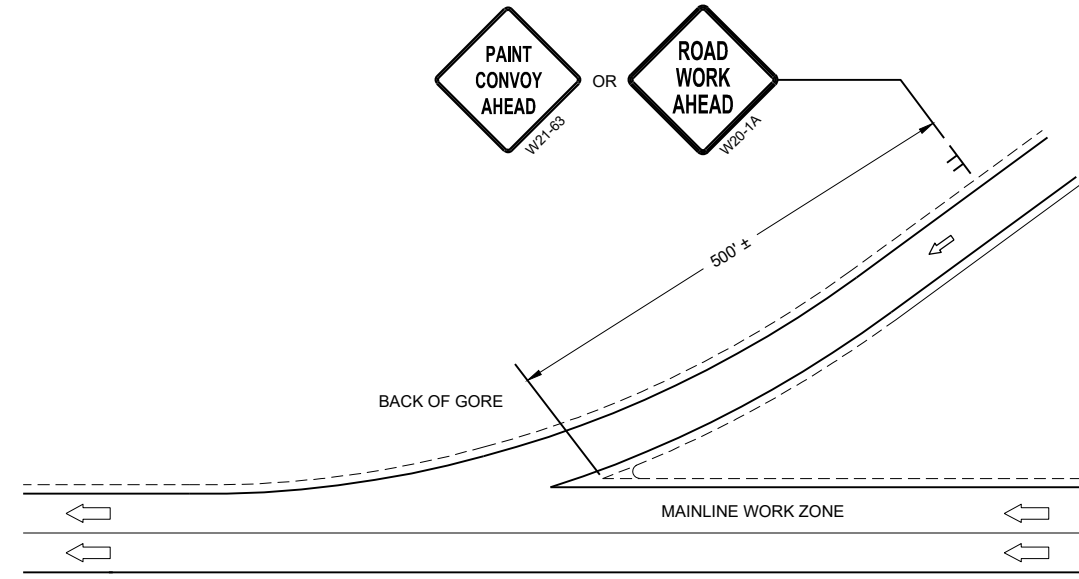
**PERMANENT LONGITUDINAL PAVEMENT MARKINGS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2023 /S/ Jeannie Silver  
DATE STATEWIDE SIGNING AND MARKING ENGINEER

**LEGEND**

- V1 MARKING VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (MERGE)
-  FLASHING ARROW PANEL (CAUTION)



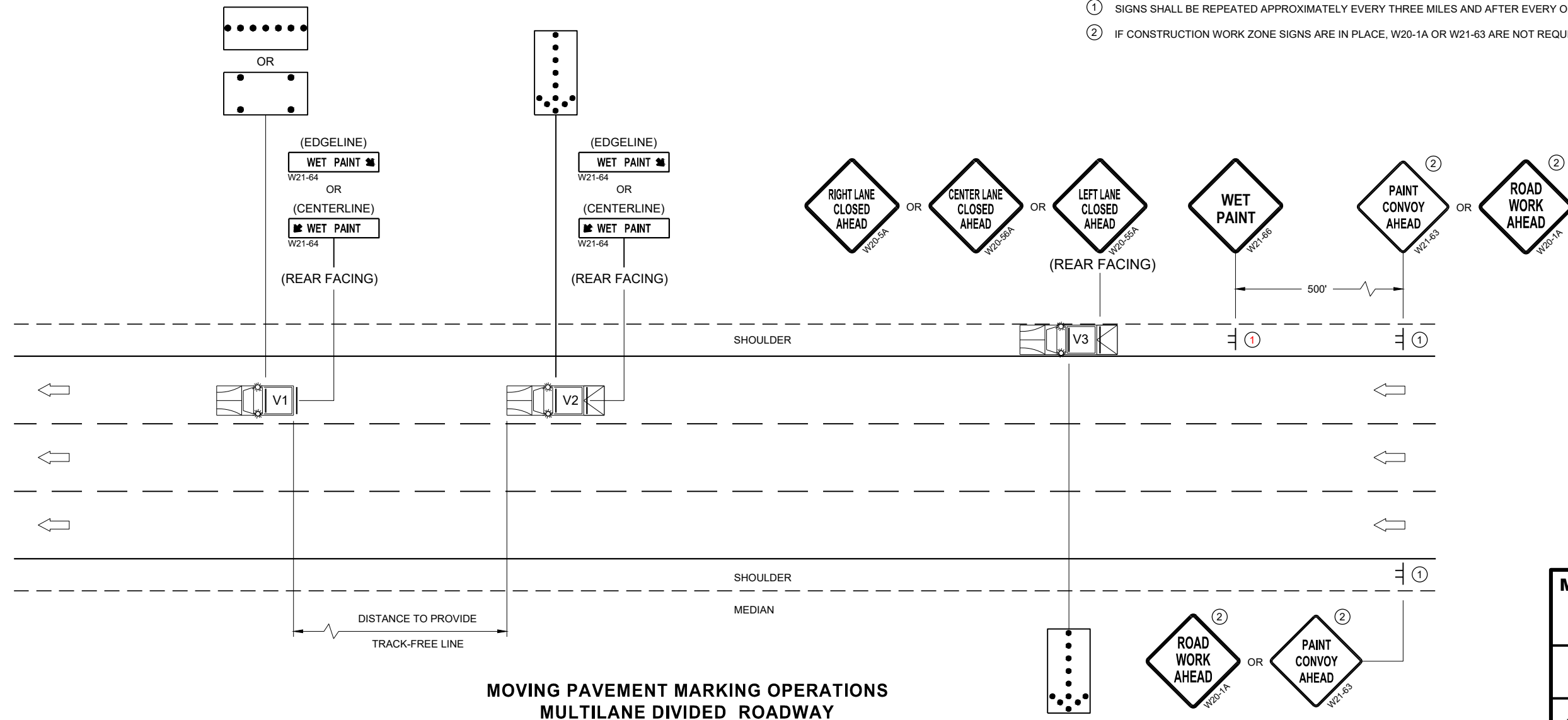
**GENERAL NOTES**

- ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.
- ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.
- DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.
- WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.
- USE AN ATTENUATOR ON THE REAR MOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.
- IF THE SHOULDER IS TOO NARROW TO ACCOMMODATE THE LAST TRAILING VEHICLE, THE VEHICLE SHOULD STRADDLE THE EDGE LINE.
- WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC
- CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- CONES SHALL BE A MINIMUM HEIGHT OF 28" FOR WET PAVEMENT MARKINGS

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES AND AFTER EVERY ON RAMP.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

6

6

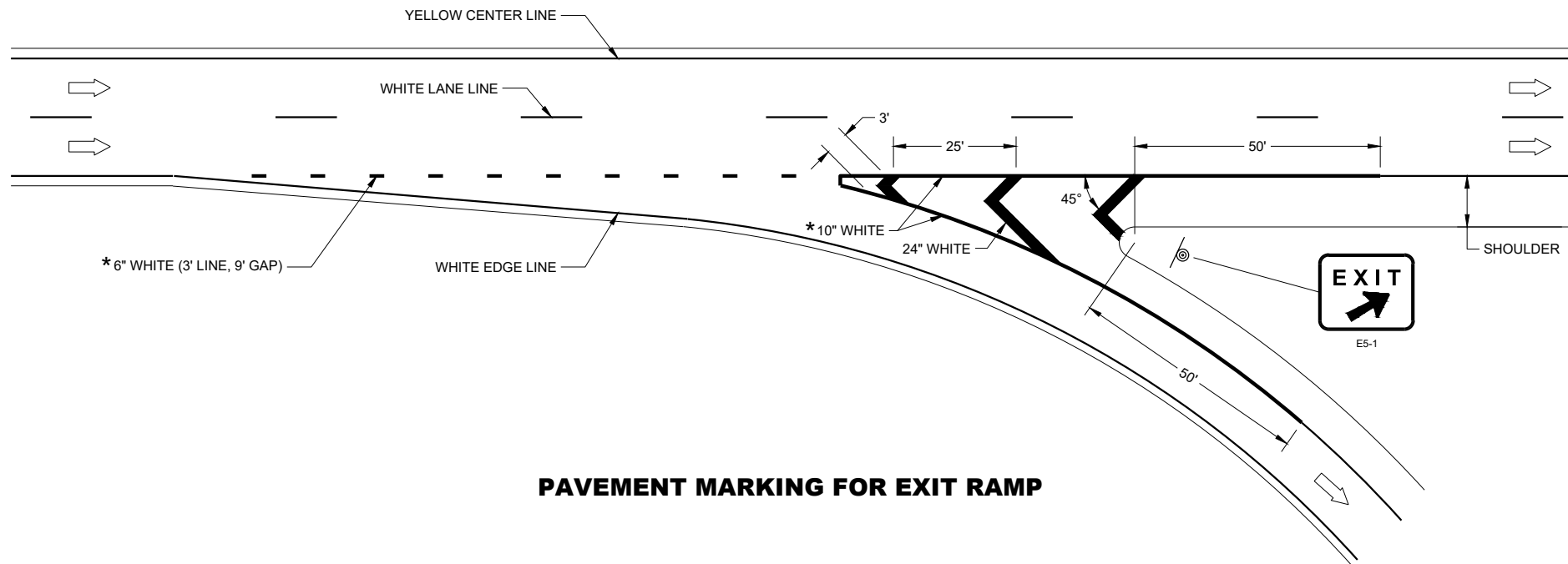


SDD 15C19-08c

SDD 15C19-08c

**MOVING PAVEMENT MARKING OPERATIONS  
MULTILANE DIVIDED ROADWAY**

<b>MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	





**PAVEMENT MARKING FOR EXIT RAMP**

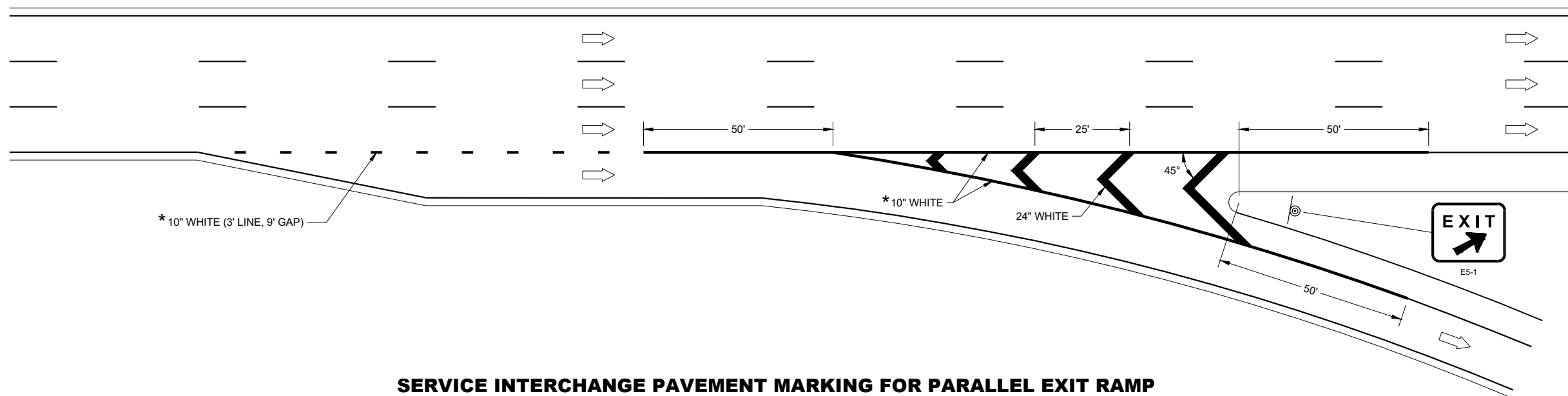
**GENERAL NOTES**

PLACE GROOVE 3 INCHES LEFT OF JOINT.

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAVEL

\* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



**SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL EXIT RAMP**

**PAVEMENT MARKING,  
EXIT RAMP AND  
PARALLEL EXIT RAMP**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

6

6

SDD 15C31-05a

SDD 15C31-05a

**GENERAL NOTES**

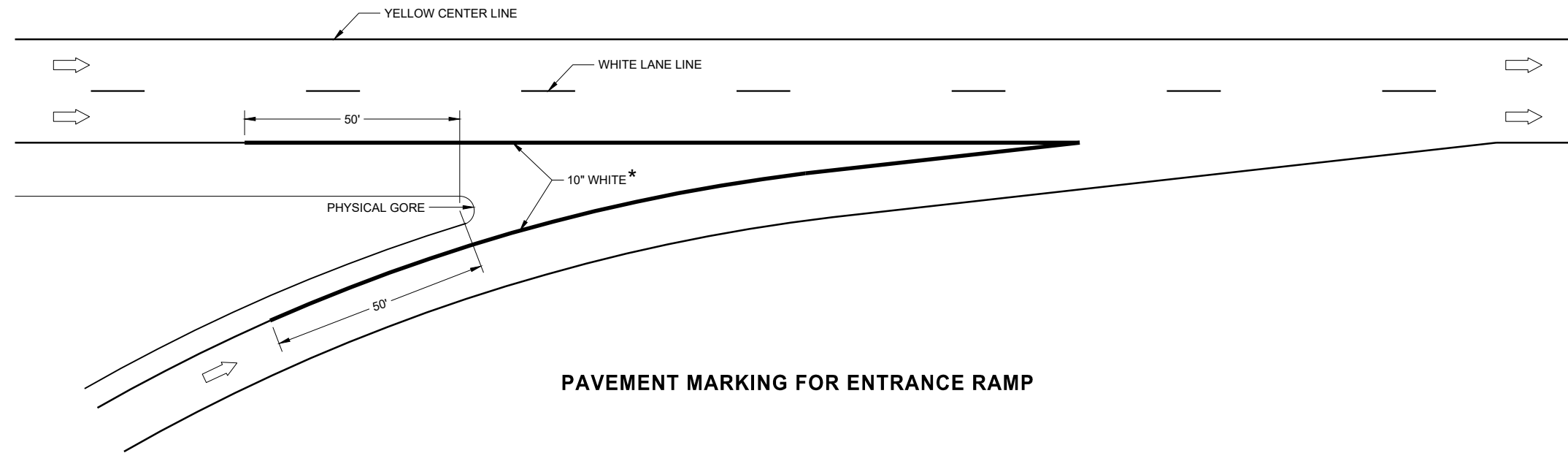
PLACE GROOVE 3 INCHES LEFT OF JOINT.

① ½ LENGTH OF FULL WIDTH ACCELERATION LANE.

**LEGEND**

➡ DIRECTION OF TRAVEL

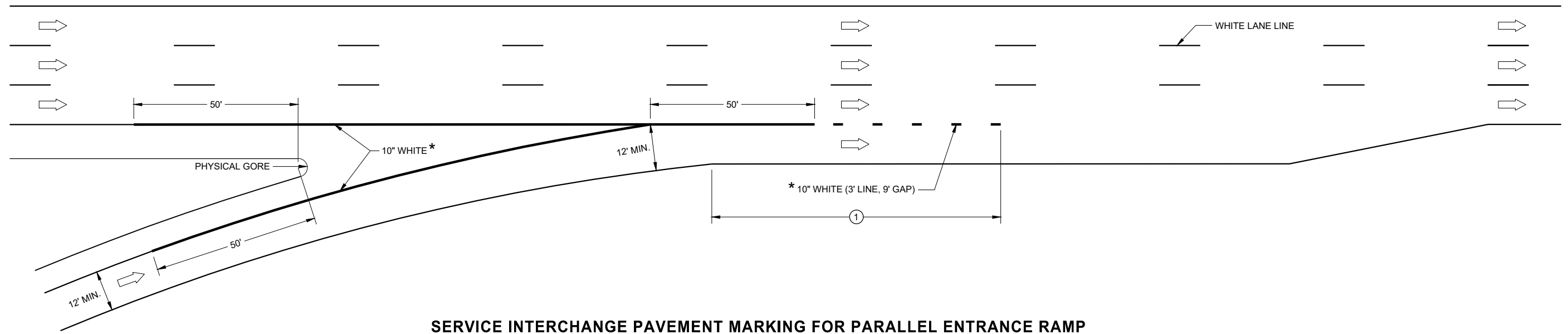
\* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



**PAVEMENT MARKING FOR ENTRANCE RAMP**

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**SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL ENTRANCE RAMP**

SDD 15C31-05c

SDD 15C31-05c

**PAVEMENT MARKING,  
ENTRANCE RAMP AND  
PARALLEL ENTRANCE RAMP**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



## GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.











WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

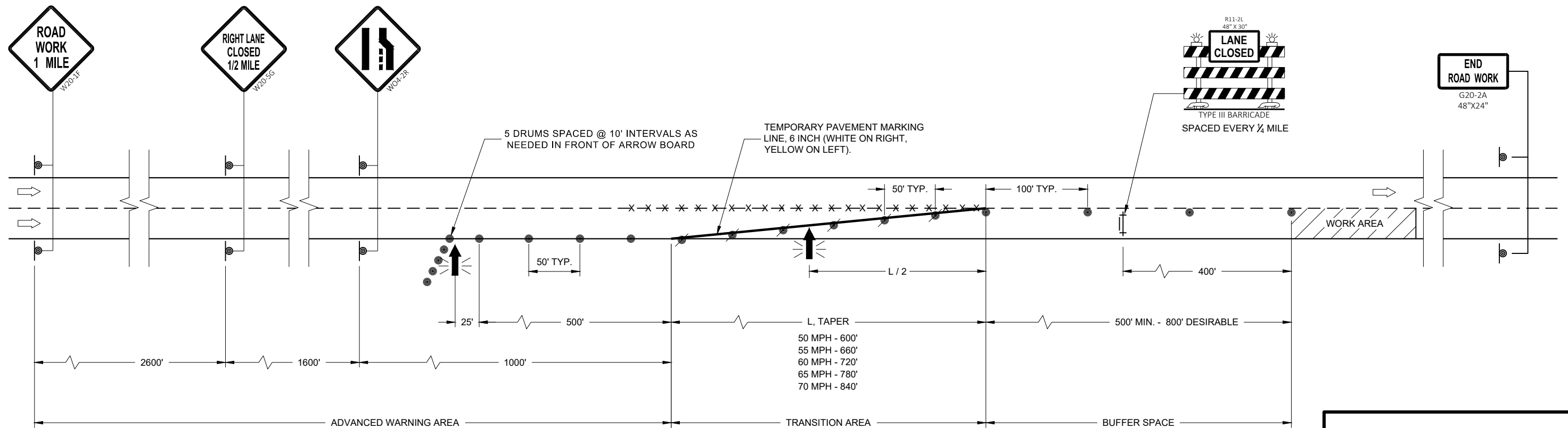
ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS

NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

## LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  -X-X-X- REMOVING PAVEMENT MARKINGS
-   DIRECTION OF TRAFFIC
-  WORK AREA
-  FLASHING ARROW BOARD



### TRAFFIC CONTROL LANE CLOSURE

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2023 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA

### GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.










WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

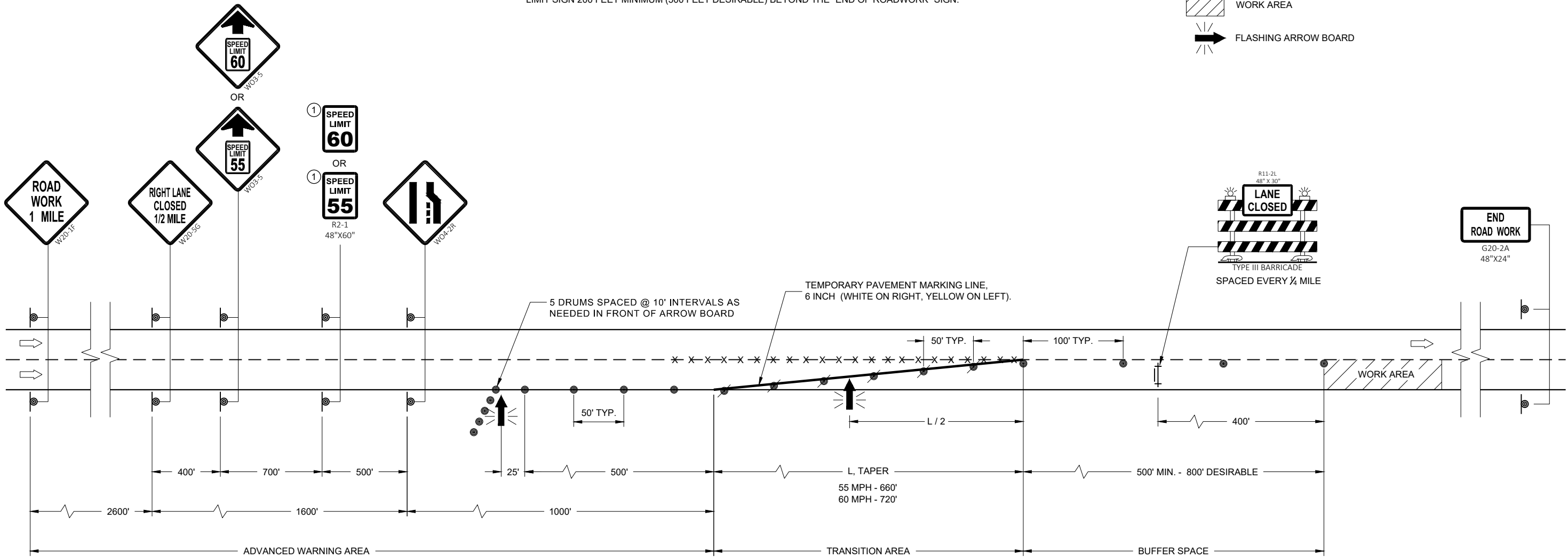
① A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. PLACE A SPEED LIMIT SIGN A MINIMUM OF EVERY 3 MILES. INCLUDE A RESUME SPEED LIMIT SIGN 200 FEET MINIMUM (500 FEET DESIRABLE) BEYOND THE "END OF ROADWORK" SIGN.

### LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  REMOVING PAVEMENT MARKINGS
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLASHING ARROW BOARD

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SDD 15D12 - 11b














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SDD 15D12 - 11b

<b>TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  REMOVING PAVEMENT MARKINGS
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLASHING ARROW BOARD
-  PORTABLE TRAFFIC SENSOR (PTS)
-  FLASHING BEACON SIGN

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS, INCLUDING FBS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS

AND NIGHTS.  
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

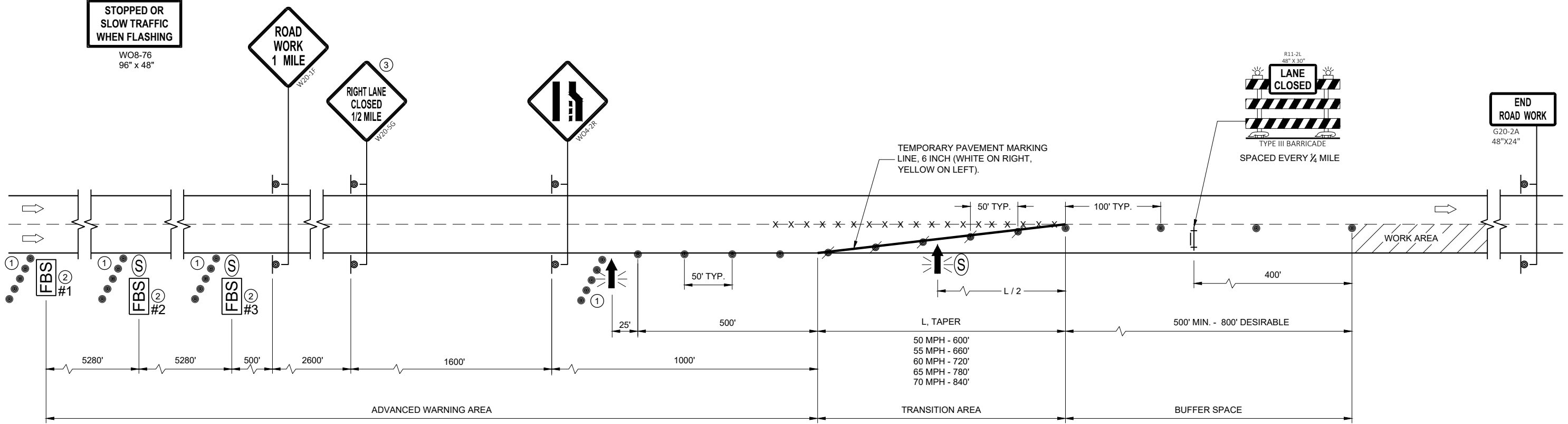
CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

PORTABLE TRAFFIC SENSOR (PTS) MAY BE MOUNTED ON THE FBS, ARROW BOARD OR OTHER TRAILER DEVICES.

- ① 5 DRUMS SPACED AT 10 FOOT INTERVALS AS NEEDED.
- ② IF THERE ARE MORE THAN TWO LANES OR IF SPECIFIED IN THE PLANS, PLACE FBS ON BOTH SIDES OF THE ROADWAY.
- ③ IF THERE IS AN APPROVED TEMPORARY SPEED DECLARATION, ADD WO-3-5 SIGNS 400 FEET AFTER THE W20-5G SIGNS AND ADD R2-1 SIGNS (48"x60") 700 FEET AFTER THE WO3-5 SIGNS. A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. PLACE A SPEED LIMIT SIGN A MINIMUM OF EVERY 3 MILES. INCLUDE A "RESUME SPEED LIMIT" SIGN 200 FEET MINIMUM (800 FEET DESIRABLE) BEYOND THE G30-3A "END ROAD WORK" SIGN.

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SDD 15D12 - 11d

SDD 15D12 - 11d

**TRAFFIC CONTROL, LANE CLOSURE, BASIC TRAFFIC QUEUE WARNING SYSTEM**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2023 /S/ Erin Schwark  
DATE WORK ZONE ENGINEER

FHWA

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  DIRECTION OF TRAFFIC

**GENERAL NOTES**

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONSECUTIVE DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS. USE SUPPORTS THAT PROVIDE A MINIMUM OF 5 FEET FROM THE BOTTOM OF THE SIGN TO THE PAVEMENT.

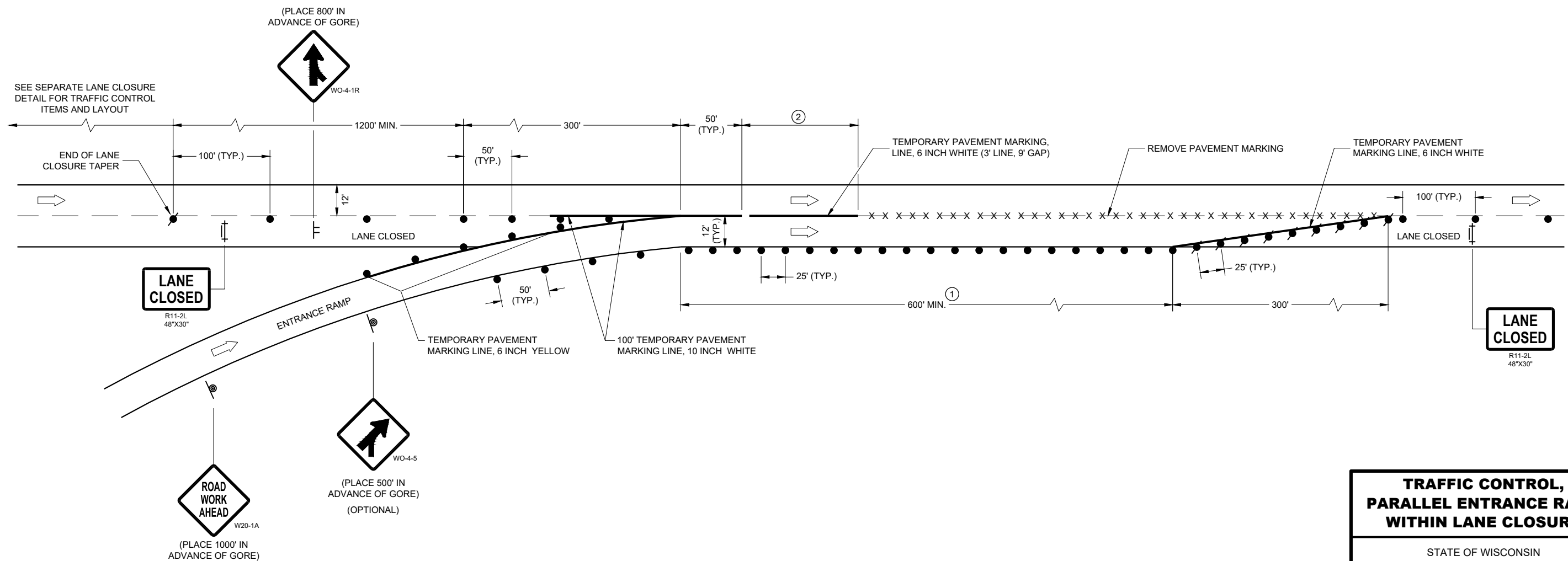
IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① EXTEND THE LENGTH OF THE MERGE ARE IF THE ENTERING (DESIGN) SPEED IS LESS THAN 50MPH OR IF THE MAINLINE GRADE EXCEEDS ±2.2%.
- ② END TEMPORARY PAVEMENT MARKING LINE AT ½ THE LENGTH OF FULL WIDTH OF THE ACCELERATION LANE.



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SDD 15D15-07a

SDD 15D15-07a

<b>TRAFFIC CONTROL, PARALLEL ENTRANCE RAMP WITHIN LANE CLOSURE</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

**LEGEND**

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊞ SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- ×-×-× REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- ⊞ TYPE III BARRICADE WITH ATTACHED SIGN
- ➡ DIRECTION OF TRAFFIC

**GENERAL NOTES**

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

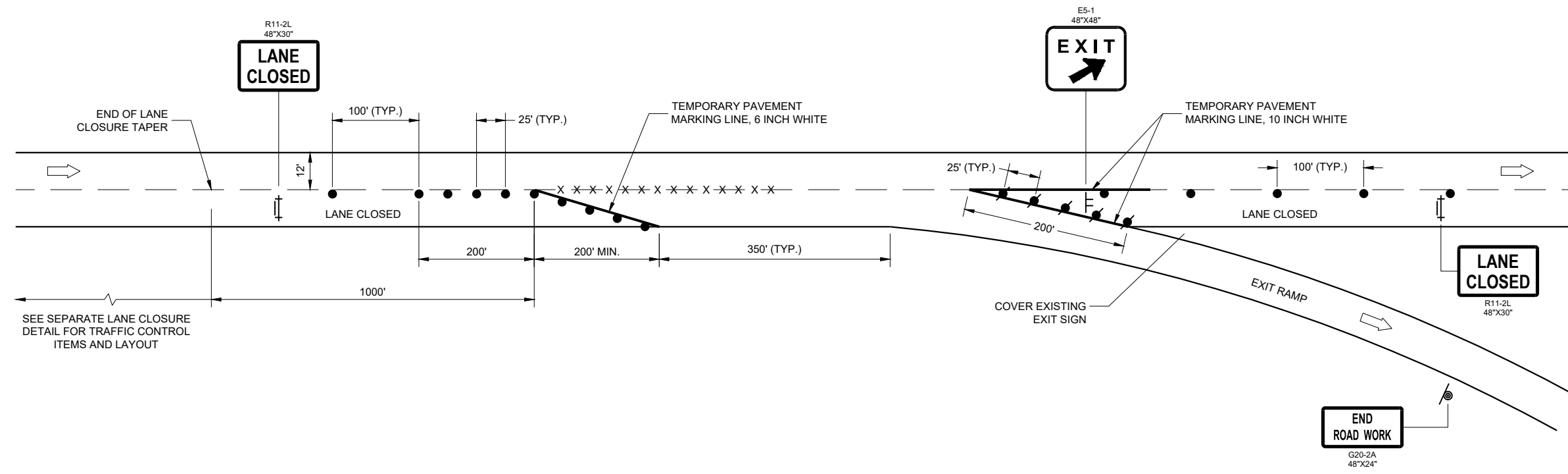
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONSECUTIVE DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE EXIT RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.



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SDD 15D15-07e

SDD 15D15-07e

<b>TRAFFIC CONTROL, PARALLEL EXIT RAMP WITHIN LANE CLOSURE</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

### GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT SHOULDER. FOR CLOSING THE LEFT SHOULDER, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR DIVIDED ROADWAYS WITH ANY NUMBER OF TRAVEL LANES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

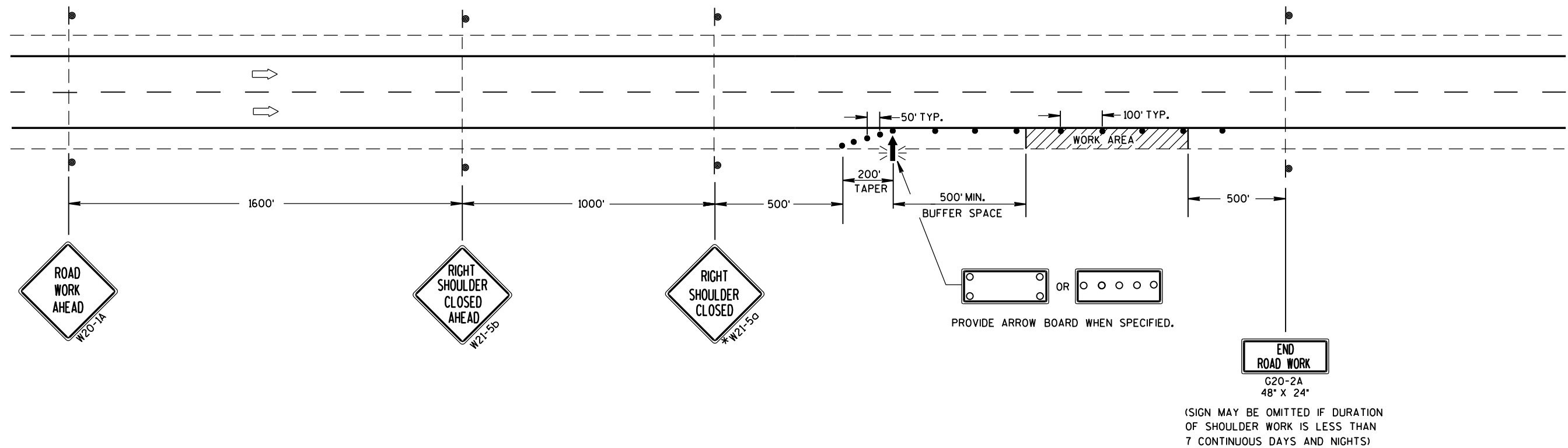
CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

\*FOR SHORT DURATION SHOULDER WORK OF LESS THAN ONE HOUR, THE W21-50 SIGN MAY BE OMITTED.

### LEGEND

- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ⚡➡ FLASHING ARROW BOARD
- ▨ WORK AREA



<b>TRAFFIC CONTROL SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2016 DATE	/s/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

Notes



## *Wisconsin Department of Transportation*

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>