

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 90



44

DESIGN DESIGNATION 8829-00-01

A.A.D.T.	2021	=	15,550
A.A.D.T.	2044	=	19,900
D.H.V.		=	1,805
D.D.		=	50/50
T.		=	9%
DESIGN SPEED		=	50 MPH
ESALS		=	30,477,500

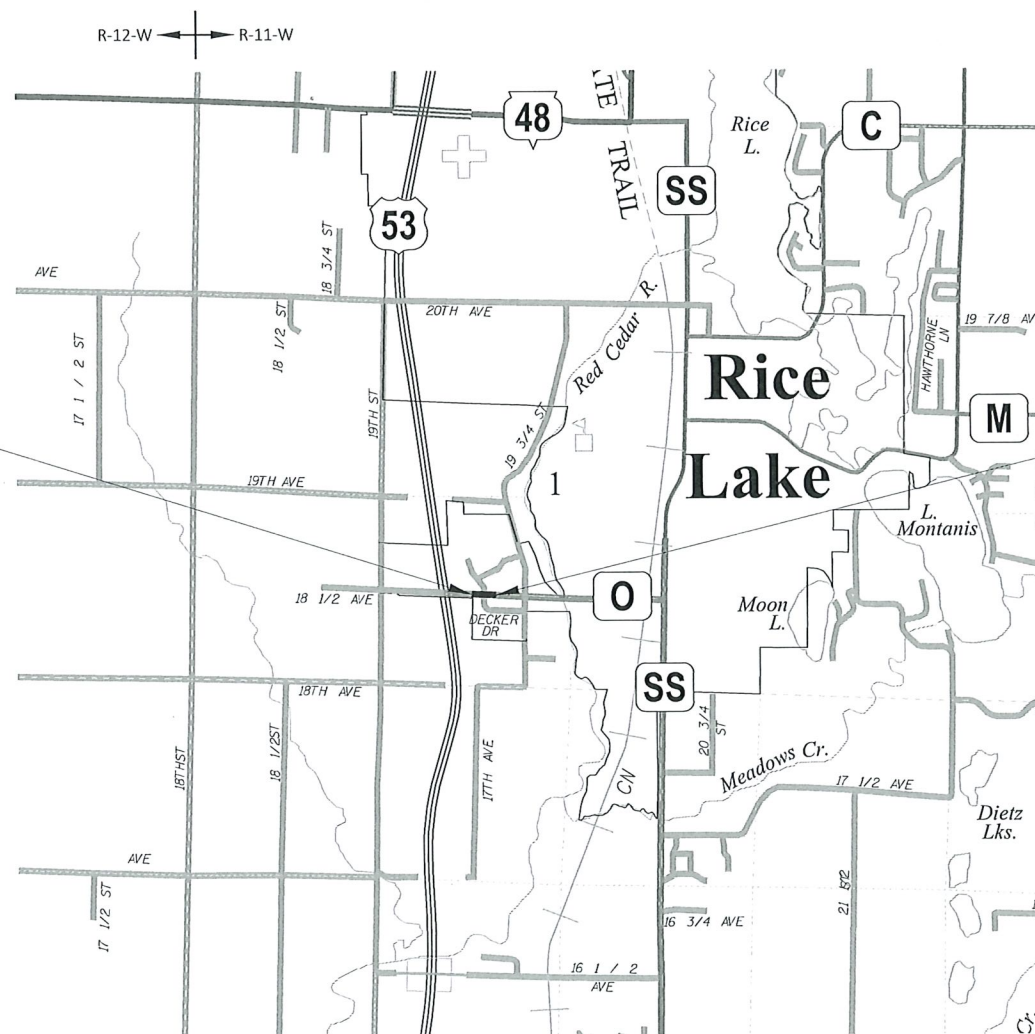
CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

BEGIN PROJECT
 STA 102+42.67
 X = 329,814.893
 Y = 123,919.329

END PROJECT
 STA 109+43.72



LAYOUT
 SCALE 0 1 MI
 TOTAL NET LENGTH OF CENTERLINE = 0.133 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), BARRON COUNTY NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88.

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION
 PLAN OF PROPOSED IMPROVEMENT
 DALLAS - RICE LAKE
 DECKER DRIVE INTERSECTION
 CTH O
 BARRON COUNTY

STATE PROJECT NUMBER
 8829-00-71

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8829-00-71	WISC 2024032	1

ACCEPTED FOR
 BARRON COUNTY
 Date: 7/25/23
 ORIGINAL PLANS PREPARED BY

 RACHEL A. BURNHAM
 E-43588
 MADISON WIS.
 PROFESSIONAL ENGINEER
 DATE: 7/25/23
 STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION
 PREPARED BY
 Surveyor: SRF CONSULTING GROUP, INC.
 Designer: SRF CONSULTING GROUP, INC.
 Project Manager: PAULA GROOM
 Regional Examiner: NORTHWEST REGION
 Regional Supervisor: TYLER RONGSTAD
 APPROVED FOR THE DEPARTMENT
 DATE: 07/27/2023
 E

GENERAL NOTES

THE CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND RESTORATION (INCLUDING, BUT NOT LIMITED TO, SEED, FERTILIZER, MULCH, AND EROSION MAT) OF ANY DISTURBED AREAS OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS AS DETERMINED BY THE ENGINEER.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY SHALL BE FINISHED WITH 6" TOPSOIL/SALVAGED TOPSOIL, SEED, FERTILIZED MULCH, AND RESTORED AS DIRECTED BY THE ENGINEER.

PURSUANT TO CHAPTER 59 OF THE WISCONSIN STATUES, THE CONTRACTOR SHALL CAREFULLY MAKE A SEARCH FOR EVIDENCE OF ALL LANDMARKS, BENCHMARKS, AND OTHER CONTROL POINTS IN ALL AREAS WHERE SUCH LANDMARKS, BENCHMARKS, OR OTHER CONTROL POINTS MAY EXIST.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT AREA NOT SHOWN. THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

EXISTING STORM WERE SIZES SHOWN ARE APPROXIMATE AND THE CONTRACTOR WILL BASE ITS BID ON THE ACTUAL FIELD SIZES. STORM SEWER PIPE ELEVATIONS, LENGTH, AND LOCATIONS AS SHOWN IN THE PLANS MAY BE ADJUSTED TO FIT FIELD CONDITIONS WITH THE APPROVAL OF THE ENGINEER.

RADIUS DIMENSIONS FOR THE CURB AND GUTTER ARE TO THE FLANGE LINE UNLESS OTHERWISE NOTED.

EROSION CONTROL FEATURES SHOWN IN THE PLANS ARE AT SUGGESTED LOCATIONS. THE ENGINEER MAY MODIFY LOCATIONS AS NEEDED. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH A TIME AS THE ENGINEER DETERMINES THE MEASURES NO LONGER NECESSARY.

APPLY TACK COAT AT A RATE OF 0.05 GAL/SY BETWEEN LAYERS OF ASPHALTIC PAVEMENT.

ASPHALTIC WEIGHT CALCULATIONS BASED ON 112 LBS/SY/IN.

6-INCH ASPHALTIC PAVEMENT SHALL BE CONSTRUCTED WITH A 2.5-INCH UPPER LAYER OF 4 MT MIX AND 3.5-INCH LOWER LAYER OF 3 MT MIX.

UTILITY CONTACTS

LUMEN TECHNOLOGIES
COMMUNICATIONS
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20 S WILSON AVE
RICE LAKE, WI 54868
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SPECTRUM
COMMUNICATIONS
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MOSAIC TECHNOLOGIES
COMMUNICATIONS
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401 S 1ST STREET
CAMERON, WI 54822
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WE ENERGIES
GAS & ELECTRIC
STEVEN CHAVERS
104 W SOUTH STREET
RICE LAKE, WI 54868
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CITY OF RICE LAKE
WATER
CHAD PAULSON
320 W COLEMAN STREET
RICE LAKE, WI 54868
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SEWER
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ELECTRIC
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WISCONSIN DNR LIASON

AMY CRONK
NW REGION
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COUNTY HIGHWAY COMMISSIONER

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BARRON, WI 54812
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DESIGN PROJECT LEADER

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SRF CONSULTING GROUP
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rburnham@srfconsulting.com

CITY OF RICE LAKE CONTACT

JIM ANDERSON
DIRECTOR OF COMMUNITY SERVICES
910 SOUTH WISCONSIN AVENUE
RICE LAKE, WI 54868
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EMAIL: janderson@ricelakegov.org

ORDER OF SECTION 2 SHEETS

GENERAL NOTES

PROJECT OVERVIEW

TYPICAL SECTIONS

MEDIAN ISLAND DETAILS

INTERSECTION DETAIL

CURB RAMP DETAIL

EROSION CONTROL

STORM SEWER

PERMANENT SIGNING

PAVEMENT MARKING

TRAFFIC CONTROL

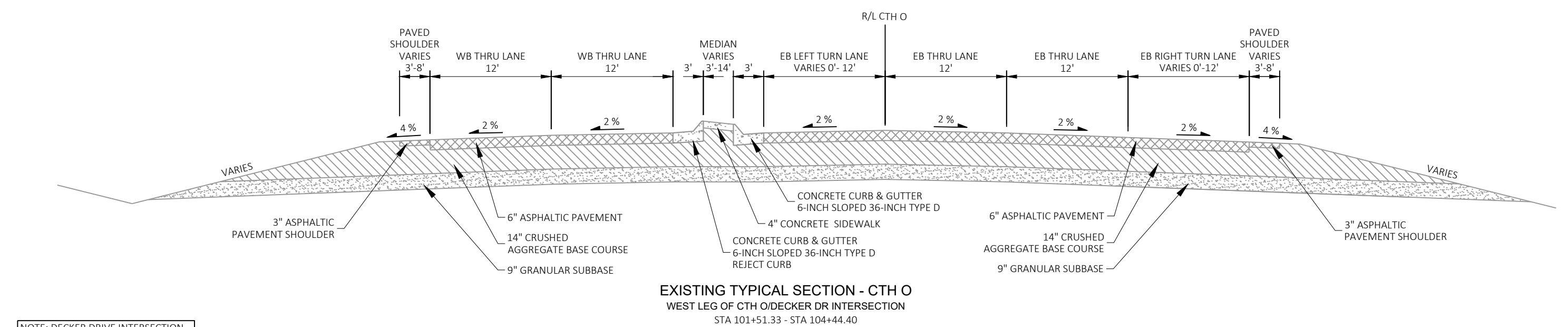
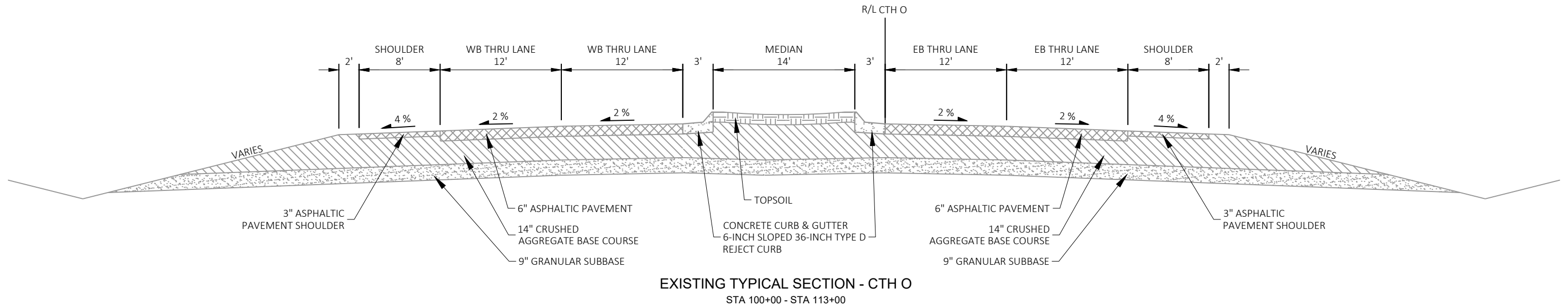
ALIGNMENT DETAIL

STANDARD ABBREVIATIONS

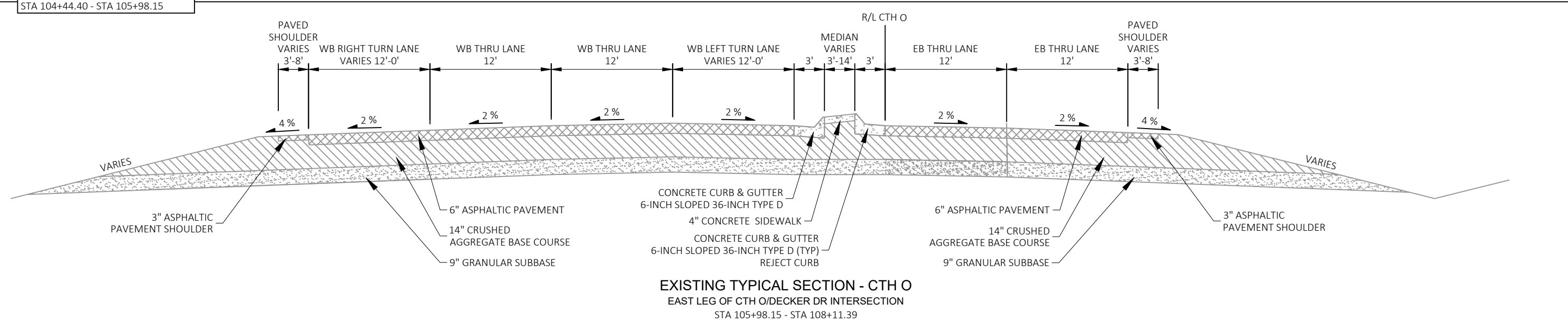
AP	Access Point	INV	Invert
AC	Acre	IP	Iron Pipe or Pin
AGG	Aggregate	LT	Left
AADT	Annual Average Daily Traffic	LF	Linear Foot
ASPH	Asphaltic	ML or M/L	Match Line
BL	Base Line	MH	Manhole
BM	Bench Mark	MP	Marker Post
CE	Commercial Entrance	MB	Message Board
CL	Center Line	NOM	Nominal
CONC	Concrete	NC	Normal Crown
CO	County	NB	Northbound
CABC	Crushed Aggregate Base Course	PAVT	Pavement
CTH	County Trunk Highway	PERM	Permanent
CY	Cubic Yard	PE	Private Entrance
CULV	Culvert	PCC	Portland Cement Concrete
CP	Culvert Pipe	PLE	Permanent Limited Easement
C&G	Curb & Gutter	PROJ	Project
DIA	Diameter	PL	Property Line
DHV	Design Hour Volume	RL	Reference Line
DD	Directional	RT	Right
DWY	Driveway	R/W	Right-of-Way
EB	Eastbound	RD	Road
ELEC	Electric (al)	RDWY	Roadway
EL or ELEV	Elevation	SHLDR	Shoulder
EW	Endwall	SB	Southbound
ESALS	Equivalent Single Axle Loads	STH	State Trunk Highways
EXC	Excavation	STA	Station
EBS	Excavation Below Subgrade	SE	Superelevation
FP	Fence Post	SI	Slope Intercept
FERT	Fertilizer	SS	Storm Sewer
F	Fill	TEL	Telephone
FG	Finished Grade	TEMP	Temporary
FL or F/L	Flow Line	TLE	Temporary Limited Easement
FO	Fiber Optic	TV	Television
FT	Foot	UG	Underground
CWT	Hundredweight	USH	United States Highway
HYD	Hydrant	VOL	Volume
IN DIA	Inch Diameter	W	Water
INL	Inlet	WB	Westbound

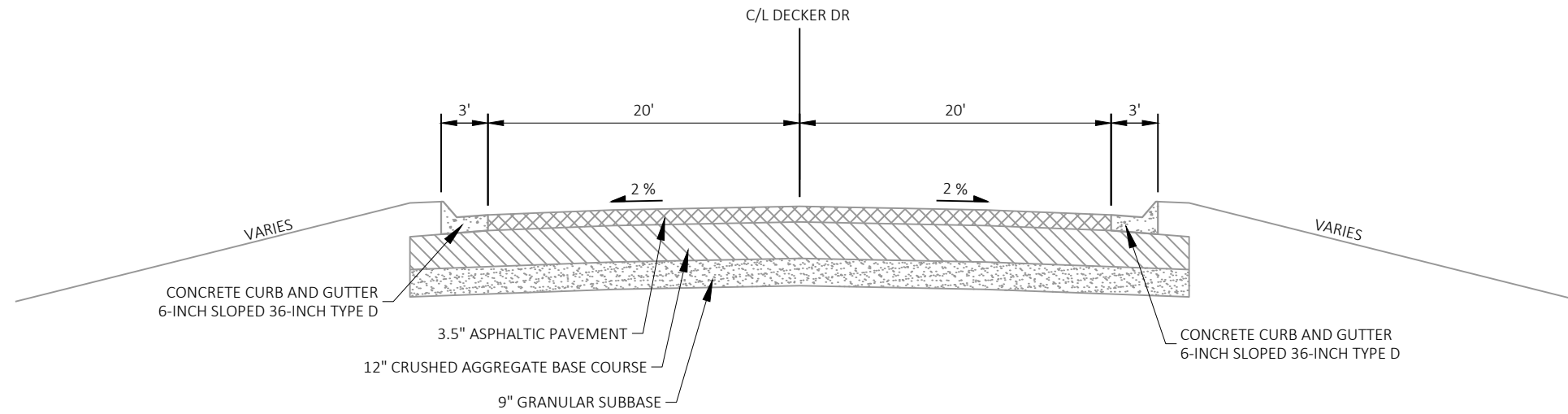


PROJECT NO: 8829-00-71	HWY: CTH O	COUNTY: BARRON	PROJECT OVERVIEW	SHEET	E
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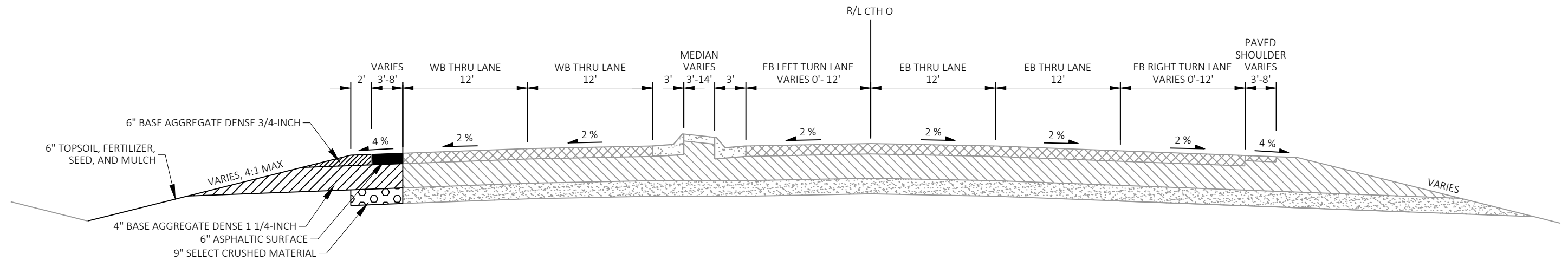


NOTE: DECKER DRIVE INTERSECTION
STA 104+44.40 - STA 105+98.15

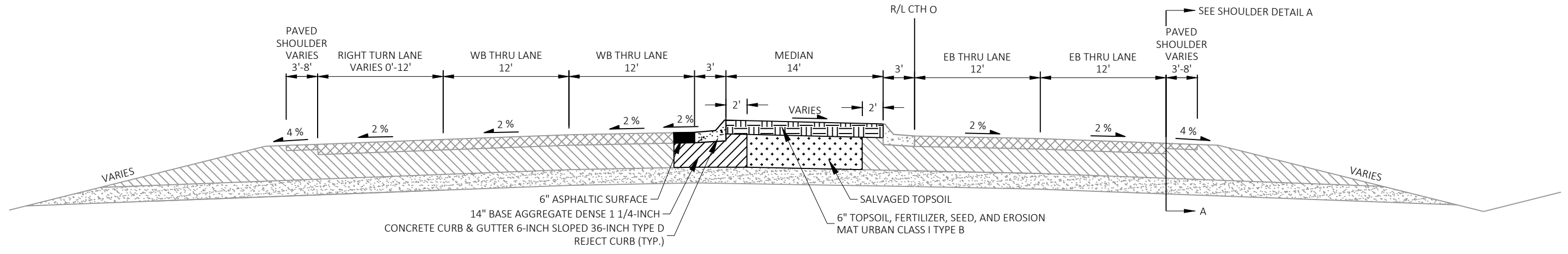




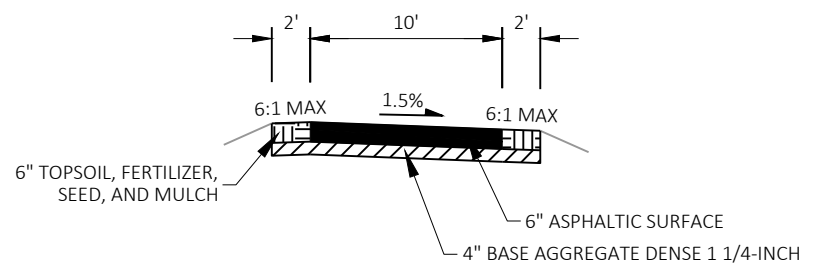
EXISTING TYPICAL SECTION - DECKER DRIVE
STA 12+00 - STA 19+00



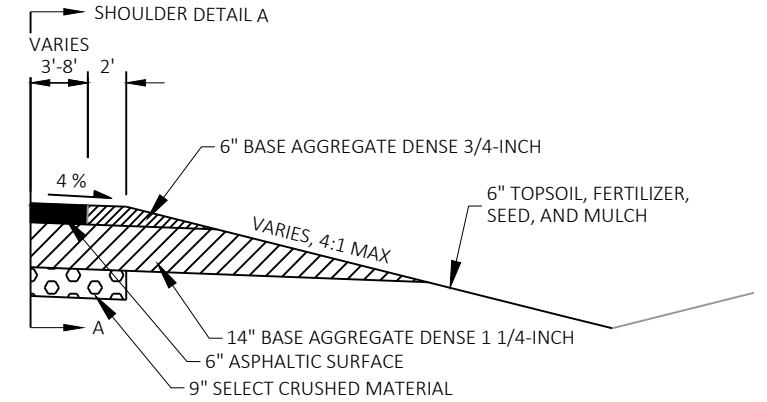
PROPOSED TYPICAL SECTION - CTH O
WEST LEG OF CTH O/DECKER DR INTERSECTION
STA 103+54.76 - STA 104+00.74



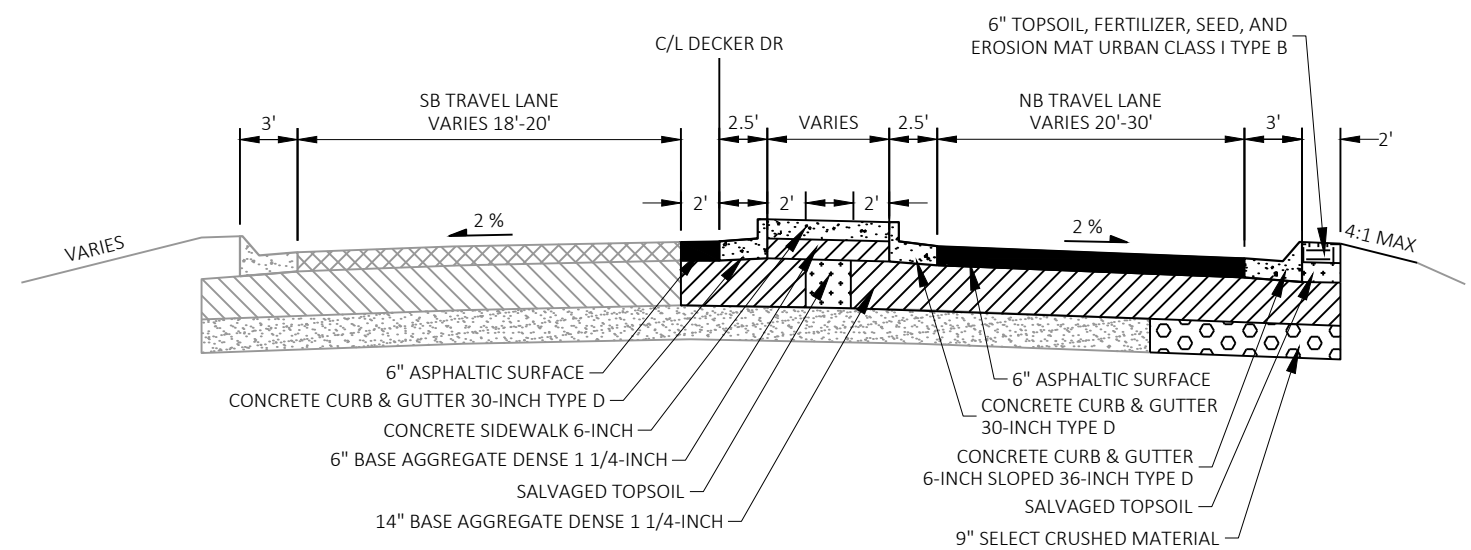
PROPOSED TYPICAL SECTION - CTH O
 EAST LEG OF CTH O/DECKER DR INTERSECTION
 STA 105+98.15 - STA 108+11.39



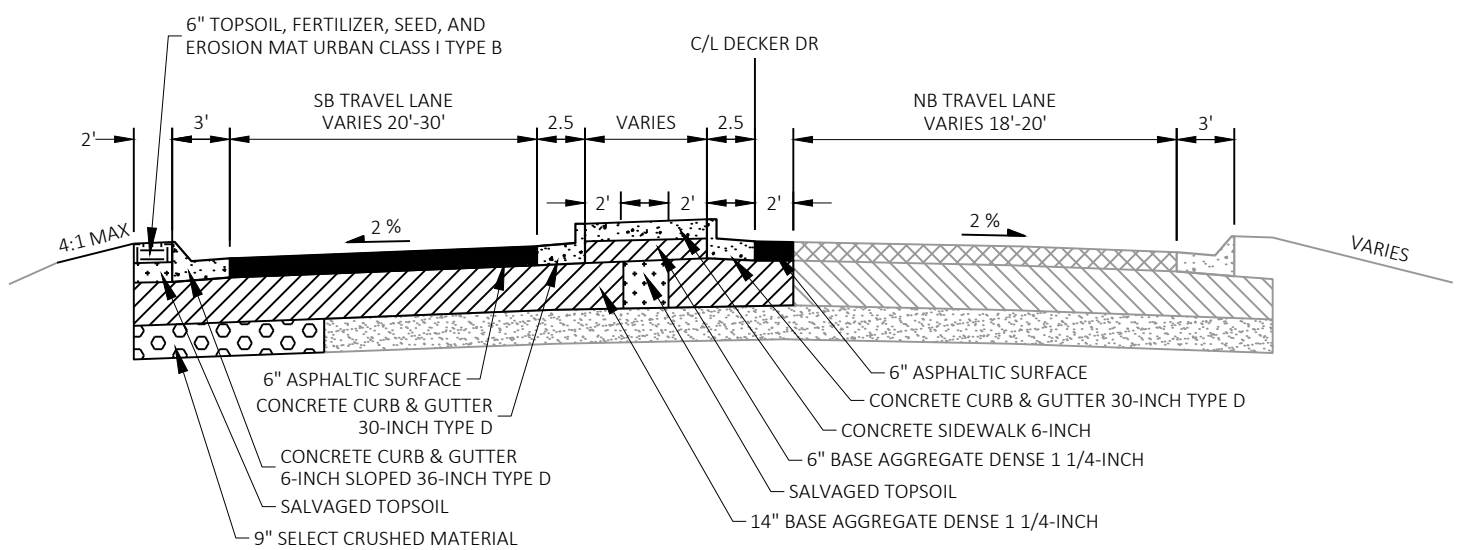
PROPOSED TYPICAL SECTION - MULTI-USE PATH
 STA 104+30.56 - STA 104+67.46



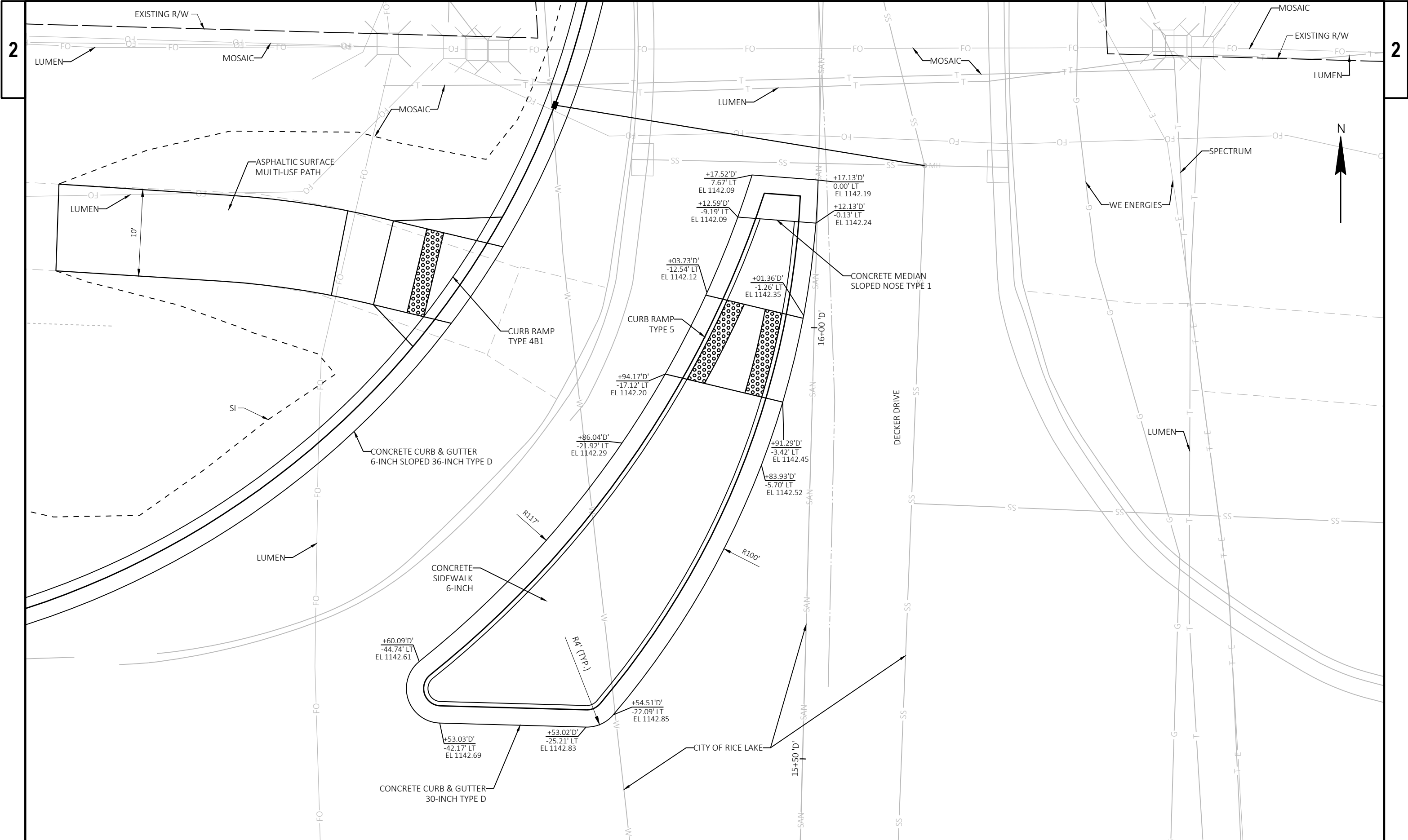
STA 106+00.31 - STA 107+26.59



PROPOSED TYPICAL SECTION - DECKER DRIVE SOUTH LEG
 STA 13+79.52 - STA 14+76.26

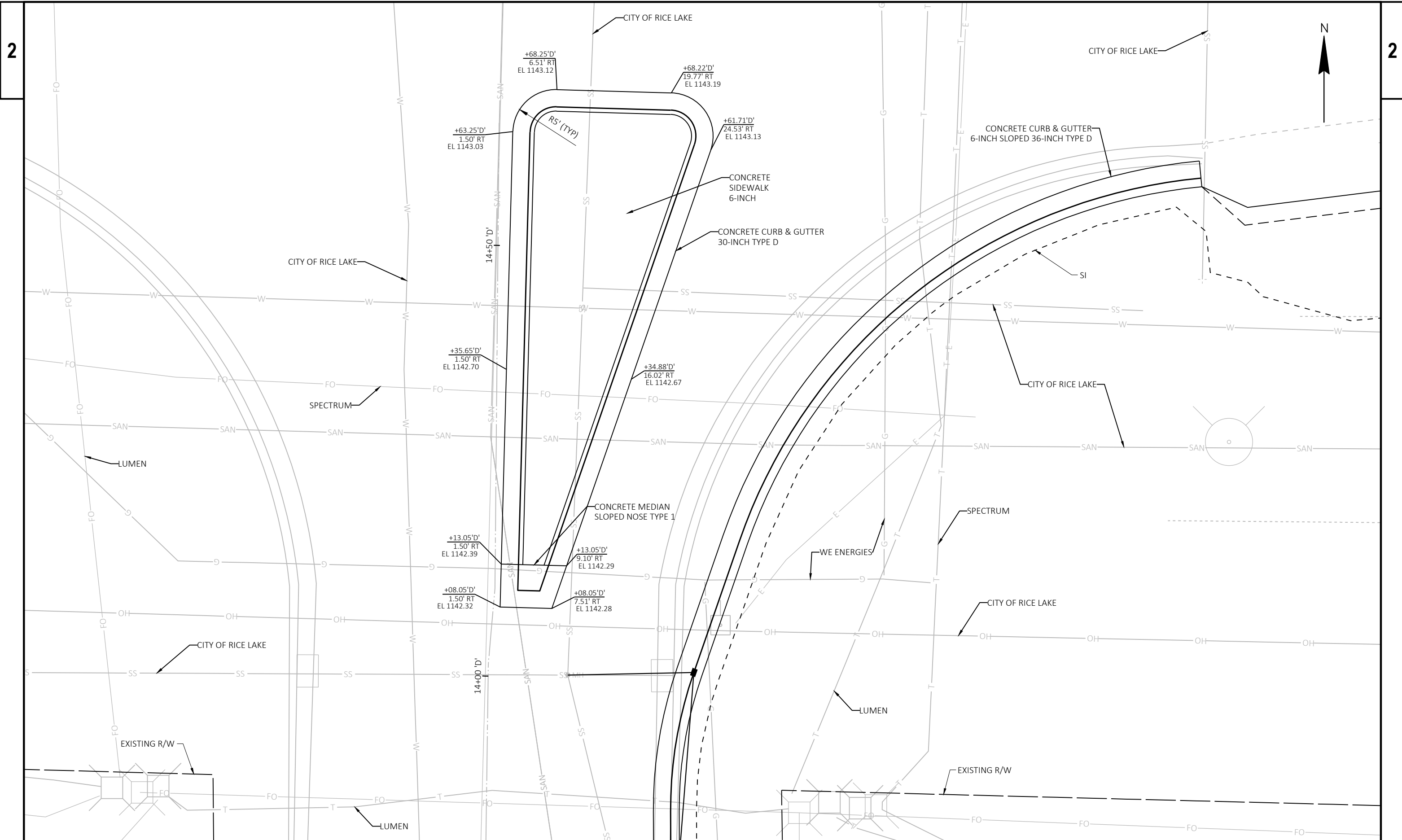


PROPOSED TYPICAL SECTION - DECKER DRIVE NORTH LEG
 STA 15+52.08 - STA 16+77.97



PROJECT NO: 8829-00-71 HWY: CTH O COUNTY: BARRON NW MEDIAN ISLAND DETAIL SHEET E

FILE NAME: H:\PROJECTS\16000\16044\DESIGN\CAD_BIM\88290001\16044\16044\02\1201-PD.DWG PLOT DATE: 9/18/2023 11:46 PM PLOT BY: RACHEL BURNHAM PLOT NAME: PLOT SCALE: 1 IN:10 FT WISDOT/CADD SHEET 42



2

2

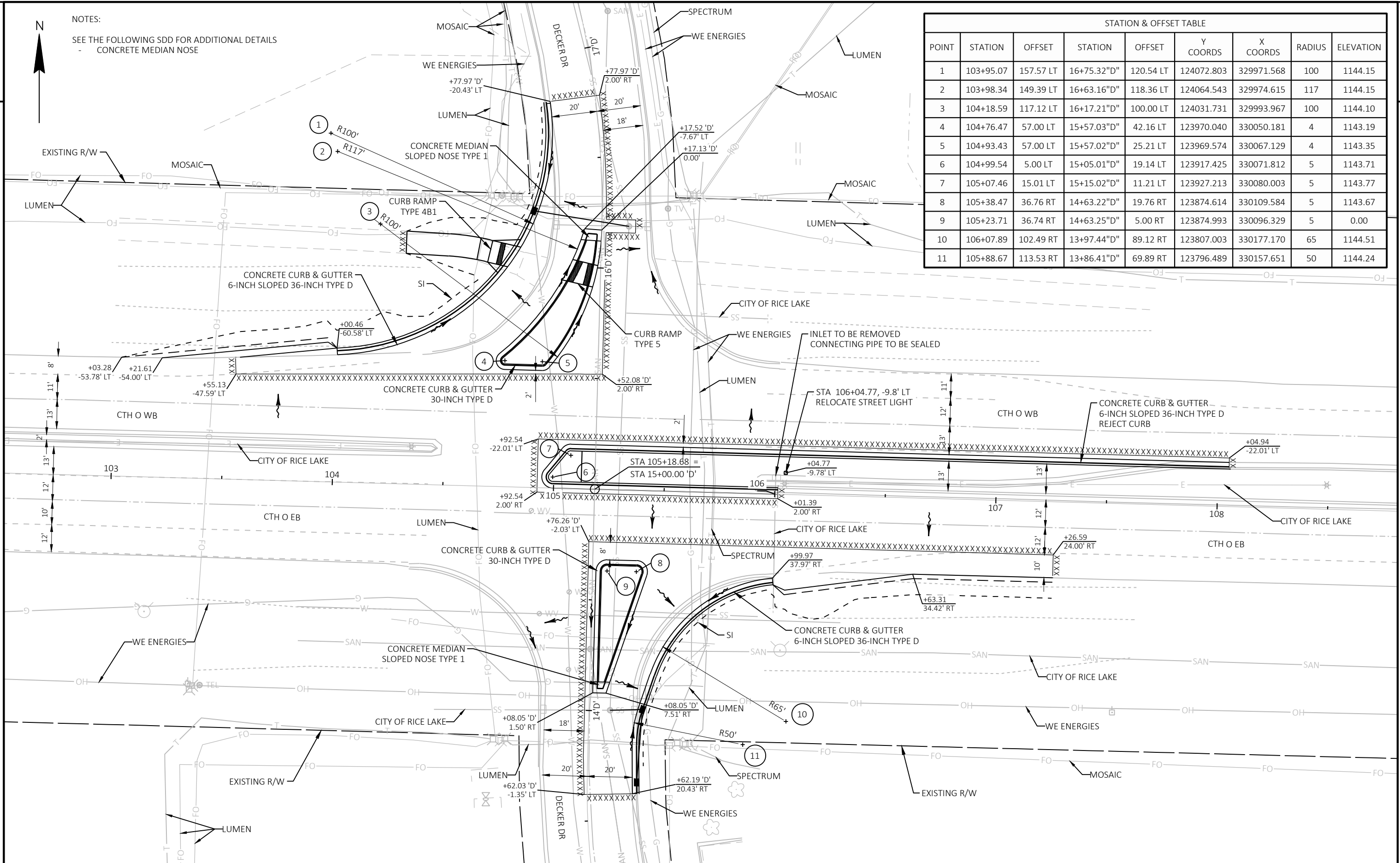


PROJECT NO: 8829-00-71	HWY: CTH O	COUNTY: BARRON	SE MEDIAN ISLAND DETAIL	SHEET	E
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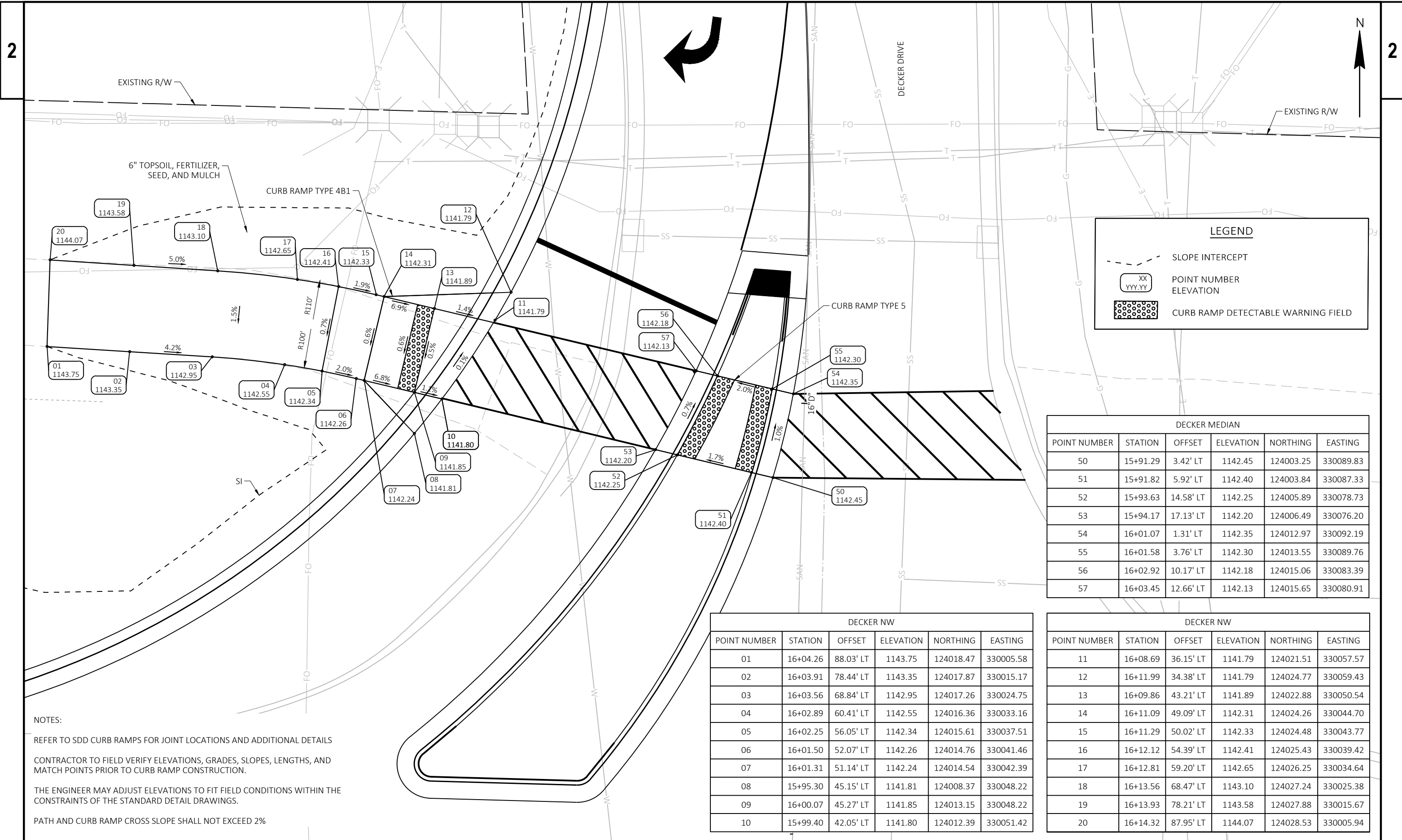


NOTES:
SEE THE FOLLOWING SDD FOR ADDITIONAL DETAILS
- CONCRETE MEDIAN NOSE

STATION & OFFSET TABLE								
POINT	STATION	OFFSET	STATION	OFFSET	Y COORDS	X COORDS	RADIUS	ELEVATION
1	103+95.07	157.57 LT	16+75.32"D"	120.54 LT	124072.803	329971.568	100	1144.15
2	103+98.34	149.39 LT	16+63.16"D"	118.36 LT	124064.543	329974.615	117	1144.15
3	104+18.59	117.12 LT	16+17.21"D"	100.00 LT	124031.731	329993.967	100	1144.10
4	104+76.47	57.00 LT	15+57.03"D"	42.16 LT	123970.040	330050.181	4	1143.19
5	104+93.43	57.00 LT	15+57.02"D"	25.21 LT	123969.574	330067.129	4	1143.35
6	104+99.54	5.00 LT	15+05.01"D"	19.14 LT	123917.425	330071.812	5	1143.71
7	105+07.46	15.01 LT	15+15.02"D"	11.21 LT	123927.213	330080.003	5	1143.77
8	105+38.47	36.76 RT	14+63.22"D"	19.76 RT	123874.614	330109.584	5	1143.67
9	105+23.71	36.74 RT	14+63.25"D"	5.00 RT	123874.993	330096.329	5	0.00
10	106+07.89	102.49 RT	13+97.44"D"	89.12 RT	123807.003	330177.170	65	1144.51
11	105+88.67	113.53 RT	13+86.41"D"	69.89 RT	123796.489	330157.651	50	1144.24



PROJECT NO: 8829-00-71 HWY: CTH O COUNTY: BARRON INTERSECTION DETAIL SHEET E



LEGEND

- - - SLOPE INTERCEPT
- XX
YY.YY POINT NUMBER
ELEVATION
- [Pattern] CURB RAMP DETECTABLE WARNING FIELD

DECKER MEDIAN					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
50	15+91.29	3.42' LT	1142.45	124003.25	330089.83
51	15+91.82	5.92' LT	1142.40	124003.84	330087.33
52	15+93.63	14.58' LT	1142.25	124005.89	330078.73
53	15+94.17	17.13' LT	1142.20	124006.49	330076.20
54	16+01.07	1.31' LT	1142.35	124012.97	330092.19
55	16+01.58	3.76' LT	1142.30	124013.55	330089.76
56	16+02.92	10.17' LT	1142.18	124015.06	330083.39
57	16+03.45	12.66' LT	1142.13	124015.65	330080.91

DECKER NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
01	16+04.26	88.03' LT	1143.75	124018.47	330005.58
02	16+03.91	78.44' LT	1143.35	124017.87	330015.17
03	16+03.56	68.84' LT	1142.95	124017.26	330024.75
04	16+02.89	60.41' LT	1142.55	124016.36	330033.16
05	16+02.25	56.05' LT	1142.34	124015.61	330037.51
06	16+01.50	52.07' LT	1142.26	124014.76	330041.46
07	16+01.31	51.14' LT	1142.24	124014.54	330042.39
08	15+95.30	45.15' LT	1141.81	124008.37	330048.22
09	16+00.07	45.27' LT	1141.85	124013.15	330048.22
10	15+99.40	42.05' LT	1141.80	124012.39	330051.42

DECKER NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
11	16+08.69	36.15' LT	1141.79	124021.51	330057.57
12	16+11.99	34.38' LT	1141.79	124024.77	330059.43
13	16+09.86	43.21' LT	1141.89	124022.88	330050.54
14	16+11.09	49.09' LT	1142.31	124024.26	330044.70
15	16+11.29	50.02' LT	1142.33	124024.48	330043.77
16	16+12.12	54.39' LT	1142.41	124025.43	330039.42
17	16+12.81	59.20' LT	1142.65	124026.25	330034.64
18	16+13.56	68.47' LT	1143.10	124027.24	330025.38
19	16+13.93	78.21' LT	1143.58	124027.88	330015.67
20	16+14.32	87.95' LT	1144.07	124028.53	330005.94

NOTES:
 REFER TO SDD CURB RAMPS FOR JOINT LOCATIONS AND ADDITIONAL DETAILS
 CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP CONSTRUCTION.
 THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE CONSTRAINTS OF THE STANDARD DETAIL DRAWINGS.
 PATH AND CURB RAMP CROSS SLOPE SHALL NOT EXCEED 2%

LEGEND

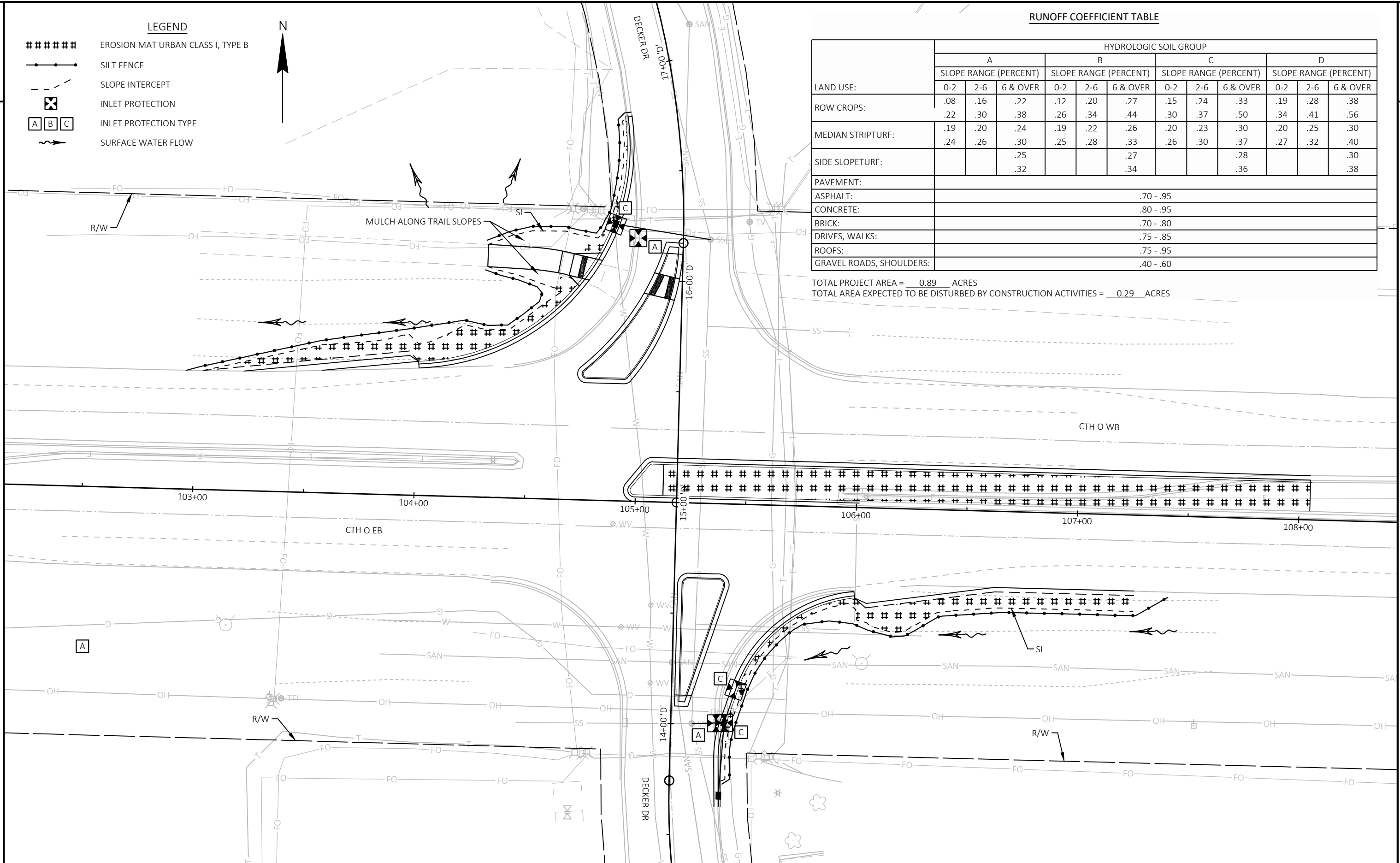
- ##### EROSION MAT URBAN CLASS I, TYPE B
- SILT FENCE
- - - SLOPE INTERCEPT
- ⊗ INLET PROTECTION
- [A] [B] [C] INLET PROTECTION TYPE
- ~> SURFACE WATER FLOW

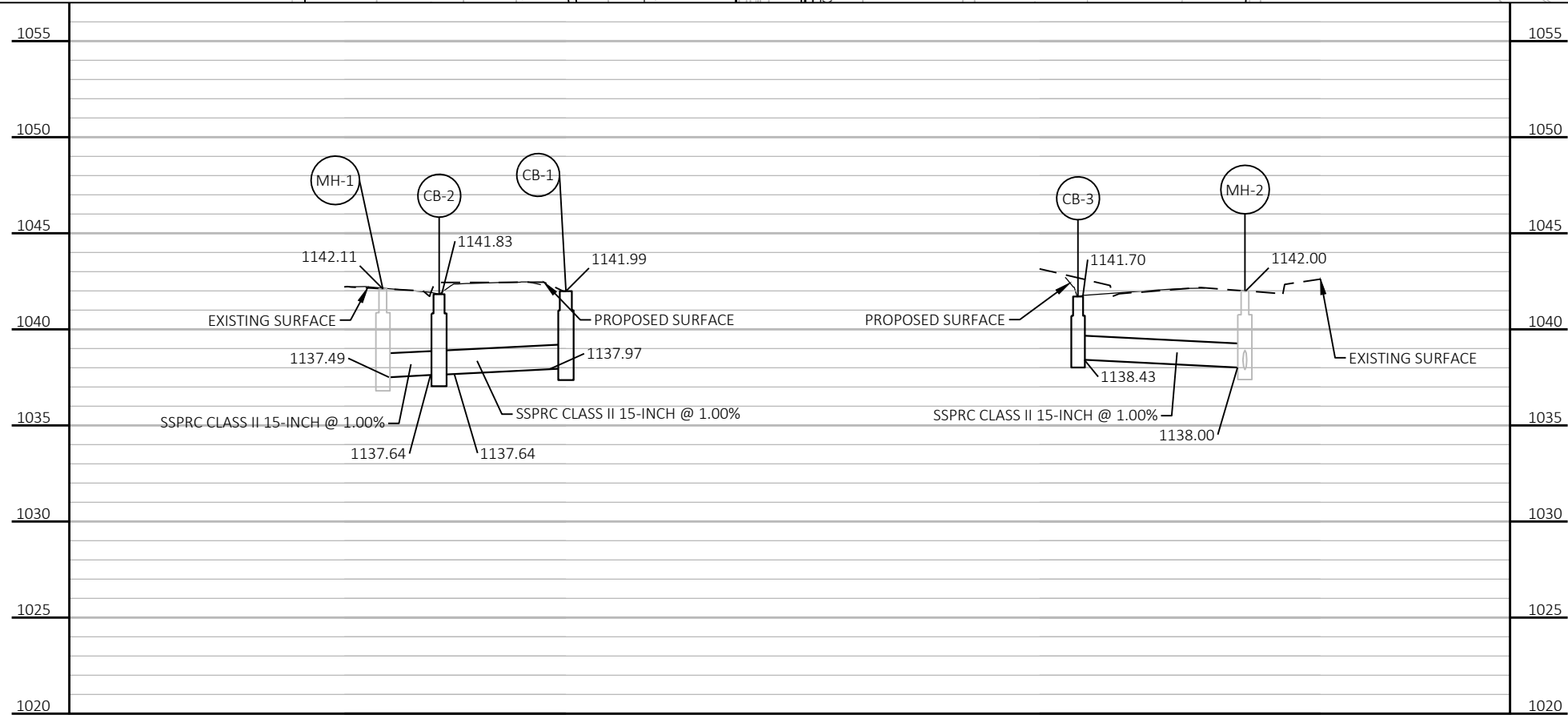
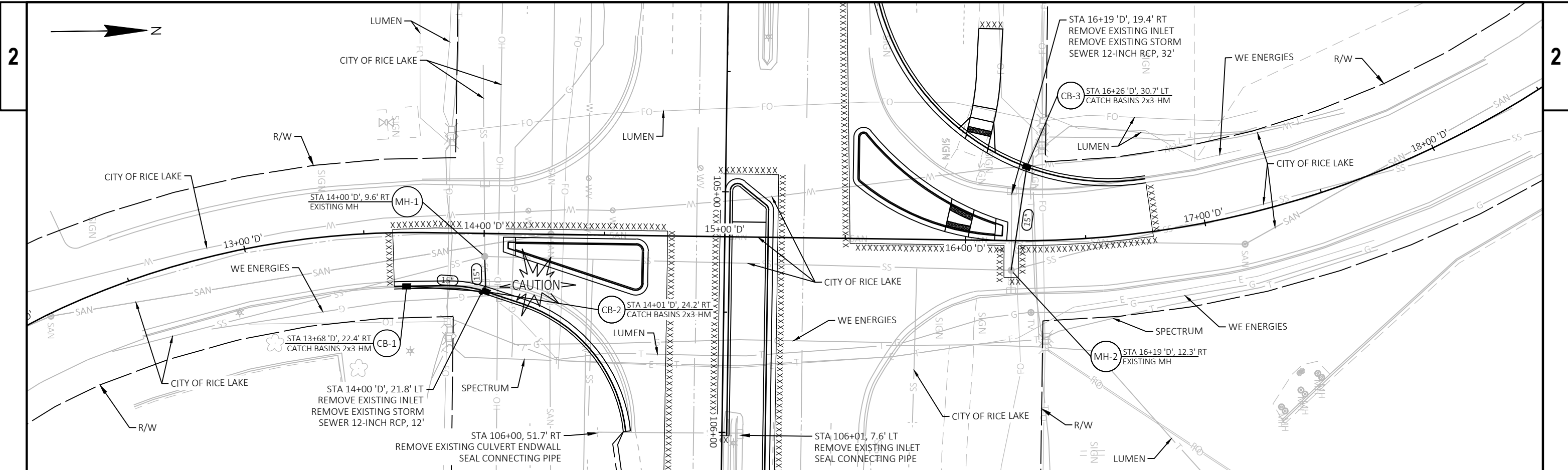


RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS:	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38
	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56
MEDIAN STRIPTURF:	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30
	.24	.26	.30	.25	.28	.33	.26	.30	.37	.27	.32	.40
SIDE SLOPETURF:			.25			.27			.28			.30
			.32			.34			.36			.38
PAVEMENT:												
ASPHALT:	.70 -.95											
CONCRETE:	.80 -.95											
BRICK:	.70 -.80											
DRIVES, WALKS:	.75 -.85											
ROOFS:	.75 -.95											
GRAVEL ROADS, SHOULDERS:	.40 -.60											

TOTAL PROJECT AREA = 0.89 ACRES
 TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.29 ACRES





PROJECT NO: 8829-00-71

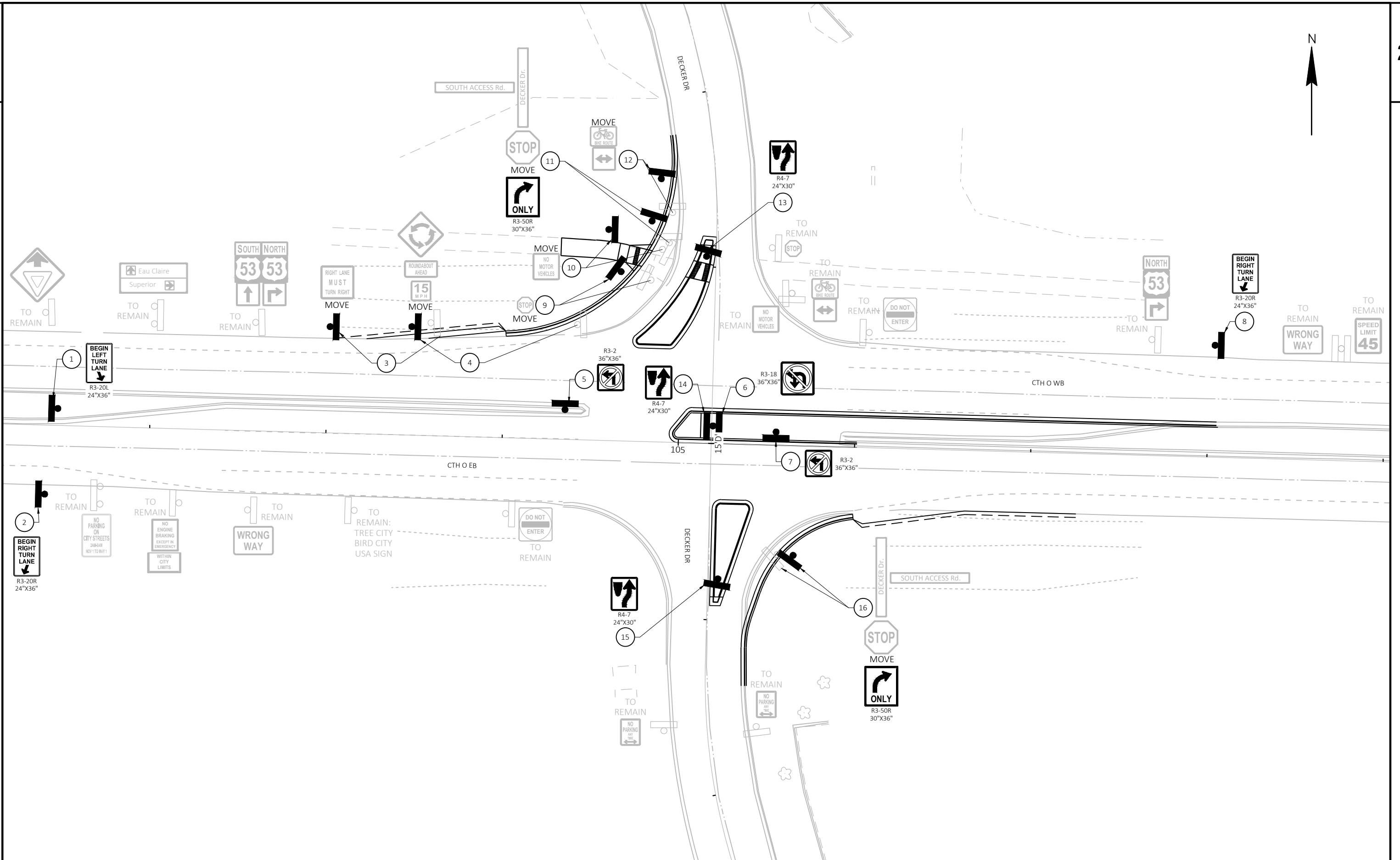
HWY: CTH O

COUNTY: BARRON

STORM SEWER

SHEET

E



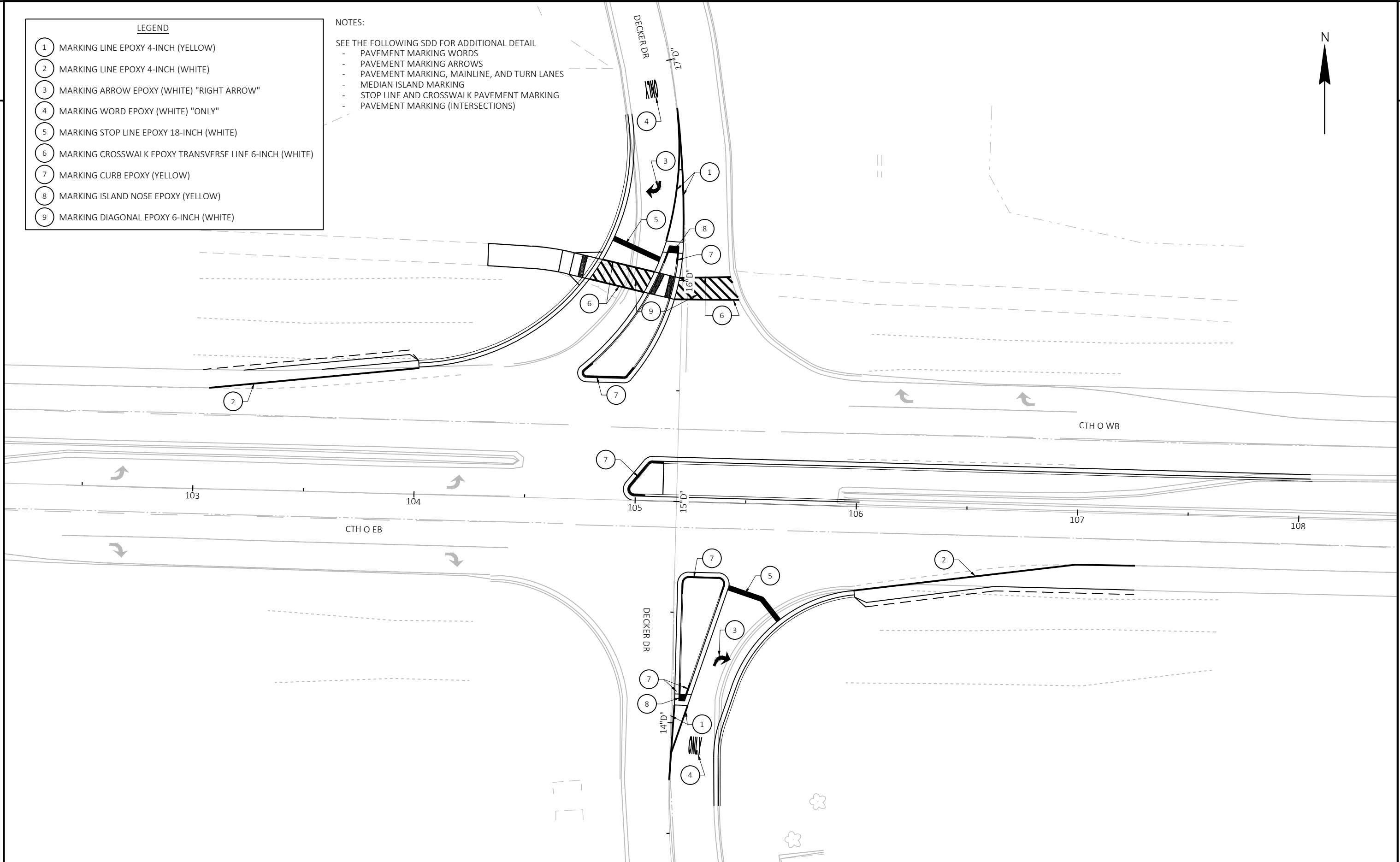
PROJECT NO: 8829-00-71	HWY: CTH O	COUNTY: BARRON	PERMANENT SIGNING	SHEET	E
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LEGEND

- 1 MARKING LINE EPOXY 4-INCH (YELLOW)
- 2 MARKING LINE EPOXY 4-INCH (WHITE)
- 3 MARKING ARROW EPOXY (WHITE) "RIGHT ARROW"
- 4 MARKING WORD EPOXY (WHITE) "ONLY"
- 5 MARKING STOP LINE EPOXY 18-INCH (WHITE)
- 6 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- 7 MARKING CURB EPOXY (YELLOW)
- 8 MARKING ISLAND NOSE EPOXY (YELLOW)
- 9 MARKING DIAGONAL EPOXY 6-INCH (WHITE)

NOTES:

- SEE THE FOLLOWING SDD FOR ADDITIONAL DETAIL
- PAVEMENT MARKING WORDS
 - PAVEMENT MARKING ARROWS
 - PAVEMENT MARKING, MAINLINE, AND TURN LANES
 - MEDIAN ISLAND MARKING
 - STOP LINE AND CROSSWALK PAVEMENT MARKING
 - PAVEMENT MARKING (INTERSECTIONS)



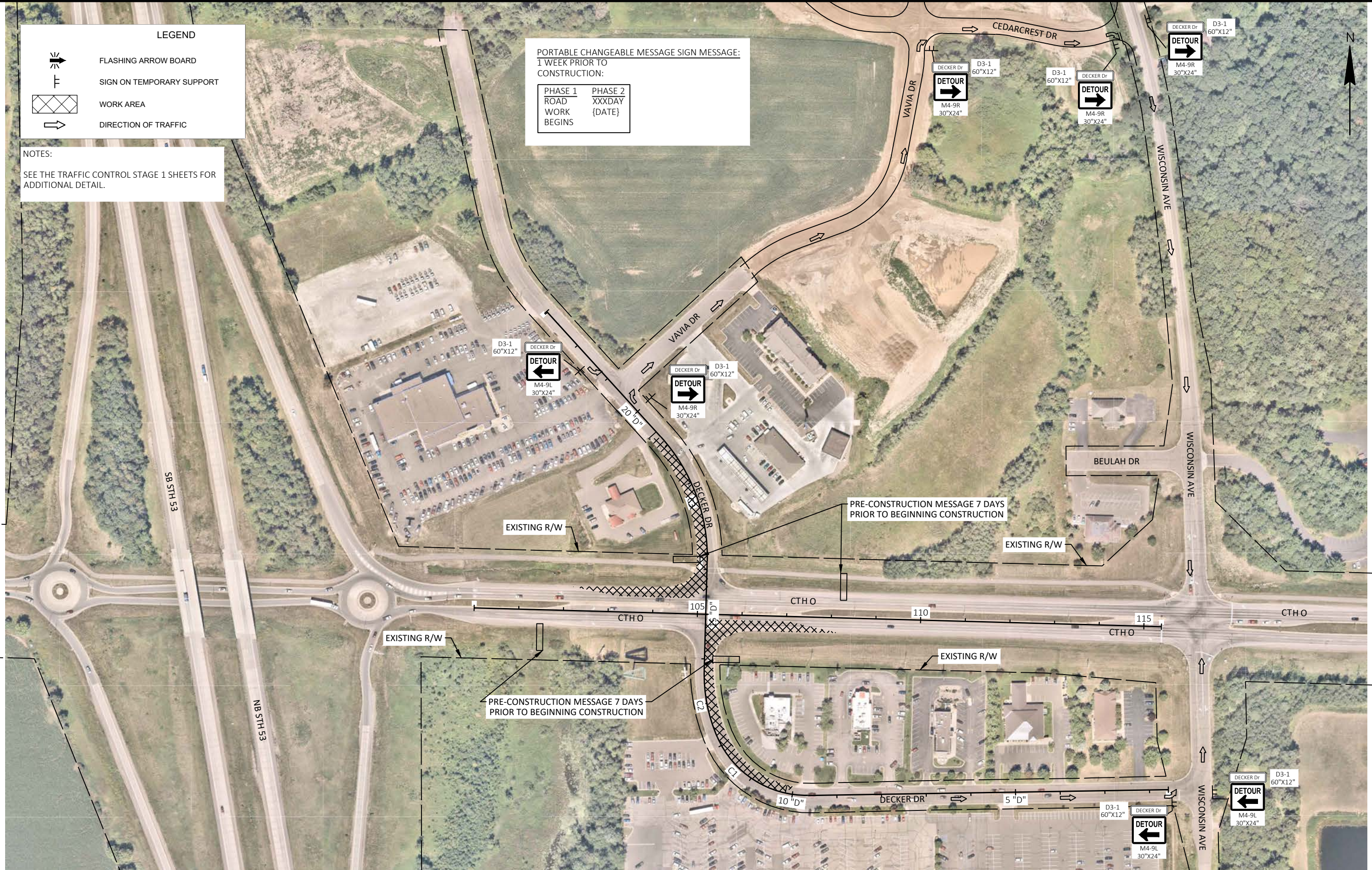
LEGEND

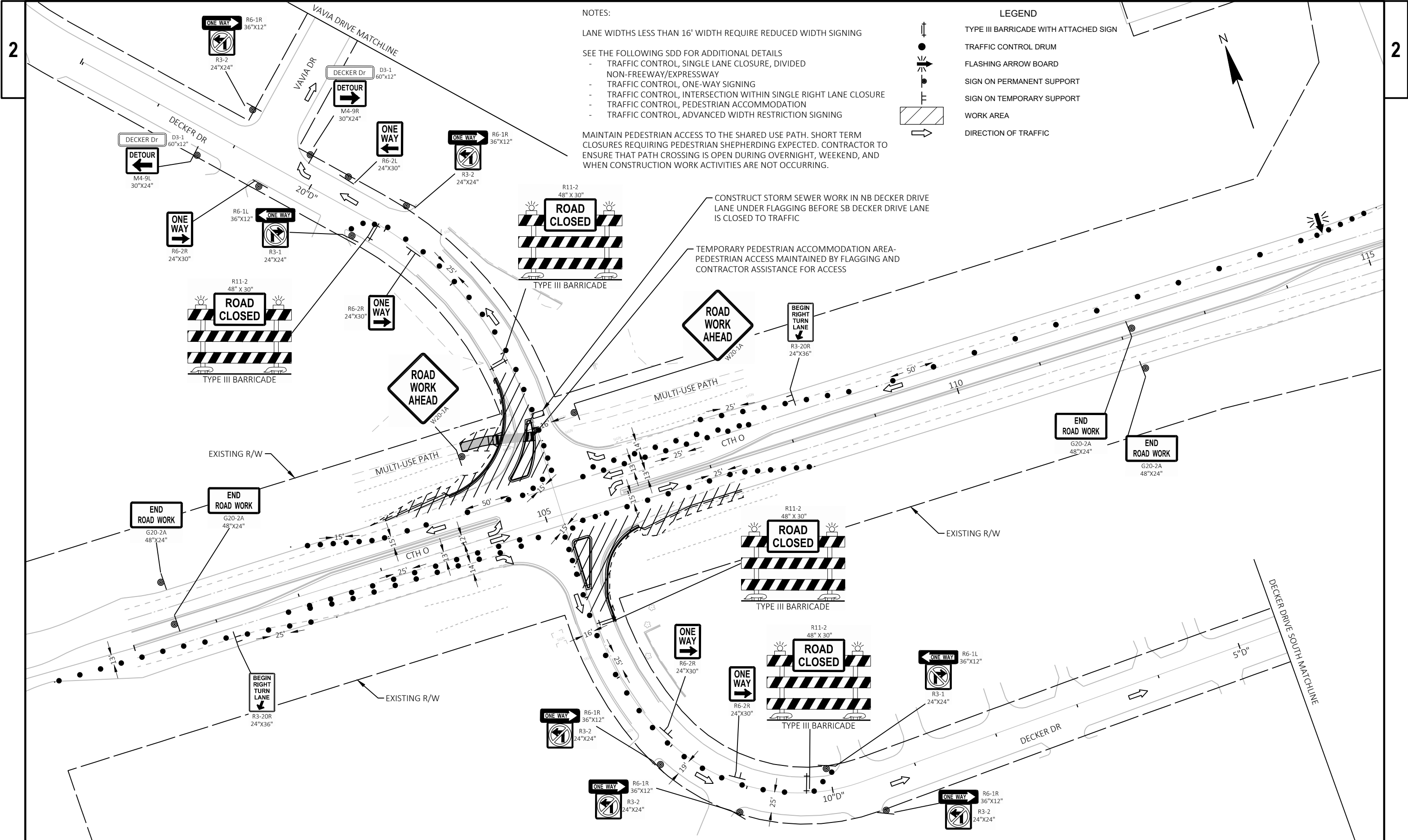
- FLASHING ARROW BOARD
- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- DIRECTION OF TRAFFIC

NOTES:
SEE THE TRAFFIC CONTROL STAGE 1 SHEETS FOR ADDITIONAL DETAIL.

PORTABLE CHANGEABLE MESSAGE SIGN MESSAGE:
1 WEEK PRIOR TO CONSTRUCTION:

PHASE 1 ROAD WORK BEGINS	PHASE 2 XXXDAY {DATE}
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NOTES:

LANE WIDTHS LESS THAN 16' WIDTH REQUIRE REDUCED WIDTH SIGNING






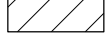

SEE THE FOLLOWING SDD FOR ADDITIONAL DETAILS

- TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED
- NON-FREWAY/EXPRESSWAY
- TRAFFIC CONTROL, ONE-WAY SIGNING
- TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE RIGHT LANE CLOSURE
- TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
- TRAFFIC CONTROL, ADVANCED WIDTH RESTRICTION SIGNING

MAINTAIN PEDESTRIAN ACCESS TO THE SHARED USE PATH. SHORT TERM CLOSURES REQUIRING PEDESTRIAN SHEPHERDING EXPECTED. CONTRACTOR TO ENSURE THAT PATH CROSSING IS OPEN DURING OVERNIGHT, WEEKEND, AND WHEN CONSTRUCTION WORK ACTIVITIES ARE NOT OCCURRING.

- LEGEND
- TYPE III BARRICADE WITH ATTACHED SIGN
 - TRAFFIC CONTROL DRUM
 - FLASHING ARROW BOARD
 - SIGN ON PERMANENT SUPPORT
 - SIGN ON TEMPORARY SUPPORT
 - WORK AREA
 - DIRECTION OF TRAFFIC

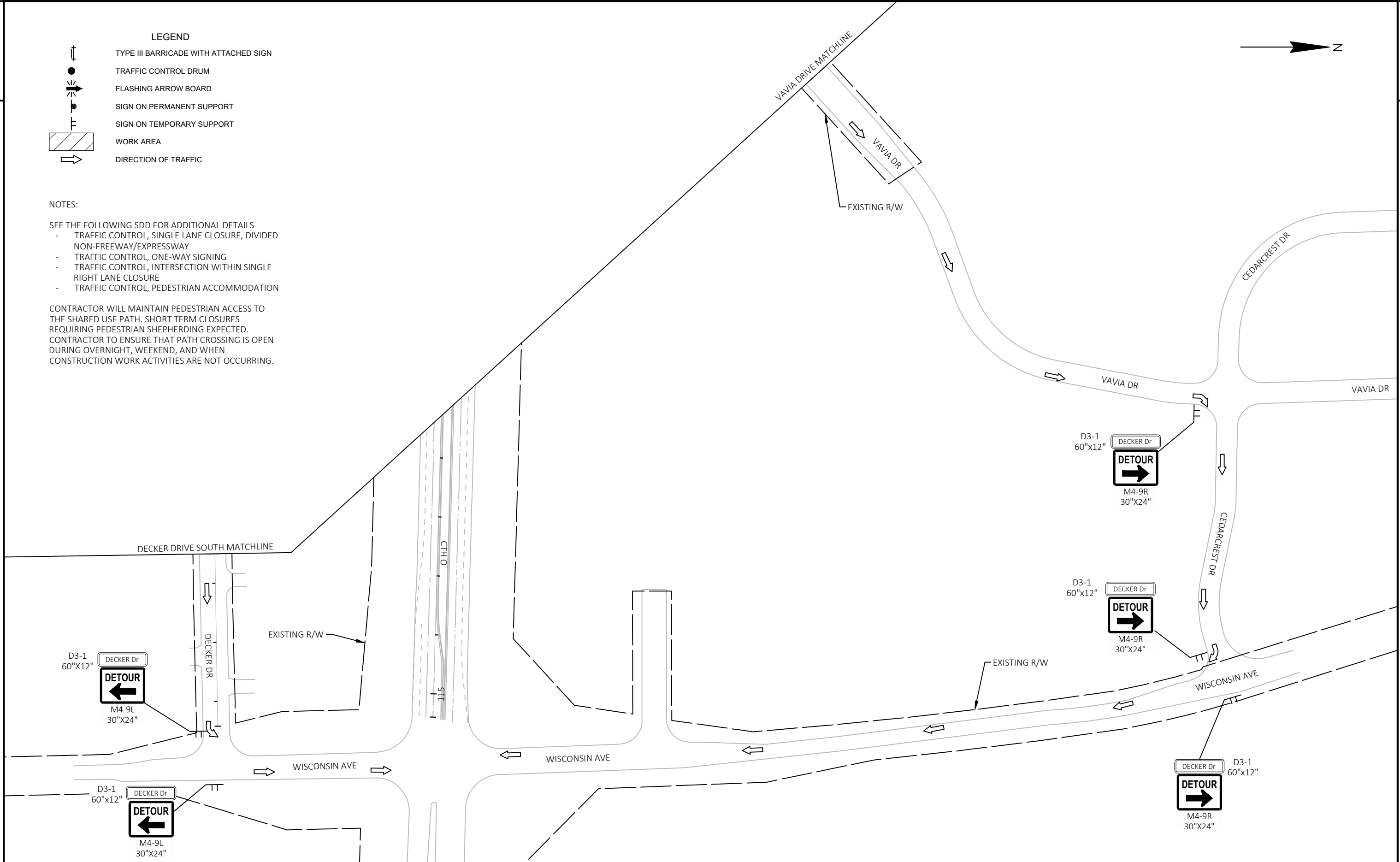
LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  FLASHING ARROW BOARD
-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  DIRECTION OF TRAFFIC

NOTES:

- SEE THE FOLLOWING SDD FOR ADDITIONAL DETAILS
- TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY
 - TRAFFIC CONTROL, ONE-WAY SIGNING
 - TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE RIGHT LANE CLOSURE
 - TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

CONTRACTOR WILL MAINTAIN PEDESTRIAN ACCESS TO THE SHARED USE PATH. SHORT TERM CLOSURES REQUIRING PEDESTRIAN SHEPHERDING EXPECTED. CONTRACTOR TO ENSURE THAT PATH CROSSING IS OPEN DURING OVERNIGHT, WEEKEND, AND WHEN CONSTRUCTION WORK ACTIVITIES ARE NOT OCCURRING.



PROJECT NO: 8829-00-71

HWY: CTH O

COUNTY: BARRON

TRAFFIC CONTROL- STAGE 1

SHEET

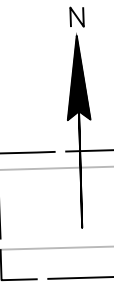
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NOTES:

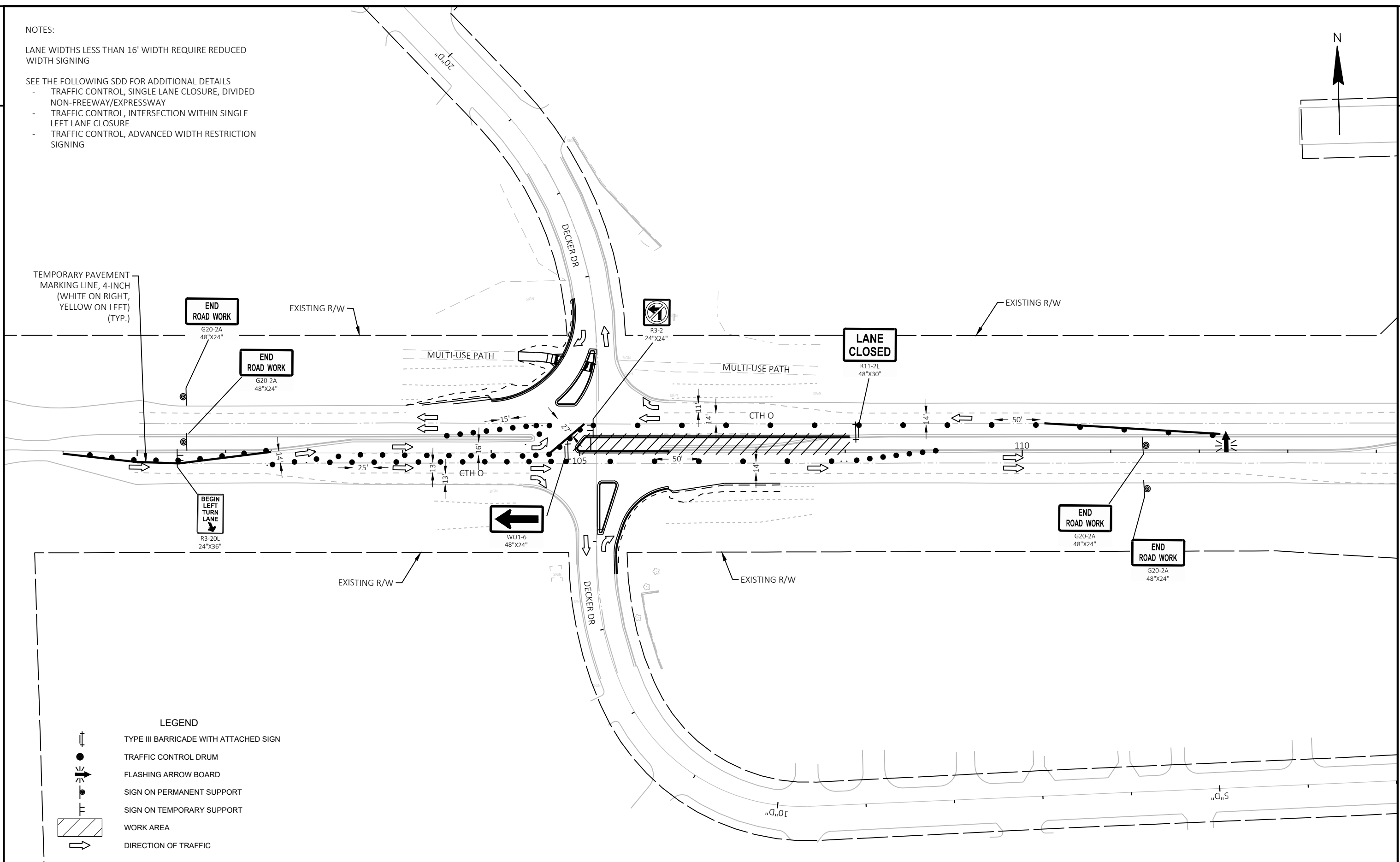
LANE WIDTHS LESS THAN 16' WIDTH REQUIRE REDUCED WIDTH SIGNING

SEE THE FOLLOWING SDD FOR ADDITIONAL DETAILS

- TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREWAY/EXPRESSWAY
- TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LEFT LANE CLOSURE
- TRAFFIC CONTROL, ADVANCED WIDTH RESTRICTION SIGNING

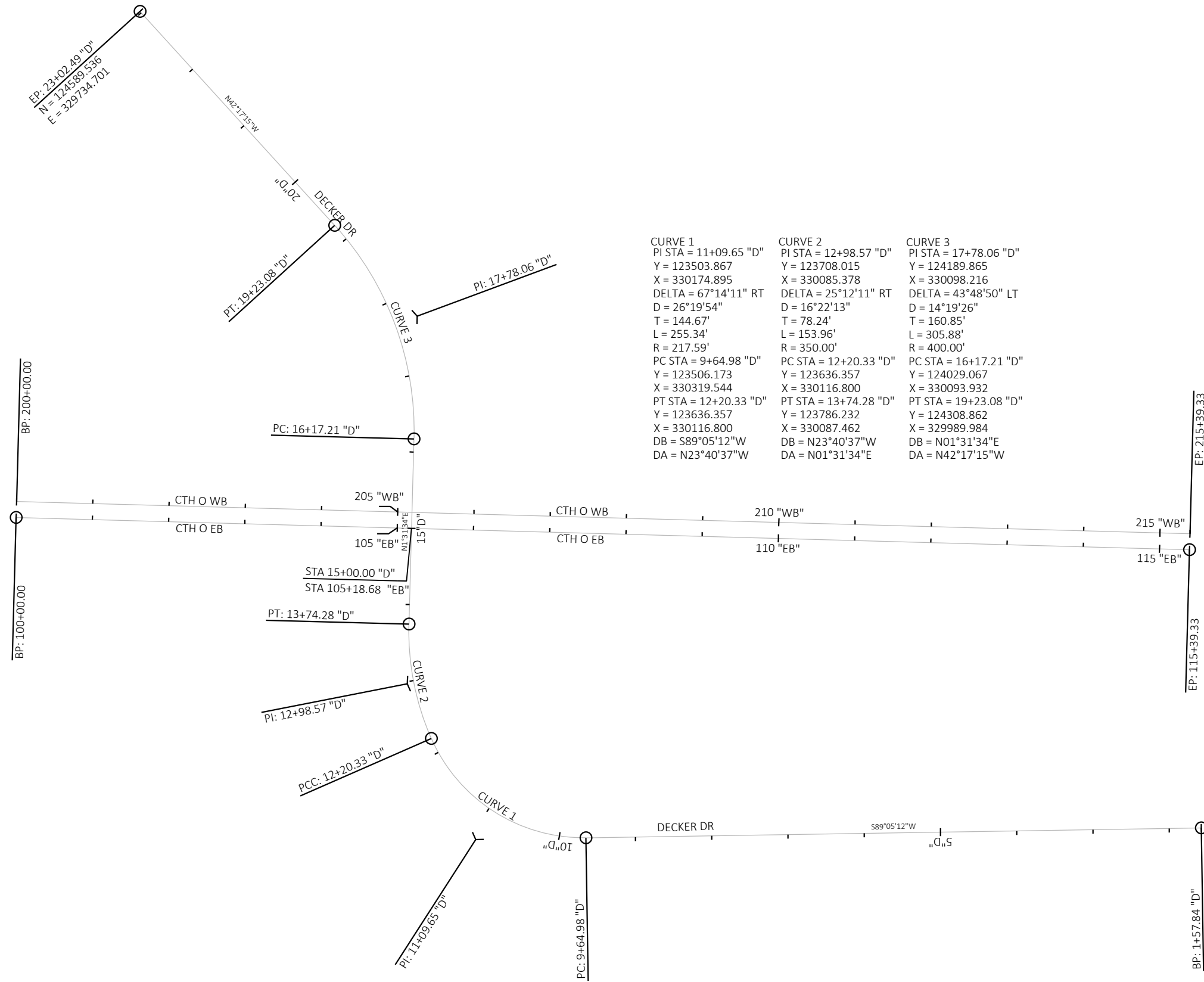


TEMPORARY PAVEMENT MARKING LINE, 4-INCH (WHITE ON RIGHT, YELLOW ON LEFT) (TYP.)



LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- DIRECTION OF TRAFFIC



Estimate Of Quantities

8829-00-71

Line	Item	Item Description	Unit	Total	Qty
0002	203.0100	Removing Small Pipe Culverts	EACH	1.000	1.000
0004	204.0110	Removing Asphaltic Surface	SY	1,045.000	1,045.000
0006	204.0150	Removing Curb & Gutter	LF	490.000	490.000
0008	204.0155	Removing Concrete Sidewalk	SY	34.000	34.000
0010	204.0215	Removing Catch Basins	EACH	2.000	2.000
0012	204.0220	Removing Inlets	EACH	2.000	2.000
0014	204.0245	Removing Storm Sewer (size) 01. 12-INCH, RPC	LF	44.000	44.000
0016	204.0280	Sealing Pipes	EACH	1.000	1.000
0018	205.0100	Excavation Common	CY	598.000	598.000
0020	213.0100	Finishing Roadway (project) 01. 8829-00-71	EACH	1.000	1.000
0022	305.0110	Base Aggregate Dense 3/4-Inch	TON	26.000	26.000
0024	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	1,568.000	1,568.000
0026	305.0500	Shaping Shoulders	STA	4.000	4.000
0028	312.0110	Select Crushed Material	TON	230.000	230.000
0030	455.0605	Tack Coat	GAL	64.000	64.000
0032	465.0105	Asphaltic Surface	TON	436.000	436.000
0034	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	295.000	295.000
0036	601.0557	Concrete Curb & Gutter 6-Inch Sloped 36-Inch Type D	LF	703.000	703.000
0038	602.0415	Concrete Sidewalk 6-Inch	SF	1,577.000	1,577.000
0040	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	60.000	60.000
0042	608.0215	Storm Sewer Pipe Reinforced Concrete Class II 15-Inch	LF	92.000	92.000
0044	611.0627	Inlet Covers Type HM	EACH	3.000	3.000
0046	611.1230	Catch Basins 2x3-FT	EACH	3.000	3.000
0048	618.0100	Maintenance And Repair of Haul Roads (project) 01. 8829-00-71	EACH	1.000	1.000
0050	619.1000	Mobilization	EACH	1.000	1.000
0052	620.0300	Concrete Median Sloped Nose	SF	76.000	76.000
0054	624.0100	Water	MGAL	24.000	24.000
0056	625.0100	Topsoil	SY	834.000	834.000
0058	625.0500	Salvaged Topsoil	SY	417.000	417.000
0060	627.0200	Mulching	SY	50.000	50.000
0062	628.1504	Silt Fence	LF	567.000	567.000
0064	628.1520	Silt Fence Maintenance	LF	567.000	567.000
0066	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0068	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0070	628.2008	Erosion Mat Urban Class I Type B	SY	714.000	714.000
0072	628.7005	Inlet Protection Type A	EACH	2.000	2.000
0074	628.7015	Inlet Protection Type C	EACH	3.000	3.000
0076	629.0210	Fertilizer Type B	CWT	60.000	60.000
0078	630.0130	Seeding Mixture No. 30	LB	8.500	8.500
0080	630.0500	Seed Water	MGAL	19.000	19.000
0082	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	8.000	8.000
0084	637.2210	Signs Type II Reflective H	SF	75.000	75.000
0086	638.2102	Moving Signs Type II	EACH	14.000	14.000
0088	638.4000	Moving Small Sign Supports	EACH	7.000	7.000
0090	642.5001	Field Office Type B	EACH	1.000	1.000
0092	643.0300	Traffic Control Drums	DAY	5,468.000	5,468.000
0094	643.0420	Traffic Control Barricades Type III	DAY	112.000	112.000
0096	643.0800	Traffic Control Arrow Boards	DAY	44.000	44.000
0098	643.0900	Traffic Control Signs	DAY	1,777.000	1,777.000

Estimate Of Quantities

8829-00-71

Line	Item	Item Description	Unit	Total	Qty
0100	643.1050	Traffic Control Signs PCMS	DAY	28.000	28.000
0102	643.3120	Temporary Marking Line Epoxy 4-Inch	LF	485.000	485.000
0104	643.5000	Traffic Control	EACH	1.000	1.000
0106	644.1440	Temporary Pedestrian Surface Matting	SF	500.000	500.000
0108	644.1601	Temporary Pedestrian Curb Ramp	DAY	56.000	56.000
0110	644.1605	Temporary Pedestrian Detectable Warning Field	SF	40.000	40.000
0112	644.1810	Temporary Pedestrian Barricade	LF	40.000	40.000
0114	646.1020	Marking Line Epoxy 4-Inch	LF	403.000	403.000
0116	646.5020	Marking Arrow Epoxy	EACH	2.000	2.000
0118	646.5120	Marking Word Epoxy	EACH	2.000	2.000
0120	646.6120	Marking Stop Line Epoxy 18-Inch	LF	52.000	52.000
0122	646.7020	Marking Diagonal Epoxy 6-Inch	LF	175.000	175.000
0124	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	103.000	103.000
0126	646.8120	Marking Curb Epoxy	LF	120.000	120.000
0128	646.8220	Marking Island Nose Epoxy	EACH	2.000	2.000
0130	650.4000	Construction Staking Storm Sewer	EACH	3.000	3.000
0132	650.4500	Construction Staking Subgrade	LF	972.000	972.000
0134	650.5000	Construction Staking Base	LF	972.000	972.000
0136	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	998.000	998.000
0138	650.9000	Construction Staking Curb Ramps	EACH	2.000	2.000
0140	650.9500	Construction Staking Sidewalk (project) 01. 8829-00-71	EACH	1.000	1.000
0142	650.9911	Construction Staking Supplemental Control (project) 01. 8829-00-71	EACH	1.000	1.000
0144	650.9920	Construction Staking Slope Stakes	LF	325.000	325.000
0146	654.0102	Concrete Bases Type 2	EACH	1.000	1.000
0148	655.0640	Electrical Wire Lighting 1 AWG	LF	10.000	10.000
0150	657.0100	Pedestal Bases	EACH	1.000	1.000
0152	690.0150	Sawing Asphalt	LF	1,161.000	1,161.000
0154	690.0250	Sawing Concrete	LF	12.000	12.000
0156	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0158	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000
0160	SPV.0060	Special 01. Relocate Street Light	EACH	1.000	1.000

REMOVALS

CTH O STA	DECKER DR STA	LOCATION	203.0100 REMOVING SMALL PIPE CULVERTS EACH	204.0215 REMOVING CATCH BASINS EACH	204.0220 REMOVING INLETS EACH	204.0245 REMOVING STORM SEWER 12-INCH LF	204.0280 SEALING PIPES EACH	204.0150 REMOVING CURB AND GUTTER LF	204.0155 REMOVING CONCRETE SIDEWALK SY	204.0110 REMOVING ASPHALTIC SURFACE SY	205.0100 EXCAVATION COMMON CY	REMARKS
103+55 - 105+20	15+52 - 16+78	LT	-	-	-	-	-	-	-	514	365	NW QUADRANT
104+98	16+19 'D'	LT	-	1	1	32	-	-	-	-	-	NW QUADRANT
104+39 - 104+92	15+59 'D' - 16+78 'D'	LT	-	-	-	-	-	148	-	-	-	NW QUADRANT
104+30 - 104+82	-	LT	-	-	-	-	-	-	-	17	-	MULTI-USE PATH
104+93 - 108+05	-	LT	-	-	-	-	-	-	-	514	233	CTH O MEDIAN
105+40	14+00 'D'	RT	-	1	1	12	-	-	-	-	-	SE QUADRANT
105+41 - 106+00	13+75 'D' - 14+59 'D'	RT	-	-	-	-	-	122	-	-	-	SE QUADRANT
105+92 - 108+05	-	LT	-	-	-	-	-	220	34	-	-	CTH O MEDIAN
106+00 - 106+01	14+48 'D' - 15+07 'D'	RT/LT	1	-	-	-	1	-	-	-	-	CTH O MEDIAN
ITEM TOTAL:			1	2	2	44	1	490	34	1045	598	

AGGREGATE

STATION - STATION	LOCATION	305.0500 SHAPING SHOULDERS STA	305.0110 BASE AGGREGATE 3/4-INCH TON	305.0120 BASE AGGREGATE 1-1/4-INCH TON	312.0110 SELECT CRUSHED MATERIAL TON	624.0100 WATER MGAL	REMARKS
103+03 - 104+00	LT	2	11	-	-	0.2	LT SHOULDER
104+31 - 104+82	LT	-	-	17	-	0.3	MULTI-USE PATH
103+55 - 105+21	LT	-	-	643	148	9.6	NW QUADRANT
104+75 - 105+16	LT	-	-	26	-	0.4	NW MEDIAN ISLAND
105+12 - 108+05	LT	-	-	272	-	4.1	CTH O MEDIAN
105+17 - 107+26	RT	-	-	589	82	8.8	SW QUADRANT
105+23 - 105+41	RT	-	-	21	-	0.3	SW MEDIAN ISLAND
106+00 - 107+26	RT	2	15	-	-	0.2	RT SHOULDER
ITEM TOTALS		4	26	1568	230	24	

ASPHALTIC SURFACE ITEMS

STATION - STATION	LOCATION	455.0605 TACK COAT GAL	465.0105 ASPHALTIC SURFACE TON	REMARKS
103+55 - 105+21	LT	30	203	NW QUADRANT
105+12 - 108+05	LT	2	12	MULTI-USE PATH
104+92 - 108+05	LT	5	36	CTH O MEDIAN
105+17 - 107+27	RT	27	185	SW QUADRANT
ITEM TOTAL		64	436	

CONCRETE CURB & GUTTER

STATION - STATION	LOCATION	601.0557		REMARKS
		601.0411 30-INCH TYPE D LF	6-INCH SLOPED 36-INCH TYPE D LF	
104+00 - 104+92	LT	-	166	NW QUADRANT
104+75 - 105+17	LT	161	-	NW MEDIAN ISLAND
104+97 - 108+05	LT	-	422	CTH O MEDIAN
105+22 - 105+41	RT	-	115	SW QUADRANT
105+41 - 106+00	RT	134	-	SW MEDIAN ISLAND
ITEM TOTAL		295	703	

DRAINAGE

CTH O STA	LOCATION	DECKER DR		RIM EL	INVERT EL	608.0215 STORM SEWER PIPE REINFORCE CONCRETE CLASS II 15-INCH			REMARKS
		STA	LOCATION			LF	611.0627 INLET COVERS TYPE HM EACH	611.1230 CATCH BASINS 2X3-FT EACH	
105+42	RT	13+68 'D'	22.4' RT	1141.99'	1137.97'	-	1	1	CB-1
105+43	RT	14+01 'D'	24.2' RT	1141.83'	1137.64'	-	1	1	CB-2
104+88	LT	16+26 'D'	30.7' LT	1141.70'	1138.43'	-	1	1	CB-3
105+42 - 105+43	RT	13+68 'D' - 14+01 'D'	RT			33	-	-	CB1 TO CB-2
105+28 - 105+43	RT	14+00 'D' - 14+01 'D'	RT			15	-	-	MH-1 TO CB-2
104+88 - 105+31	LT	16+26 'D' - 16+19 'D'	LT			44	-	-	MH-2 TO CB-3
ITEM TOTALS						92	3	3	

CONCRETE ITEMS

STATION - STATION	LOCATION	602.0505		620.0300 CONCRETE MEDIAN SLOPED NOSE SF	REMARKS
		602.0415 CONCRETE SIDEWALK 6-INCH SF	CURB RAMP DETECTABLE WARNING FIELD YELLOW SF		
104+63 - 104+81	LT	161	20	-	NW CURB RAMP
104+75 - 105+16	LT	693	40	42	NW MEDIAN ISLAND
104+98 - 105+12	LT	157	-	-	CTH O MEDIAN
105+23 - 105+41	RT	566	-	34	SW MEDIAN ISLAND
ITEM TOTAL		1577	60	76	

EROSION CONTROL ITEMS

STATION	LOCATION	628.1504 SILT FENCE LF		628.2008 EROSION MAT URBAN CLASS I TYPE B SY	628.7005 INLET PROTECTION TYPE A EACH	628.7015 INLET PROTECTION TYPE C EACH	REMARKS
		628.1520 SILT FENCE MAINTENANCE LF	628.1504 SILT FENCE LF				
102+97 - 104+94	LT	310	310	-	-	-	NW QUADRANT
103+15 - 104+89	LT	-	-	132	-	-	NW QUADRANT
104+88	LT	-	-	-	1	1	NW QUADRANT
105+11 - 108+05	LT	-	-	446	-	-	CTH O MEDIAN
105+41 - 107+43	RT	257	257	136	-	-	SE QUADRANT
105+42	RT	-	-	-	1	1	SE QUADRANT
105+48	RT	-	-	-	-	1	SE QUADRANT
ITEM TOTALS		567	567	714	2	3	

FINISHING ITEMS

STATION - STATION	LOCATION	625.0100	625.0500	627.0200	629.0210	630.0130	630.0500	REMARKS
		TOPSOIL SY	SALVAGED TOPSOIL SY	MULCHING SY	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 30 LB	SEED WATER MGAL	
103+17 - 104+92	LT	176	-	45	11	1.6	4	NW QUADRANT
104+75 - 105+16	LT	-	37	-	-	-	-	NW MEDIAN ISLAND
105+12 - 108+05	LT	446	349	-	28	4.0	10	CTH O MEDIAN
105+42 - 107+26	RT	136	-	-	9	1.2	3	SW QUADRANT
105+23 - 105+41	RT	-	31	-	-	-	-	SW MEDIAN ISLAND
UNDISTRIBUTED		76	-	5	12	1.7	2	
ITEM TOTALS		834	417	50	60	8.5	19	

MOVING SIGNS

SIGN NO.	FROM		TO		SIGN CODE	SIGN TYPE II	SIGN MOVING SMALL SIGN SUPPORTS	SIGN MOVING SMALL SIGN SUPPORTS	SIGN MOUNTED ON SAME POST AS
	STATION	OFFSET	STATION	OFFSET					
3	103+60	64.1 LT	103+03	60.3 LT	R3-7R	1	1	1	-
4A	104+43	65.4 LT	103+49	61.57 LT	W2-6	1	1	1	3B, 3C
4B	104+43	65.4 LT	103+49	61.57 LT	W2-6P	1	-	-	3A, 3C
4C	104+43	65.4 LT	103+49	61.57 LT	W13-1	1	-	-	3A, 3B
9	15+92 'D'	38.2 LT	15+97 'D'	55.13 LT	R1-1B	1	1	1	-
10	16+09 'D'	29.1 LT	16+20 'D'	59.52 LT	R5-3	1	1	1	-
11A	16+15 'D'	27.0 LT	16+28 'D'	36.29 LT	D1-1	1	1	1	11B, 11C, 11D
11B	16+15 'D'	27.0 LT	16+28 'D'	36.29 LT	D1-1	1	-	-	11A, 11C, 11D
11C	16+15 'D'	27.0 LT	16+28 'D'	36.29 LT	R1-1	1	-	-	11A, 11B, 11D
12A	16+33 'D'	25.2 LT	16+54 'D'	30.57 LT	D11-1	1	1	1	12B
12B	16+33 'D'	25.2 LT	16+54 'D'	30.57 LT	M6-4	1	-	-	12A
17A	14+37 'D'	37.9 RT	14+36 'D'	47.14 RT	D1-1	1	1	1	17B, 17C, 17D
17B	14+37 'D'	37.9 RT	14+36 'D'	47.14 RT	D1-1	1	-	-	17A, 17C, 17D
17C	14+37 'D'	37.9 RT	14+36 'D'	47.14 RT	R1-1	1	-	-	17A, 17B, 17D
ITEM TOTAL						14	7		

PERMANENT SIGNS

SIGN NO.	STATION	OFFSET	LOCATION	SIGN CODE	SIGN (WXH) IN X IN*	637.2210	634.0616	SIGN MOUNTED ON SAME POST AS
						SIGN TYPE II REFLECTIVE H SF	POSTS WOOD 4X6-INCH 16-FT EACH	
1	101+42	9.64 LT	CTH O WEST MEDIAN	R3-20L	24X36	6	1	-
2	101+36	39.17 RT	CTH O SW CORNER	R3-20R	24X36	6	1	-
5	104+35	22.14 LT	CTH O WEST MEDIAN	R3-2	36X36	9	1	-
6	105+32	9.66 LT	CTH O EAST MEDIAN	R3-18	36X36	9	1	14
7	105+55	2.28 LT	CTH O EAST MEDIAN	R3-2	36x36	9	1	-
8	108+08	63.54 LT	CTH O NE CORNER	R3-20R	24X36	6	1	-
11D	16+28 'D'	36.29 LT	DECKER DR. NW CORNER	R3-50R	30X36	7.5	-	11A, 11B, 11C
13	16+11 'D'	4.18 LT	DECKER DR. NORTH MEDIAN	R4-7	24X30	5	1	-
14	105+32	9.66 LT	CTH O EAST MEDIAN	R4-7	24X30	5	-	6
15	14+18 'D'	5.08 RT	DECKER DR. SOUTH MEDIAN	R4-7	24X30	5	1	-
16D	14+22 'D'	5.58 RT	DECKER DR. SE CORNER	R3-50R	30X36	7.5	-	16A, 16B, 16C
ITEM TOTAL						75	8	

TRAFFIC CONTROL

LOCATION	643.0300 DRUMS		643.0420 BARRICADES TYPE III		643.3120 TEMPORARY MARKING LINE EPOXY 4-INCH		643.0900 SIGNS		643.1050 SIGNS PCMS		643.5000 TRAFFIC CONTROL		643.0800 TRAFFIC CONTROL ARROW BOARD		644.1440 TEMPORARY PEDESTRIAN SURFACE SKID-RESISTANT MATTING		644.1601 TEMPORARY PEDESTRIAN CURB RAMP		644.1605 TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD		644.1810 TEMPORARY PEDESTRIAN BARRICADE		REMARKS
	EACH	DAYS	EACH	DAYS	LF	EACH	DAYS	EACH	DAYS	EACH	EACH	DAYS	EACH	DAYS	SF	EACH	DAYS	SF	LF	LF			
PROJECT 6085-02-76	-	-	-	-	-	13	585	4	28	1	-	-	-	-	-	-	-	-	-	-	-	INCLUDES REDUCED WIDTH SIGNING	
STAGE 1	149	4172	4	112	-	38	1064	-	-	-	1	28	500	2	56	40	40						
STAGE 2	81	1296	-	-	485	8	128	-	-	-	1	16	-	-	-	-	-	-	-	-			
ITEM TOTALS	230	5468	4	112	485	59	1777	4	28	1	2	44	500	2	56	40	40						

CONSTRUCTION DURATION OF APPROXIMATELY 35 WORKING DAYS (45 TOTAL DAYS INCLUDING WEEKENDS)

STAGE 1 - APPROXIMATELY 28 TOTAL DAYS, INCLUDING WEEKENDS

STAGE 2 - APPROXIMATELY 16 TOTAL DAYS, INCLUDING WEEKENDS

PAVEMENT MARKING

STATION	LOCATION	646.1020 MARKING LINE EPOXY 4-INCH		646.5020 MARKING ARROW		646.5120 MARKING WORD		646.6120 MARKING STOP LINE EPOXY 18-INCH		646.7420 MARKING CROSSWALK TRANSVERSE LINE EPOXY 6-INCH		646.7020 MARKING DIAGONAL EPOXY 6-INCH		646.8120 MARKING CURB EPOXY		646.8220 MARKING ISLAND NOSE EPOXY		REMARKS	
		YELLOW LF	WHITE LF	EPOXY EACH	WORD EACH	WHITE LF	WHITE LF	WHITE LF	YELLOW LF	YELLOW EACH									
103+06 - 104+00	LT	-	96	-	-	-	-	-	-	-	-	-	-	-	-	-	-	CTH O WB SHOULDER	
105+02	LT	-	-	-	-	-	-	-	-	-	-	-	31	-	-	-	-	CTH O MEDIAN	
106+00 - 107+27	RT	-	128	-	-	-	-	-	-	-	-	-	-	-	-	-	-	CTH O EB SHOULDER	
13+90 'D'	RT	-	-	-	1	-	-	-	-	-	-	-	-	-	-	-	-	SOUTH DECKER DRIVE	
14+08 - 13+74 'D'	RT	59	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	SOUTH DECKER DRIVE	
14+13 'D'	RT	-	-	-	-	-	-	-	-	-	-	-	-	-	-	1	-	SE MEDIAN ISLAND	
14+16 'D'	RT	-	-	-	-	-	-	-	-	-	-	-	11	-	-	-	-	SE MEDIAN ISLAND	
14+30 'D'	RT	-	-	1	-	-	-	-	-	-	-	-	-	-	-	-	-	SOUTH DECKER DRIVE	
14+57 'D'	RT	-	-	-	-	-	29	-	-	-	-	-	-	-	-	-	-	SOUTH DECKER DRIVE	
14+66 'D'	RT	-	-	-	-	-	-	-	-	-	-	-	35	-	-	-	-	SE MEDIAN ISLAND	
15+56 'D'	LT	-	-	-	-	-	-	-	-	-	-	-	32	-	-	-	-	NW MEDIAN ISLAND	
15+97 'D'	LT	-	-	-	-	-	-	-	53	90	-	-	-	-	-	-	-	NORTH DECKER DRIVE	
16+01 'D'	LT	-	-	-	-	-	-	-	50	85	-	-	-	-	-	-	-	NORTH DECKER DRIVE	
16+10 'D'	LT	-	-	-	-	-	-	-	-	-	-	-	11	-	-	-	-	NW MEDIAN ISLAND	
16+12 'D'	LT	-	-	-	-	-	-	-	-	-	-	-	-	-	-	1	-	NW MEDIAN ISLAND	
16+14 'D'	LT	-	-	-	-	-	-	23	-	-	-	-	-	-	-	-	-	NORTH DECKER DRIVE	
16+17 - 16+78 'D'	LT	120	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	NORTH DECKER DRIVE	
16+41 'D'	LT	-	-	1	-	-	-	-	-	-	-	-	-	-	-	-	-	NORTH DECKER DRIVE	
16+88 'D'	LT	-	-	-	1	-	-	-	-	-	-	-	-	-	-	-	-	NORTH DECKER DRIVE	
ITEM TOTALS		179	224	2	2	52	103	175	120	2									
		TOTAL				403													

LIGHTING

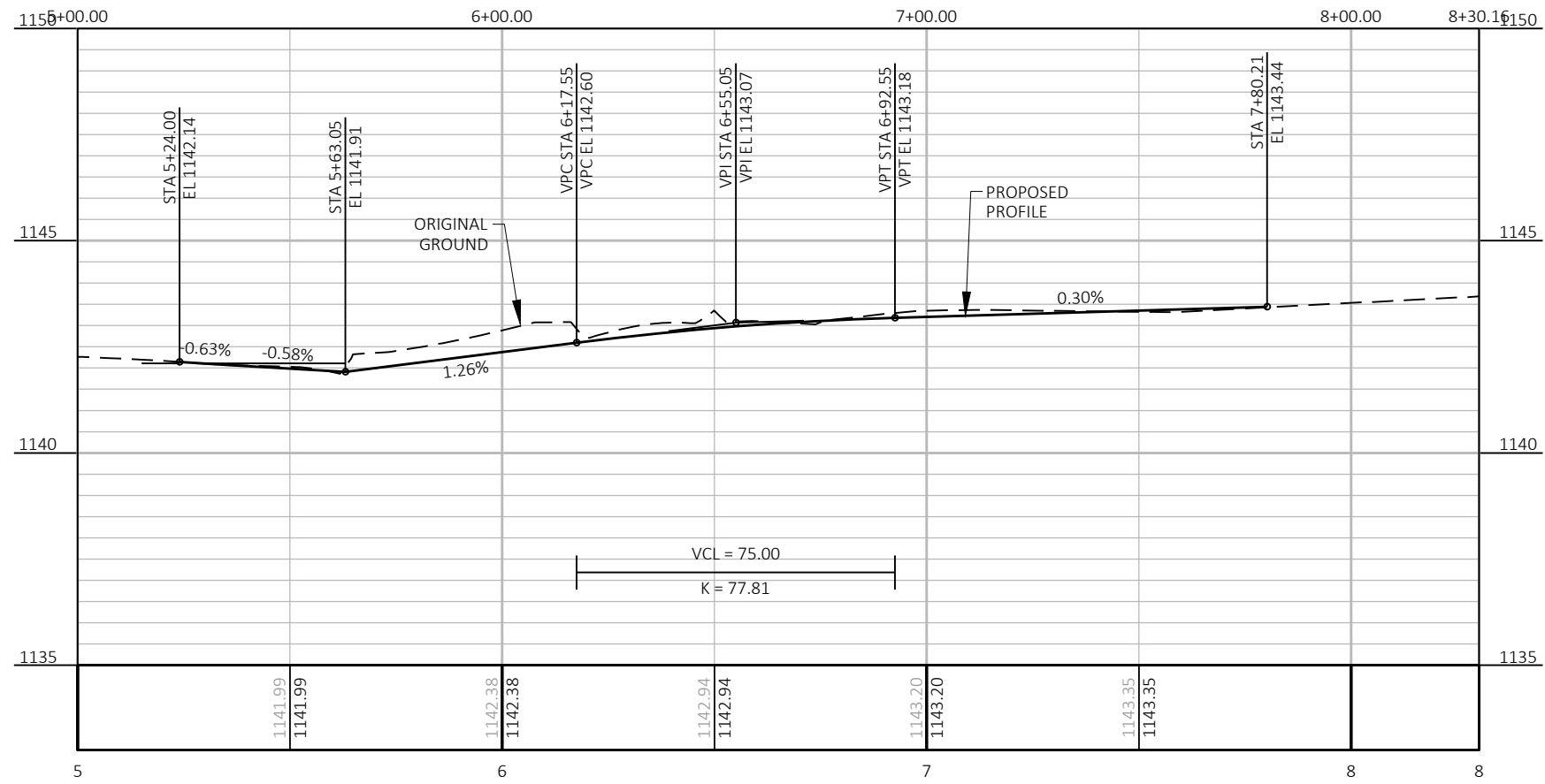
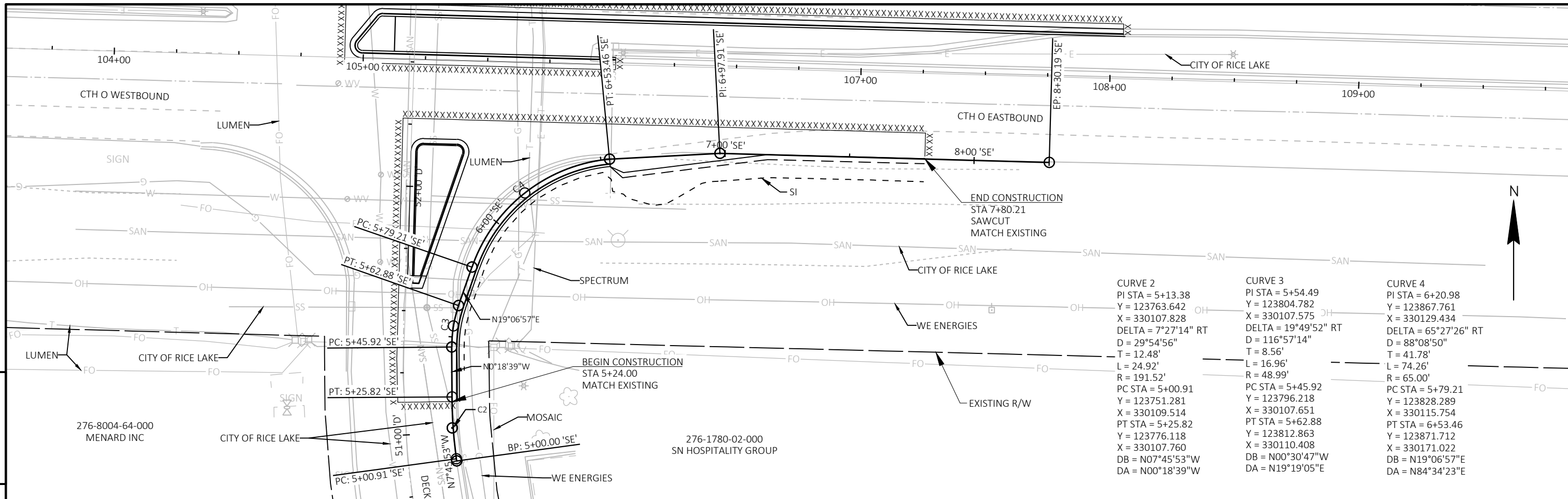
STATION	LOCATION	654.0102 CONCRETE BASE TYPE 2 EACH	655.0640 ELECTRICAL WIRE LIGHTING 1 AWG LF	657.0100 PEDESTAL BASES EACH	SPV.0105.01 RELOCATE STREET LIGHT LS	REMARKS
106+05	9.8' LT	1	10	1	1	LIGHT RELOCATION
ITEM TOTAL		1	10	1	1	

SAWING

STATION	LOCATION	690.0150 ASPHALT LF	690.0250 CONCRETE LF	REMARKS
103+55 - 105+20	LT	350	3	NW QUADRANT
104+30	LT	10	-	MULTI-USE PATH
104+93 - 108+05	LT	456	6	CTH O MEDIAN
105+17 - 107+27	RT	345	3	SW QUADRANT
ITEM TOTAL		1161	12	

CONSTRUCTION STAKING

STATION - STATION	LOCATION	650.4000 STORM SEWER EACH	650.4500 SUBGRADE LF	650.5000 BASE LF	650.5500 CURB AND GUTTER LF	650.9000 CURB RAMPS EACH	650.9500 SIDEWALK 8829-00-71 EACH	650.9911 SUPPLEMENTAL CONTROL 8829-00-71 EACH	650.9920 SLOPE STAKES LF	REMARKS
104+88	LT	1	-	-	-	-	-	-	-	CB-3
105+43	RT	1	-	-	-	-	-	-	-	CB-2
105+48	RT	1	-	-	-	-	-	-	-	CB-1
103+55 - 105+21	LT	-	290	290	166	-	-	-	175	NW QUADRANT
104+75 - 105+17	LT	-	-	-	161	1	-	-	-	NW MEDIAN ISLAND
105+12 - 108+05	LT	-	55	55	-	1	1	-	-	MULTI-USE PATH
104+92 - 108+05	LT	-	312	312	422	-	-	-	-	CTH O MEDIAN
105+17 - 107+27	RT	-	315	315	115	-	-	-	150	SW QUADRANT
105+41 - 106+00	RT	-	-	-	134	-	-	-	-	SW MEDIAN ISLAND
PROJECT 8829-00-71	-	-	-	-	-	-	-	1	-	
ITEM TOTALS		3	972	972	998	2	1	1	325	

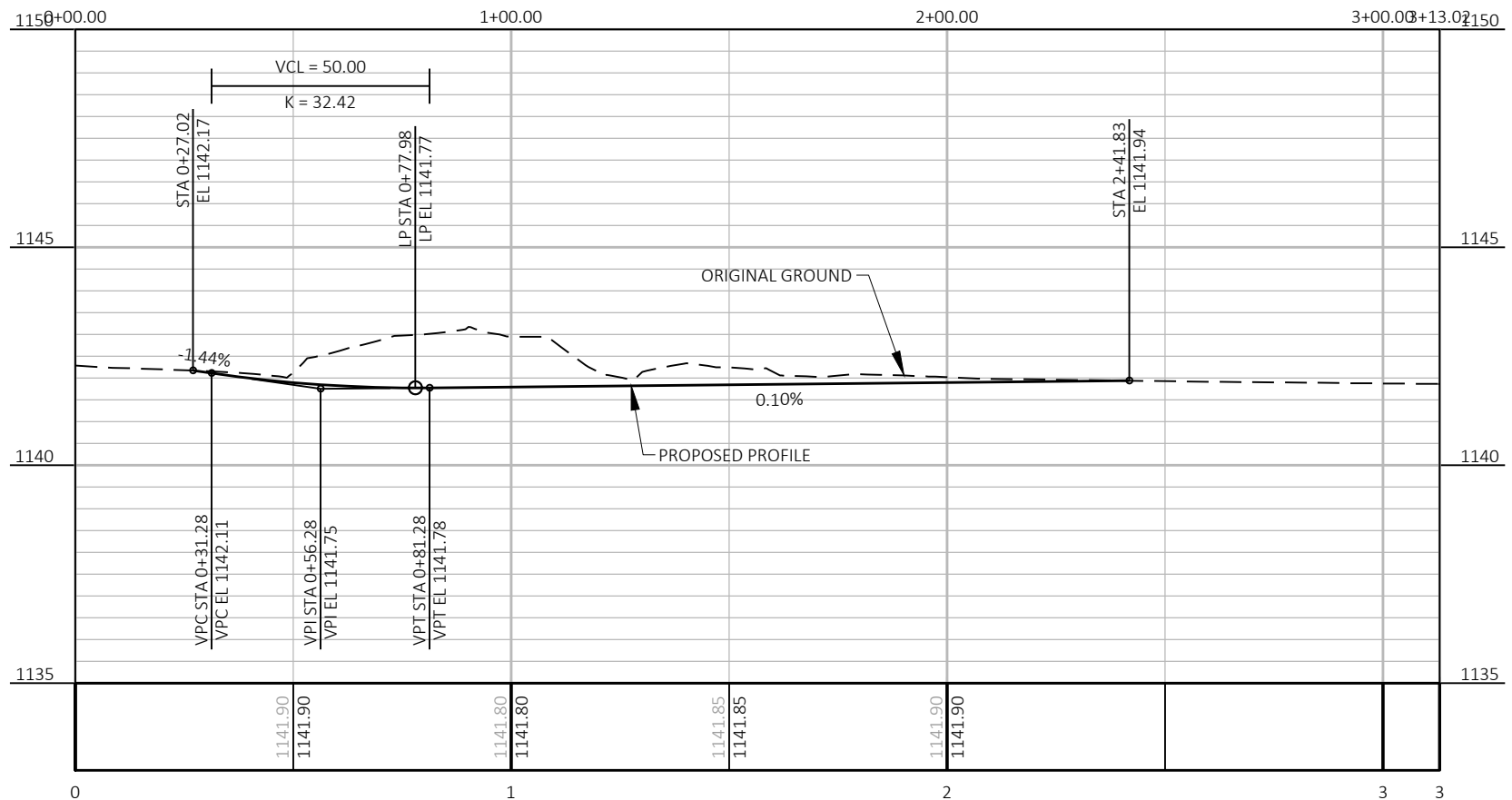
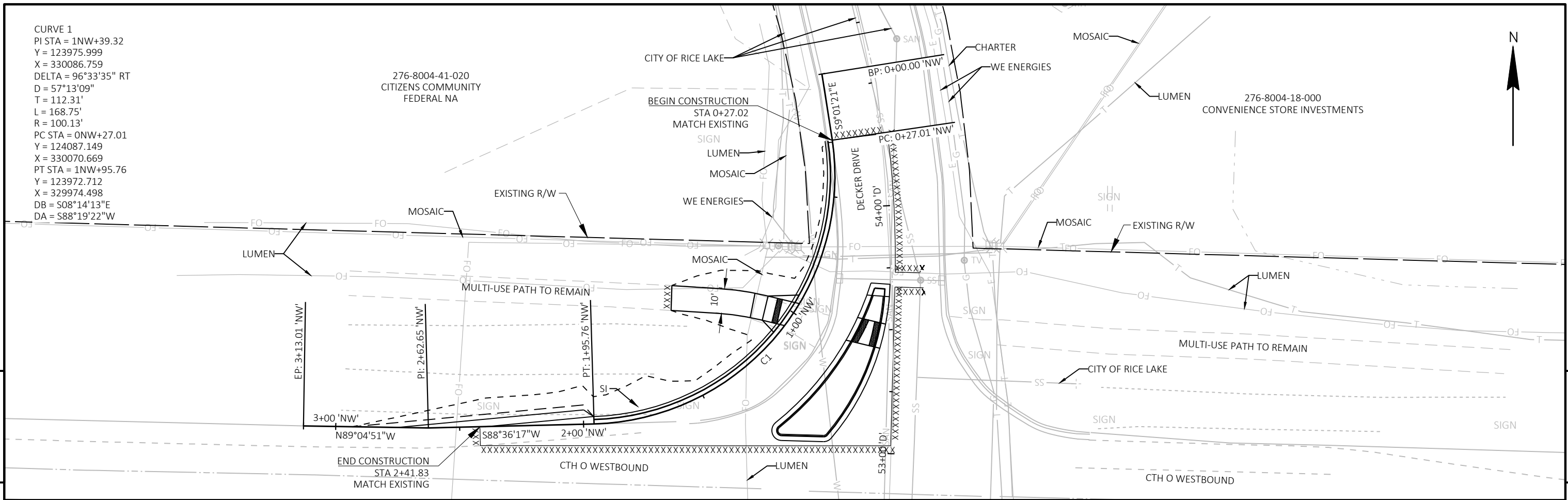


PROJECT NO: 8829-00-71 HWY: CTH O COUNTY: BARRON PLAN AND PROFILE: SE CURB RETURN SHEET: E

CURVE 1
 PI STA = 1NW+39.32
 Y = 123975.999
 X = 330086.759
 DELTA = 96°33'35" RT
 D = 57°13'09"
 T = 112.31'
 L = 168.75'
 R = 100.13'
 PC STA = 0NW+27.01
 Y = 124087.149
 X = 330070.669
 PT STA = 1NW+95.76
 Y = 123972.712
 X = 329974.498
 DB = S08°14'13"E
 DA = S88°19'22"W

276-8004-41-020
 CITIZENS COMMUNITY
 FEDERAL NA

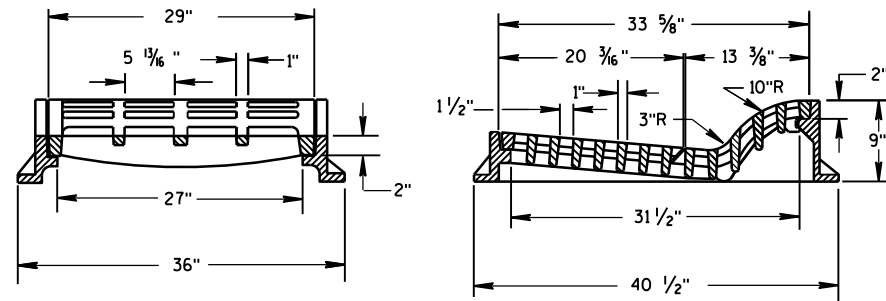
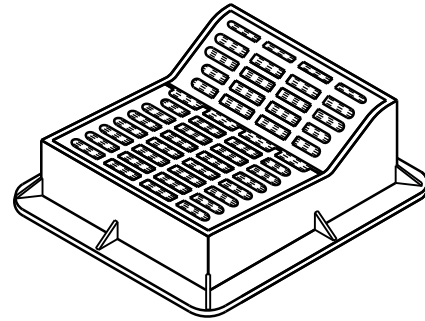
276-8004-18-000
 CONVENIENCE STORE INVESTMENTS



PROJECT NO: 8829-00-71	HWY: CTH O	COUNTY: BARRON	PLAN AND PROFILE: NW CURB RETURN	SHEET	E
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Standard Detail Drawing List

08A05-19C	INLET COVERS TYPE F, HM, HM-S, S, T, V, HM-GJ, & HM-GJ-S
08A09-02	CATCH BASINS 2X3-FT AND 2.5X3-FT
08D01-23A	CONCRETE CURB & GUTTER
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09A01-14A	AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE
09A01-14B	AT-GRADE SIDE ROAD INTERSECTION, TYPE "A1" & "A2"
09C02-09	CONCRETE BASES, TYPES 1, 2, 5, & 6
09C03-04	TRANSFORMER/PEDESTAL BASES
09E03-06	NON-FREEWAY LIGHTING UNIT POLE WIRING
11B02-02	CONCRETE MEDIAN NOSE
15C02-09F	ADVANCED WIDTH RESTRICTION SIGNING
15C07-15B	PAVEMENT MARKING WORDS
15C07-15C	PAVEMENT MARKING ARROWS
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-23C	PAVEMENT MARKING (TURN LANES)
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C18-08A	MEDIAN ISLAND MARKING PAVEMENT MARKINGS
15C18-08B	MEDIAN ISLAND MARKING MEDIAN ISLAND NOSE
15C18-08C	MEDIAN PAVEMENT MARKINGS DOUBLE ARROW WARNING SIGN PLACEMENT
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D20-07A	TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY
15D20-07B	TRAFFIC CONTROL, SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D21-07A	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D21-07B	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D46-01	TRAFFIC CONTROL, ONE - WAY SIGNING



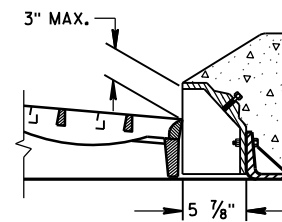
TYPE "F"

USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.

GENERAL NOTES

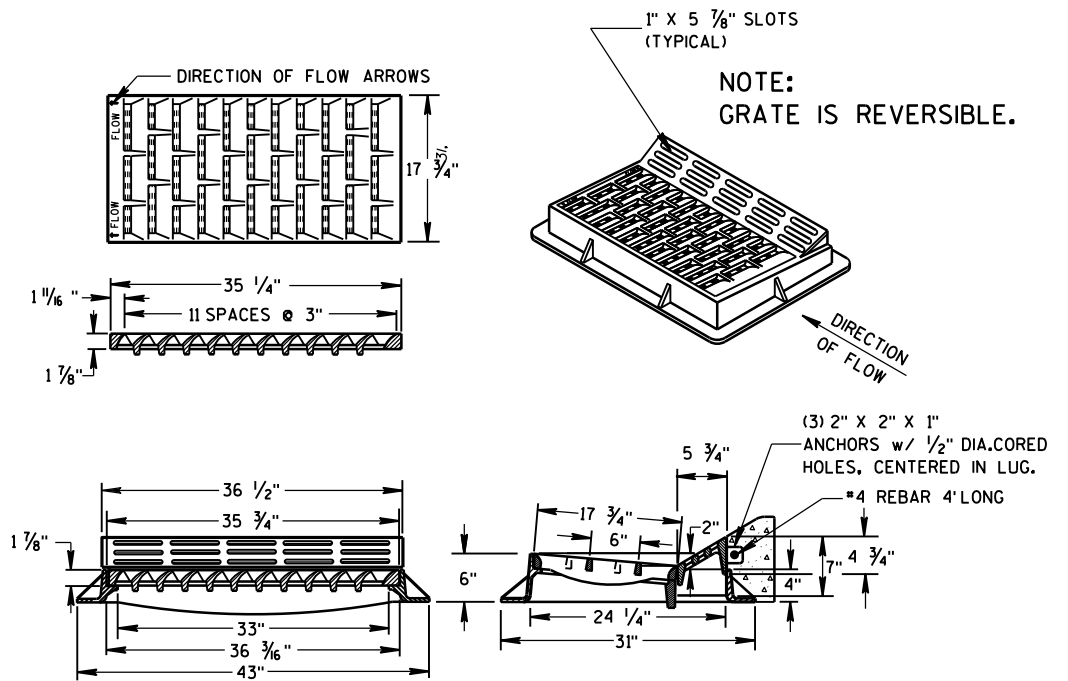
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.



ALTERNATIVE CURB BOX FOR TYPE "HM" COVER

USE WITH TYPES G & J CONCRETE CURB & GUTTER, 30 INCH NOTED AS TYPE HM-GJ ON DRAINAGE TABLE



TYPE "HM"

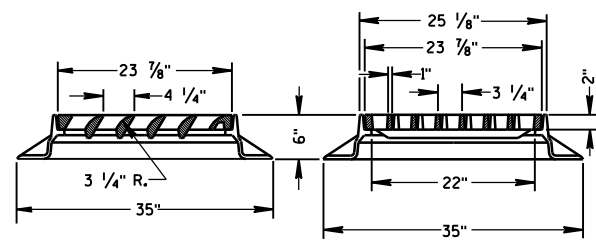
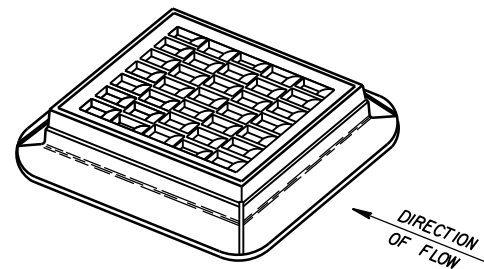
USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.

NOTE: SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM" COVER NOTED AS TYPE HM-S ON DRAINAGE TABLE

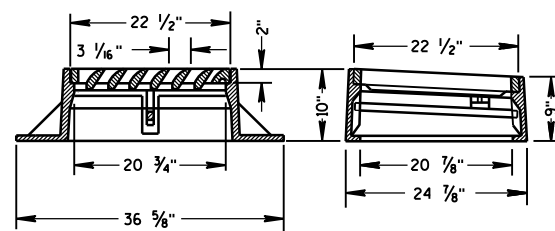
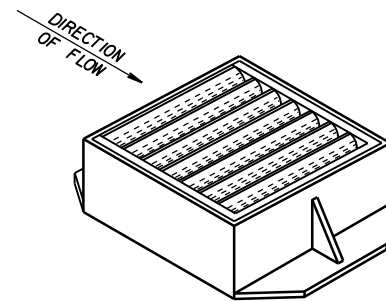
NOTE: SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM-GJ" COVER NOTED AS TYPE HM-GJ-S ON DRAINAGE TABLE

6

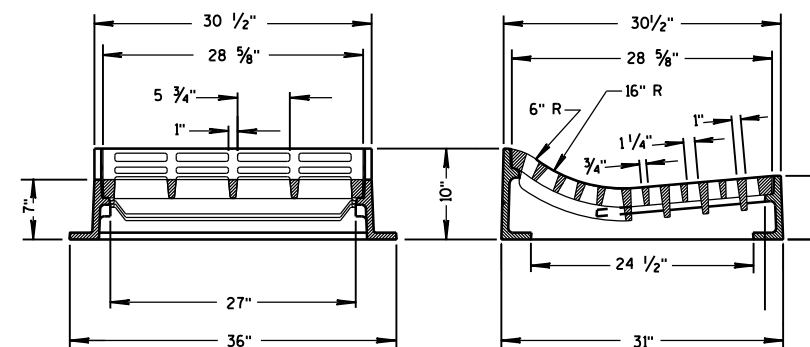
6



TYPE "S"

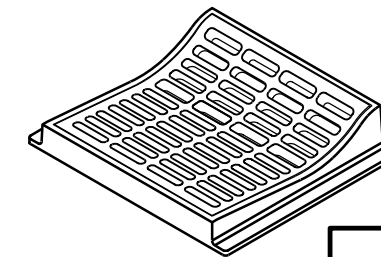


TYPE "V"



TYPE "T"

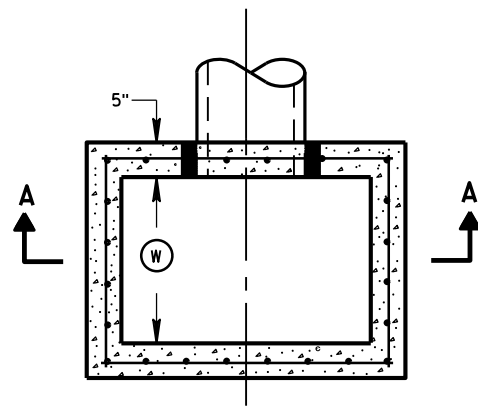
USE WITH TYPES R & T CONCRETE CURB & GUTTER, 36 INCH.



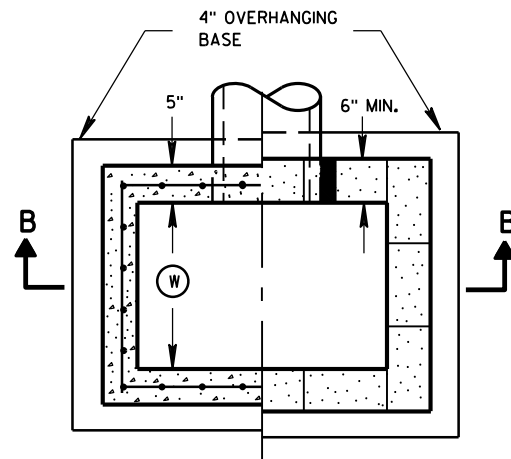
INLET COVERS
TYPE F, HM, HM-S, S, T, V,
HM-GJ, & HM-GJ-S

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

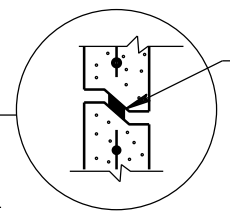
APPROVED
11/27/2013 DATE /s/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA



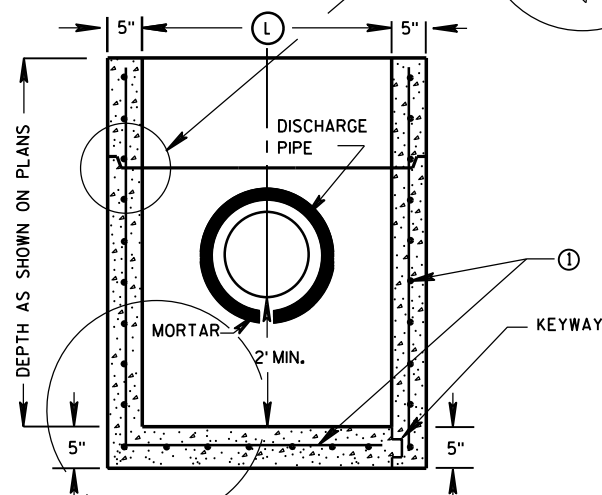
PLAN VIEW



PLAN VIEW

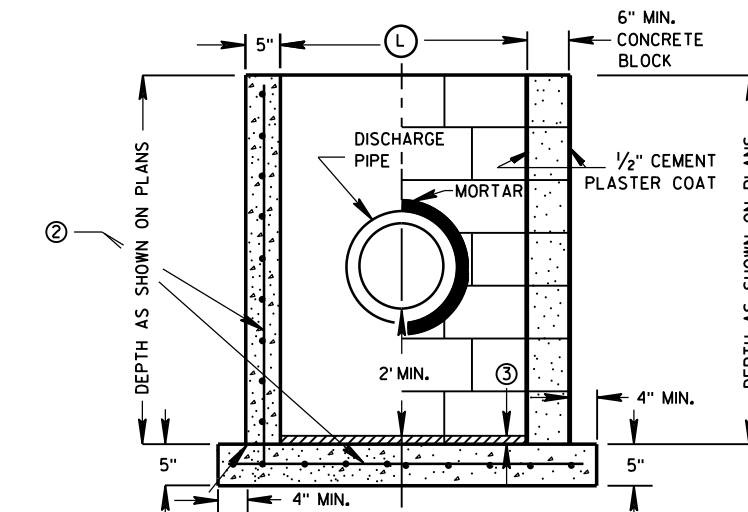


RISER JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

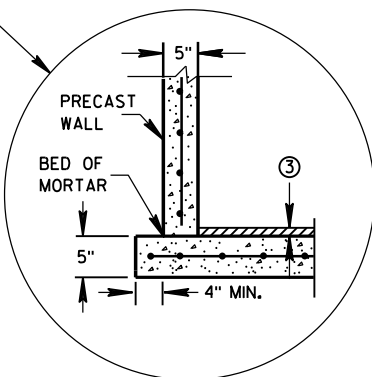
SECTION A-A



CAST-IN-PLACE REINFORCED CONCRETE

CONCRETE BLOCK ON CAST-IN-PLACE WITH PRECAST REINFORCED CONCRETE BASE ①

SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST CATCH BASIN UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

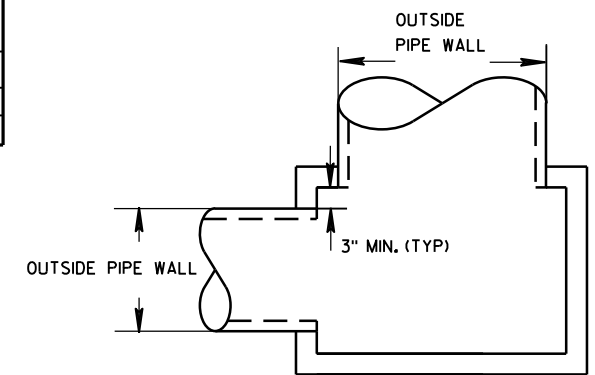
- ① FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.
- ③ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2" SUMP MEASURED FROM TOP OF KEY.

CATCH BASIN COVER MATRIX

CATCH BASIN SIZE	INLET COVER TYPE		F	ALL H'S
	WIDTH (W) (FT)	LENGTH (L) (FT)		
2X3-FT	2	3		X
2.5X3-FT	2.5	3	X	

PIPE MATRIX

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	WIDTH (IN)	LENGTH (IN)
2X3-FT	12	24
2.5X3-FT	18	24



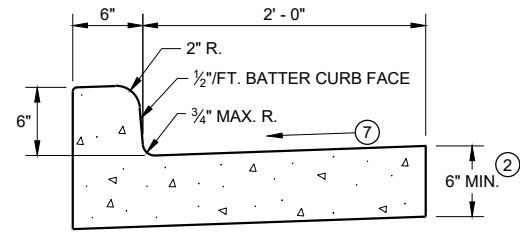
DETAIL "A"

CATCH BASINS 2X3-FT AND 2.5X3-FT

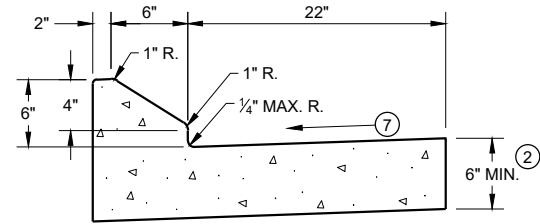
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: Sept., 2016 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

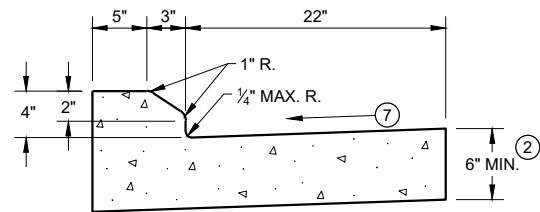
CATCH BASINS 2X3-FT AND 2.5X3-FT



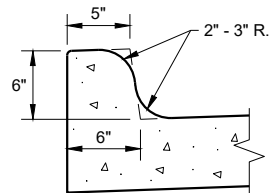
TYPES A¹ & D



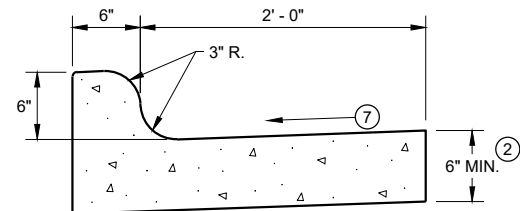
6" SLOPED CURB TYPES G¹ & J



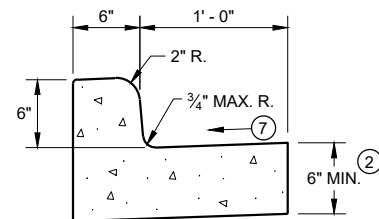
4" SLOPED CURB TYPES G¹ & J



TYPES K¹ & L
(OPTIONAL CURB SHAPE)

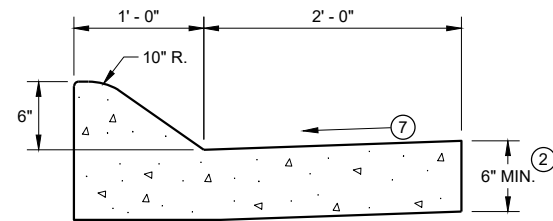


TYPES K¹ & L
CONCRETE CURB AND GUTTER 30"

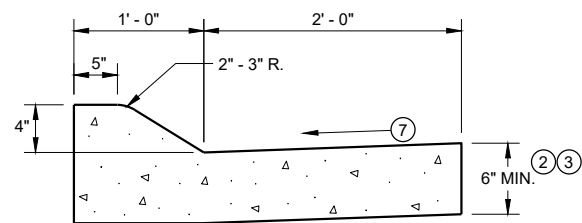


TYPES A¹ & D

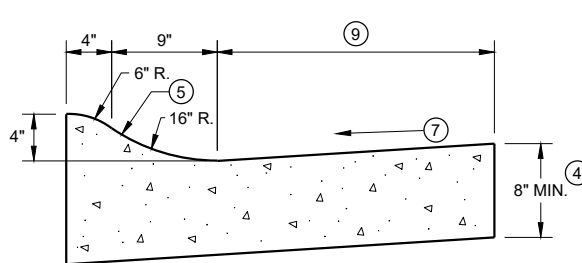
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A¹ & D

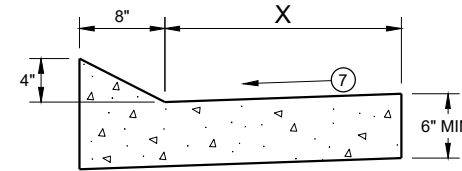


4" SLOPED CURB TYPES A¹ & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R¹ & T

TBT & TBTT	X
30"	22"
36"	28"

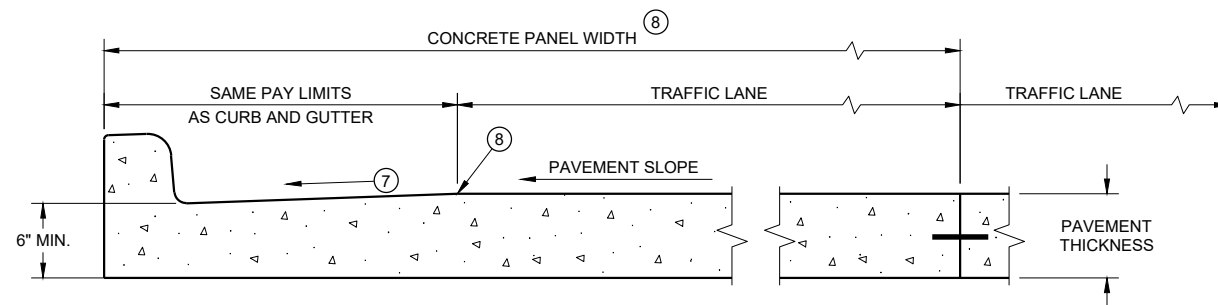


TYPES TBT & TBTT¹

CONCRETE CURB AND GUTTER

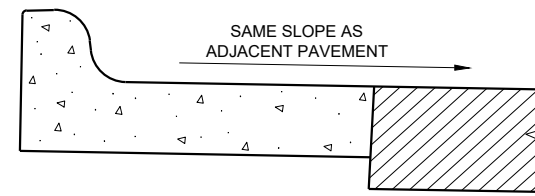
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT* WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁶
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

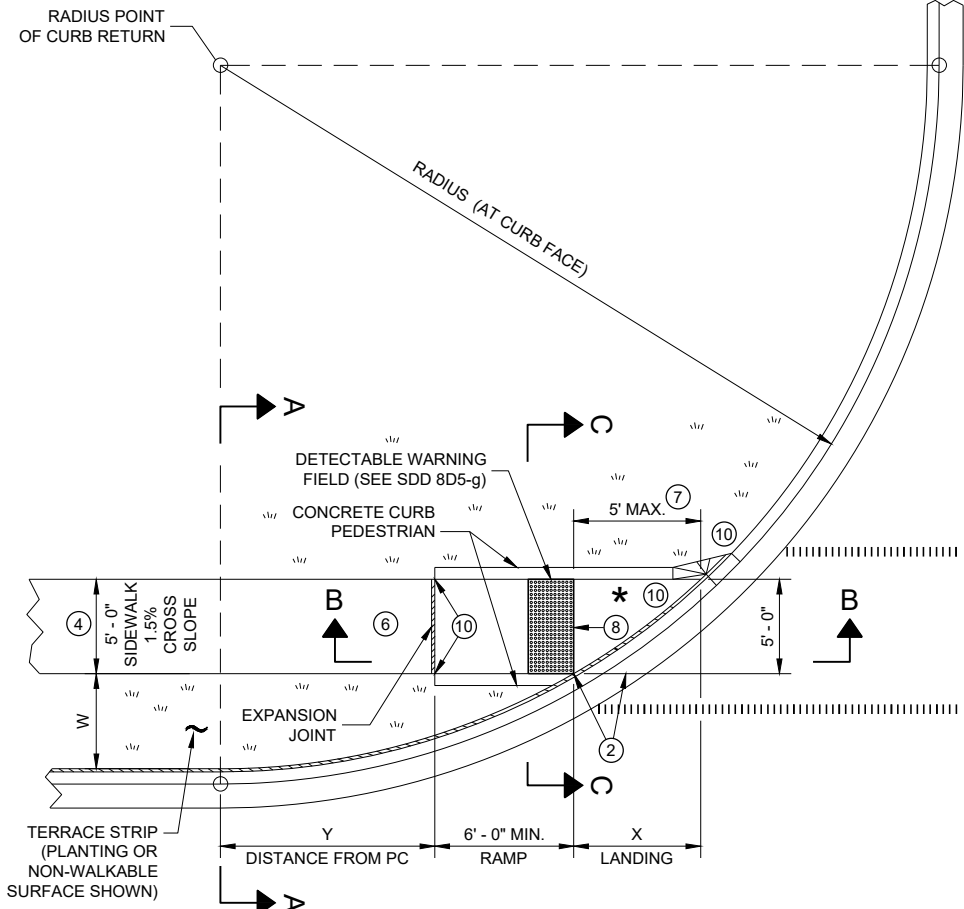
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



PLAN VIEW CURB RAMP TYPE 4B

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

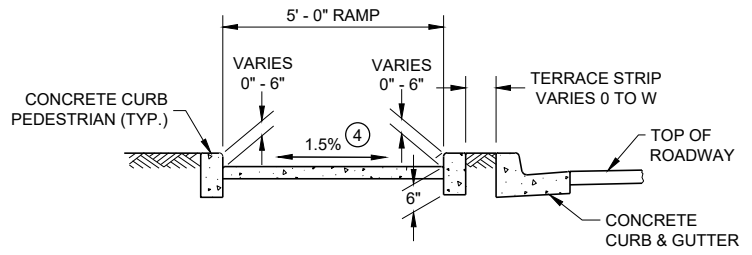
INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

LEGEND

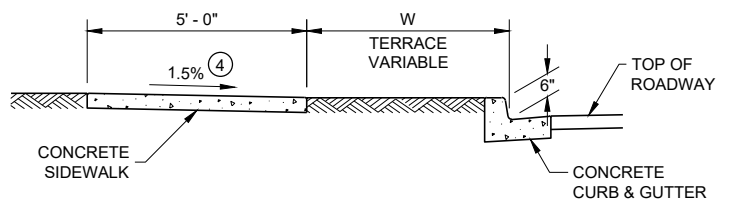
- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

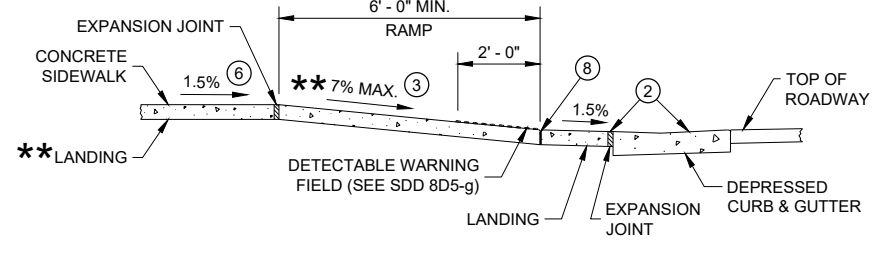


SECTION C - C FOR TYPE 4B



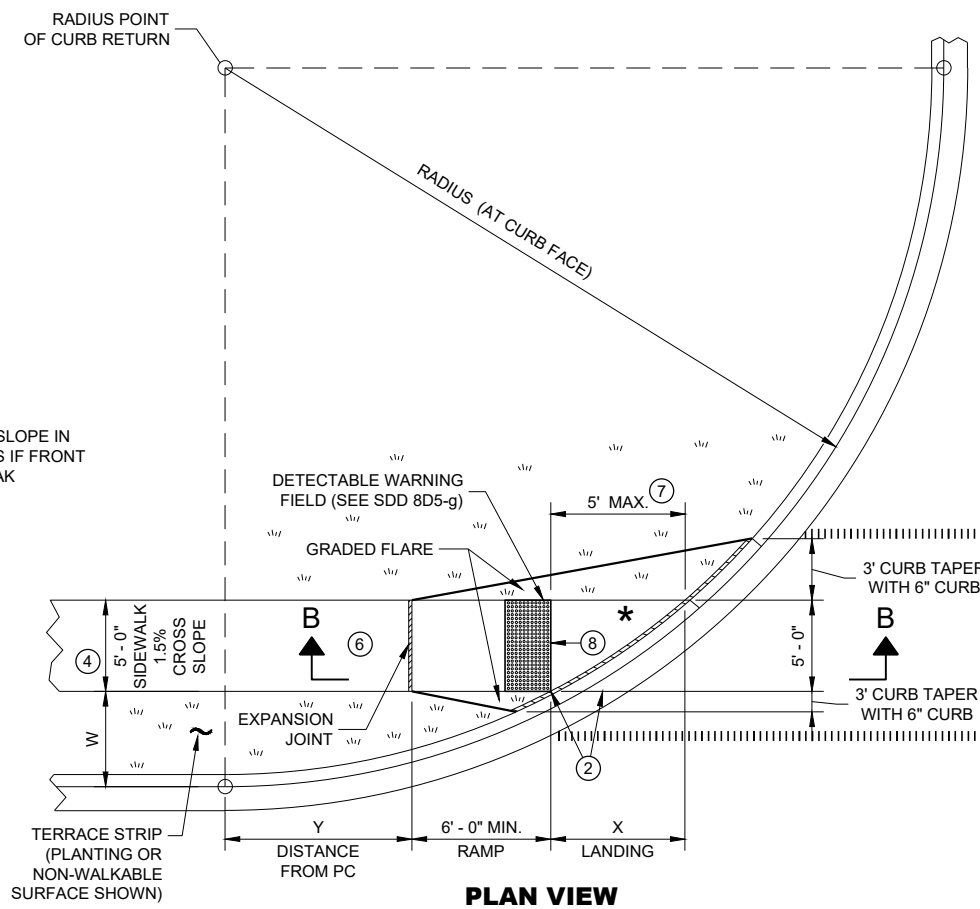
SECTION A - A FOR TYPE 4B

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

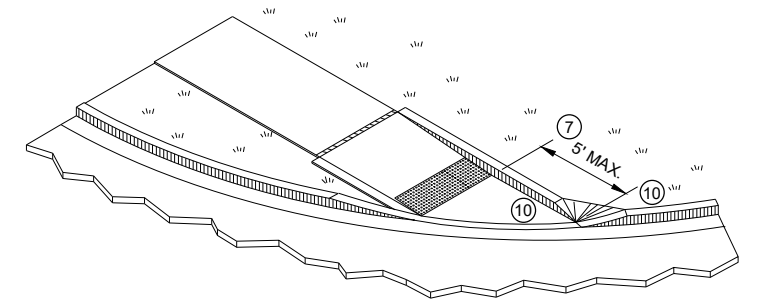


SECTION B - B FOR TYPE 4B AND TYPE 4B1

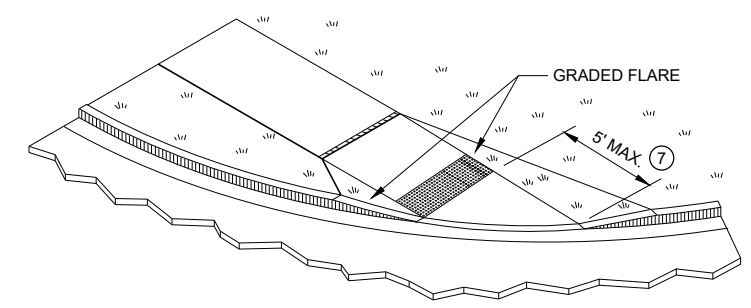
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



PLAN VIEW CURB RAMP TYPE 4B1



ISOMETRIC VIEW FOR TYPE 4B



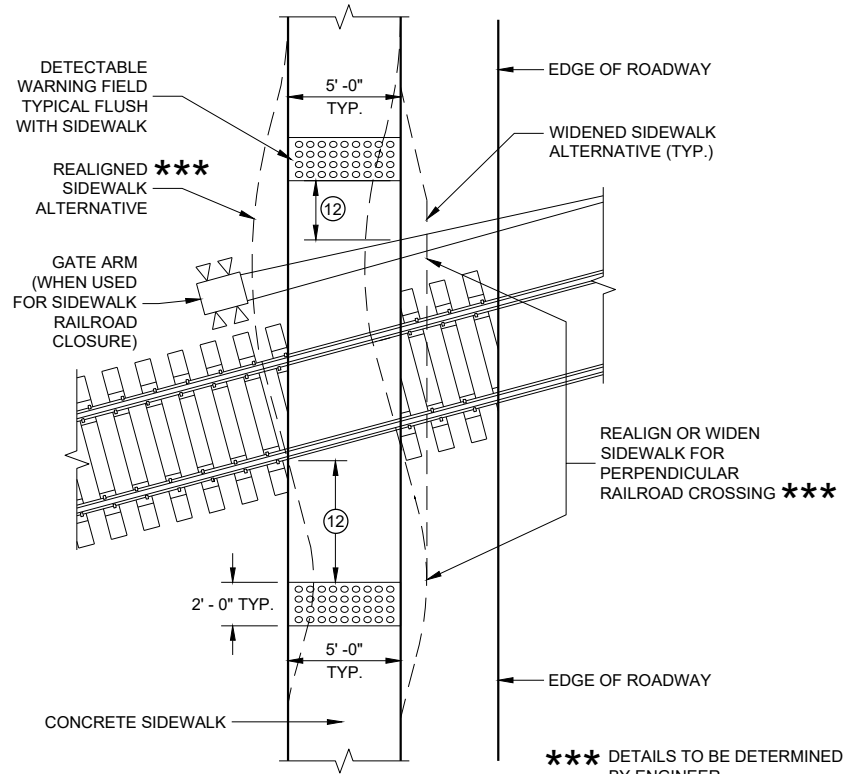
ISOMETRIC VIEW FOR TYPE 4B1

**CURB RAMPS
TYPE 4B AND 4B1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 08D05 - 20d

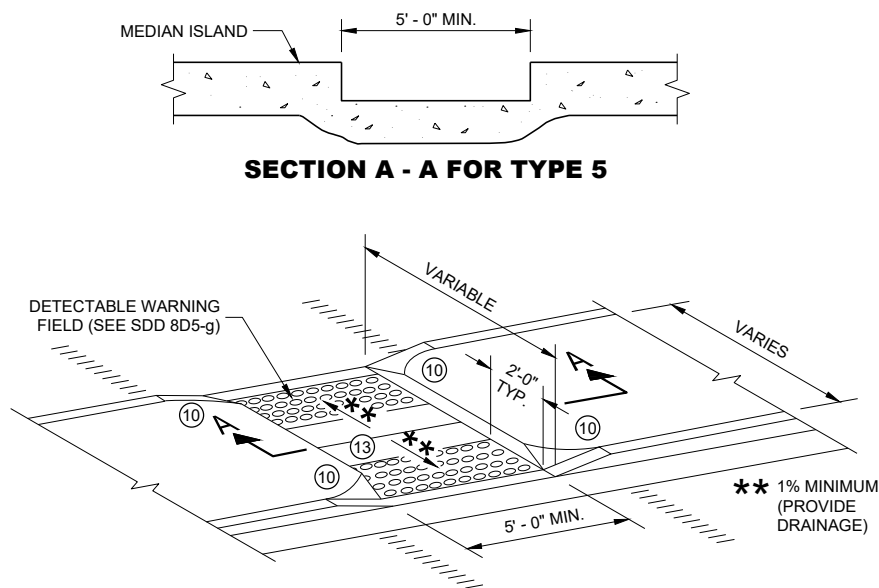
SDD 08D05 - 20d



CURB RAMP TYPE 8

DETECTABLE WARNINGS AT RAILROAD CROSSING

*** DETAILS TO BE DETERMINED BY ENGINEER



SECTION A - A FOR TYPE 5

CURB RAMP TYPE 5

**MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING**

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

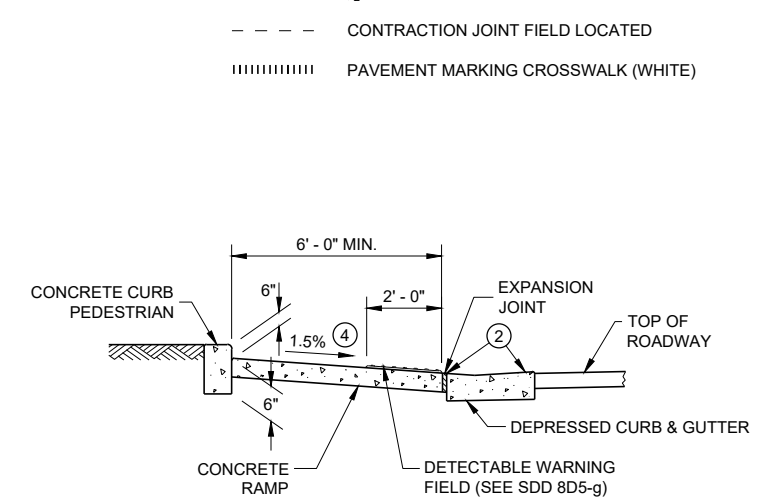
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

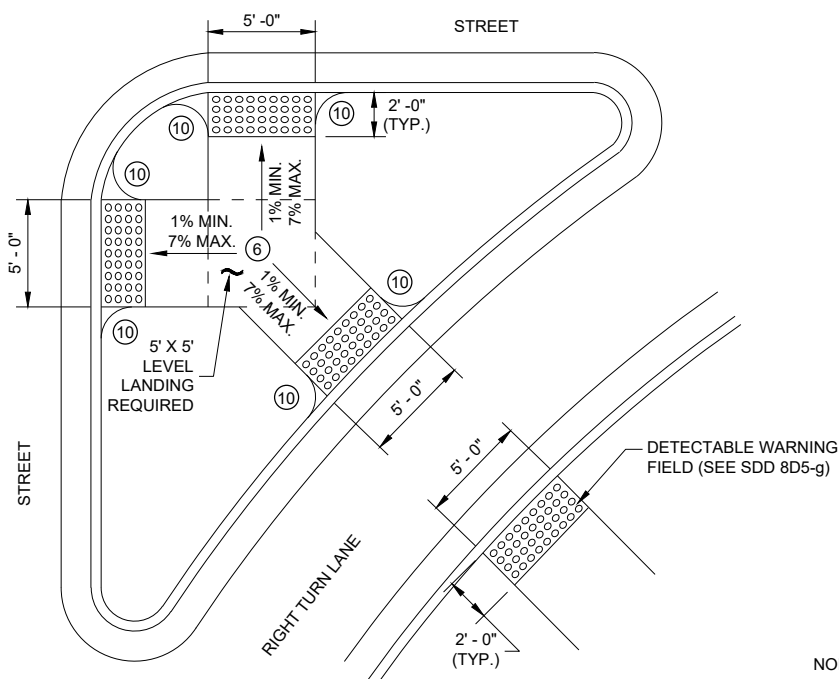
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)



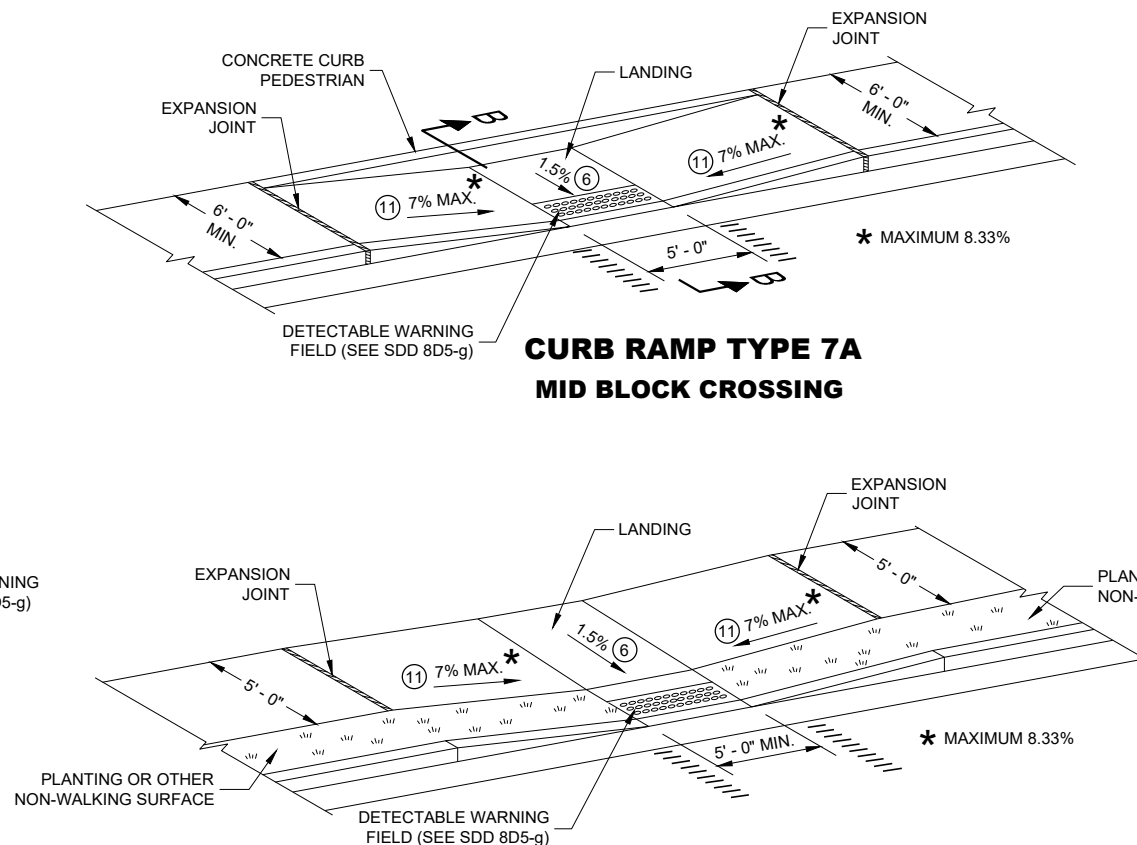
SECTION B - B FOR TYPE 7A



CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS



**CURB RAMP TYPE 7A
MID BLOCK CROSSING**

**CURB RAMP TYPE 7B
MID BLOCK CROSSING**

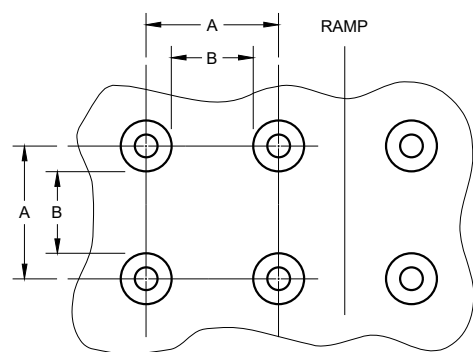
NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

**CURB RAMPS
TYPE 5, 6, 7A, 7B & 8**

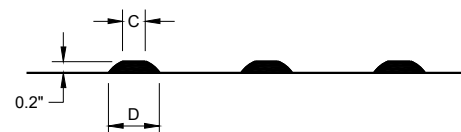
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

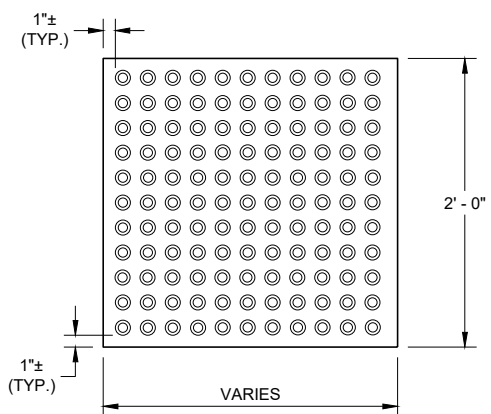


PLAN VIEW

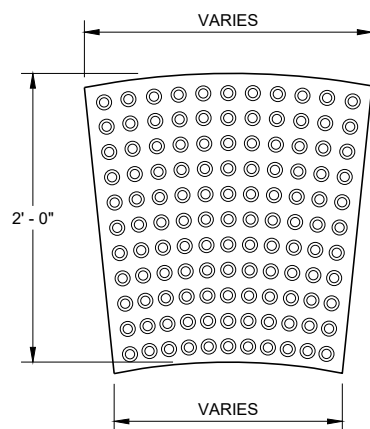


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

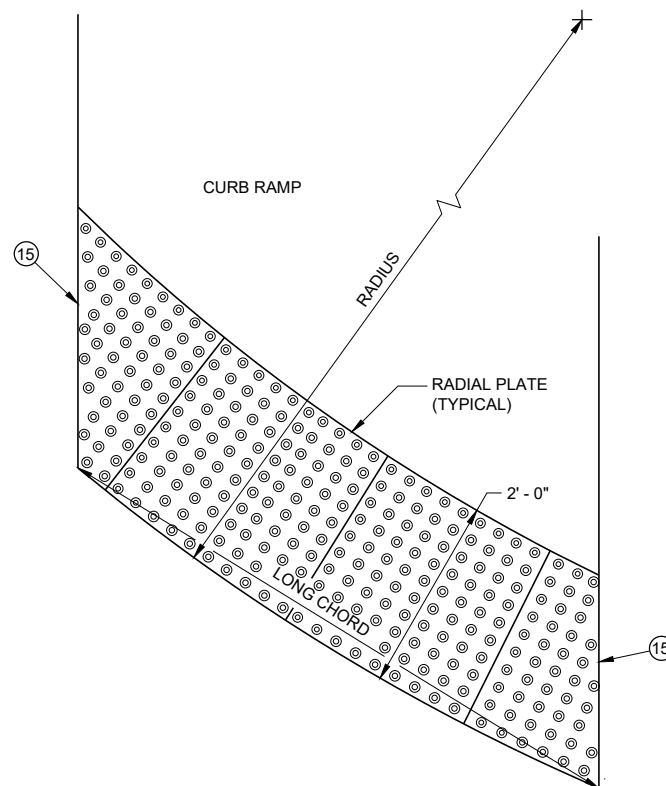


RECTANGULAR
PLATES

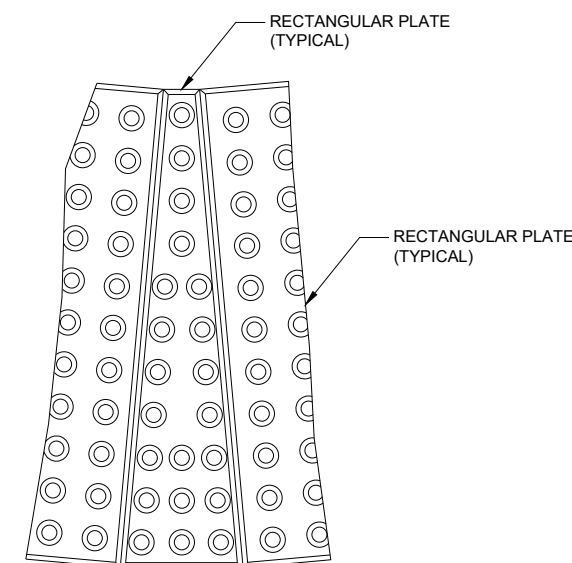


RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

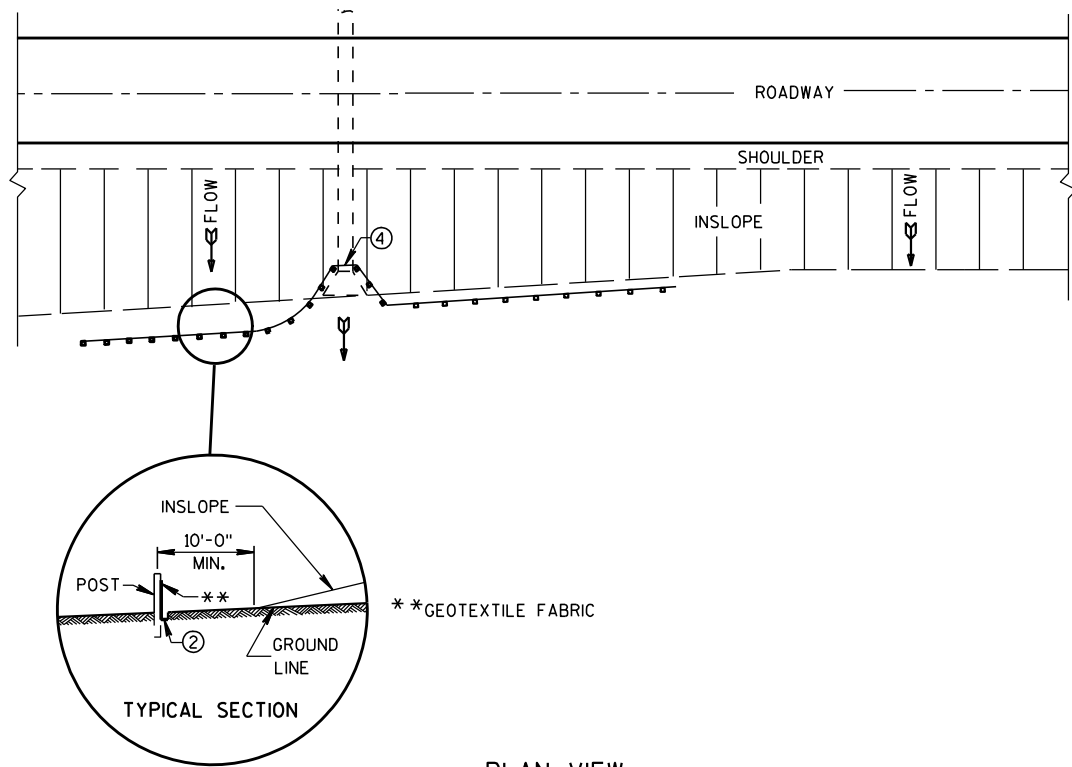
15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

**CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES**

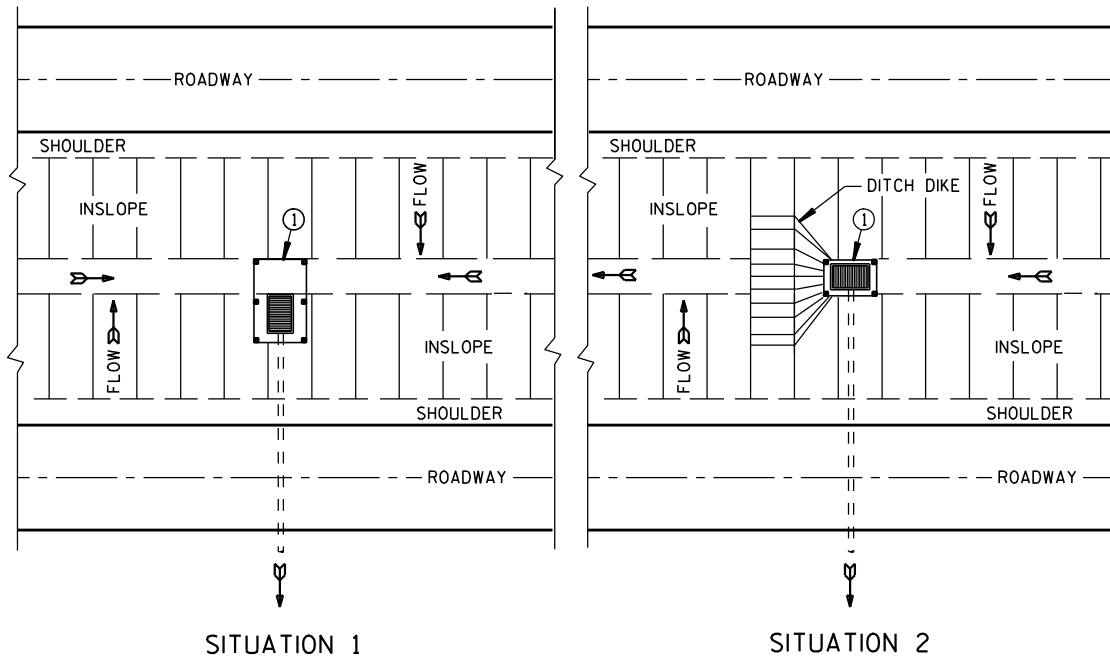
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

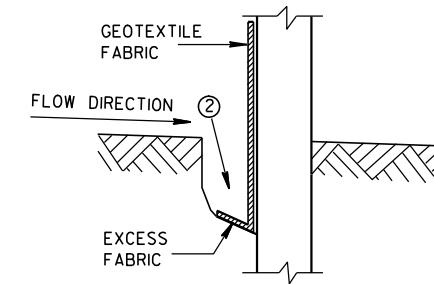


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

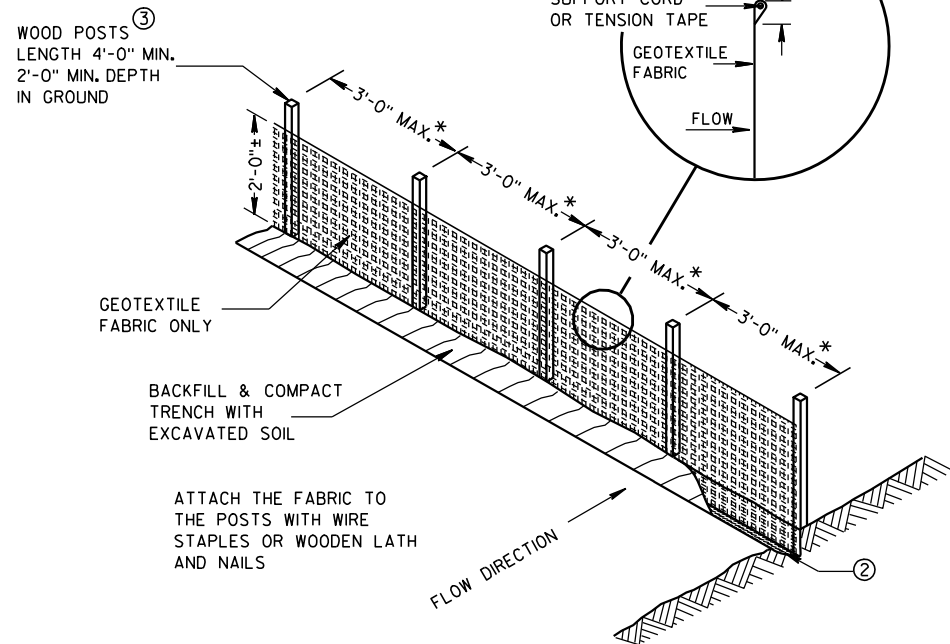
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



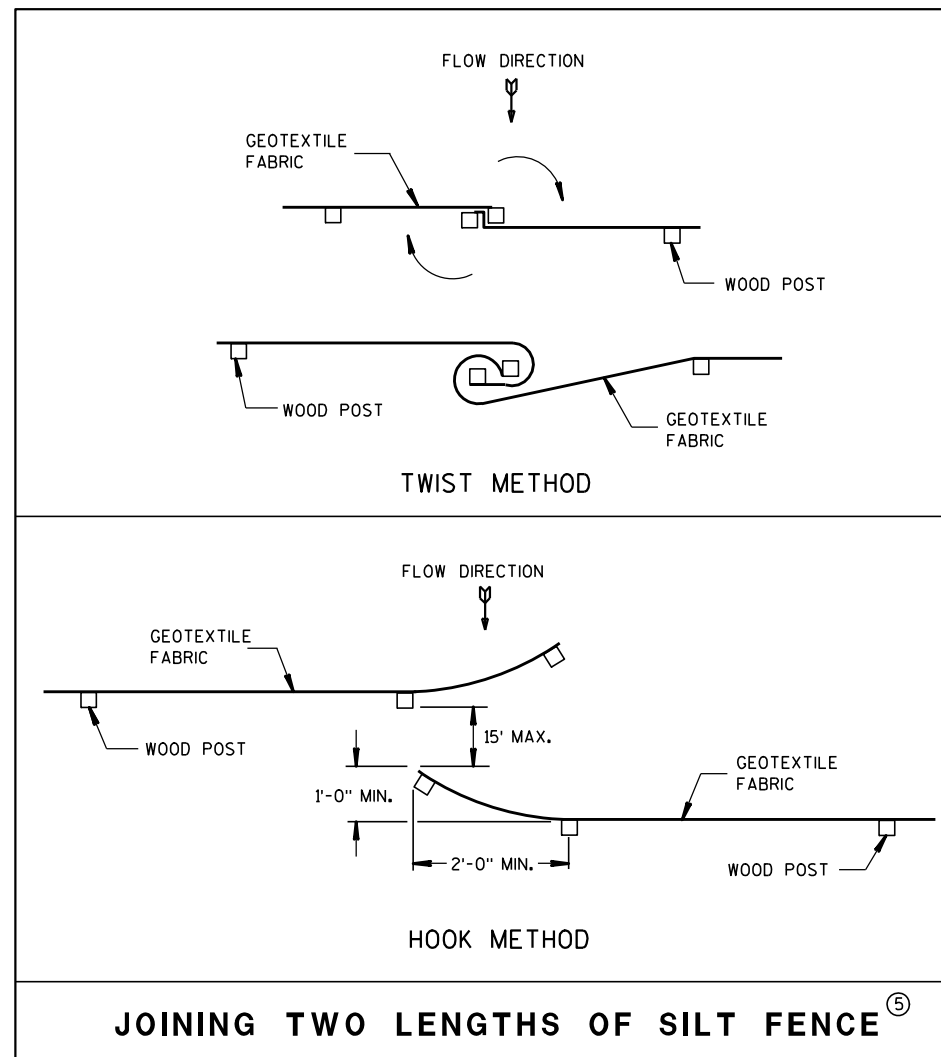
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

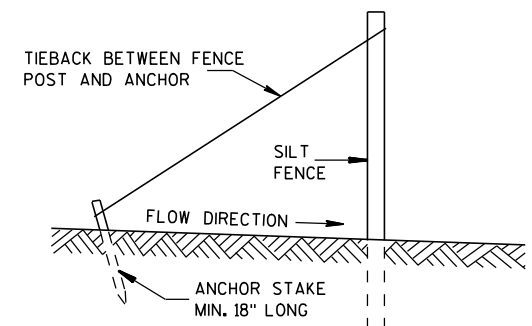


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤

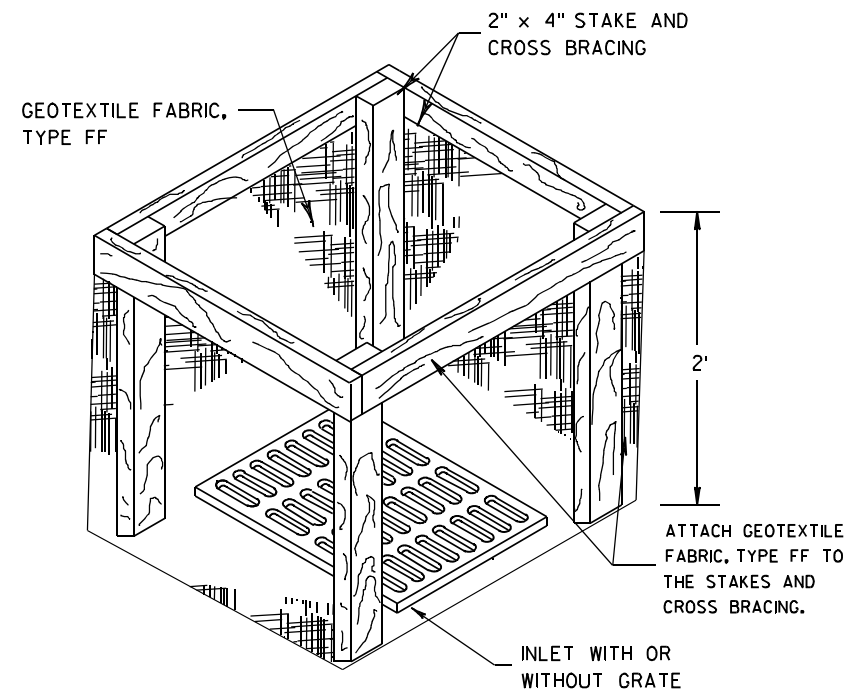
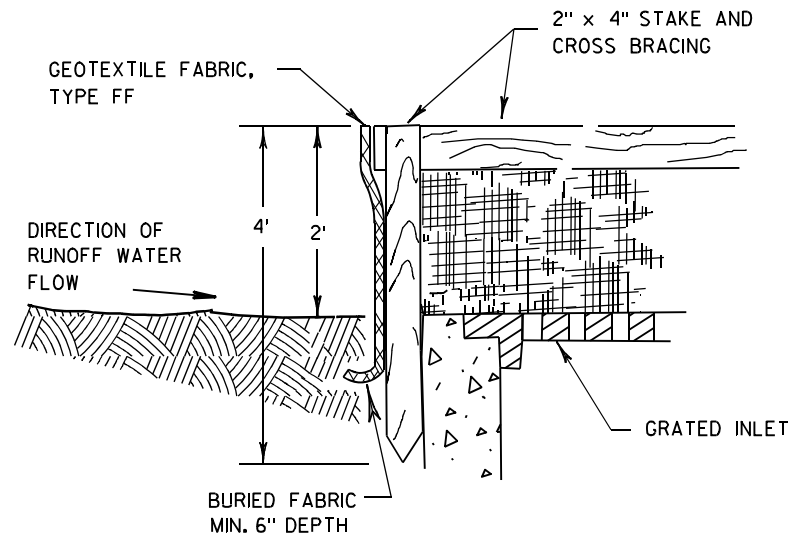


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



INLET PROTECTION, TYPE A

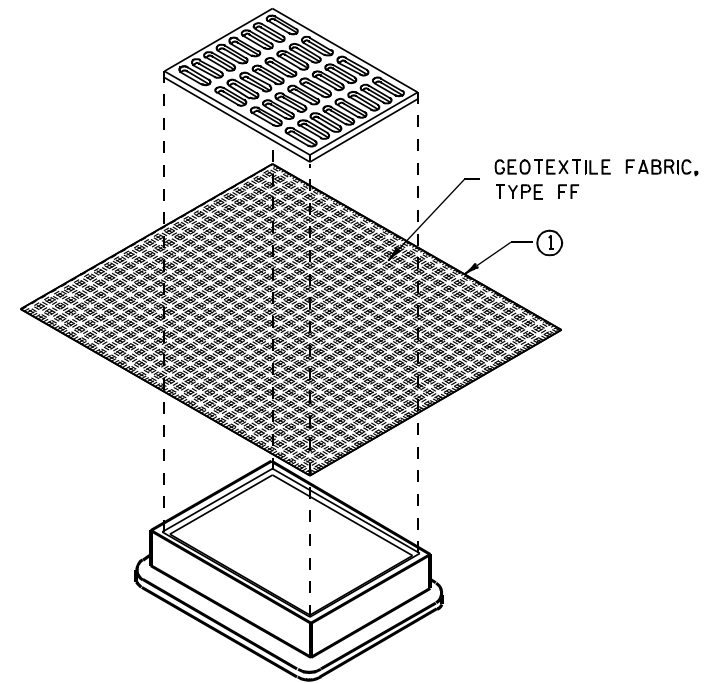
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

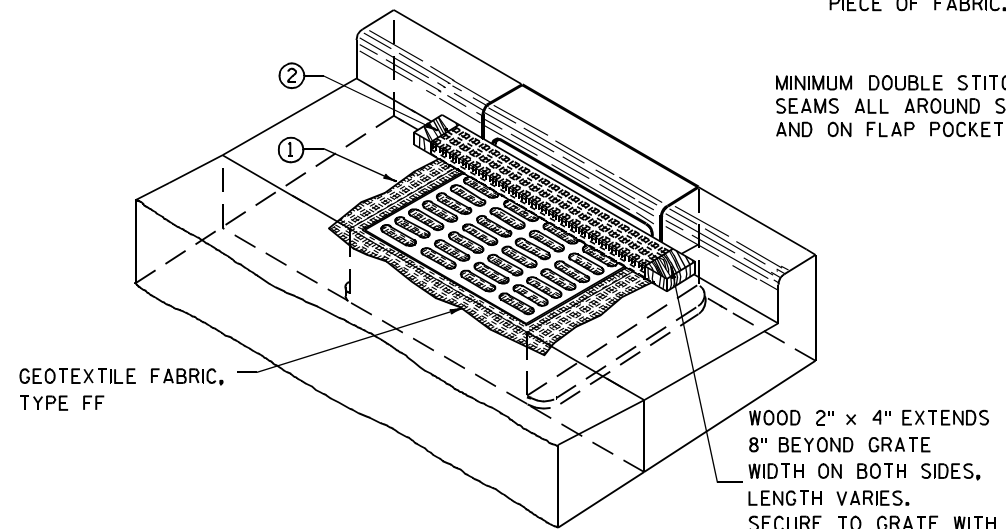
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

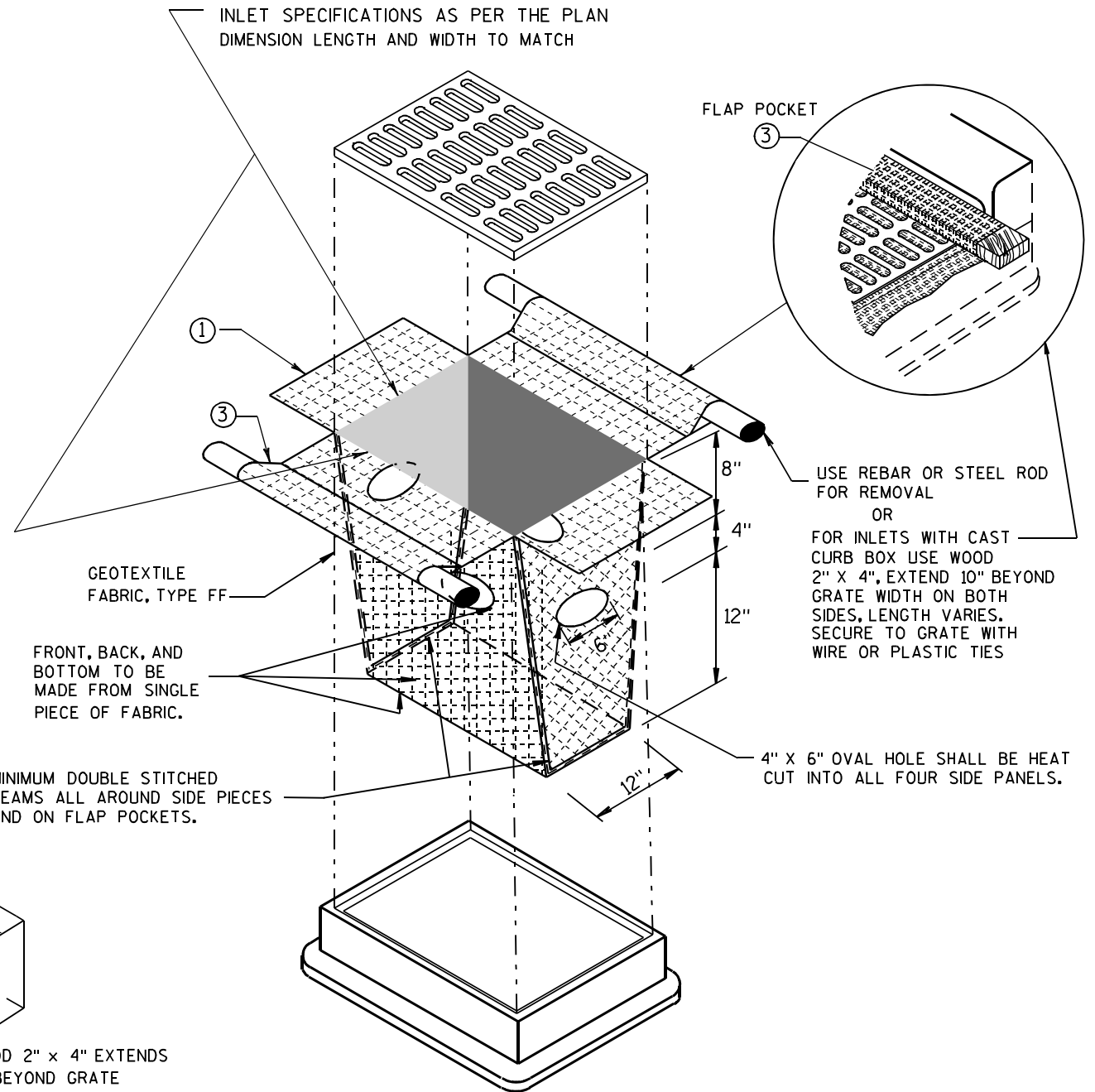
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

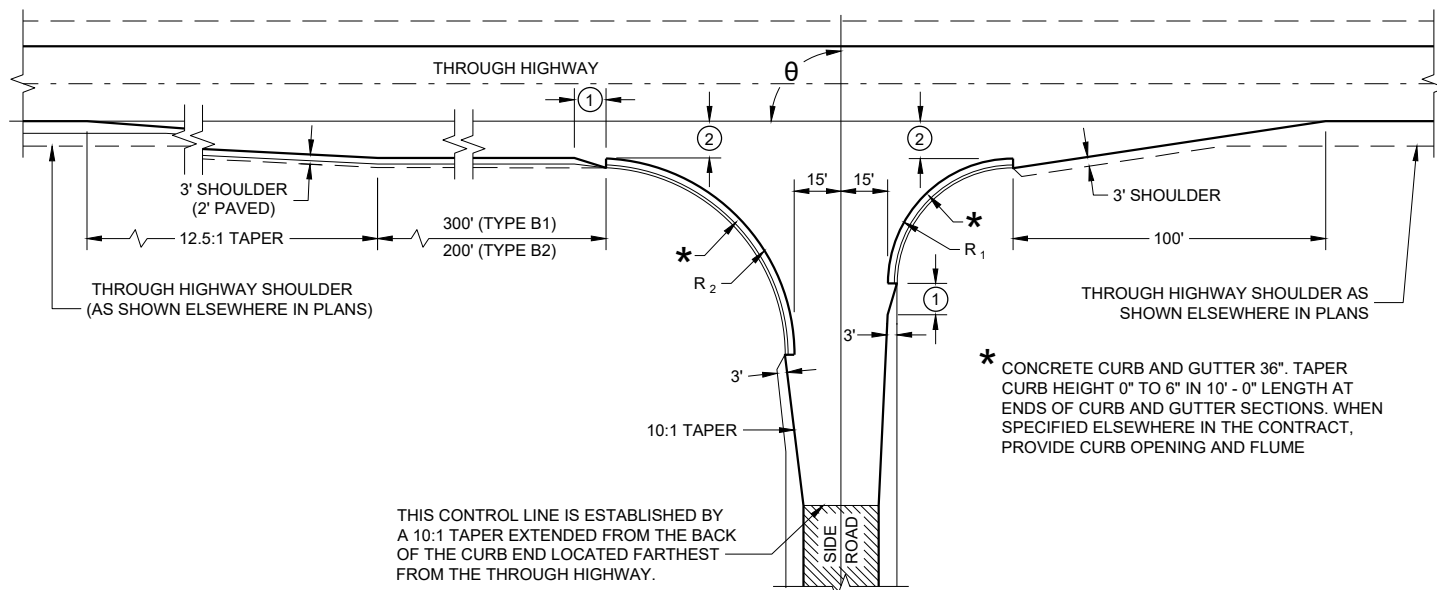
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/S/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



TYPE "B1" AND "B2"

RADII DIMENSIONS FOR TYPES "B1", "B2", "C" AND "D" INTERSECTIONS

θ	R_1	R_2
65 - 70	35	70
71 - 80	40	70
81 - 90	40	60
91 - 100	50	55
101 - 110	60	45

GENERAL NOTES

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

SIDE ROAD SURFACING NOTE

WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

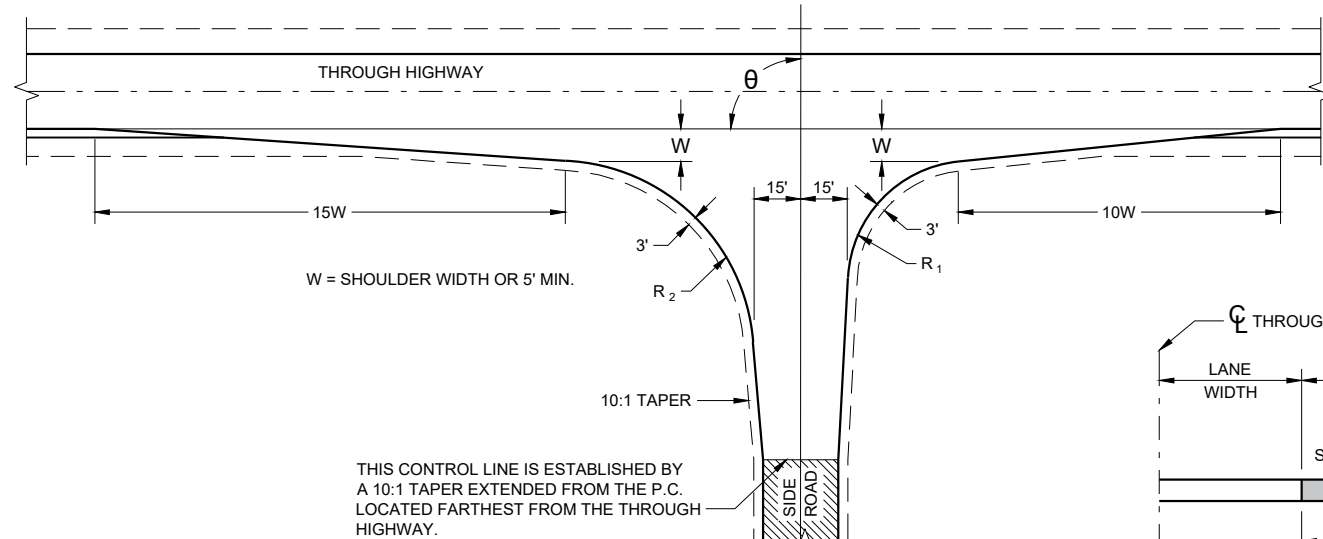
WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

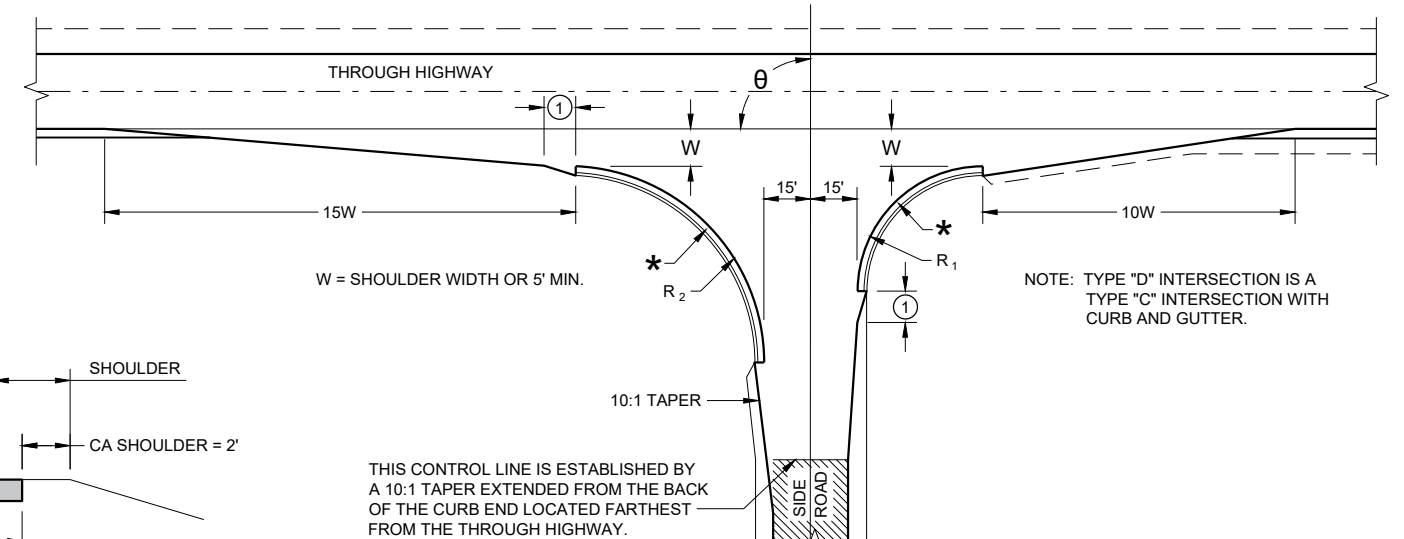
- ① 10-FT TYPICAL.
- ② 12-FT** PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.
**10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.
- ③ BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE
- ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH
- PC CONCRETE = 13-FT PLUS PAVED SHOULDER WIDTH
- ④ BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.

* CONCRETE CURB AND GUTTER 36". TAPER CURB HEIGHT 0" TO 6" IN 10' - 0" LENGTH AT ENDS OF CURB AND GUTTER SECTIONS. WHEN SPECIFIED ELSEWHERE IN THE CONTRACT, PROVIDE CURB OPENING AND FLUME

THIS CONTROL LINE IS ESTABLISHED BY A 10:1 TAPER EXTENDED FROM THE BACK OF THE CURB END LOCATED FARTHEST FROM THE THROUGH HIGHWAY.

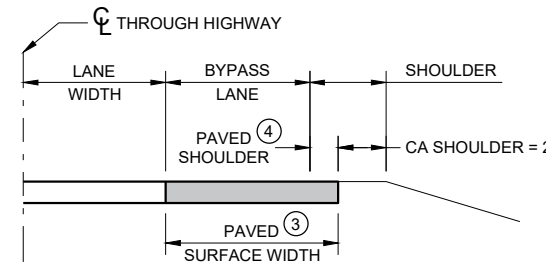


TYPE "C"

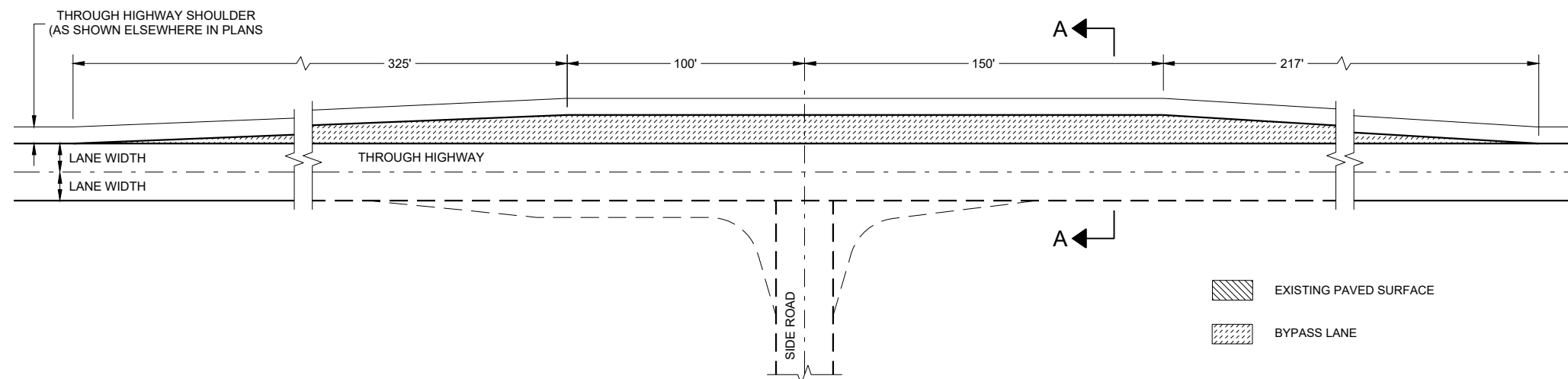


TYPE "D"

NOTE: TYPE "D" INTERSECTION IS A TYPE "C" INTERSECTION WITH CURB AND GUTTER.



SECTION A - A
(SHOWING BYPASS LANE AND SHOULDER)

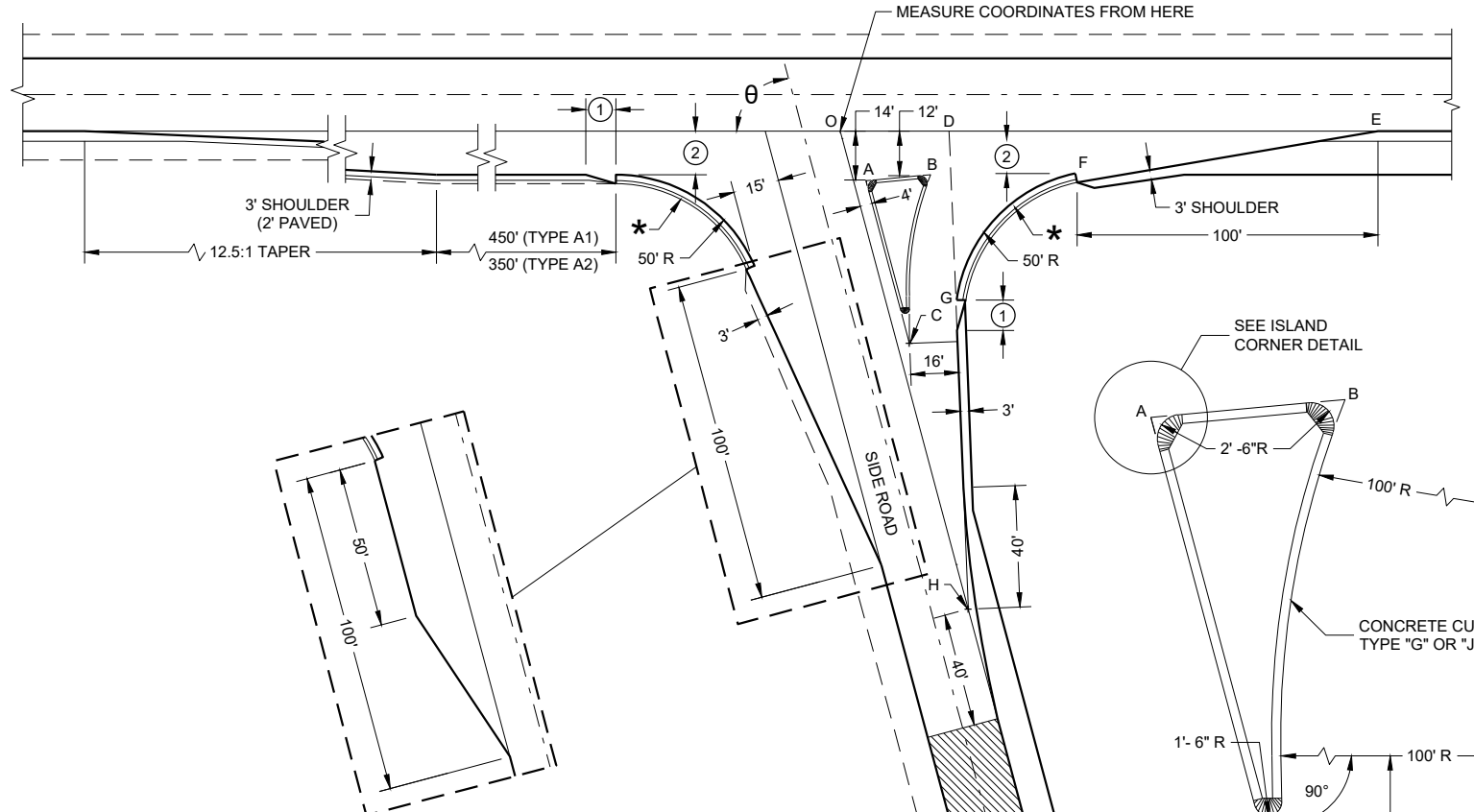
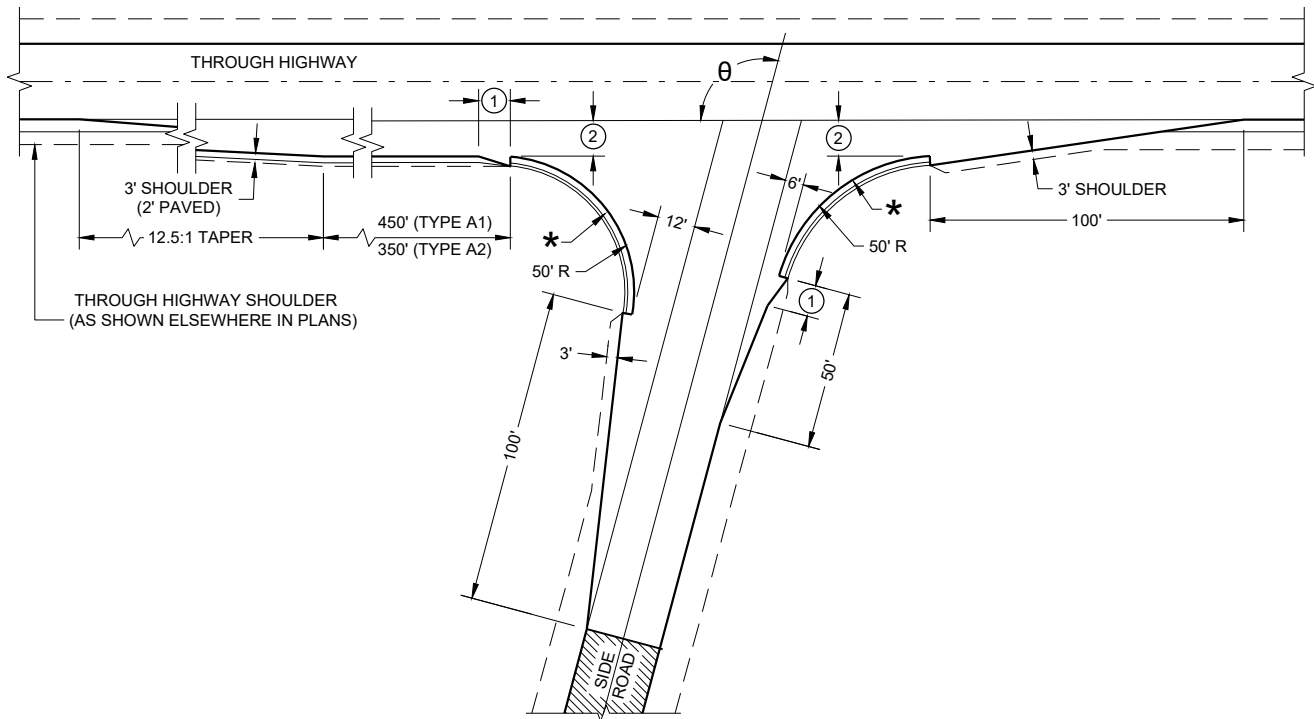


TEE INTERSECTION BYPASS LANE DETAIL

EXISTING PAVED SURFACE
BYPASS LANE

AT GRADE SIDE ROAD INTERSECTION TYPES "B1", "B2", "C", "D" AND TEE INTERSECTION BYPASS LANE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

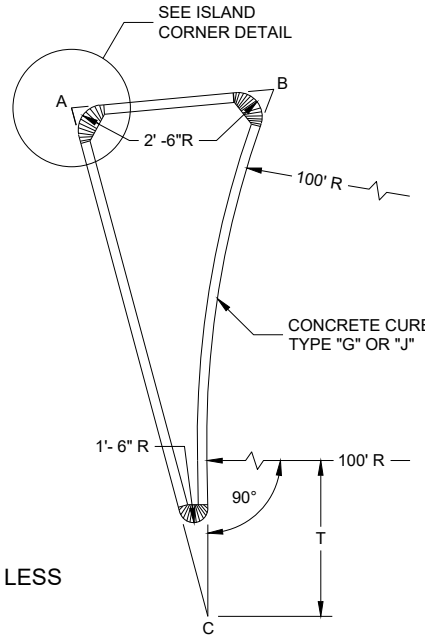


EXISTING PAVED SURFACE

$\theta = \text{MORE THAN } 80^\circ$

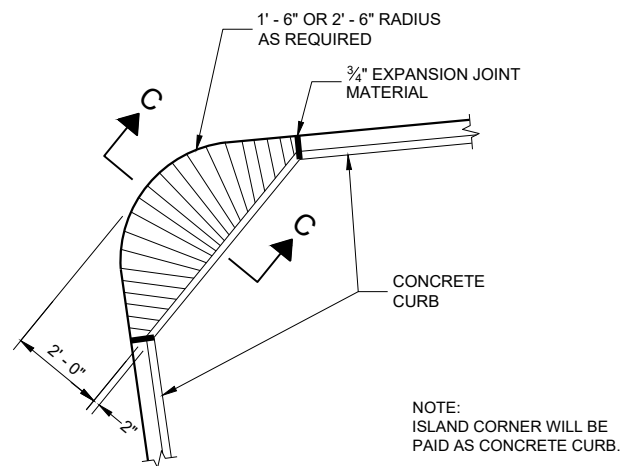
SIDE ROAD WIDENING AND TAPER REQUIRED WHERE THE THROUGH HIGHWAY CARRIES TWO-WAY TRAFFIC
 $\theta = \text{ACUTE ANGLES } 70^\circ \text{ OR LESS}$

$\theta = \text{ACUTE ANGLES } 80^\circ \text{ OR LESS}$

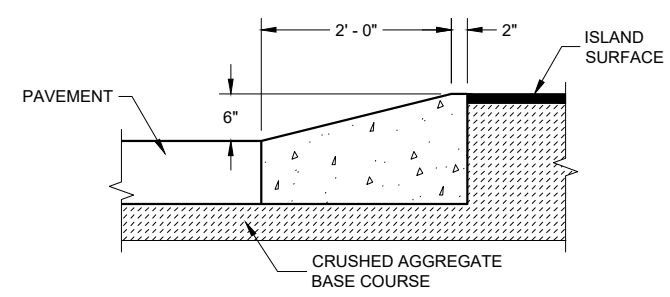


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6



PLAN VIEW



SECTION C - C

ISLAND CORNER DETAIL
 (TO BE CONSTRUCTED AT ALL ISLAND CORNERS)

TABLE OF DIMENSIONS FOR VARIABLE SIDE ROAD INTERSECTION ANGLES
 (INTERPOLATE VALUES FOR ANGLES NOT SHOWN)

ANGLE θ DEGREES	COORDINATES IN FEET (MEASURED FROM POINT "O")								LENGTH IN FEET				
	A	B	C	D	E	F	G	H	AB	AC	T	OJ	OH
60	12.7 -14.0	44.9 -12.0	46.4 -72.4	41.9 0.0	205.0 0.0	104.6 -12.0	64.0 -75.5	85.0 -147.1	32.3	67.4	4.9	85.9	169.9
65	10.9 -14.0	39.0 -12.0	37.8 -71.6	39.4 0.0	196.1 0.0	95.7 -12.0	54.1 -71.5	70.5 -151.3	28.2	63.6	8.5	80.9	166.9
70	9.4 -14.0	33.9 -12.0	29.8 -70.1	37.4 0.0	188.3 0.0	87.8 -12.0	45.6 -67.5	56.1 -154.2	24.6	59.7	11.5	76.1	164.1
75	7.9 -14.0	29.3 -12.0	22.3 -67.9	35.7 0.0	181.2 0.0	80.7 -12.0	38.2 -63.4	41.8 -155.9	21.5	55.8	13.8	71.4	161.4
80	6.5 -14.0	25.4 -12.0	15.6 -65.2	34.4 0.0	174.8 0.0	74.4 -12.0	31.8 -59.3	27.6 -156.5	18.9	52.0	15.6	66.9	158.9

TYPE 'A1" AND "A2" SIDE ROAD INTERSECTION DETAILS

**AT GRADE SIDE ROAD INTERSECTIONS
 TYPES "A1" AND "A2"**

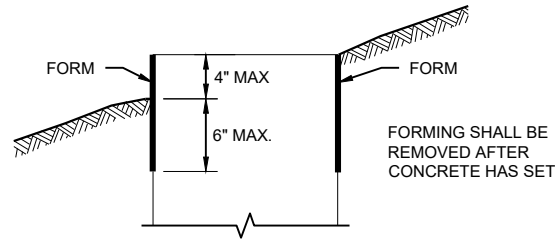
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 November 2022 /S/ John Jenkins
 DATE ROADWAY STANDARDS DEVELOPMENT
 ENGINEER
 FHWA

SDD09A01 - 14b

SDD09A01 - 14b

FORM DEPTH SHALL BE NO MORE THAN 6" BELOW GRADE ON THE LOWER SIDE OF BASE



FORMING DETAIL

QUANTITY REQUIREMENTS	CONCRETE BASE TYPE		
	1	2	5 & 6
APPROX. CUBIC YARDS OF CONCRETE	0.40	0.57	0.40
LBS. OF HOOP BAR STEEL	NONE	23	16
LBS. OF VERTICAL BAR STEEL	NONE	60	18

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE SHOWN ON THE PLANS

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN AT THE ENTRANCE OF THE BASE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FOR FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION.

WHEN REQUIRED TO CONNECT NON-METALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 2, TYPE 5 AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER ALL BASE TYPES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD. ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

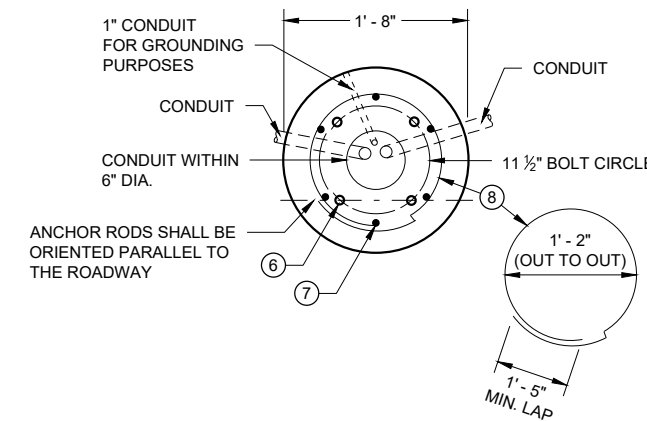
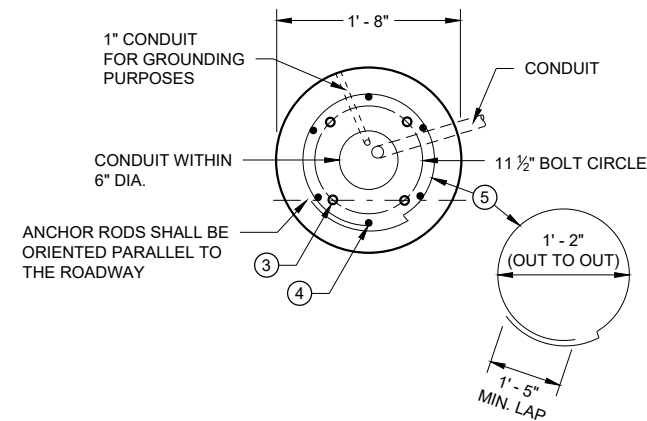
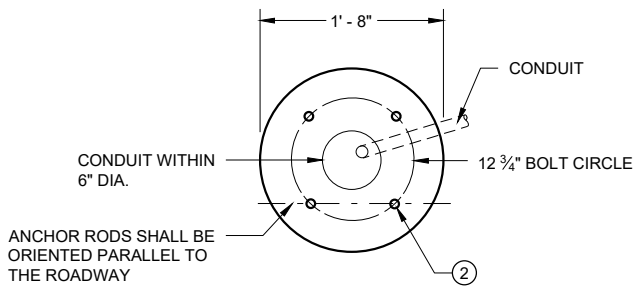
WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4 INCH "L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND SHALL NOT BE THREADED.

ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

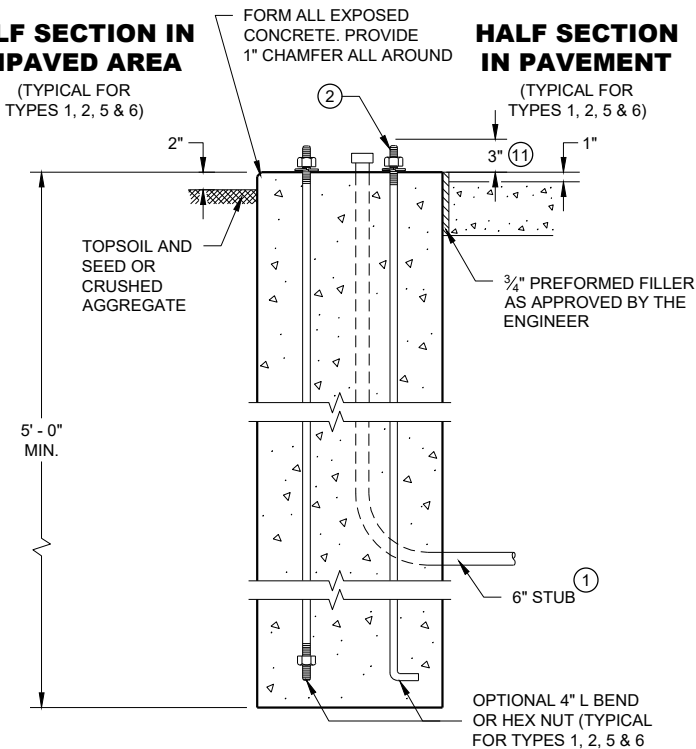
WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

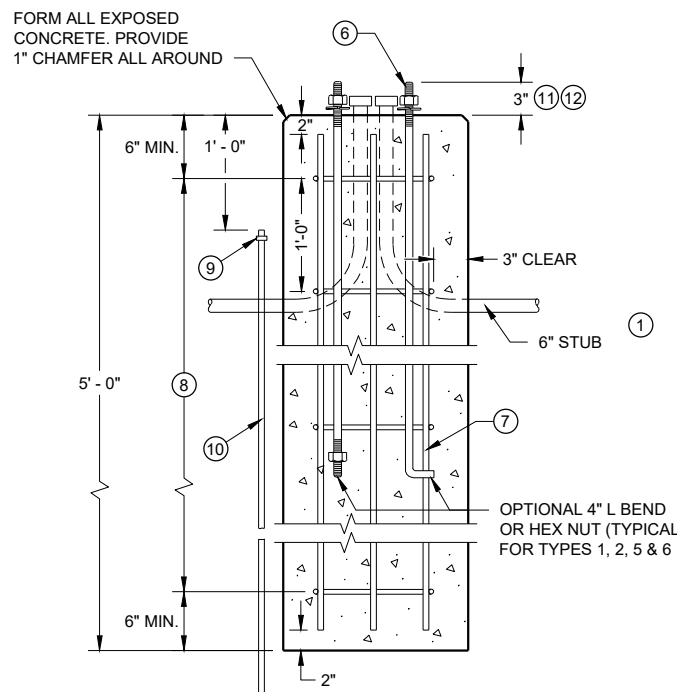
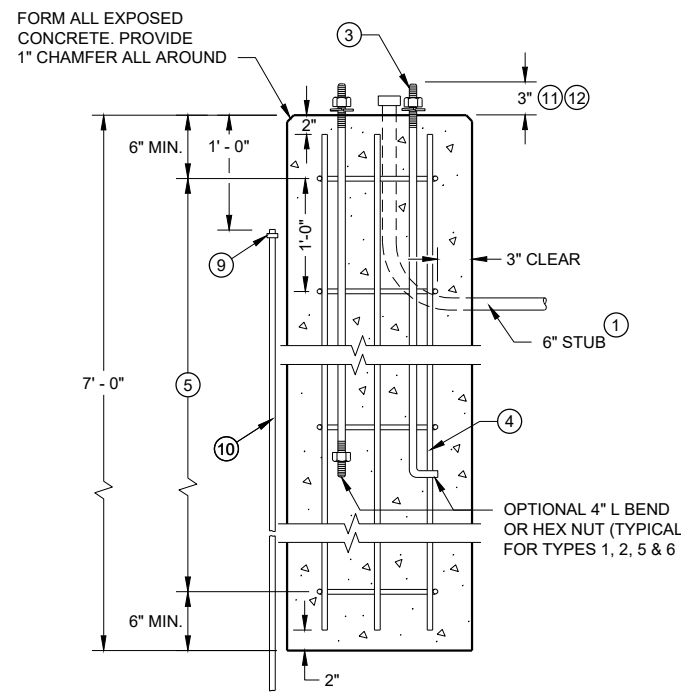
- ① THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL OF THE ENGINEER.
- ② (4) 1" DIA. X 3' - 6" ANCHOR RODS.
- ③ (4) 1" DIA. X 5' - 0" ANCHOR RODS.
- ④ (6) NO. 6 X 6' - 8" BAR STEEL REINFORCEMENT.
- ⑤ (7) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.
- ⑥ (4) 1" DIA. X 3' - 6" ANCHOR RODS.
- ⑦ (6) NO. 4 X 4' - 8" BAR STEEL REINFORCEMENT.
- ⑧ (5) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.
- ⑨ EXOTHERMIC CONNECTION TO EQUIPMENT GROUNDING CONDUCTOR
- ⑩ 5/8" DIA. X 8' - 0" COPPERCLAD EQUIPMENT GROUNDING ELECTRODE REQUIRED
- ⑪ ANY ANCHOR ROD PROJECTION SHORTER THAN 2 3/4" OR LONGER THAN 3 1/4" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.
- ⑫ FOR NON - BREAKAWAY INSTALLATIONS, 4 1/2" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.



HALF SECTION IN UNPAVED AREA



HALF SECTION IN PAVEMENT



CONCRETE BASES

**CONCRETE BASES
TYPES 1, 2, 5, & 6**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Ahmet Demirelek
DATE STATE ELECTRICAL ENGINEER

FHWA

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

FOUR (4) BOLTS SHALL BE FURNISHED WITH EACH TRANSFORMER BASE. BOLTS SHALL BE 1" DIAMETER, 4" IN LENGTH, WITH WASHERS, LOCK WASHERS AND NUTS. BOLTS, NUTS AND WASHERS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 531.2.2 OF THE STANDARD SPECIFICATIONS.

LEVELING SHIMS, IF NEEDED, SHALL BE DESIGNED FOR THE PURPOSE AND USED UNDER CAST BASES WHEN PLUMBING POLES OR STANDARDS DURING INSTALLATION. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE.

SHIM LENGTH SHALL BE LONG ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.

DOUBLE NUTTING IS NOT ACCEPTABLE FOR LEVELING OR MOUNTING PURPOSES.

A NEMA APPROVED, U.L. LISTED, COPPER WITH BRASS OR STAINLESS STEEL SET SCREW, DIRECT BURY RATED, MECHANICAL CONNECTOR (LUG), SIZED TO ACCEPT AWG. #10 TO #4 COPPER STRANDED WIRE SHALL BE FURNISHED AND INSTALLED IN THE PEDESTAL AND TRANSFORMER BASES.

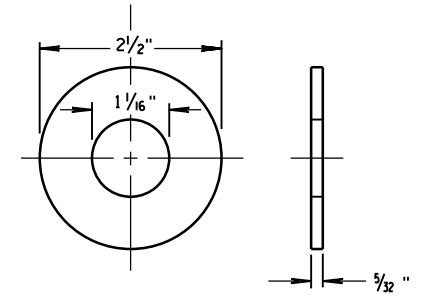
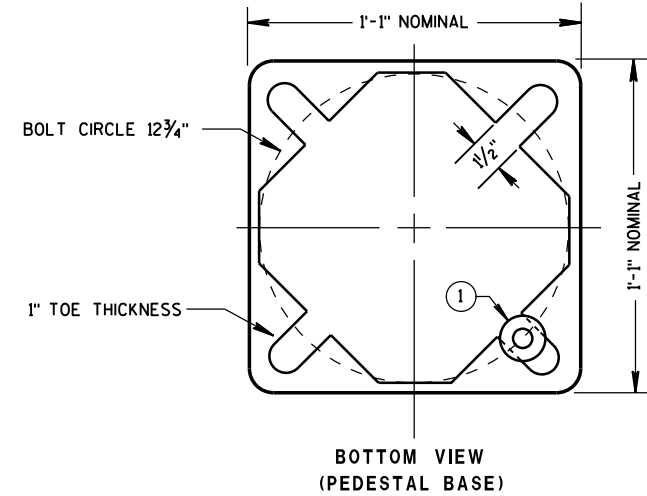
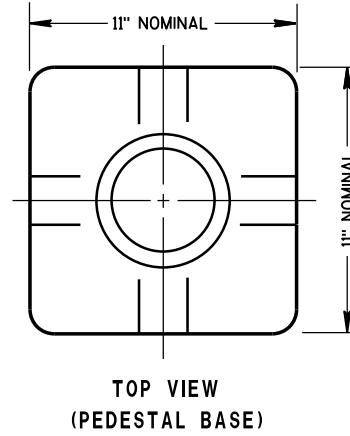
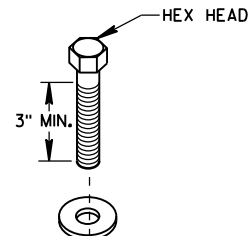
THE MECHANICAL CONNECTOR SHALL BE INSTALLED USING A 1/4" - 20 (TPI) STAINLESS STEEL HEX HEAD BOLT OF SUFFICIENT LENGTH TO FIRMLY ATTACH THE LUG TO THE BASE.

SHOULD THE MANNER OF ATTACHMENT OF THE LUG REQUIRE WASHERS, HEX NUTS, LOCK WASHER - THEY SHALL BE STAINLESS STEEL AS IS THE BOLT. THE MANNER OF ATTACHMENT SHALL NOT BLOCK ACCESSIBILITY TO WIRE PLACEMENT IN THE CONNECTOR.

PEDESTAL BASE COLLAR THREADING SHALL BE TAPERED AND IN ACCORDANCE WITH NATIONAL PIPE THREADING DIMENSIONS.

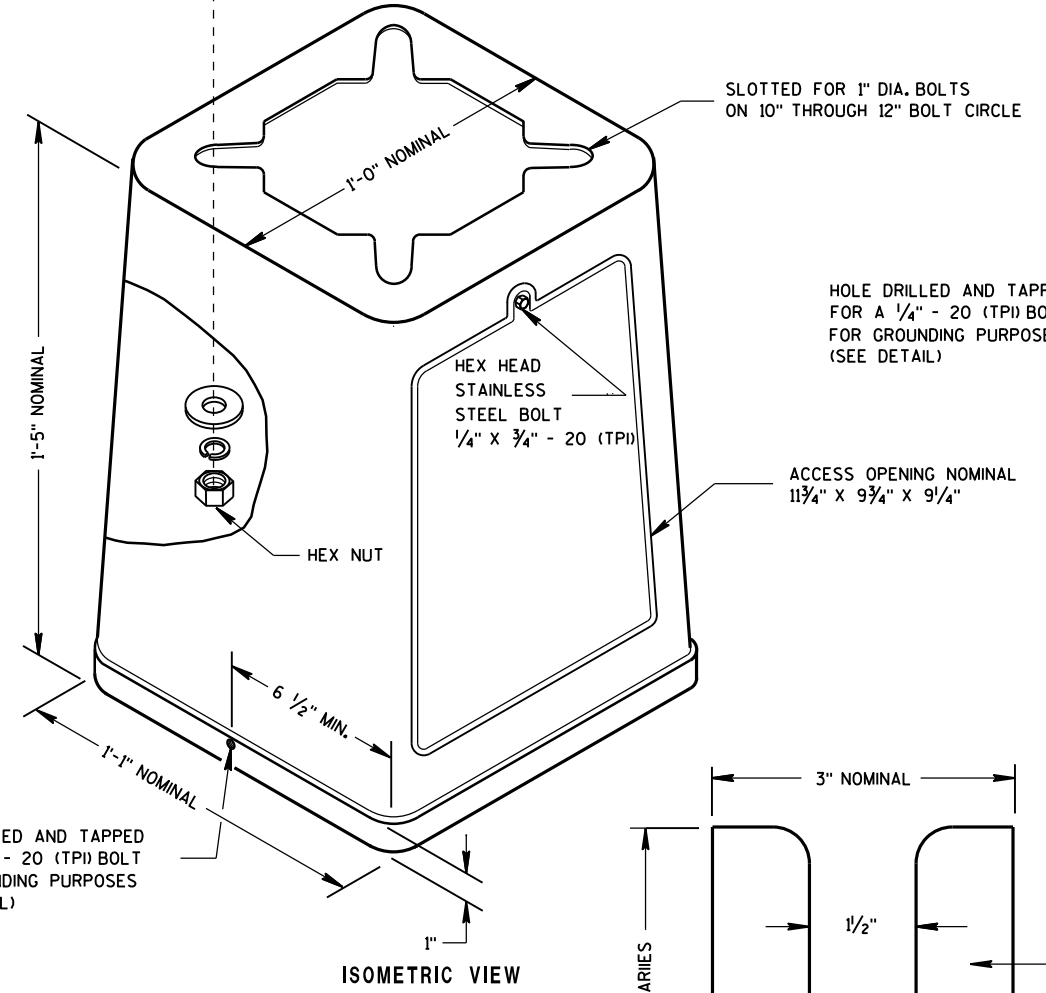
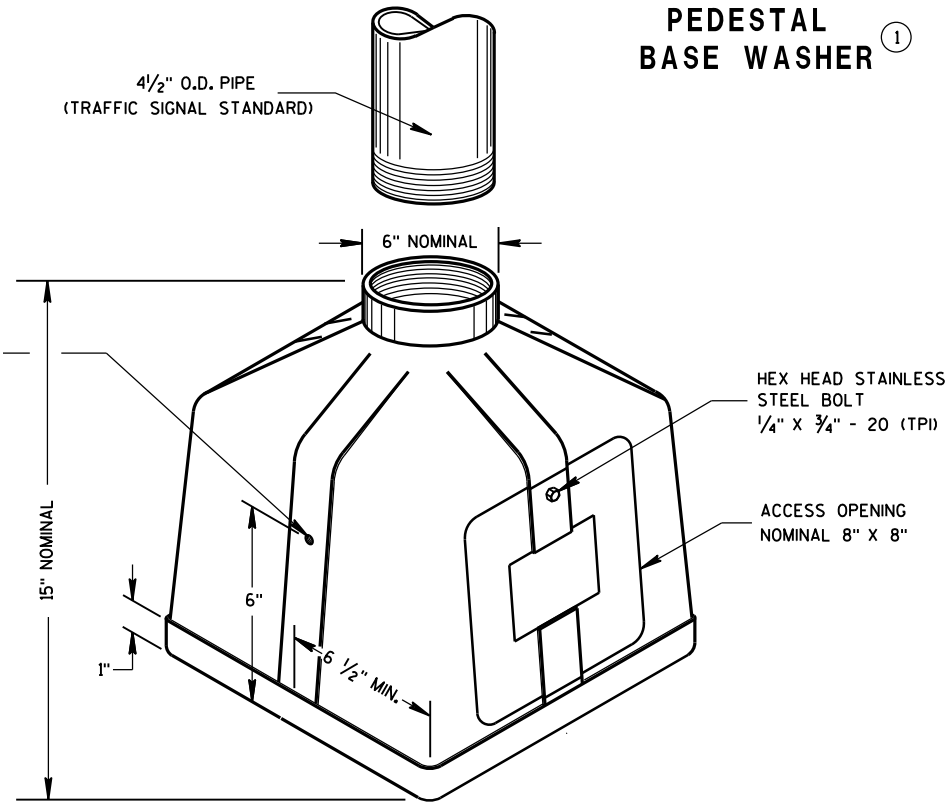
BASE COLLAR THREADING SHALL EXTEND INTO THE BASE COLLAR WITH SUFFICIENT DEPTH TO ACCEPT THE INSTALLATION OF TRAFFIC SIGNAL STANDARDS TO A DEPTH OF 1/2", THEN TIGHTENING TO A POINT OF BEING IMMOVABLE.

THE ACCESS DOOR SHALL BE OF THE SAME MATERIAL AS THE BASE.

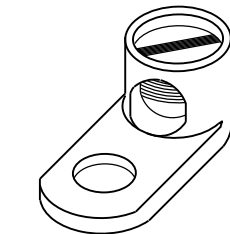
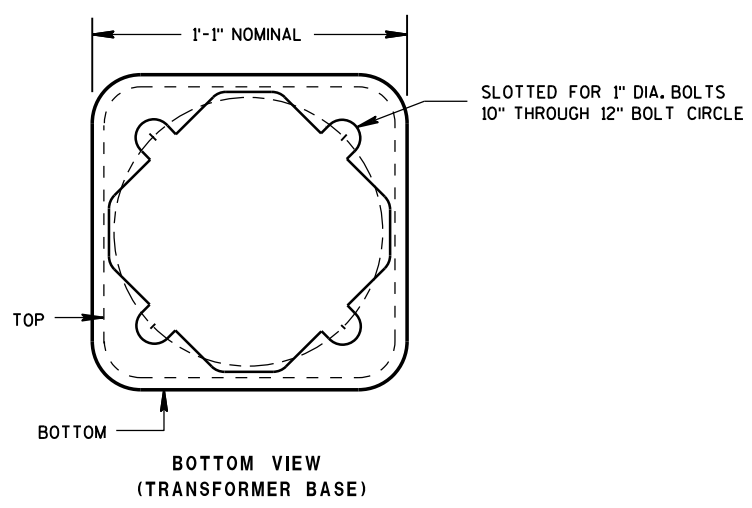
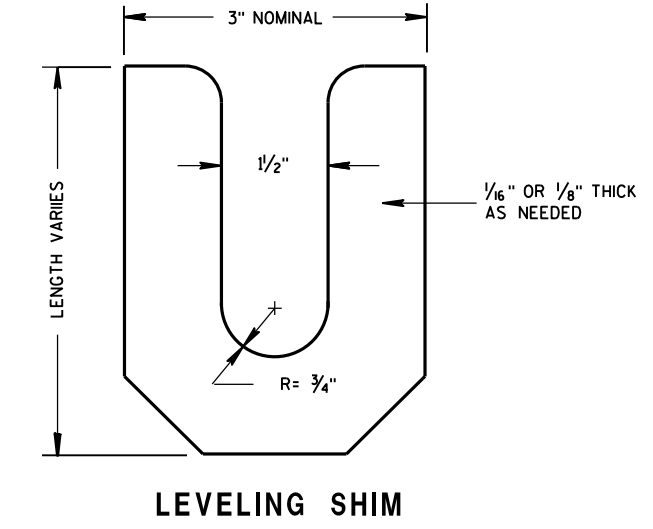


ZINC COATED STEEL WASHER
TO BE PROVIDED BY THE CONTRACTOR

**PEDESTAL
BASE WASHER** ①



HOLE DRILLED AND TAPPED
FOR A 1/4" - 20 (TPI) BOLT
FOR GROUNDING PURPOSES
(SEE DETAIL)



**TYPICAL MECHANICAL
CONNECTOR LUG**
TO BE FURNISHED WITH EACH BASE

TRANSFORMER BASE
INTENDED FOR USE WITH TYPE 2, 3, 4, 5 & 6 POLES

TRANSFORMER/PEDESTAL BASES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED Sept. 2014 DATE	/S/ Ahmet Demirbilek STATE ELECTRICAL ENGINEER
FHWA	

6

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S.D.D. 9 C 3-4

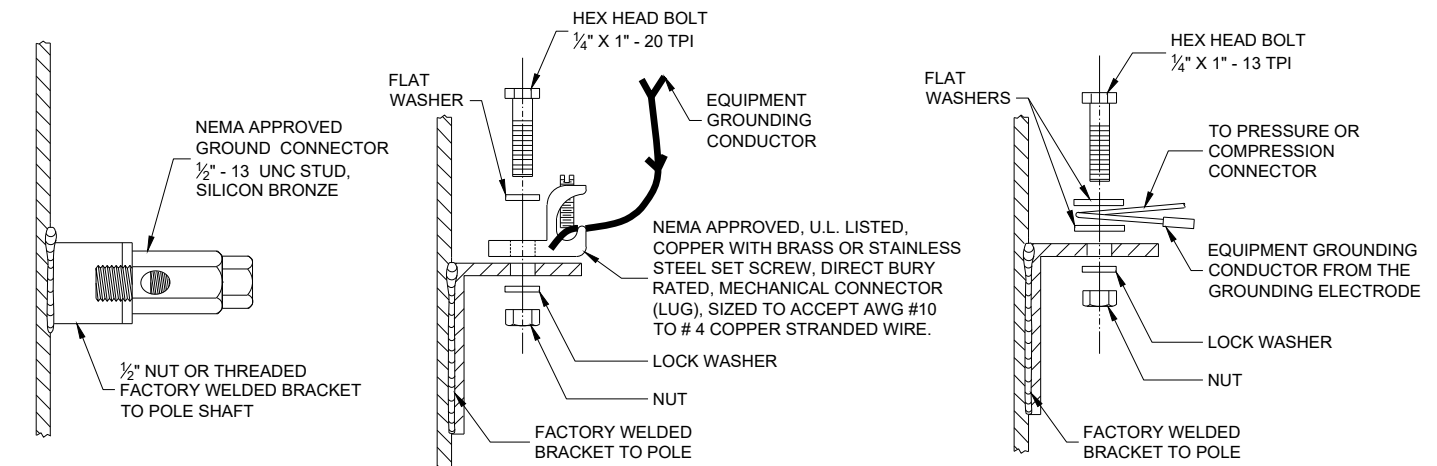
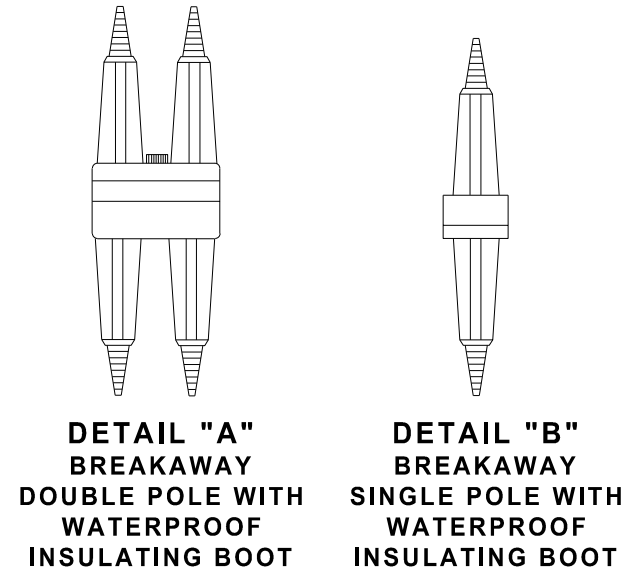
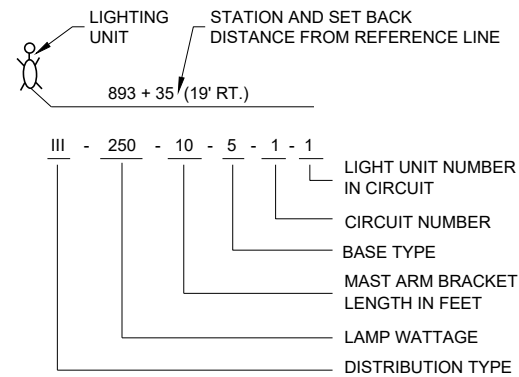
S.D.D. 9 C 3-4

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

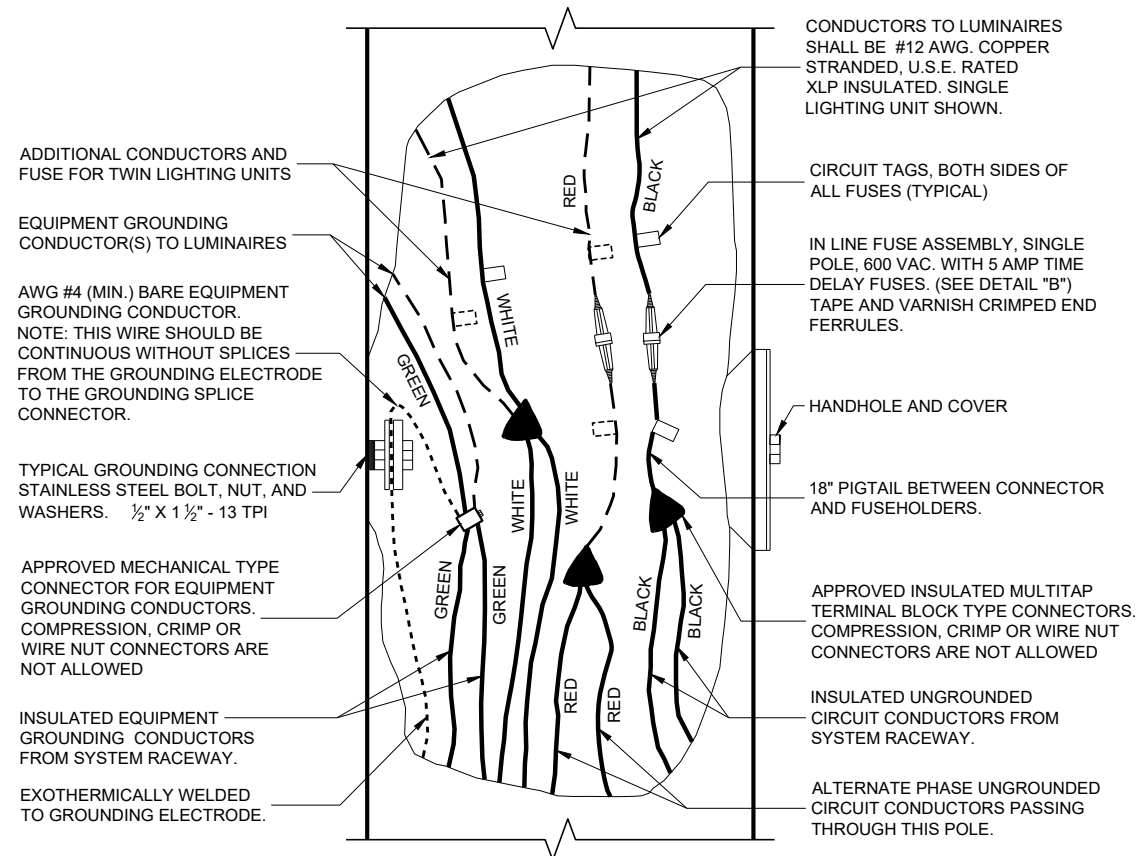
THE EQUIPMENT GROUND CONNECTOR SHALL BE TAPED WITH 3 WRAPS (MINIMUM) OF APPROVED RUBBER TAPE AND 3 WRAPS (MINIMUM) OF APPROVED VINYL TAPE TO COVER SHARP WIRE ENDS AFTER THE CONNECTION IS COMPLETED.

WHEN TRANSFORMER BASES ARE USED, ALL WIRING CONNECTIONS SHALL OCCUR WITHIN THE TRANSFORMER BASES.

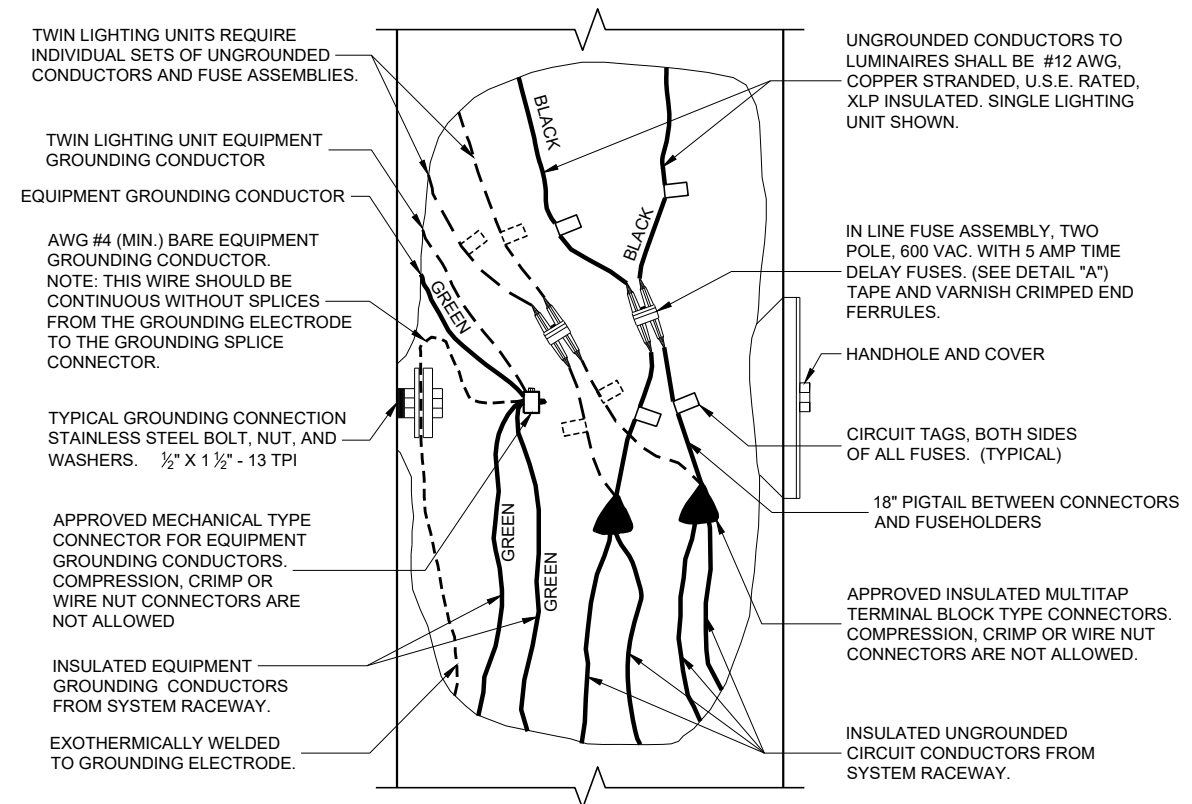


TYPICAL GROUNDING CONNECTIONS
NUT, BOLT AND WASHERS SHALL BE STAINLESS STEEL

LIGHTING UNIT CODE (TYPICAL)



3 WIRE - 120, 240 OR 480 VAC (UNGROUNDING CONDUCTORS) WITH GROUNDING CONDUCTOR AND EQUIPMENT GROUNDING CONDUCTOR



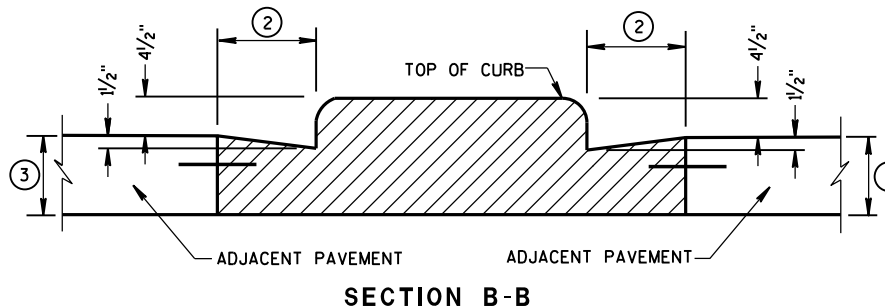
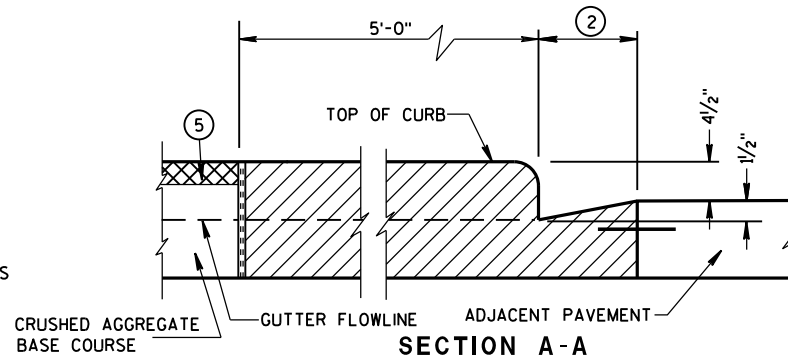
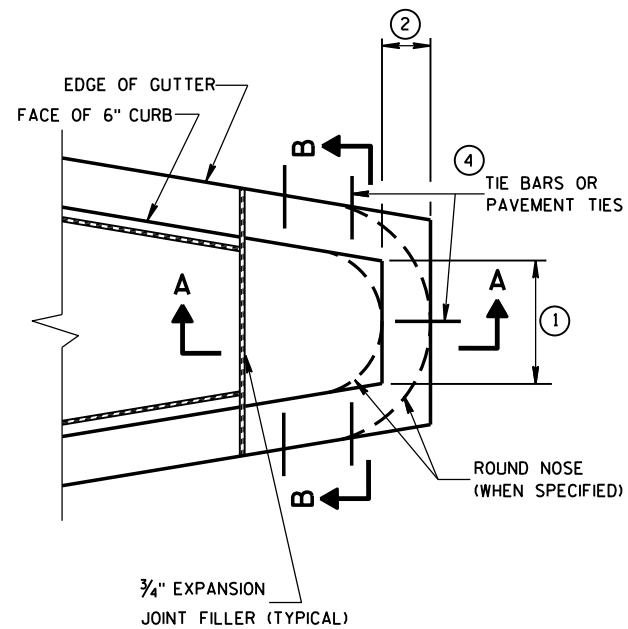
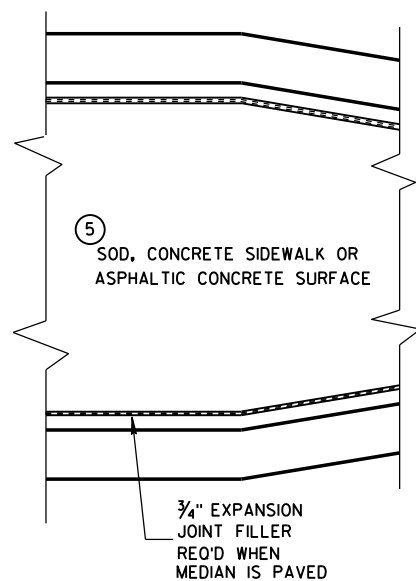
2 WIRE - 240 OR 480 VAC (UNGROUNDING CONDUCTORS) WITH EQUIPMENT GROUNDING CONDUCTOR

NON - FREEWAY LIGHTING UNIT POLE WIRING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Ahmet Demirebilek
DATE STATE ELECTRICAL ENGINEER

FHWA

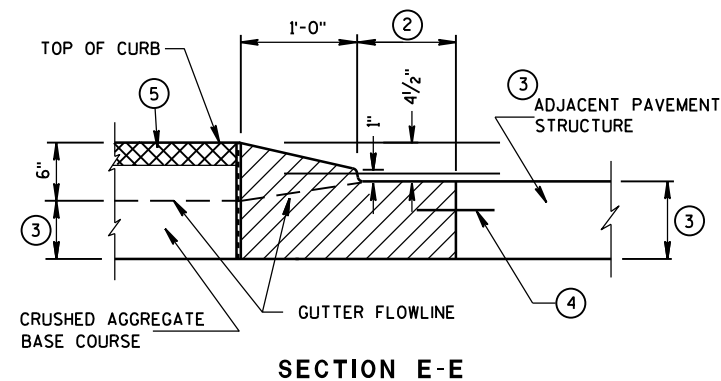
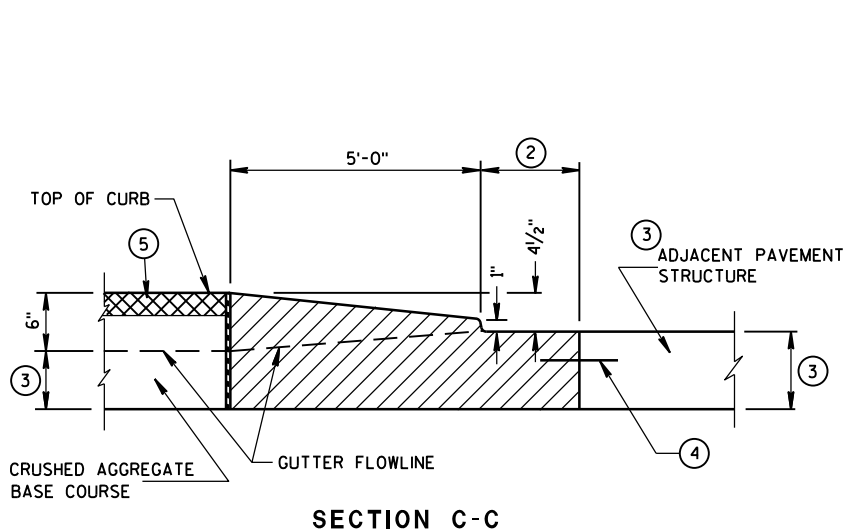
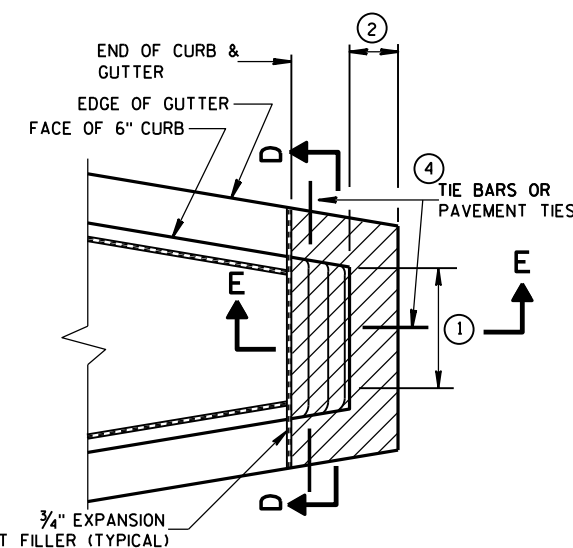


CONCRETE MEDIAN BLUNT NOSE DETAIL

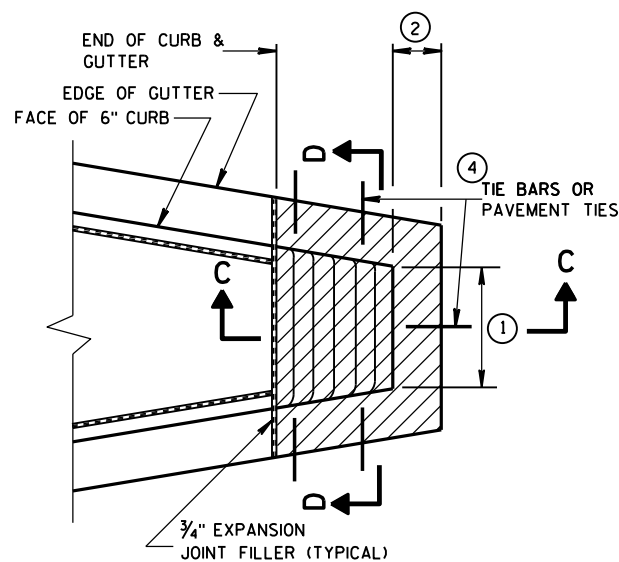
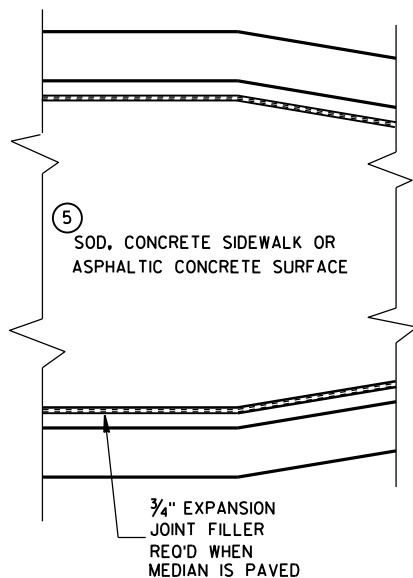
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

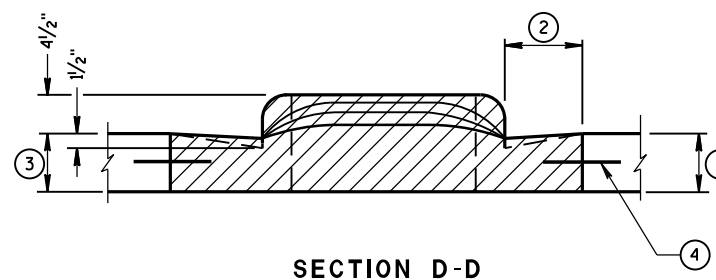
- ① SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).
- ② WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.
- ③ DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:
 - (1) NEW OR EXISTING CONCRETE PAVEMENT.
 - (2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.
 - (3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.
- ④ TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.
- PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.
- ⑤ SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.



CONCRETE MEDIAN SLOPED NOSE TYPE 2



CONCRETE MEDIAN SLOPED NOSE TYPE 1



CONCRETE MEDIAN NOSE

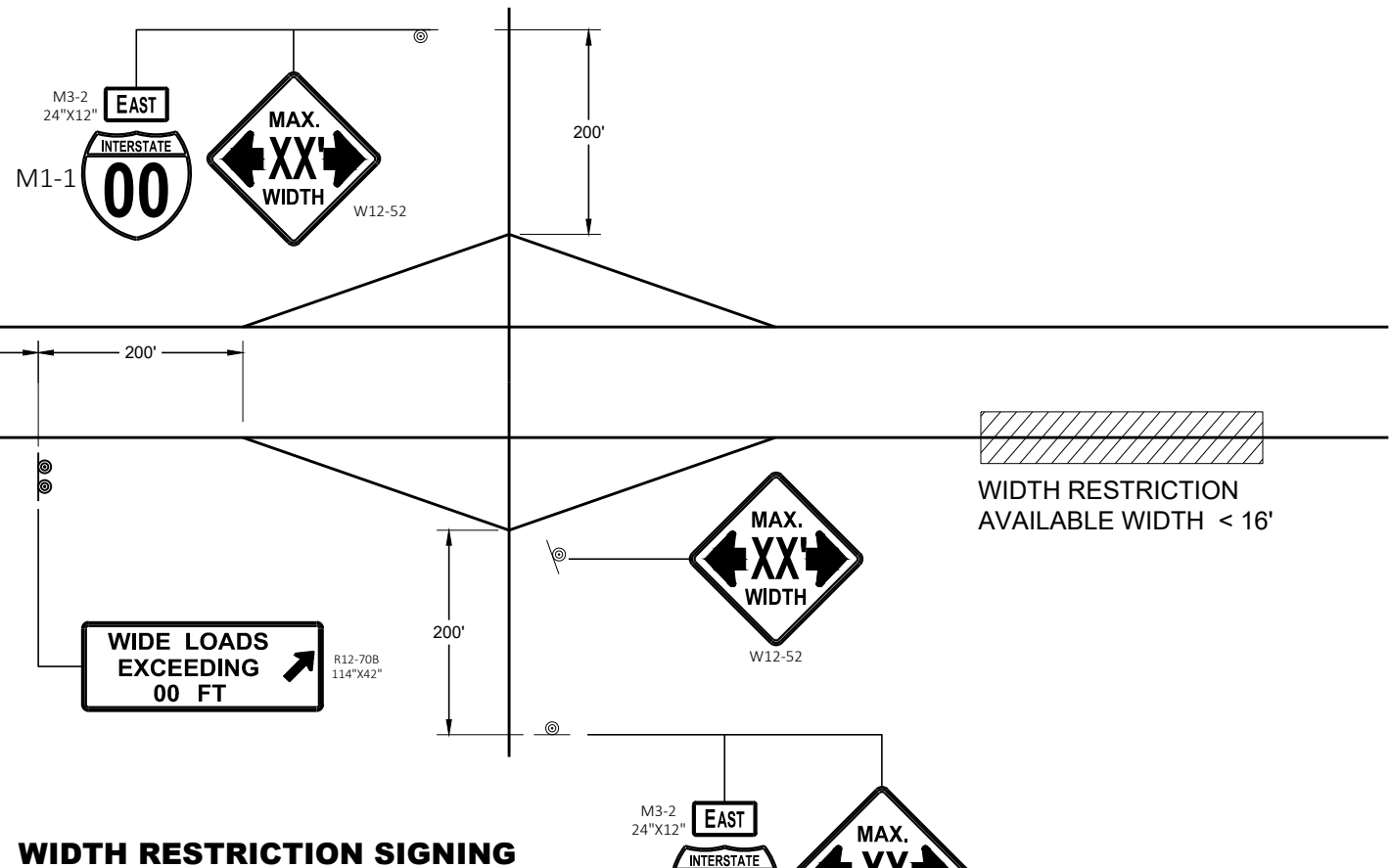
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

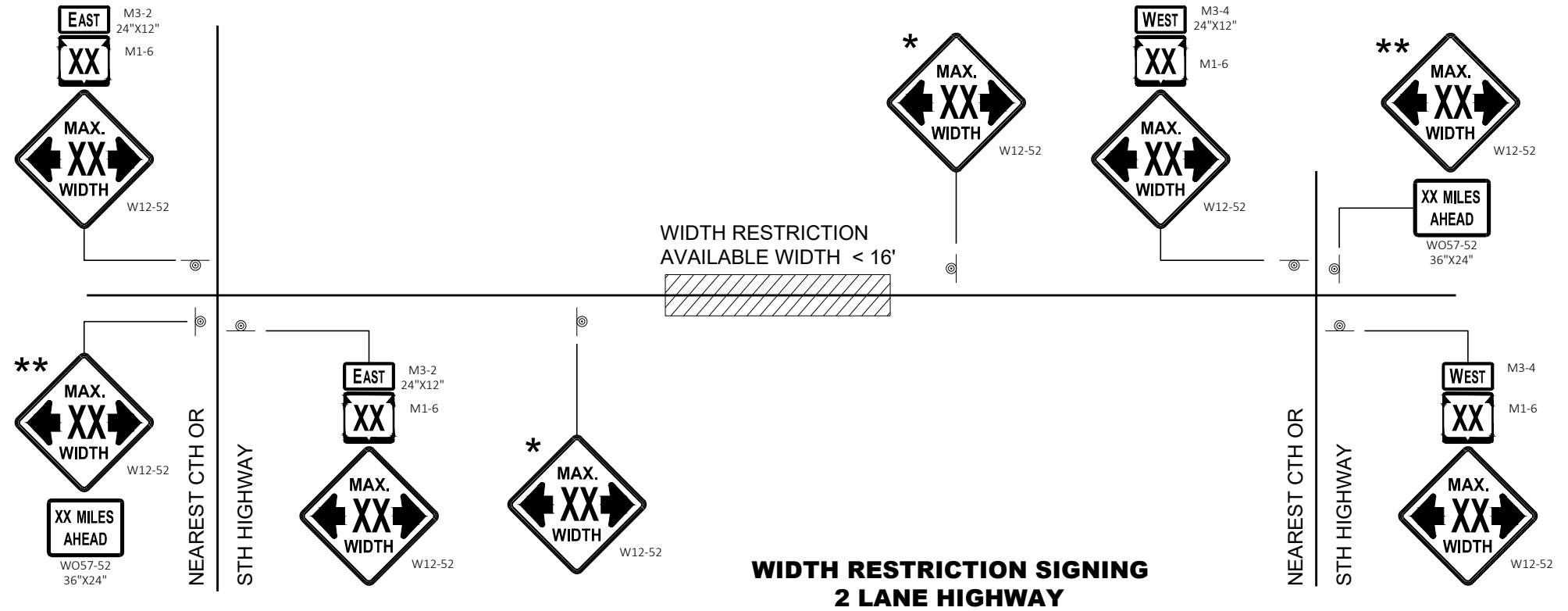
6/8/2006
DATE

FHWA

/s/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



WIDTH RESTRICTION SIGNING



**WIDTH RESTRICTION SIGNING
2 LANE HIGHWAY**

LEGEND

⊙ SIGN ON PERMANENT SUPPORT

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WIDTH ON SIGN TO BE APPROXIMATELY ONE FOOT LESS THAN AVAILABLE WIDTH.

* PLACE 500 FEET AFTER THE W20 - 1A AND 500 FEET BEFORE ADDITIONAL SIGNS FOR ROADWAYS WITH A PRE - CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE. FOR 35-40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25-30 MPH, USE 200 FOOT TYPICAL SPACING.

** SIGN SHALL BE VISIBLE FROM ROADWAY.

*** ADDITIONAL SIGNS NEEDED IF THERE IS AN ON RAMP BETWEEN SIGNS.

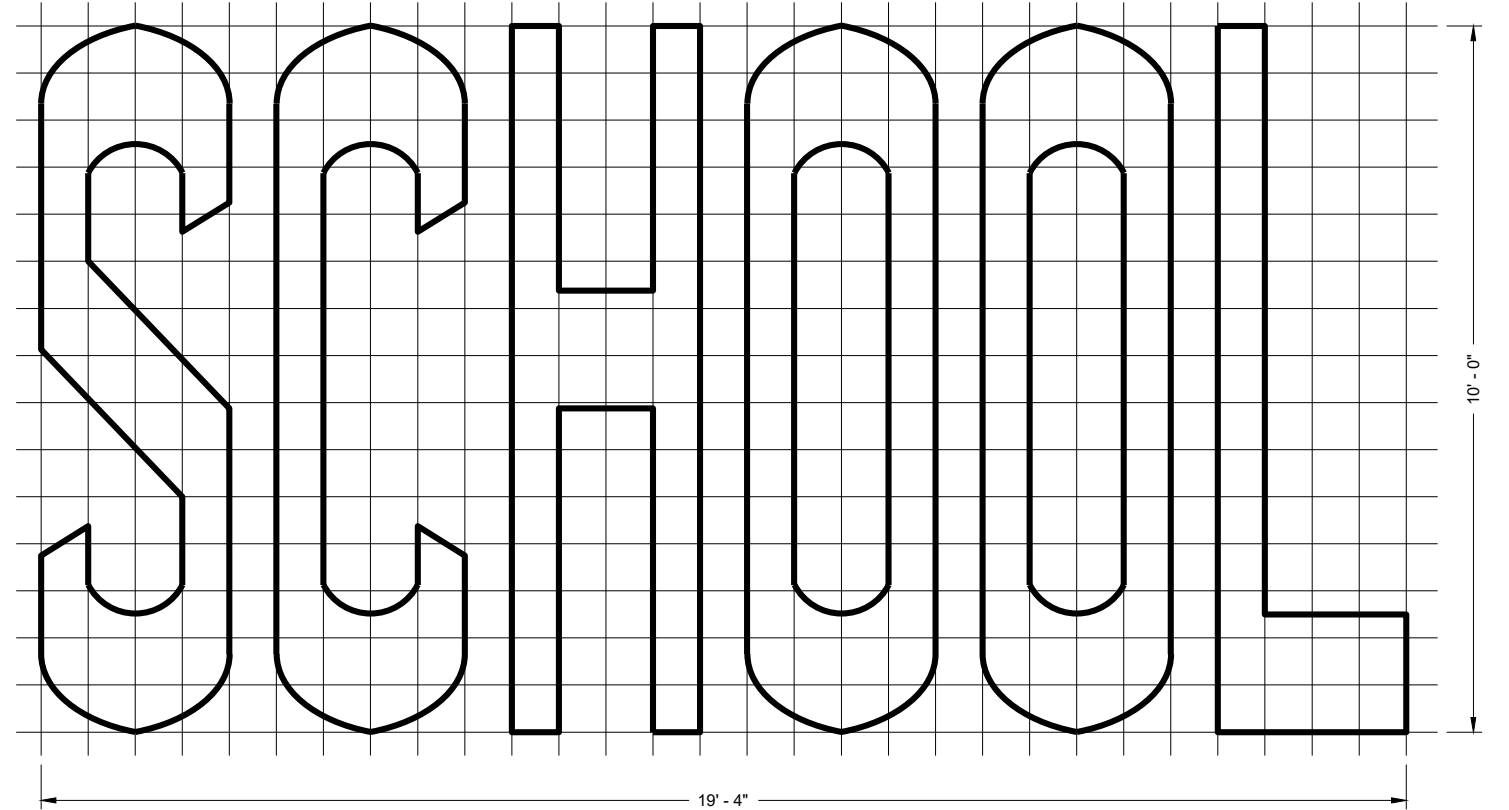
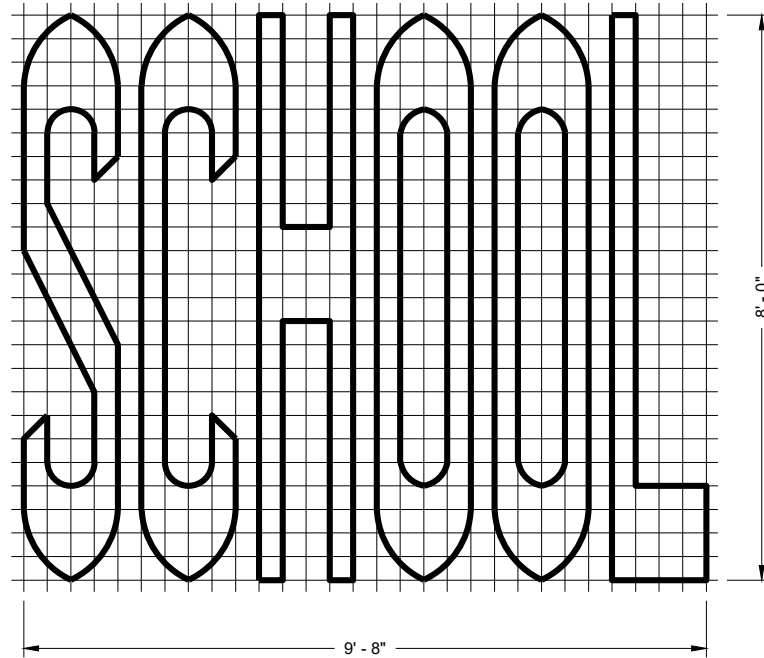
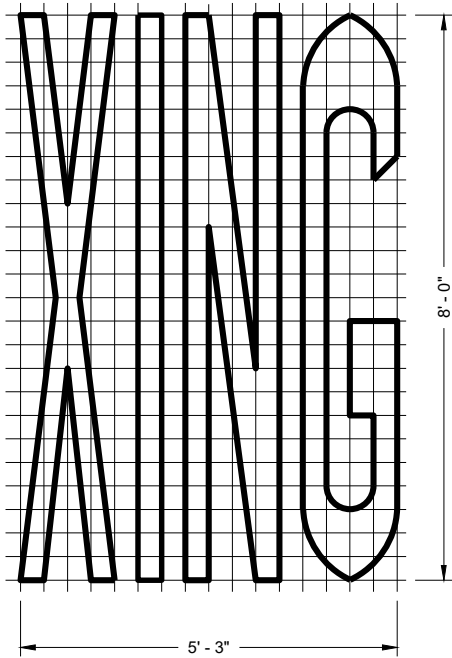
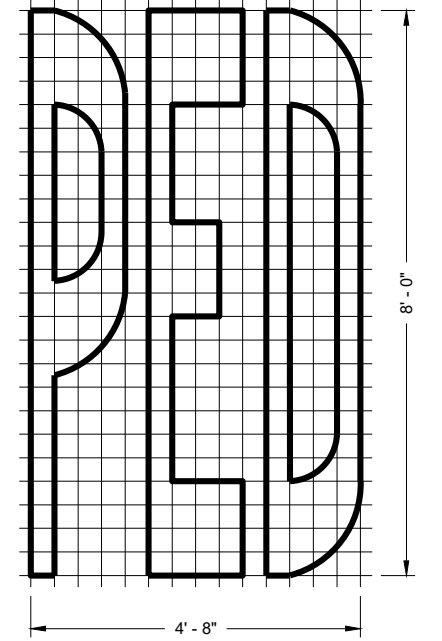
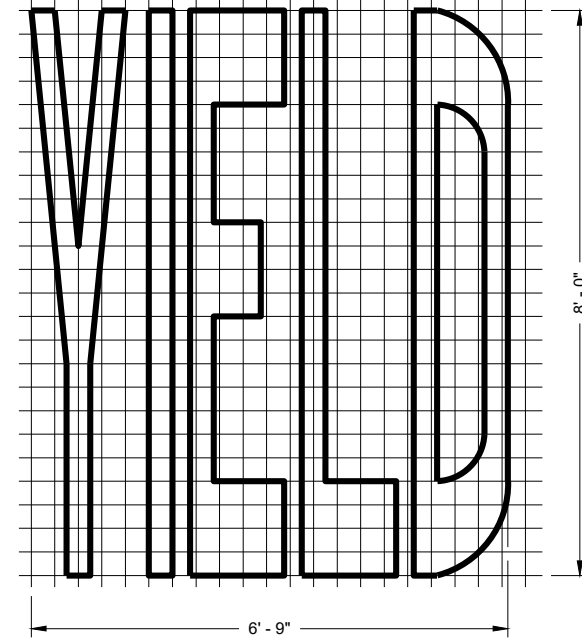
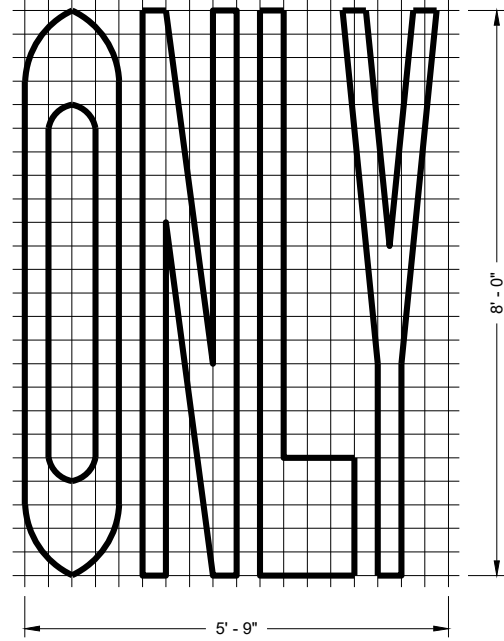
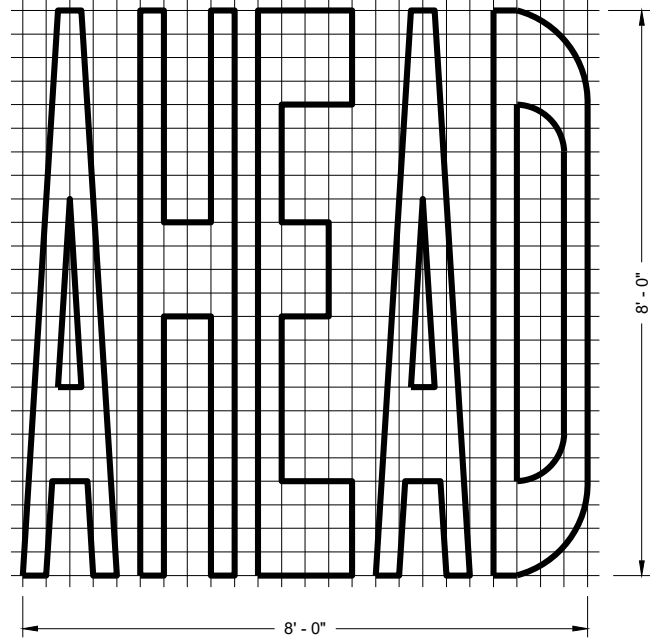
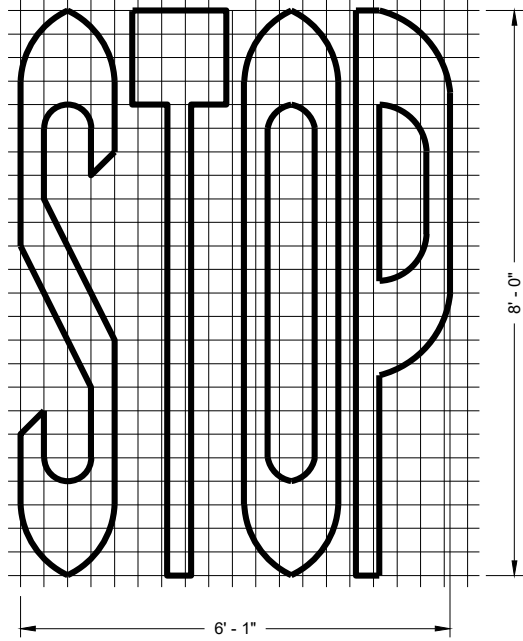


WIDTH ON SIGN TO BE APPROX. 1 - FOOT LESS THAN AVAILABLE WIDTH

ADVANCED WIDTH RESTRICTION SIGNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED	/S/ Andrew Heidtke
DATE	May 2023
FHWA	WORK ZONE ENGINEER



SINGLE LANE

TWO - LANE

GENERAL NOTES

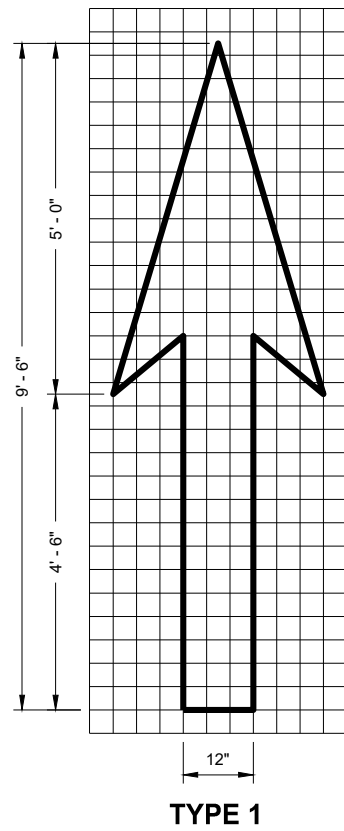
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING WORDS

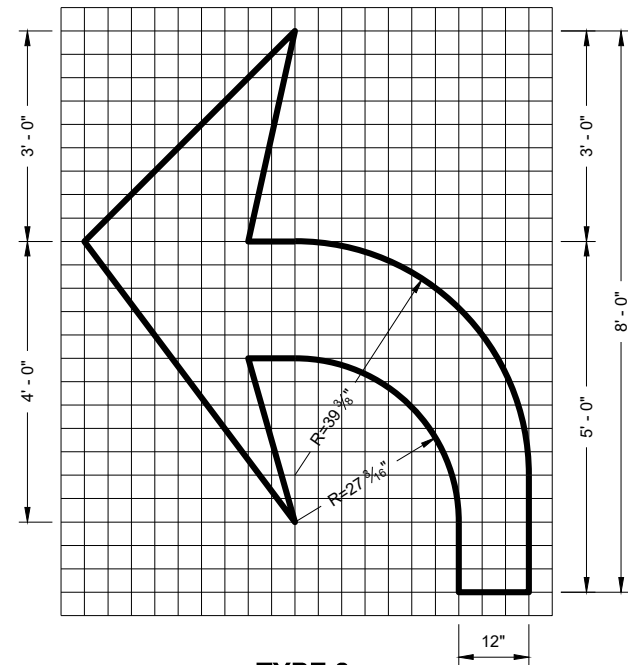
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

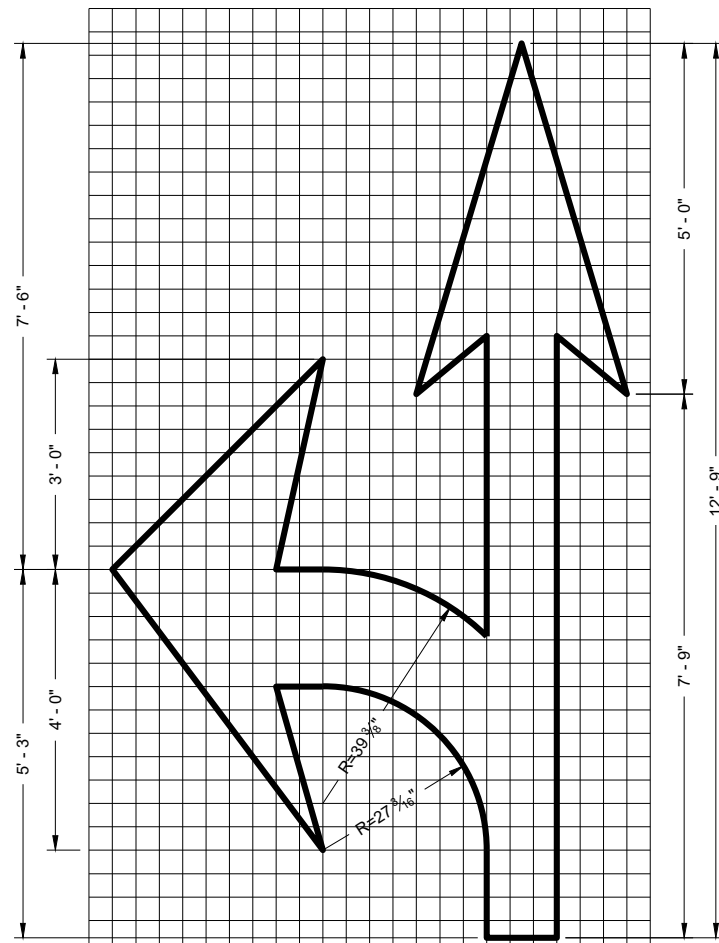
FHWA



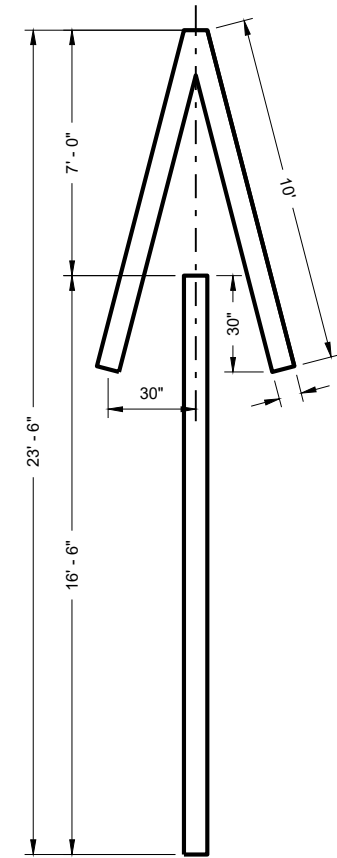
TYPE 1



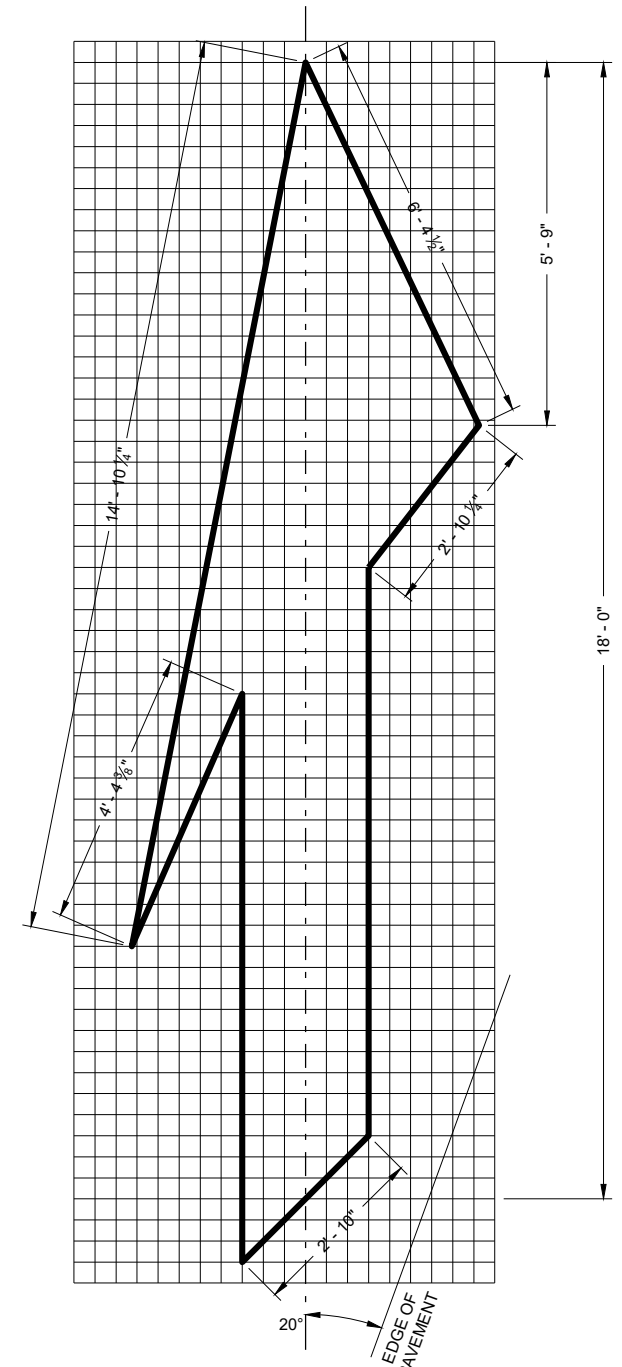
TYPE 2



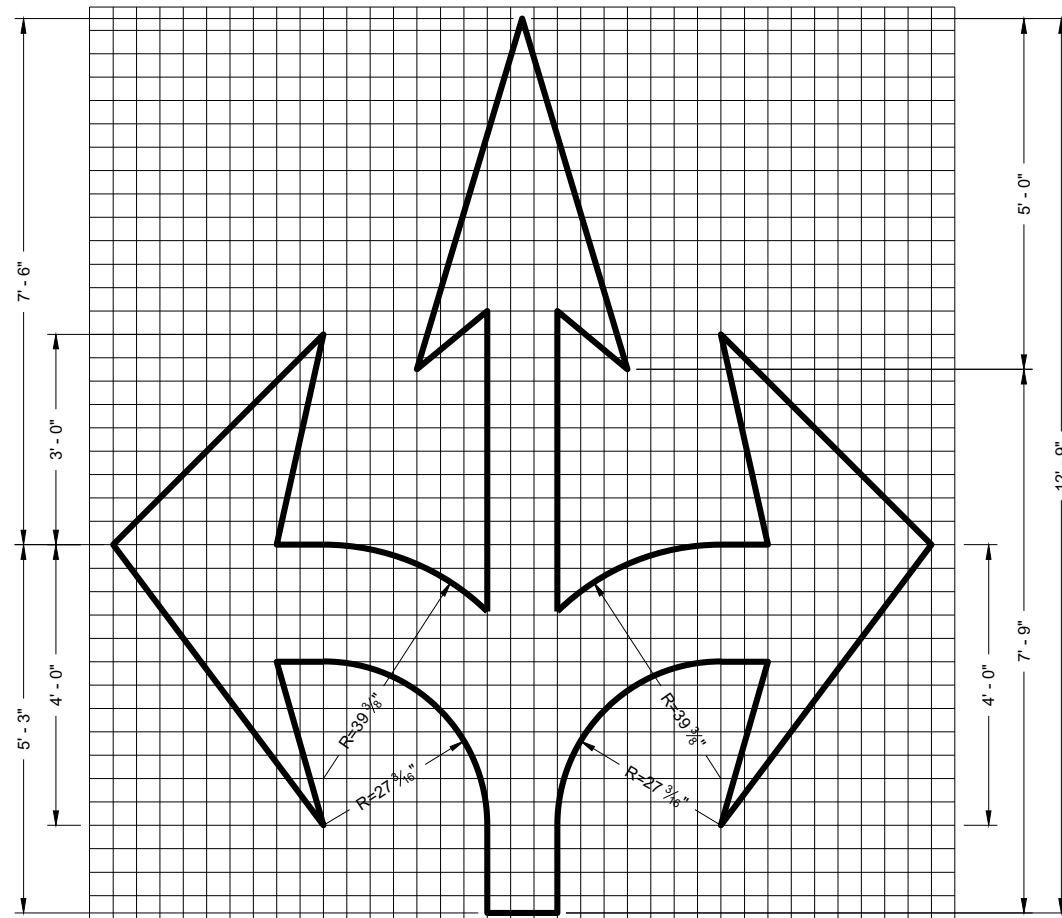
TYPE 3



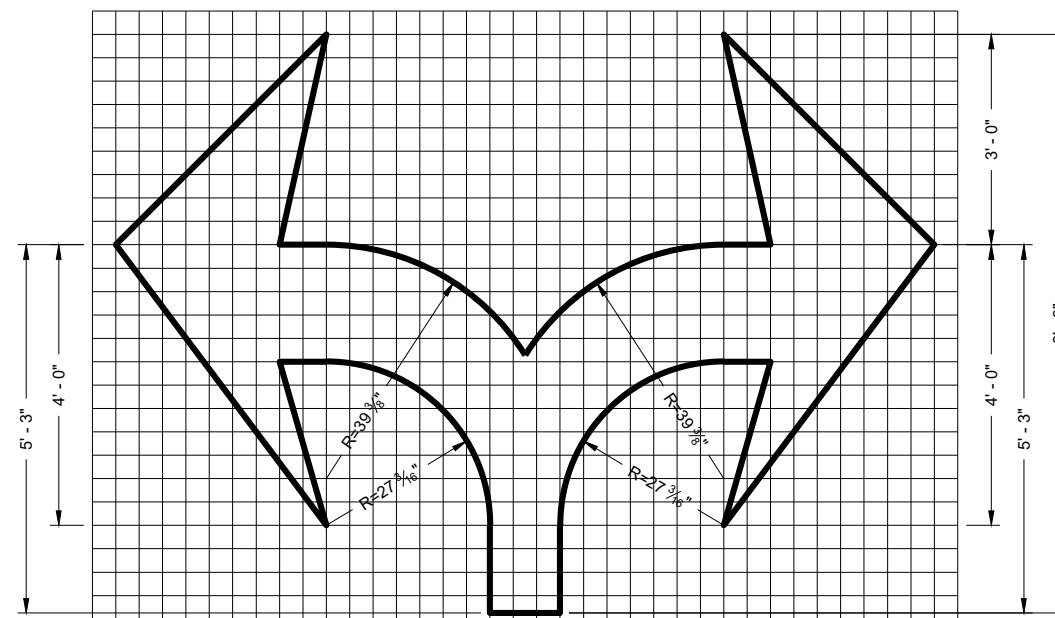
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

November 2019

DATE

FHWA




/s/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

GENERAL NOTES

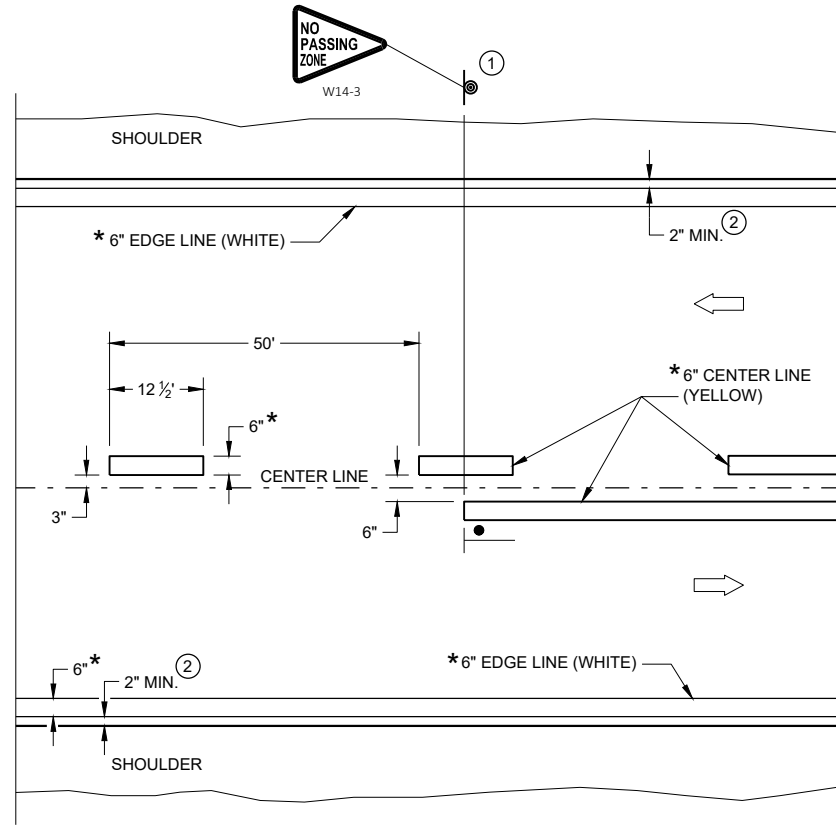
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

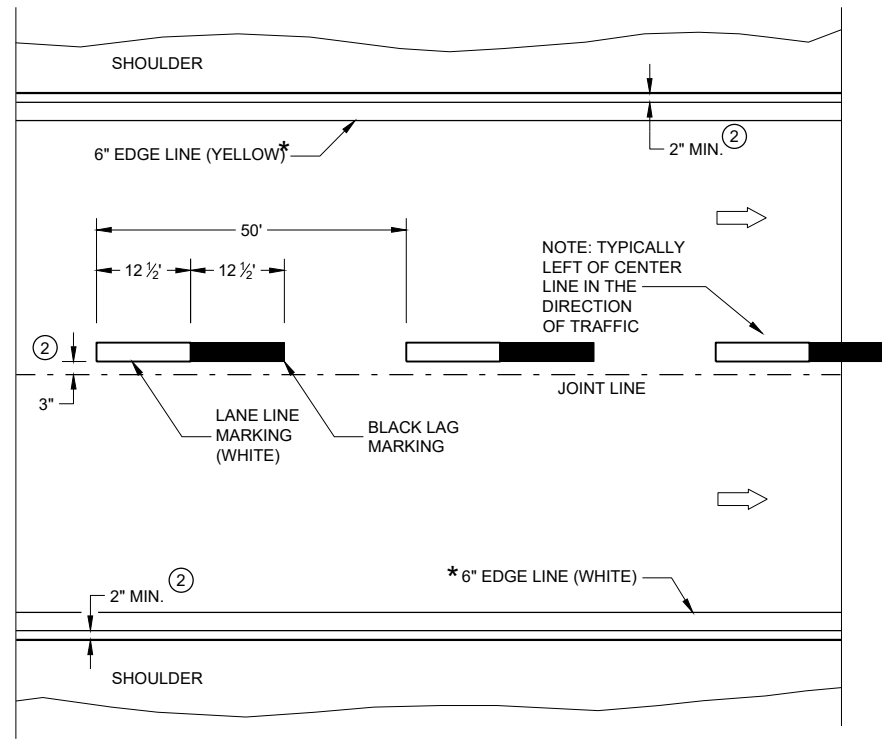
LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



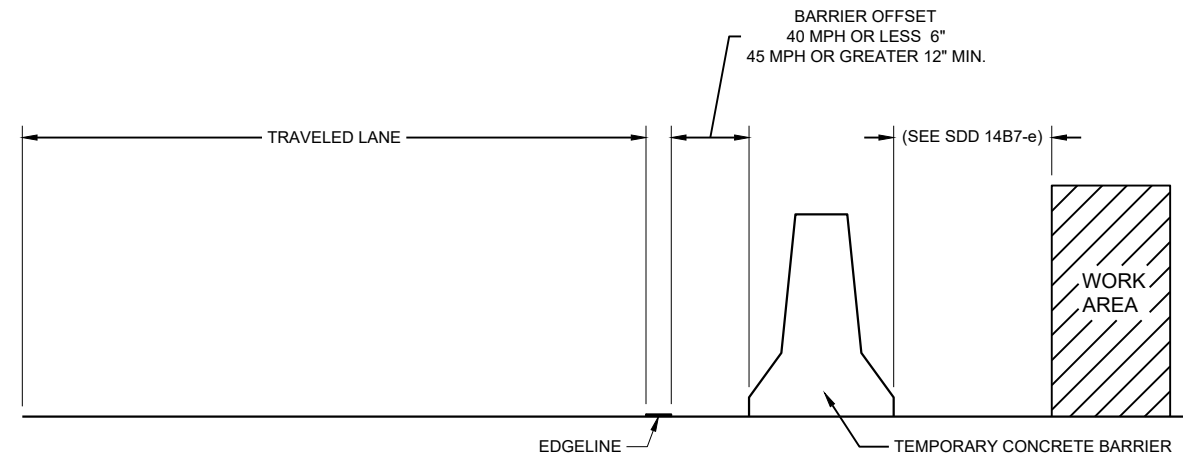
TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

PERMANENT LONGITUDINAL PAVEMENT MARKINGS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Jeannie Silver STATEWIDE SIGNING AND MARKING ENGINEER
FHWA	



TEMPORARY BARRIER OFFSET FROM EDGELINE

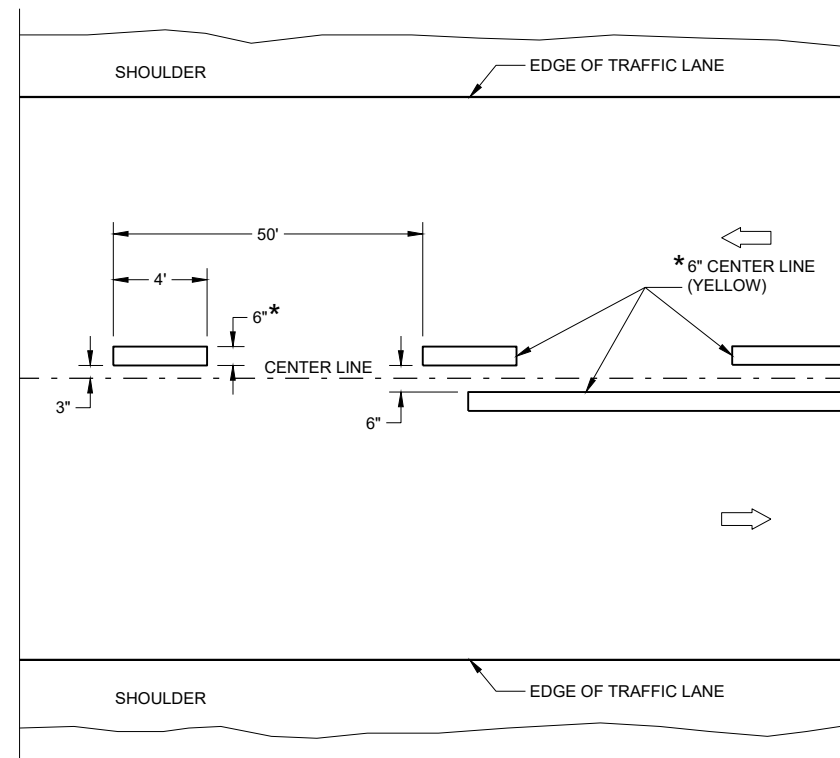
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

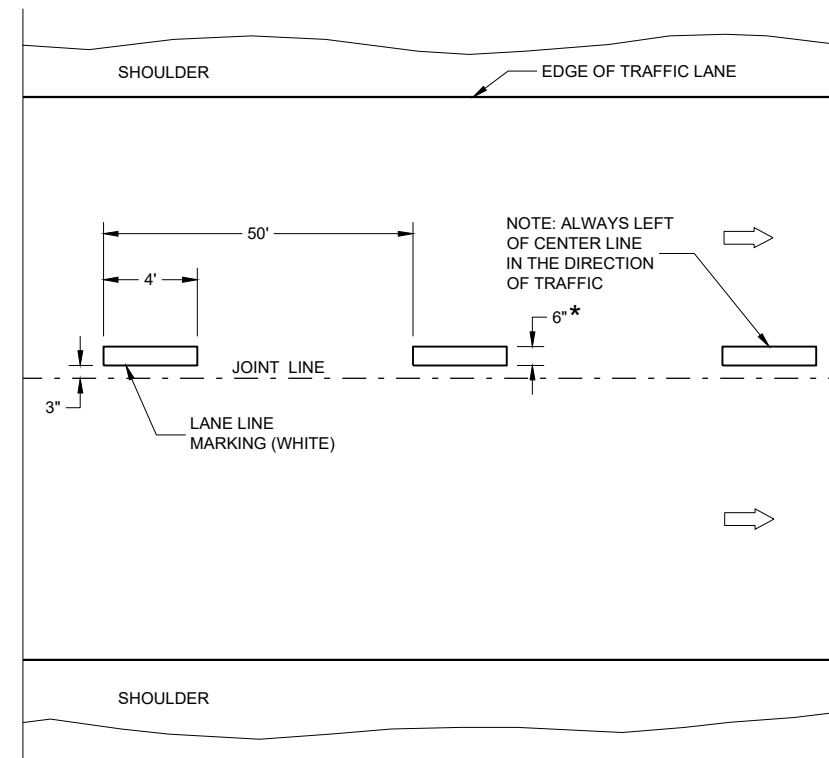
LEGEND

➡ DIRECTION OF TRAFFIC

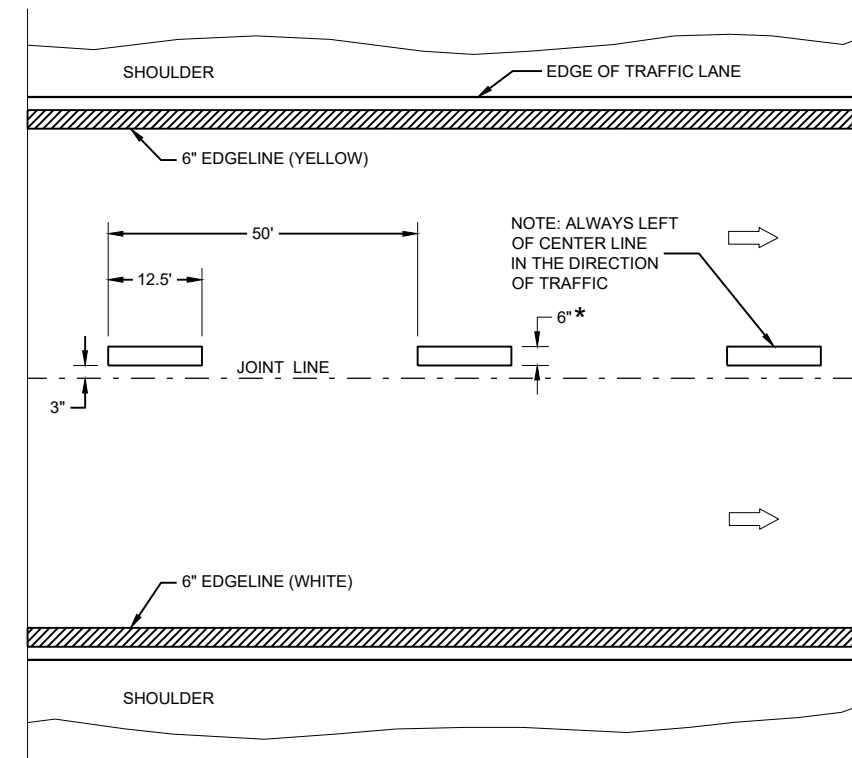
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2023 /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER

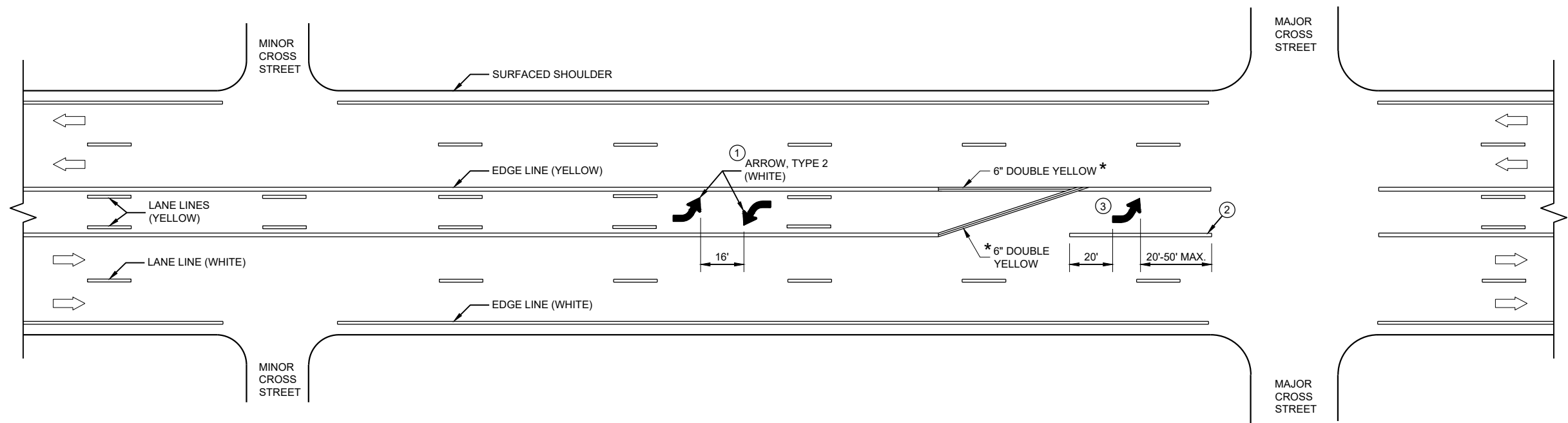
FHWA

GENERAL NOTES

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 10" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC

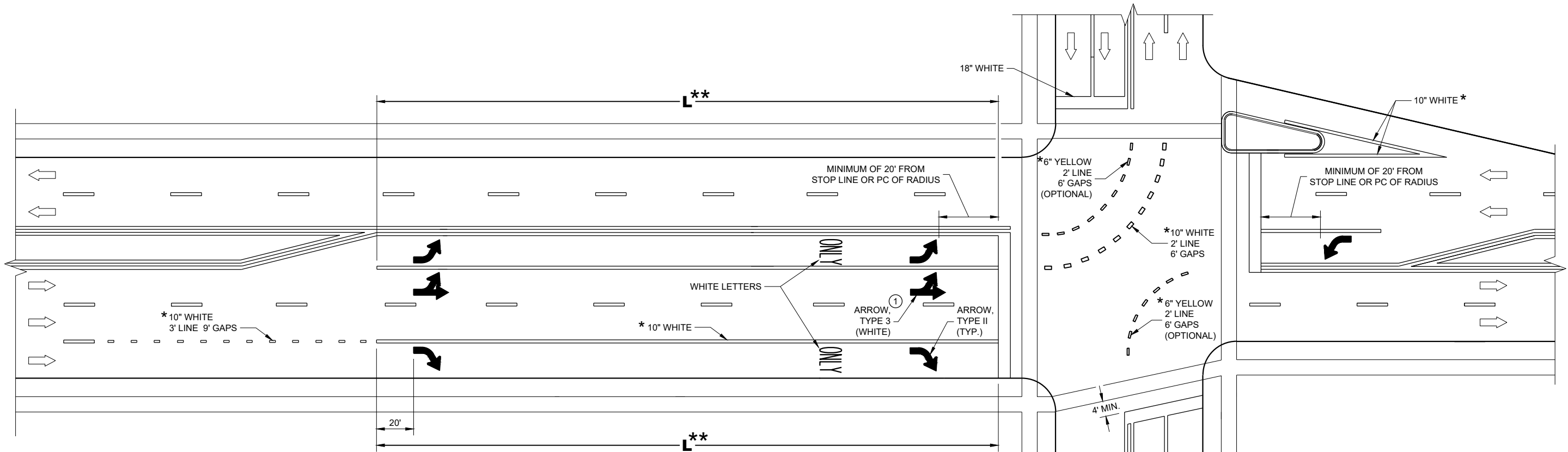
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY LEFT TURN LANE

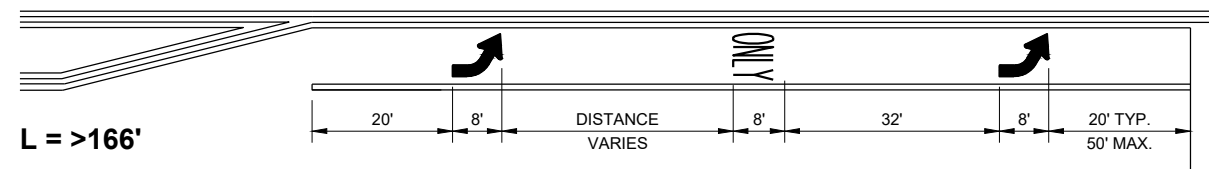
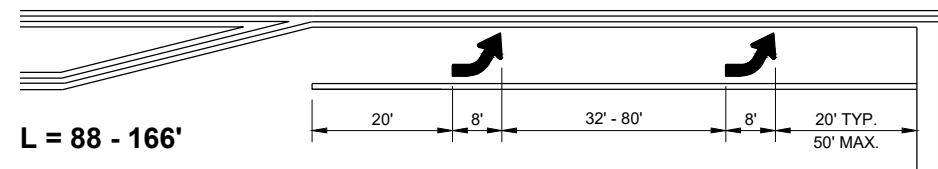
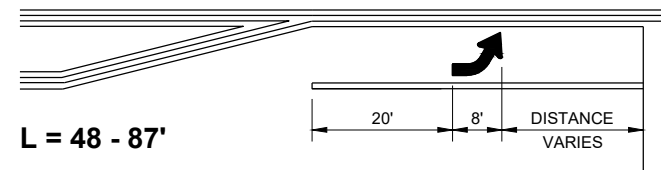
**PAVEMENT MARKING
(TURN LANES)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



TURN LANE OPTIONS

LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

① QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

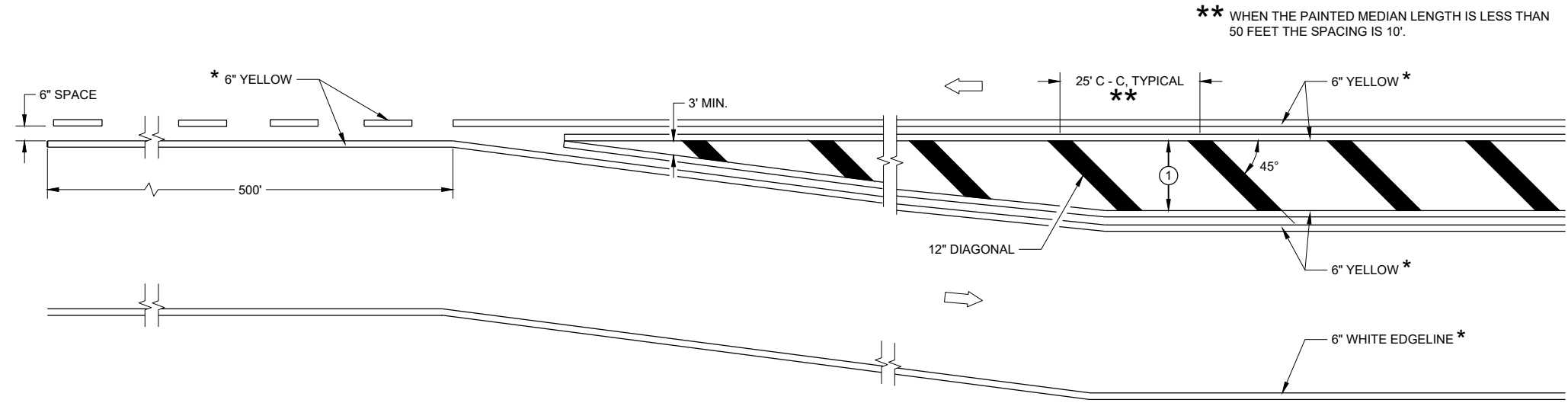
➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



MEDIAN ISLAND DETAIL

****** WHEN THE PAINTED MEDIAN LENGTH IS LESS THAN 50 FEET THE SPACING IS 10'.

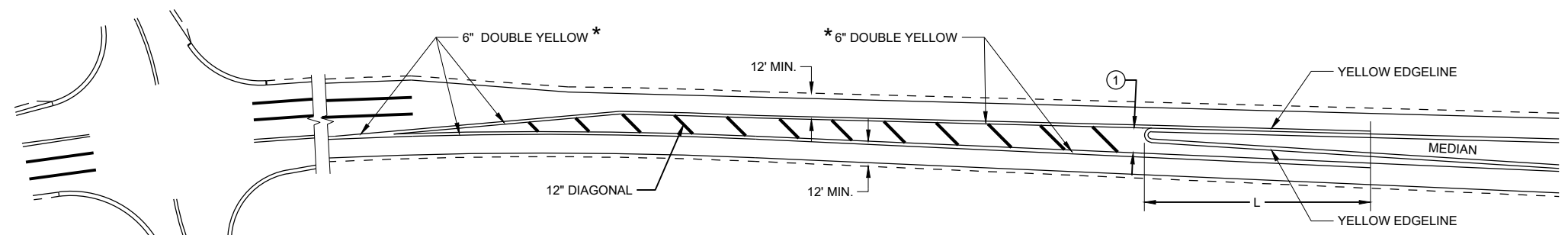
GENERAL NOTES

① DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT THE WIDEST POINT. OMIT DIAGONALS IF WIDTH IS LESS THAN 4 FEET.

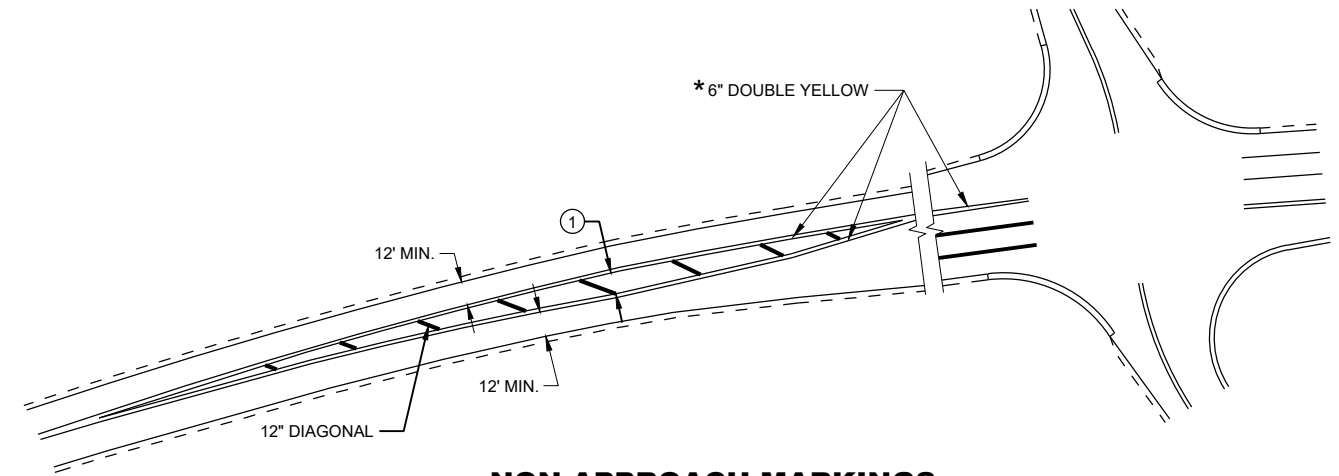
➔ DIRECTION OF TRAVEL

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

SPEED LIMIT	L
<35 MPH	5'
35> MPH	50'



APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON-APPROACH MARKINGS

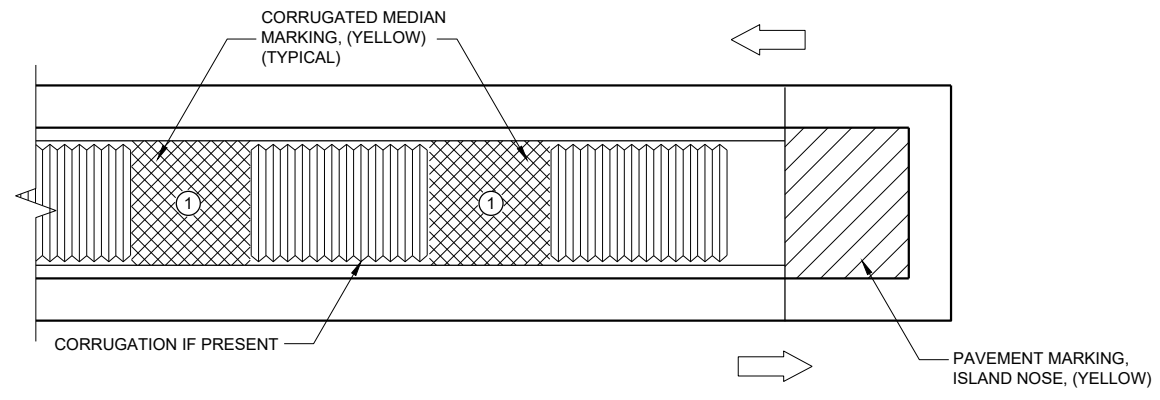
6

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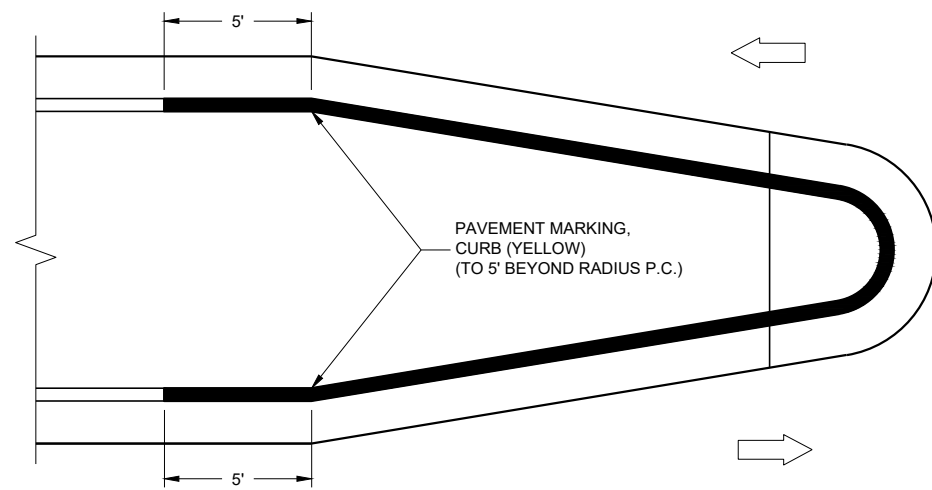
SDD 15C18-08a

SDD 15C18-08a

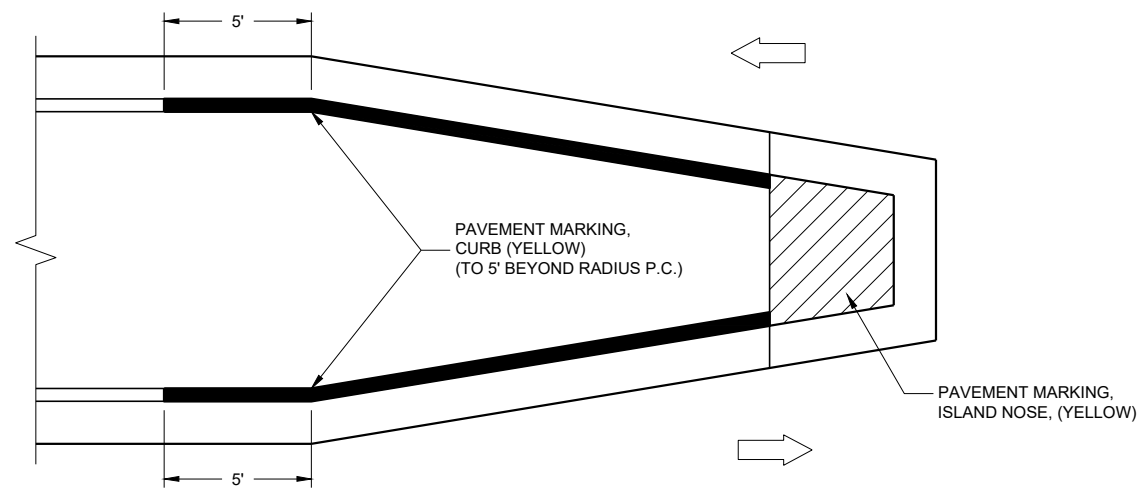
MEDIAN ISLAND PAVEMENT MARKINGS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Jeannie Silver STATE SIGNING AND MARKING ENGINEER
<small>FHWA</small>	



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE







MEDIAN ISLAND WITH SLOPED NOSE

TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

- ① APPLY PAVEMENT MARKING TO THE FLAT PORTION OF CORRUGATED MEDIAN.

-  ISLAND NOSE MARKING
-  CURB MARKING
-  CORRUGATED MEDIAN MARKING
-  DIRECTION OF TRAVEL

**PAVEMENT MARKINGS,
MEDIAN ISLAND NOSE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

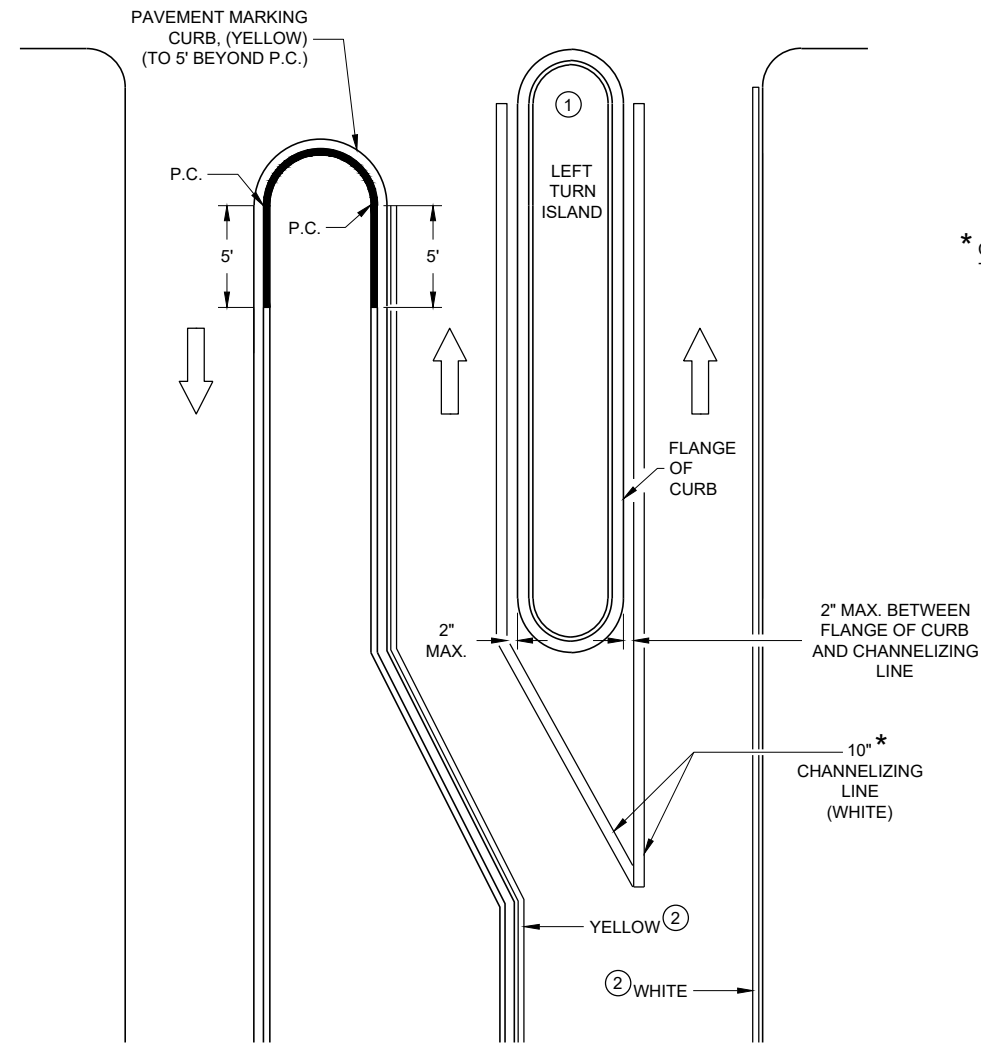
APPROVED
May 2023 /S/ Jeannie Silver
DATE STATE SIGNING AND MARKING
ENGINEER

REQUIREMENTS FOR EDGE LINES		
POSTED SPEED	IS THERE CONTINUOUS LIGHTING?	
	YES	NO
≤ 30 MPH	NO	OPTIONAL
35 OR 40 MPH	OPTIONAL	RECOMMENDED
≥ 45 MPH	RECOMMENDED	REQUIRED

GENERAL NOTES

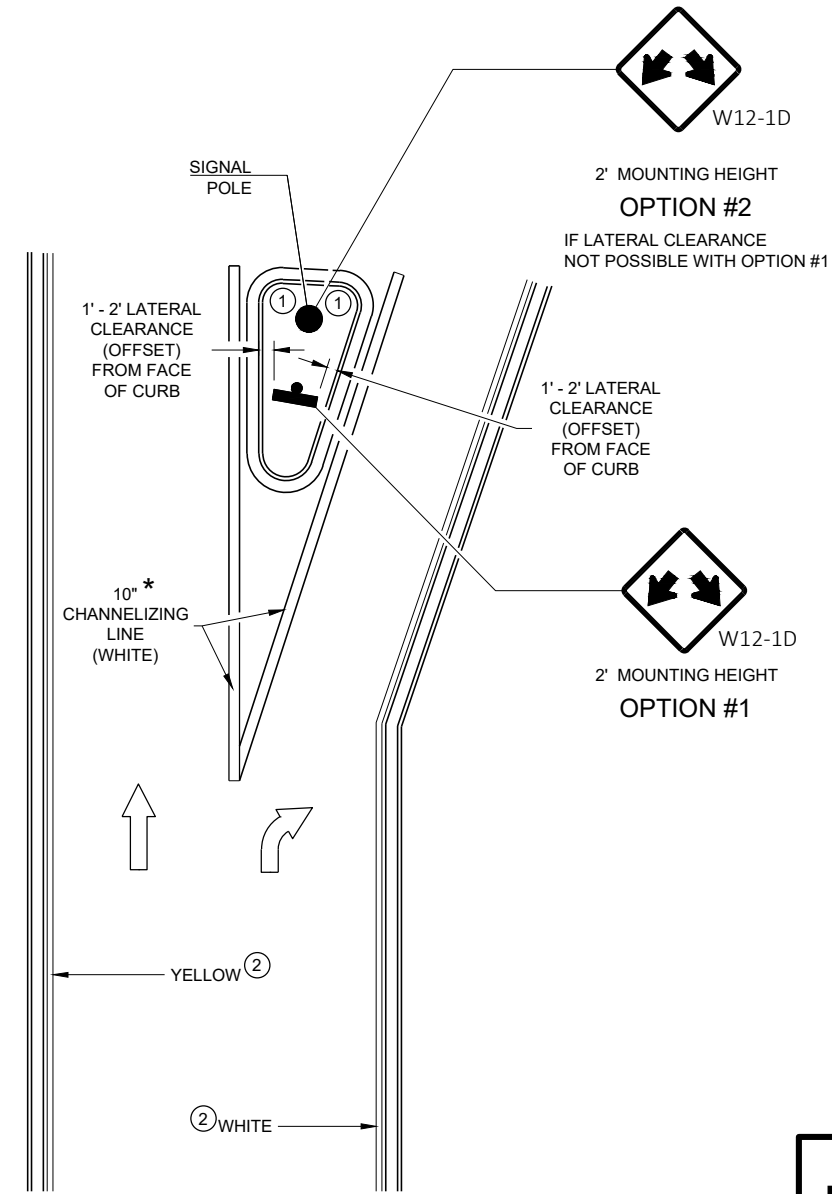
APPLIES TO ISLANDS AT LEFT TURNS AT ONE WAY ROADWAYS AS WELL.
SEE MISCELLANEOUS QUANTITIES FOR SIGN SIZE.

- ① MARK CURB NOSES YELLOW.
- ② MARK ACCORDING TO TABLE.



* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

LEFT TURN & MEDIAN ISLAND



RIGHT TURN ISLAND

6

6

SDD 15C18-08C

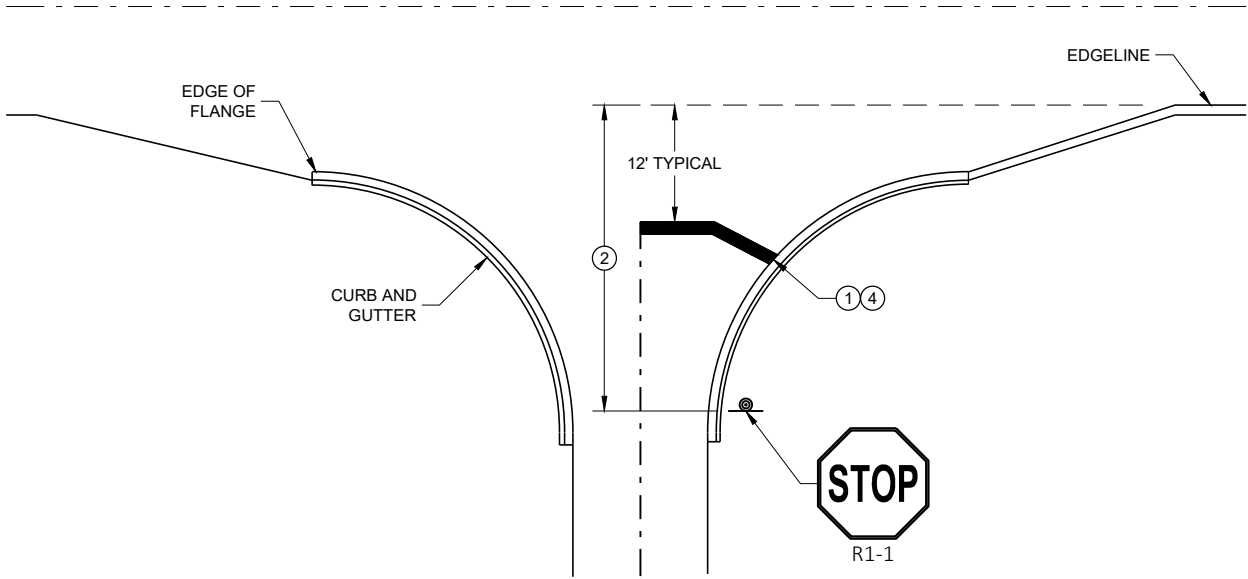
SDD 15C18-08C

MEDIAN PAVEMENT MARKINGS, DOUBLE ARROW WARNING SIGN PLACEMENT	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Jeannie Silver STATE SIGNING AND MARKING ENGINEER
FHWA	

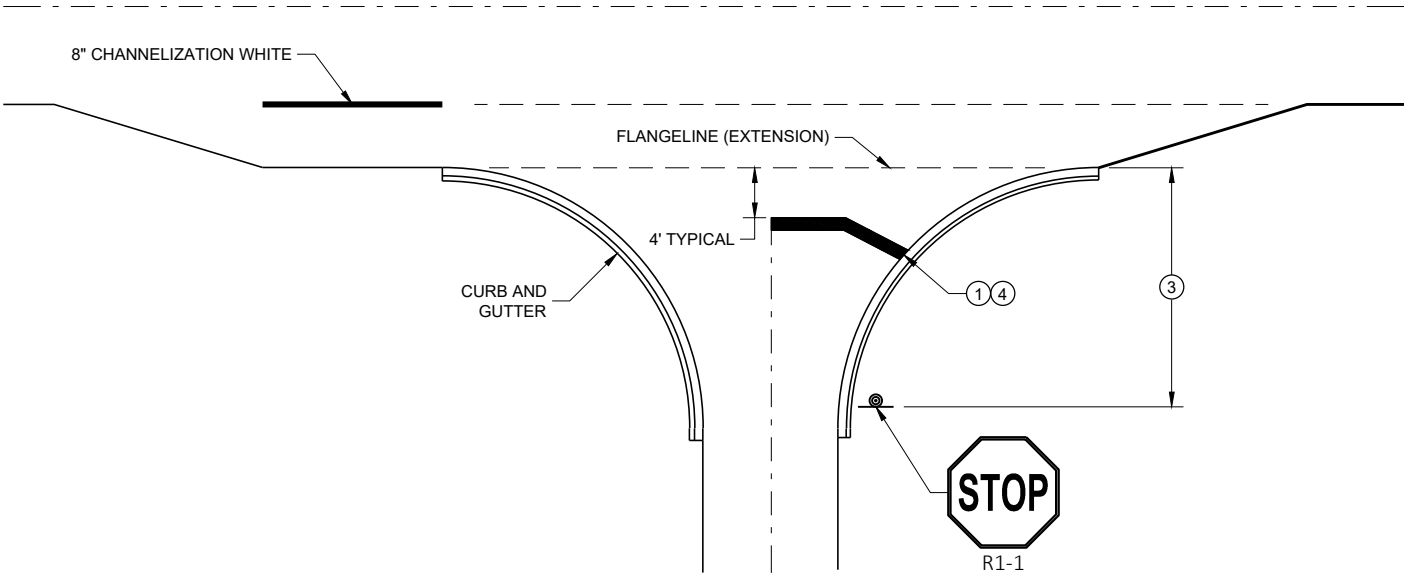
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

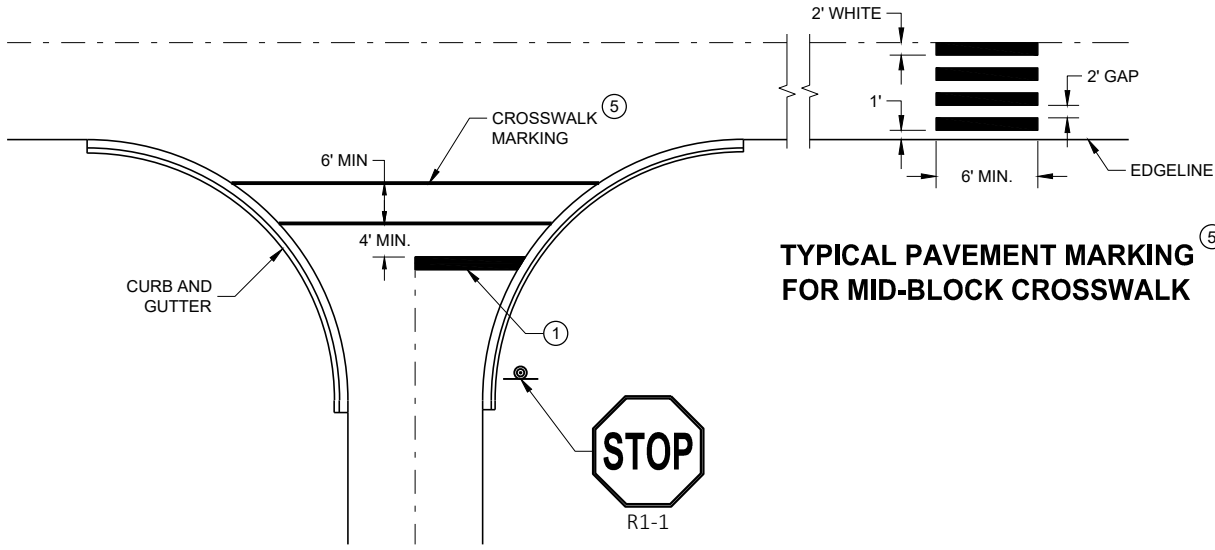
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



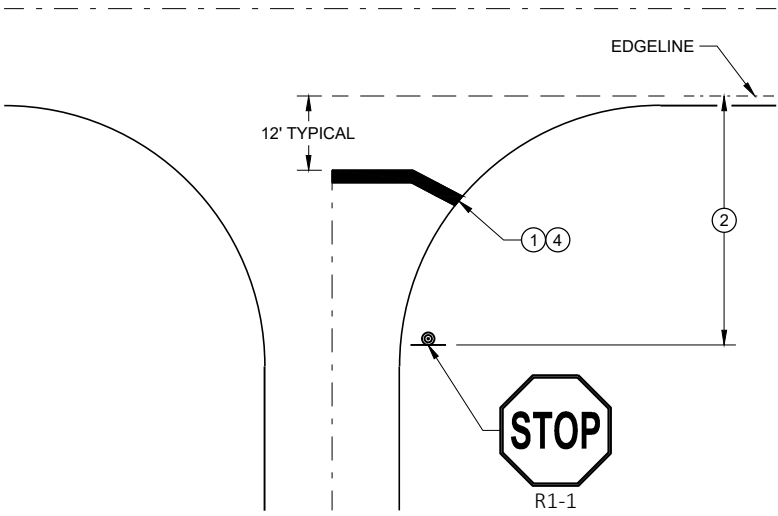
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

6

6

SDD 15C33 - 04

SDD 15C33 - 04










STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  FLASHING ARROW BOARD
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
-  WORK AREA

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45MPH, USE SDD 15D12.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

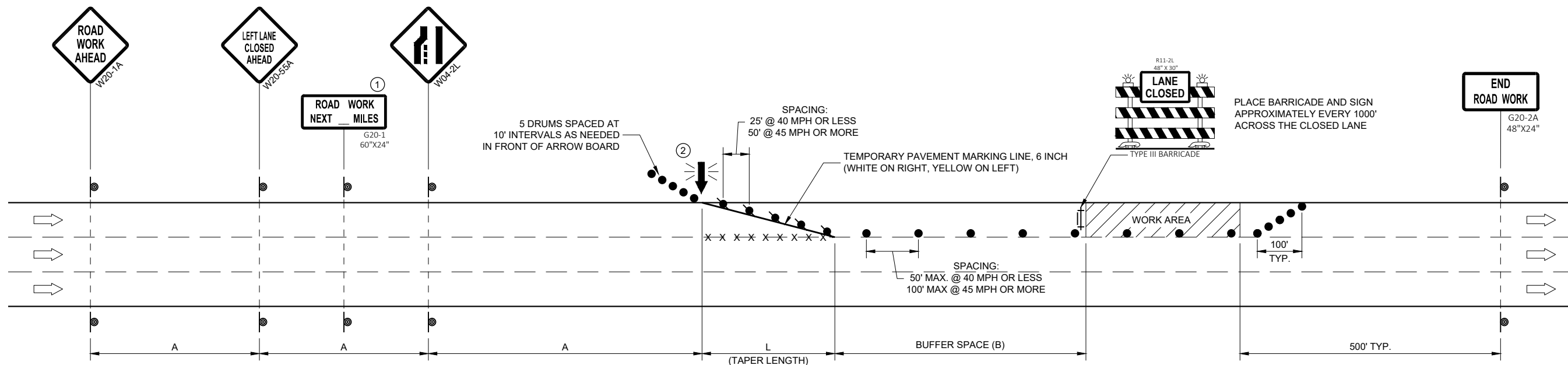
CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- ② WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO PLACE THE ARROW BOARD AS SHOWN, PLACE THE ARROW BOARD AT THE END OF THE TAPER.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'

TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA










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SDD 15D20-07a

SDD 15D20-07a

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  FLASHING ARROW BOARD
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
-  WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

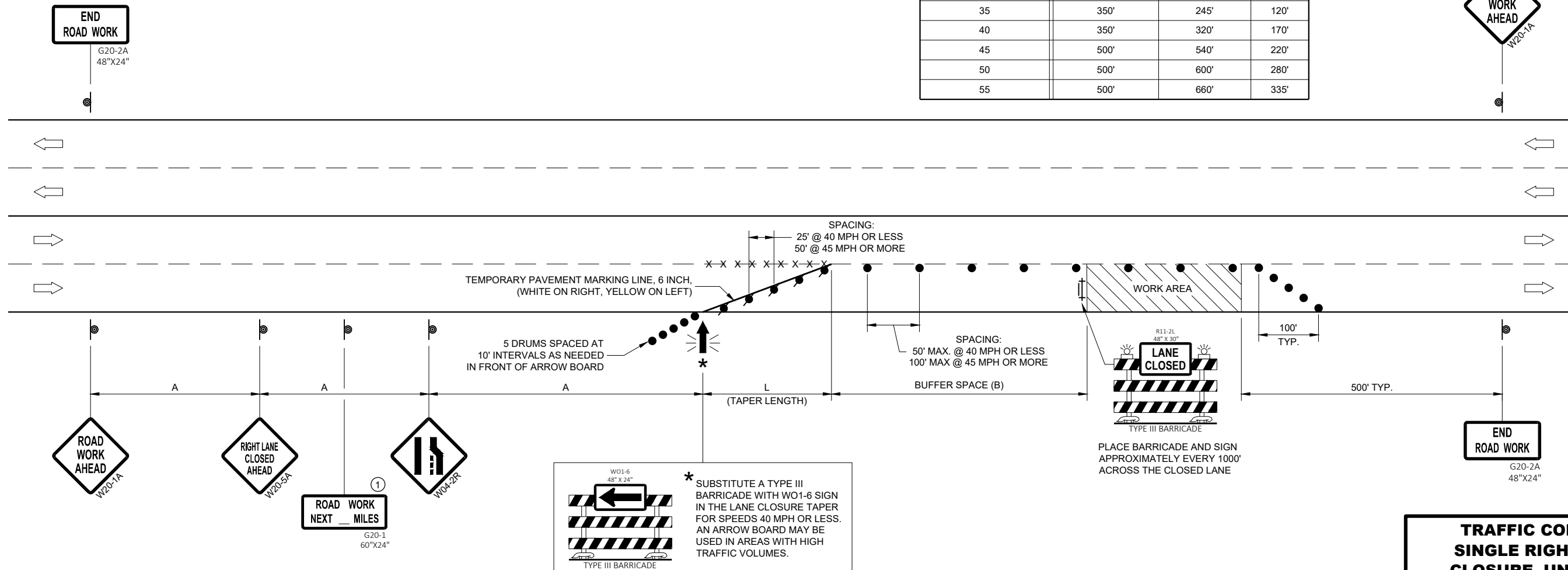
① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'



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SDD 15D20-07b

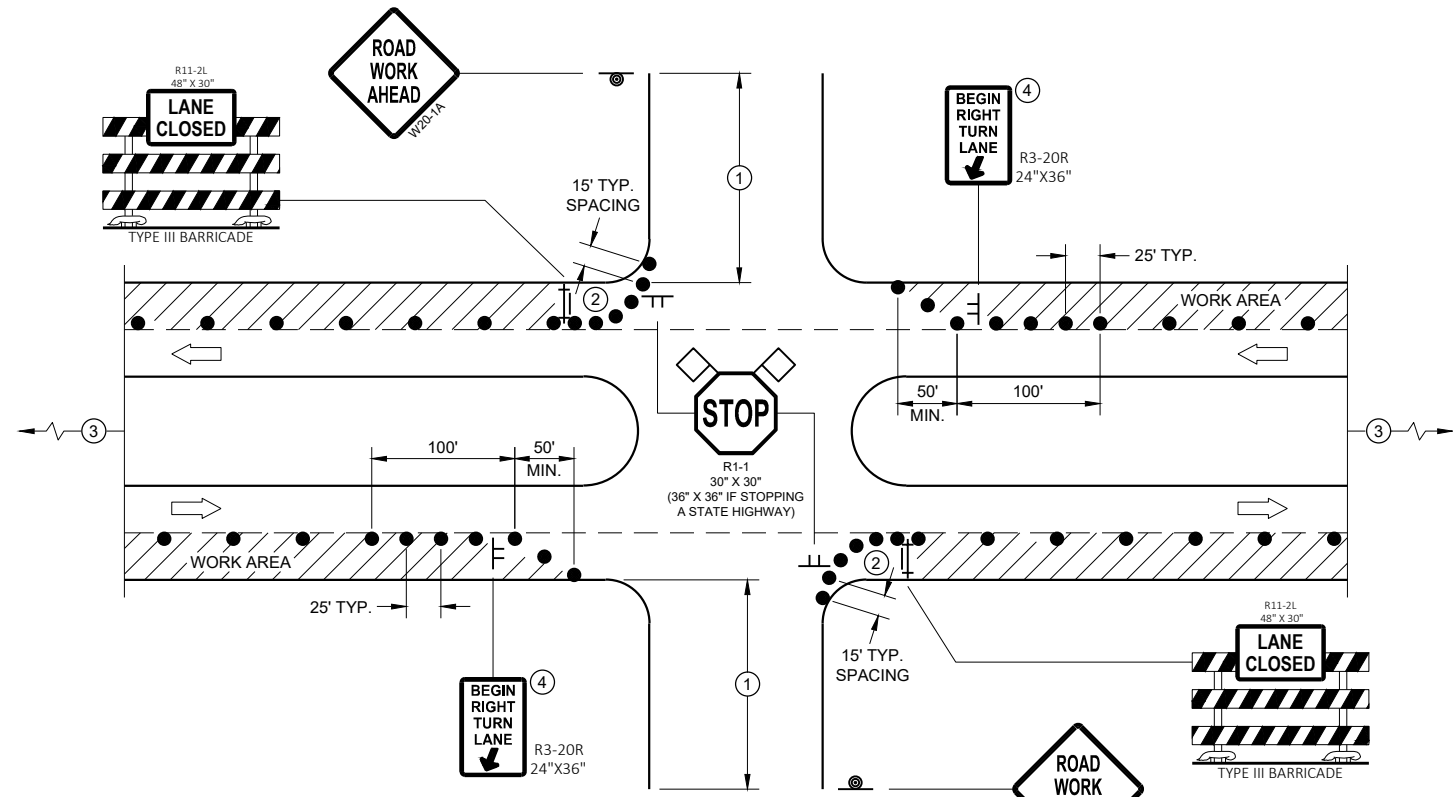
SDD 15D20-07b

**TRAFFIC CONTROL,
SINGLE RIGHT LANE
CLOSURE, UNDIVIDED
NON-FREEWAY/EXPRESSWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

FOR RIGHT LANE CLOSURE AT INTERSECTION

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

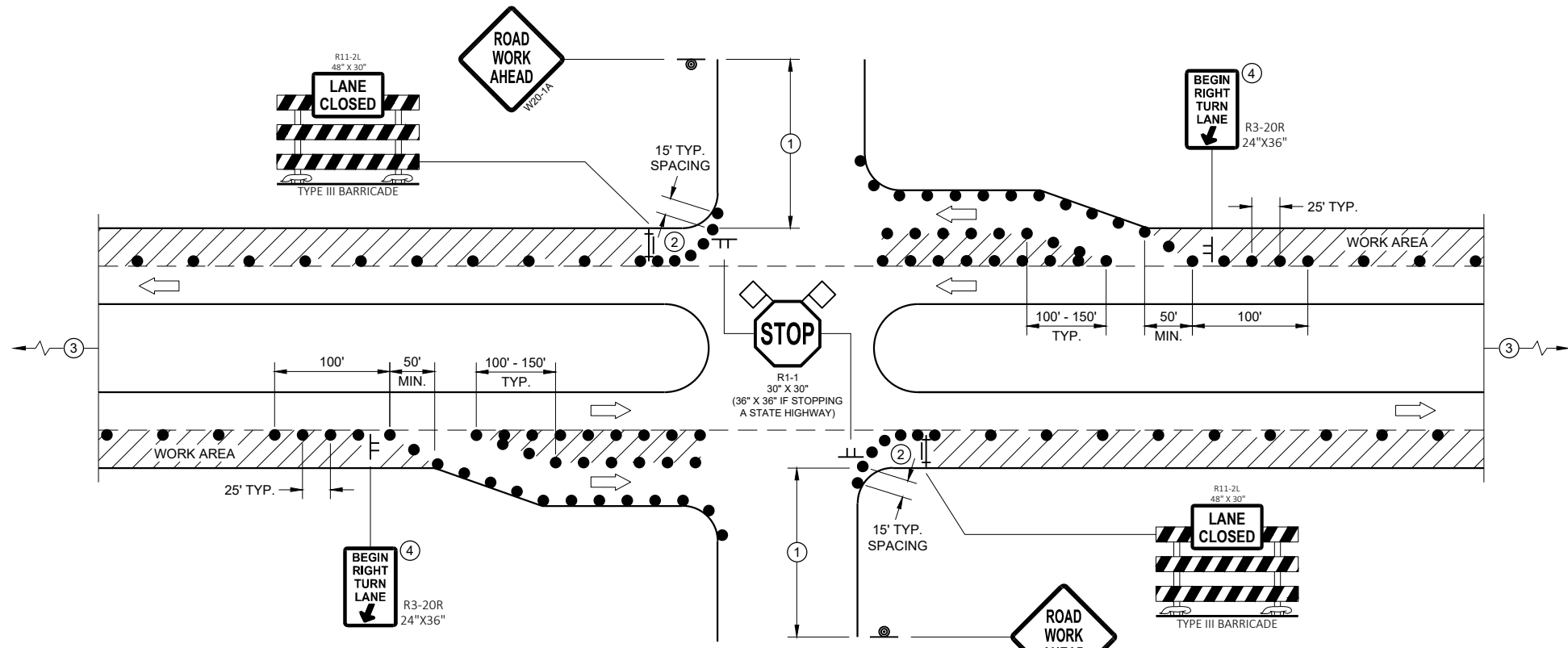
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

- ① 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.
- ② ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.
- ④ MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.



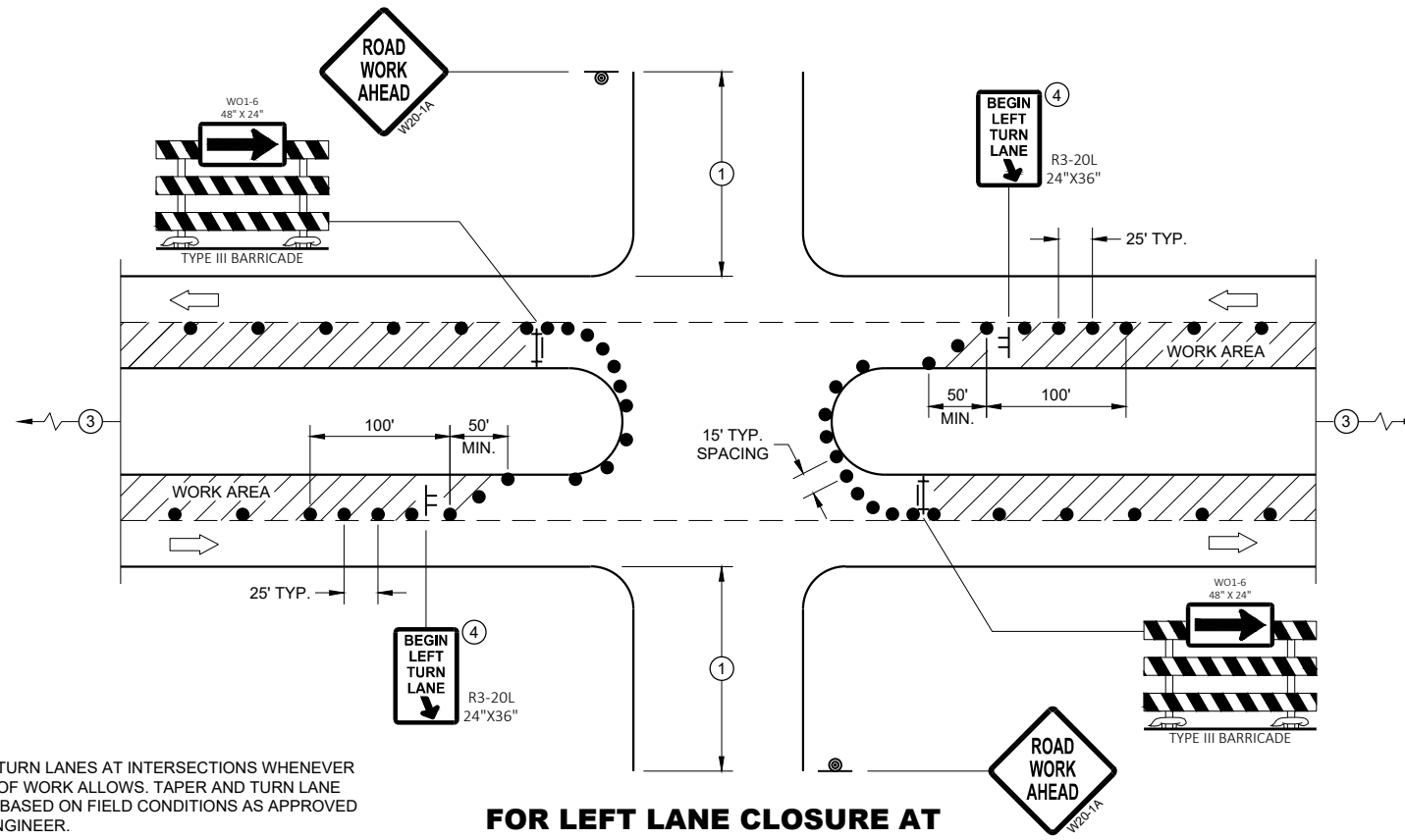
FOR RIGHT LANE CLOSURE AT INTERSECTION (WITH RIGHT TURN BAY OPEN)

LEGEND

- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

**TRAFFIC CONTROL,
INTERSECTION WITHIN SINGLE
RIGHT LANE CLOSURE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

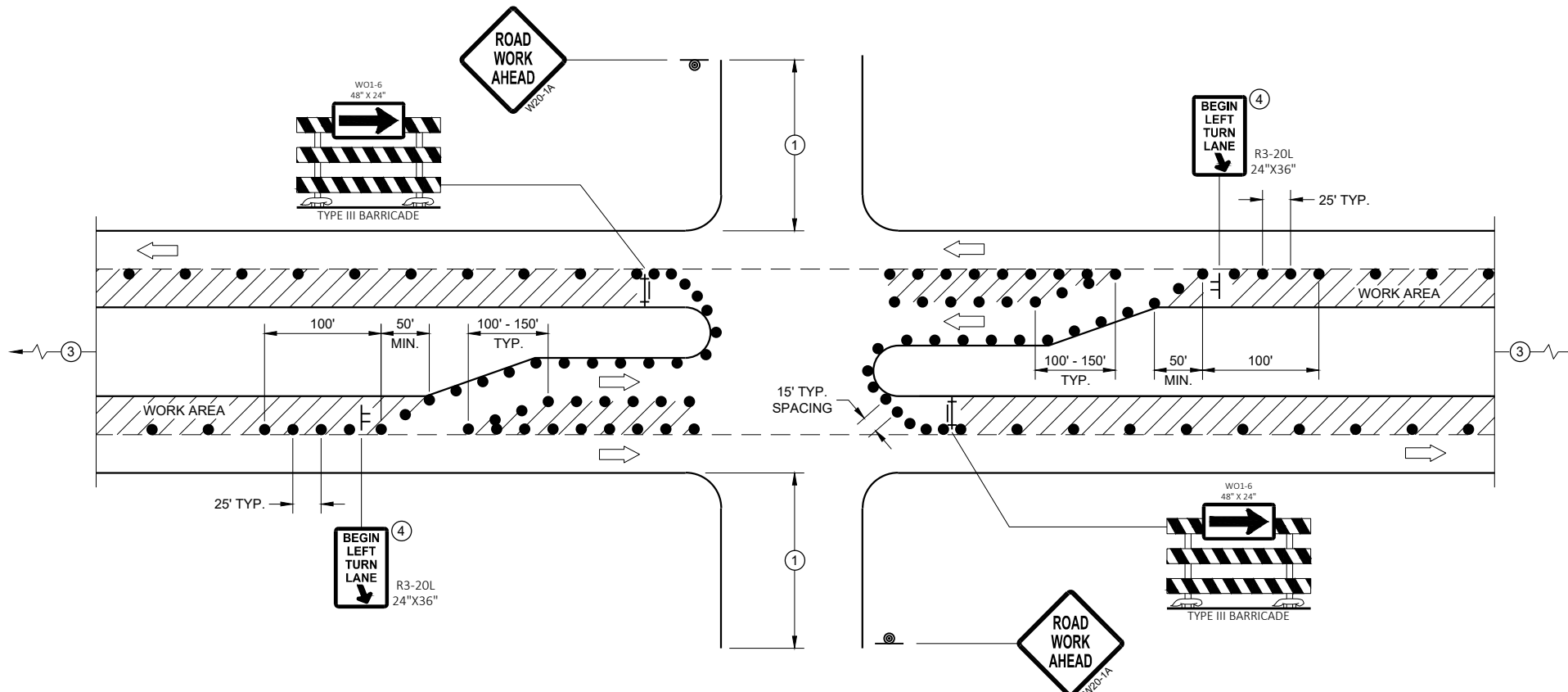
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL REMAIN IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON PORTABLE SUPPORTS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

- ① 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35 - 40 MPH.
200' IF 25 - 30 MPH.
- ② ALSO USE BARRICADE AND 15 FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.
- ④ MINIMUM MOUNTING HEIGHT OF 5 FEET FROM EDGE OF PAVEMENT (AT EDGE LINE LOCATION) TO BOTTOM OF SIGN.



FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING (WITH LEFT TURN BAY OPEN)

LEGEND

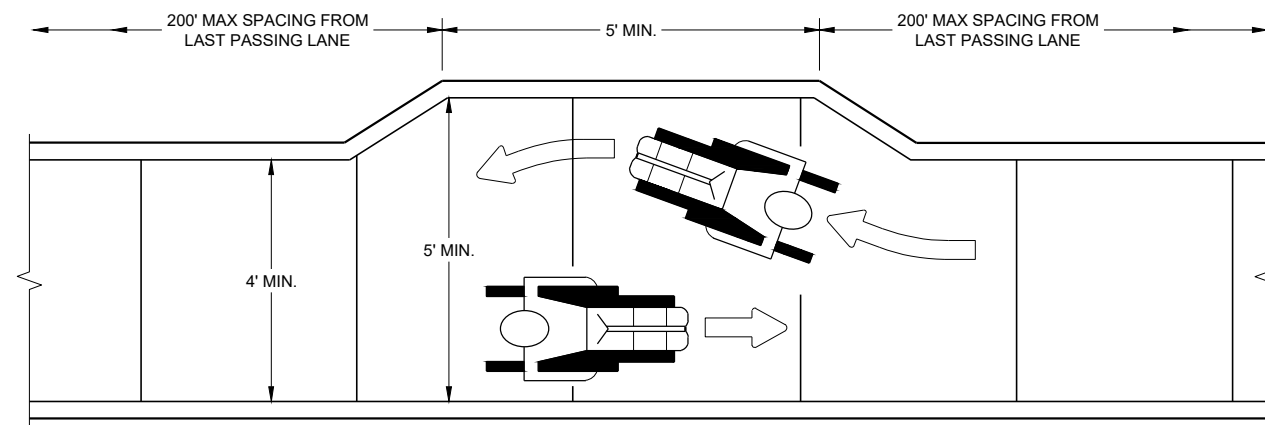
- SIGN ON TEMPORARY SUPPORT
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., ORANGE
- WORK AREA

**TRAFFIC CONTROL,
INTERSECTION WITHIN SINGLE
LEFT LANE CLOSURE**

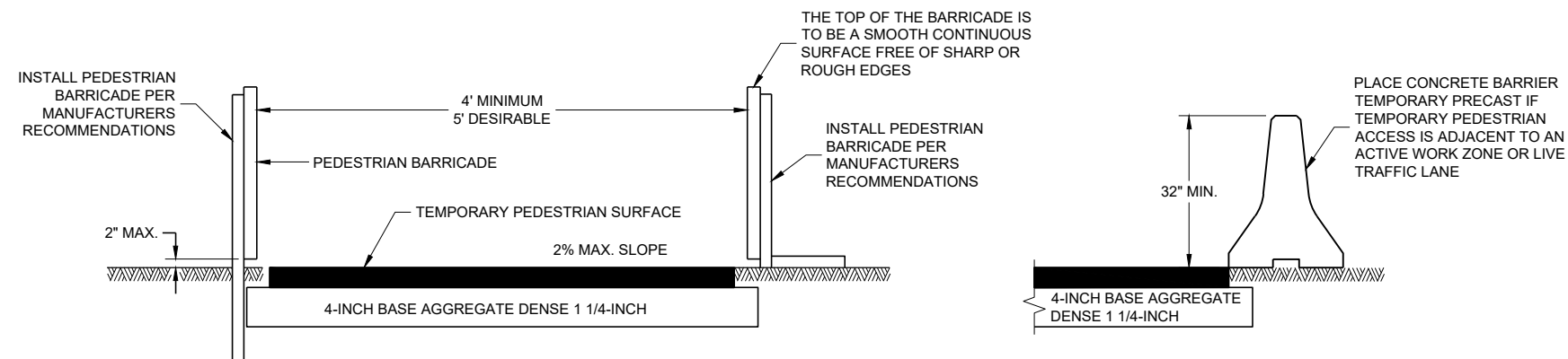
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



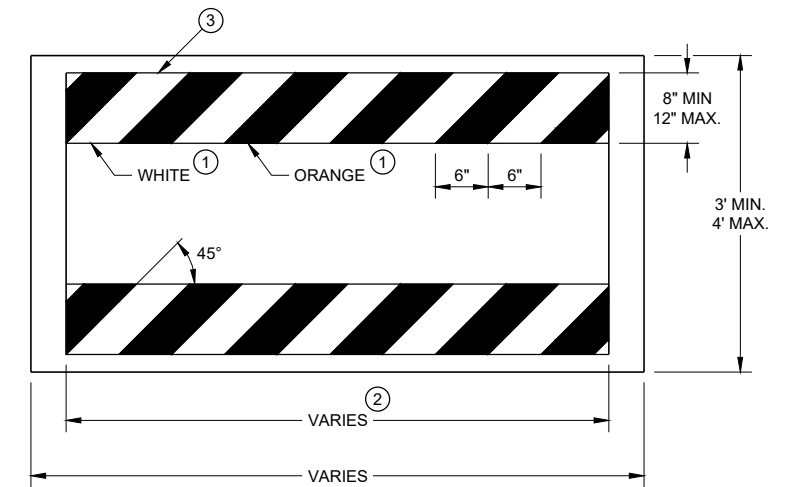
NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

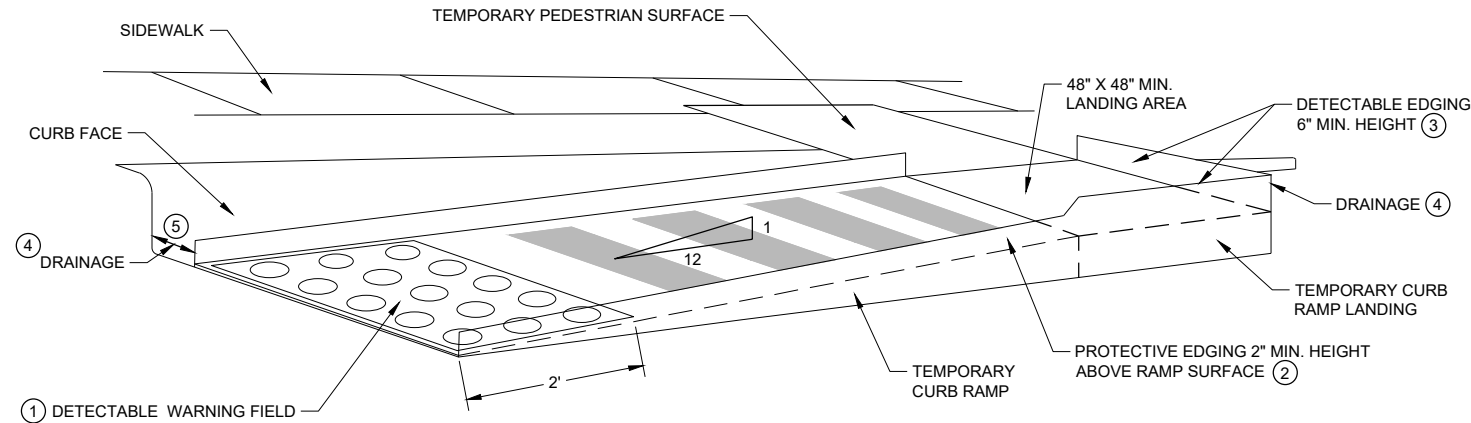


TEMPORARY PEDESTRIAN BARRICADE*

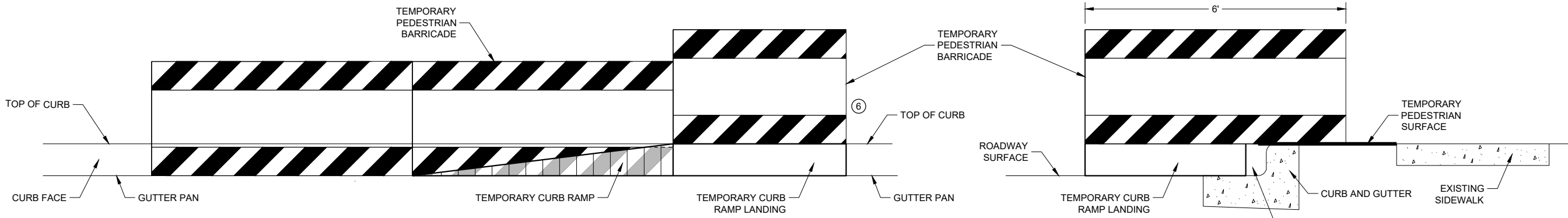
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW

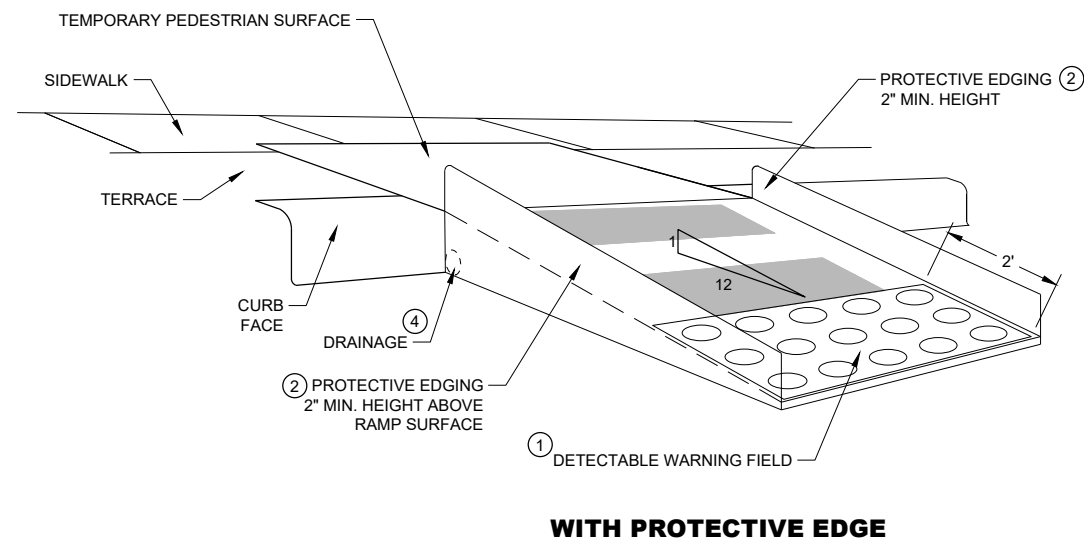
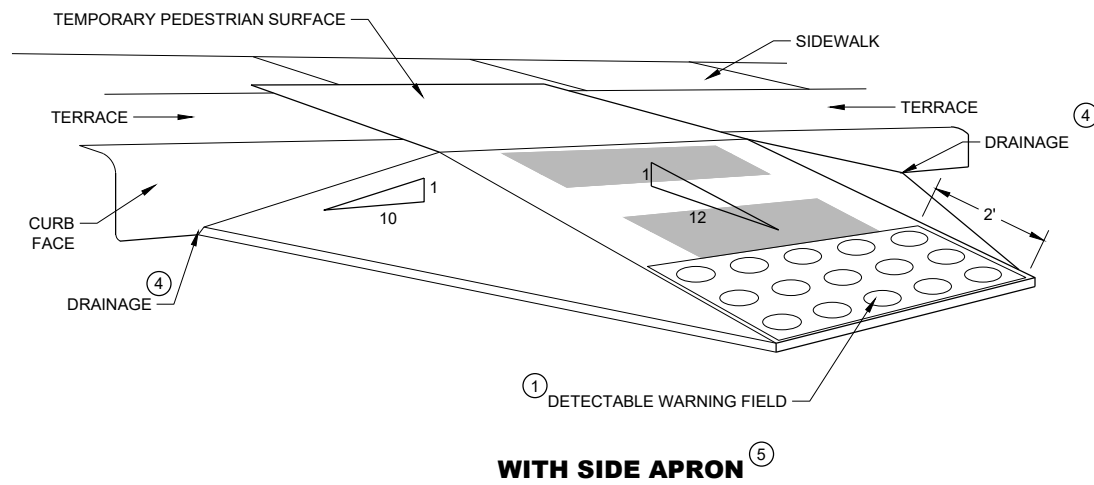


FRONT VIEW

SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

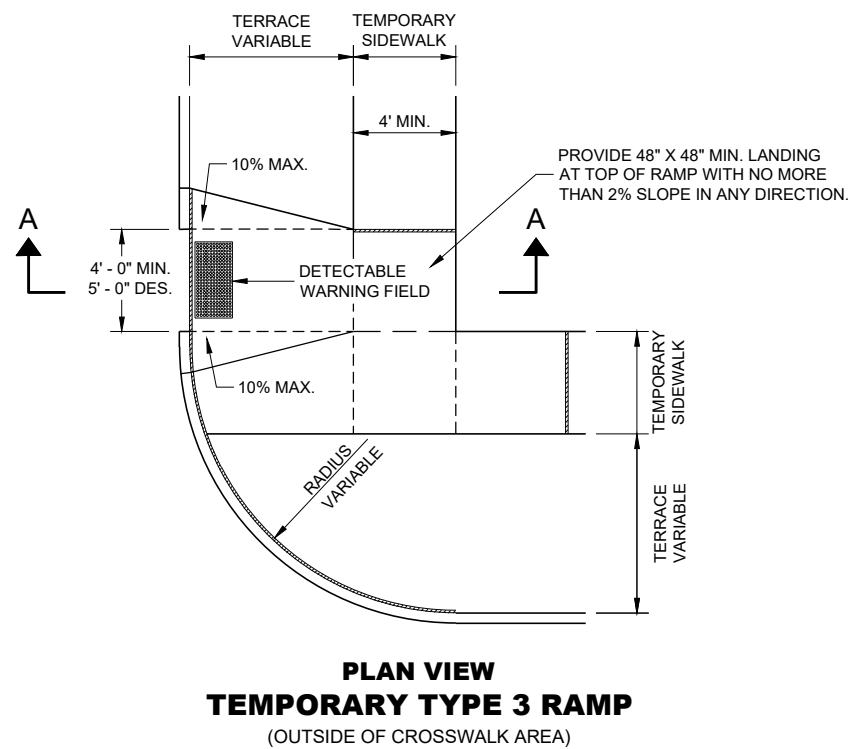
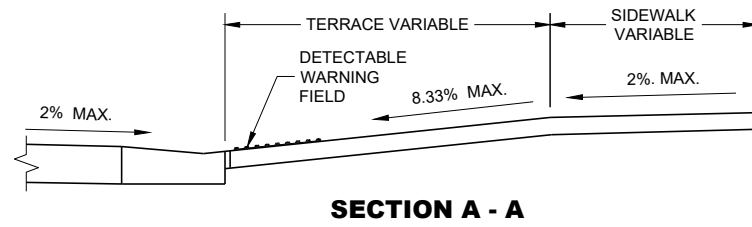
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- (1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- (2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- (3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- (4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- (5) CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



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





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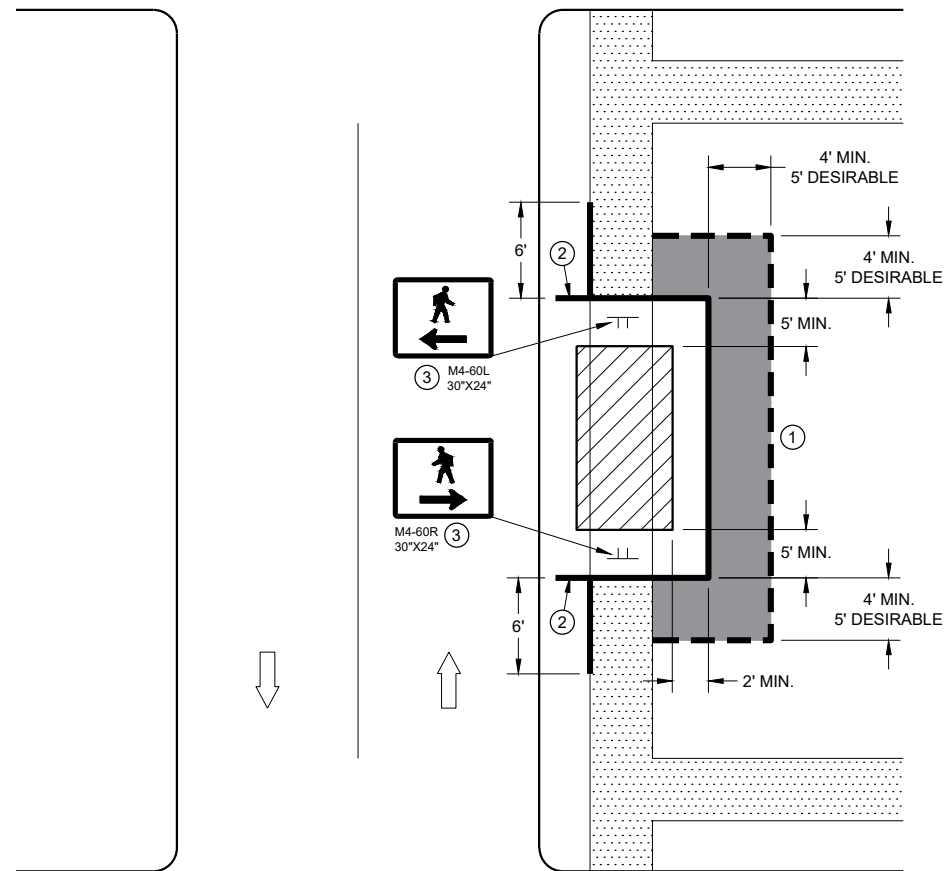
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC










GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
 - ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



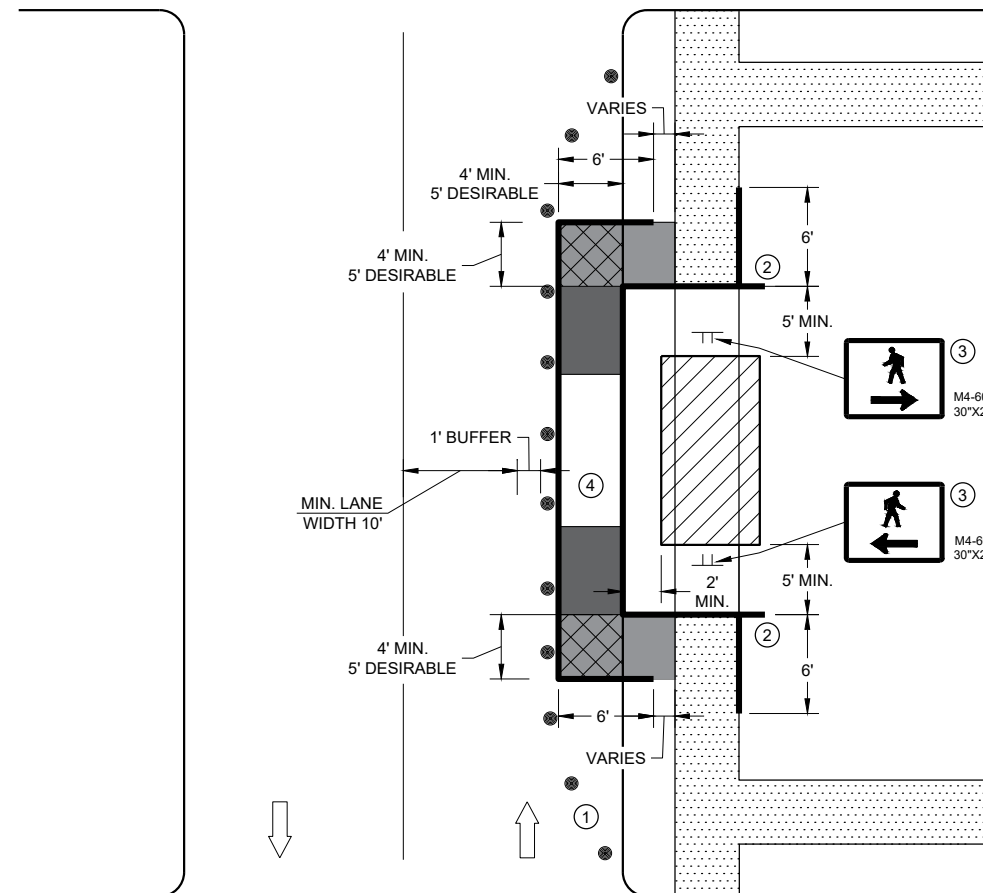
**SIDEWALK DIVERSION
SINGLE SIDE**

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

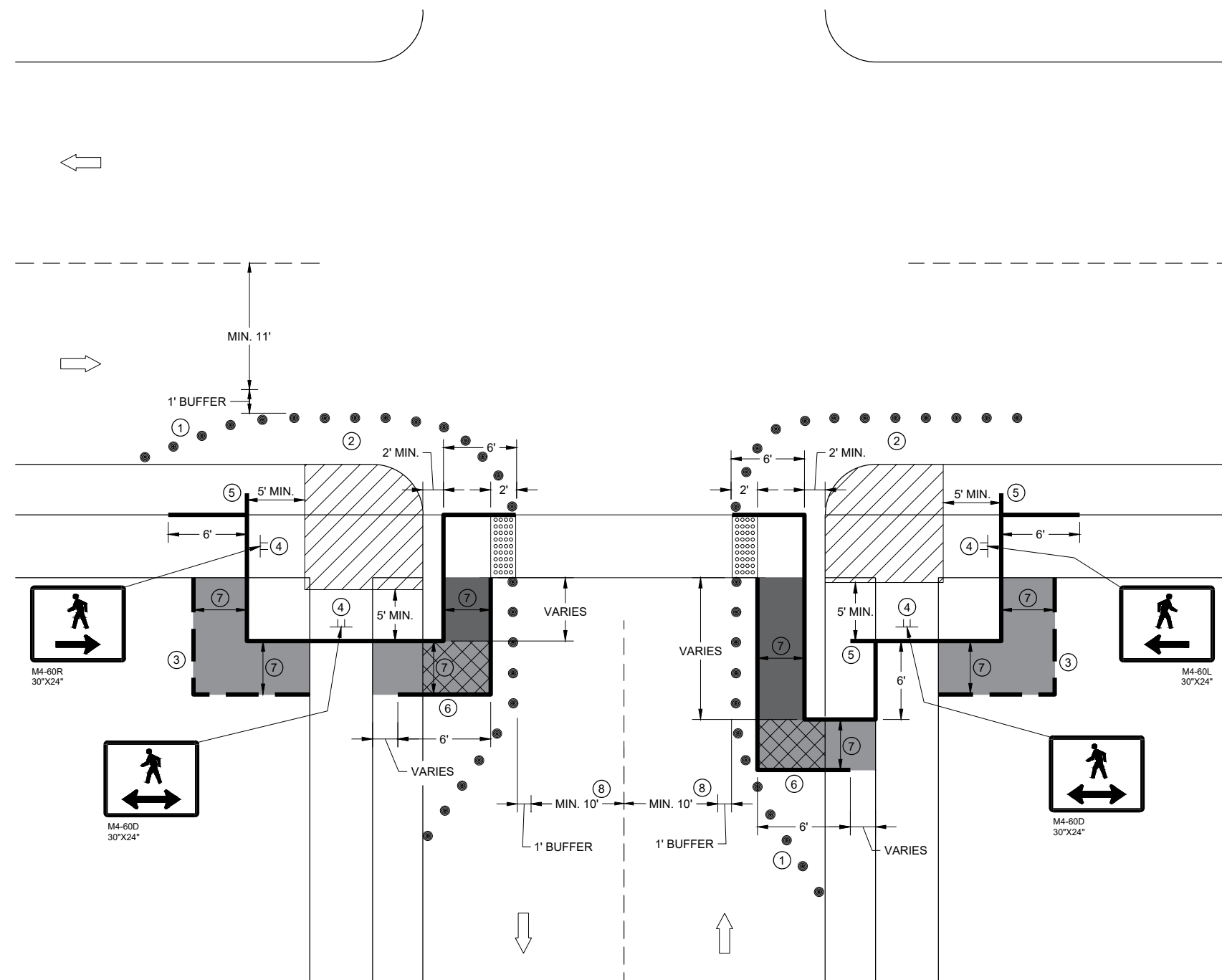
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

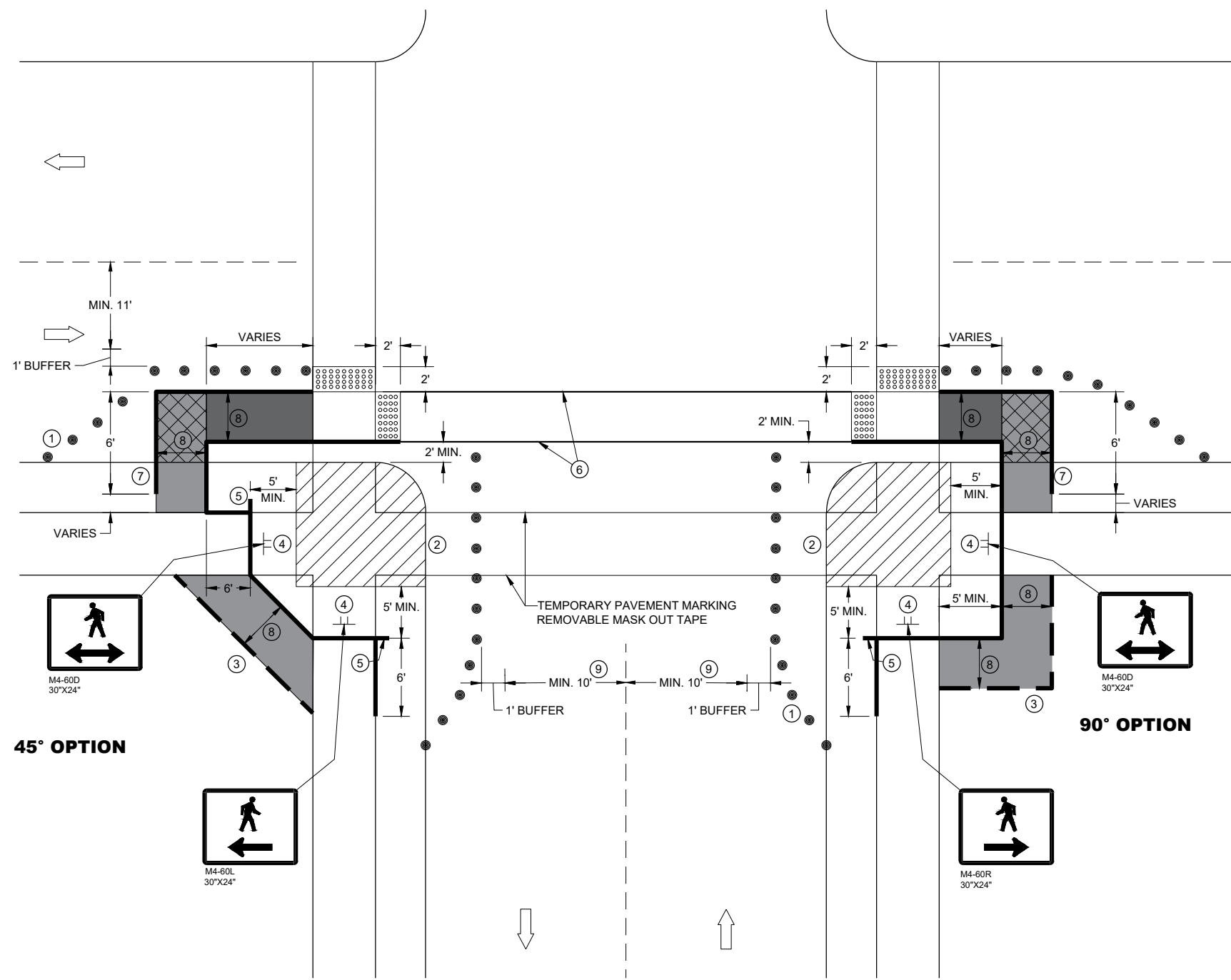
LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



CURB RAMP PEDESTRIAN TRAFFIC CONTROL

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

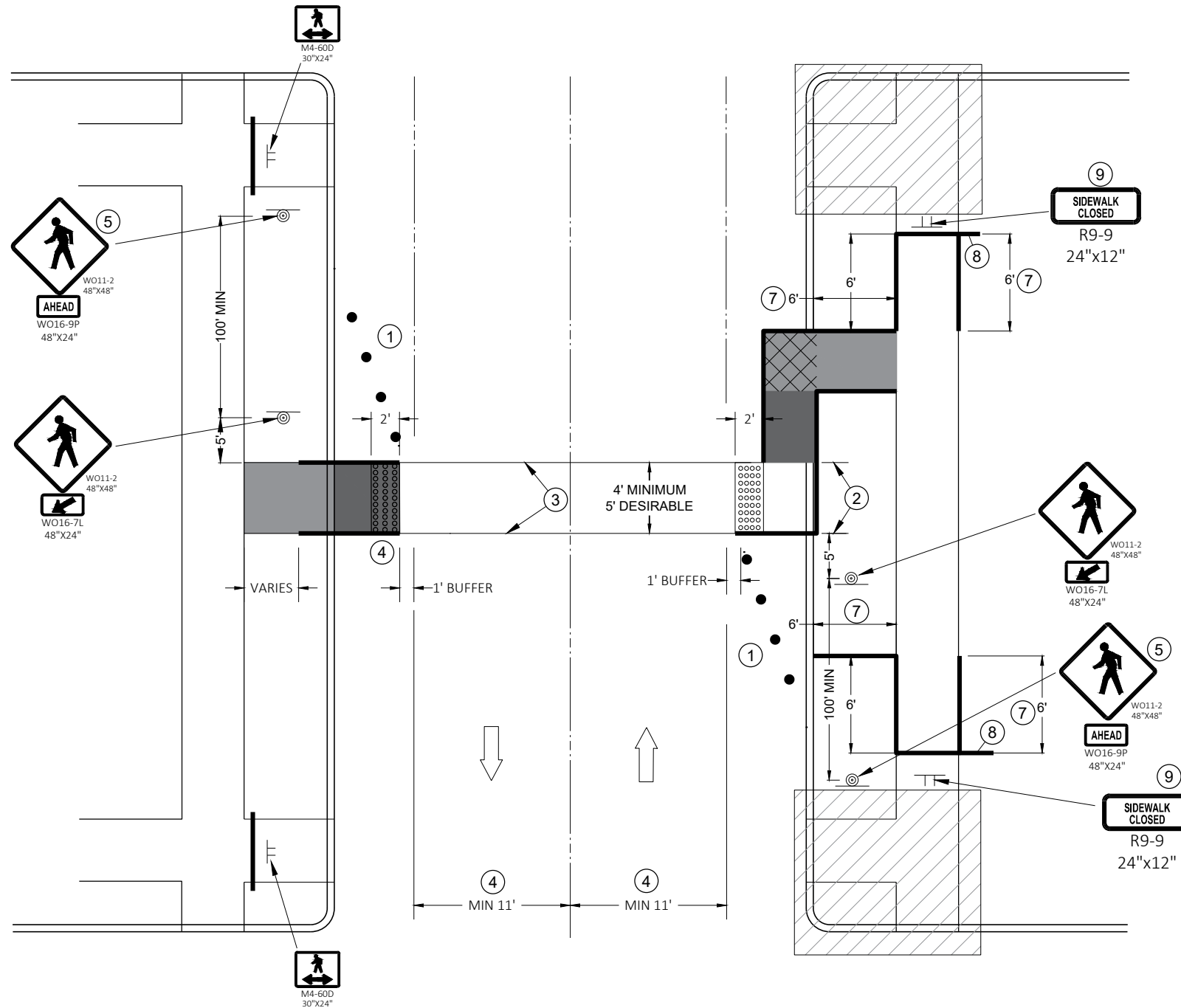
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT PEDESTRIAN FACILITIES CLOSURES.

- ① SHOULDER OR LANE CLOSURE ADVANCED WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② 4 FEET MINIMUM, 5 FEET DESIRABLE.
- ③ WHITE 6" TEMPORARY PAVEMENT MARKING.
- ④ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, PERPENDICULAR CURB RAMP MAY NEED TO BE UTILIZED.
- ⑤ IF MINIMUM 100' SPACING FROM THE MID-BLOCK CROSSING CANNOT BE ATTAINED BEFORE THE INTERSECTION, REMOVE THIS SIGN ASSEMBLY.
- ⑥ IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ⑦ PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ⑧ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF THE EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ⑨ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF THE SIGN.

LEGEND

- TRAFFIC CONTROL DRUM
- ⊥ SIGN ON TEMPORARY SUPPORT
- TEMPORARY CURB RAMP
- ◻ TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- ▨ WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- ➔ DIRECTION OF TRAFFIC








TEMPORARY PEDESTRIAN CROSSING

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  EXISTING SIGN ON PERMANENT SUPPORT
-  NEW SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

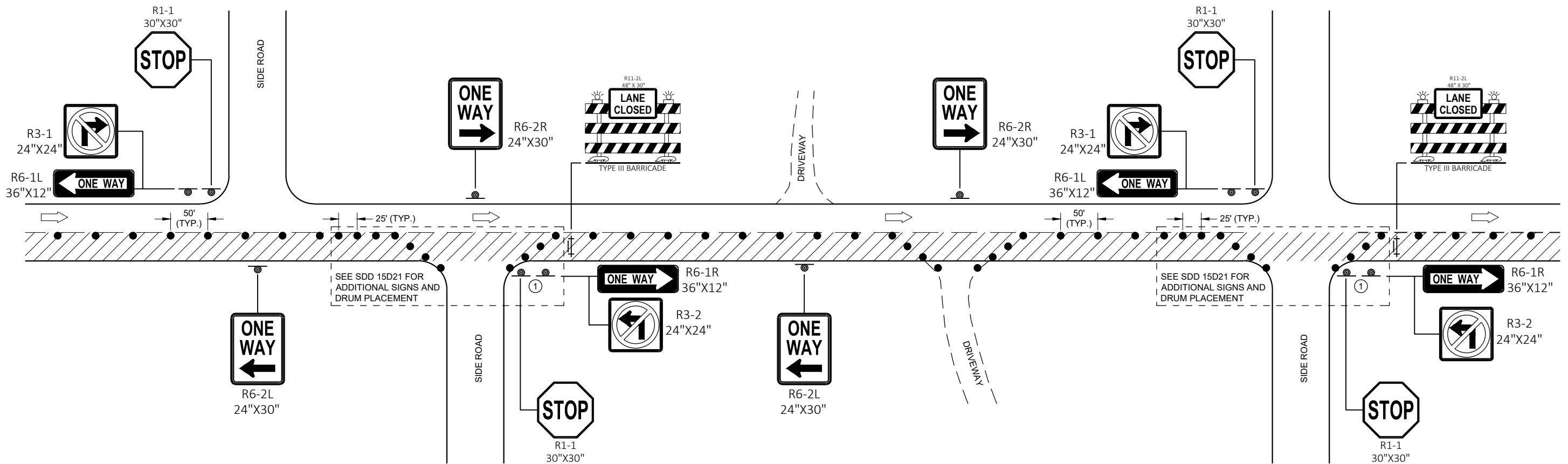
ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

① IF WORK OPERATIONS ALLOWS, MOVE R1-1, R6-1R, AND R3-2 CLOSER TO INTERSECTION.



**TRAFFIC CONTROL
ONE - WAY SIGNING**

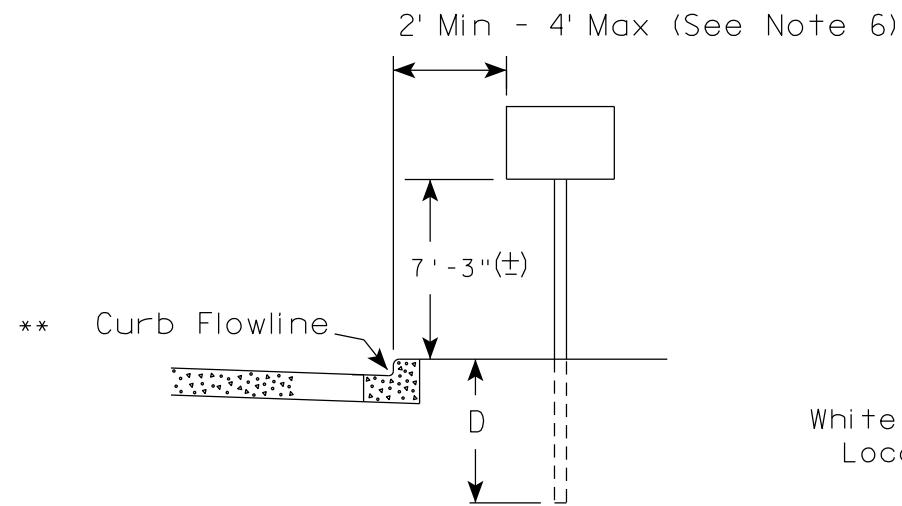
**TRAFFIC CONTROL
ONE-WAY SIGNING**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

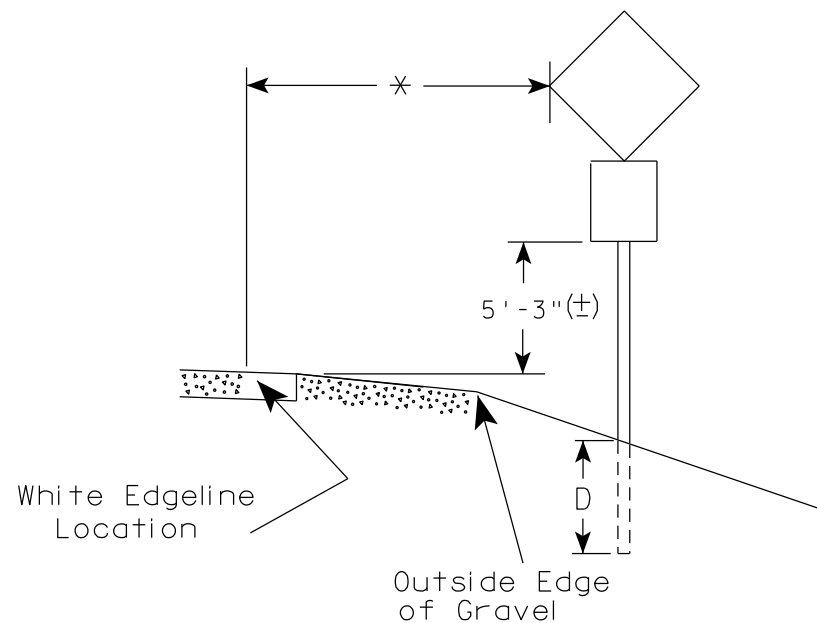
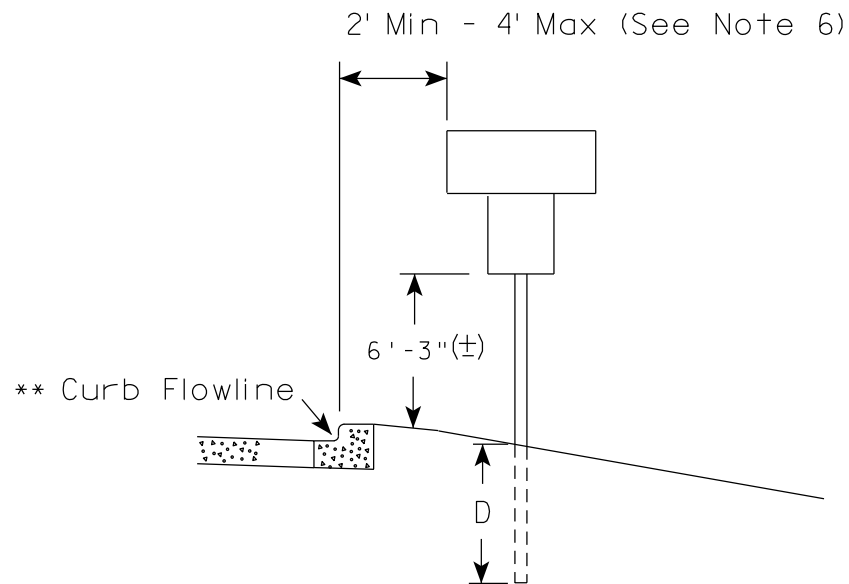
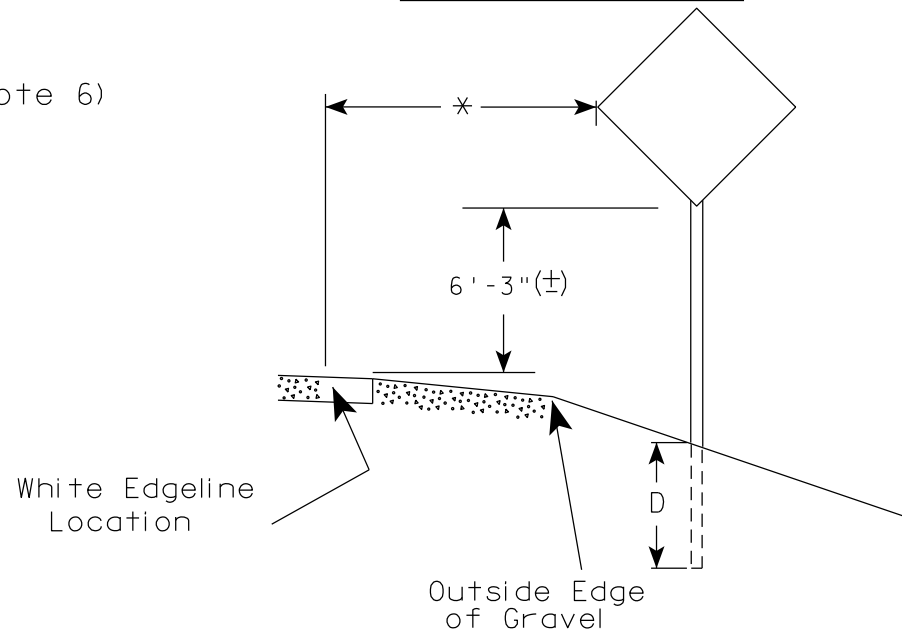
APPROVED
DATE May 2020 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

- GENERAL NOTES**
1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
 2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
 3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
 4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
 5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 6. The (±) tolerance for mounting height is 3 inches.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

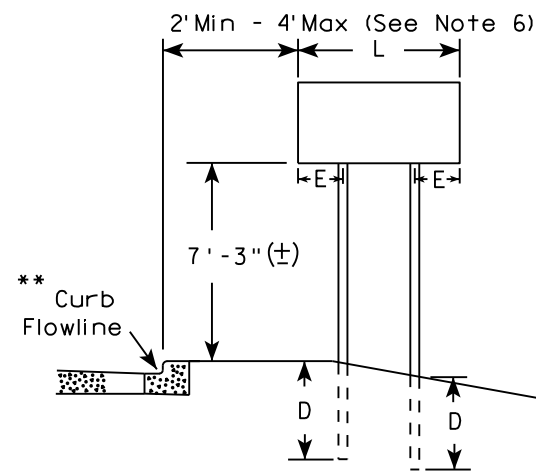
FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

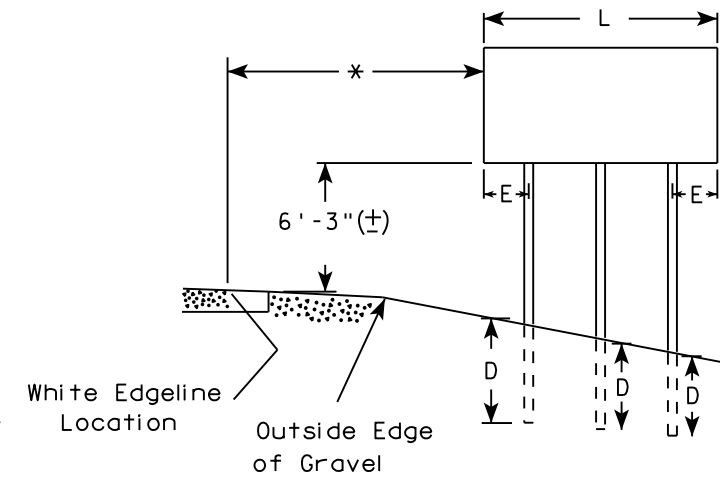
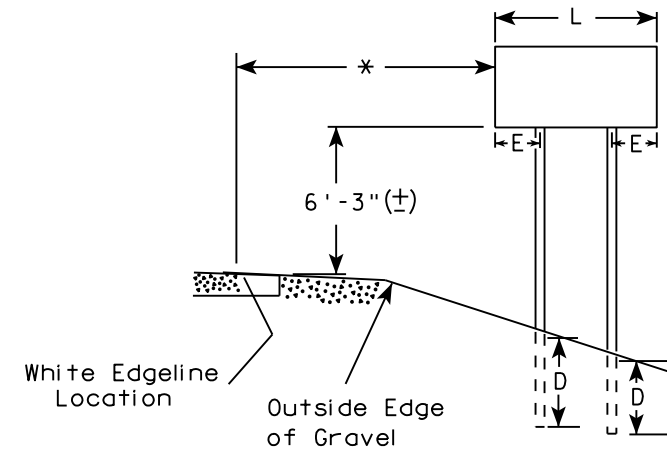
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

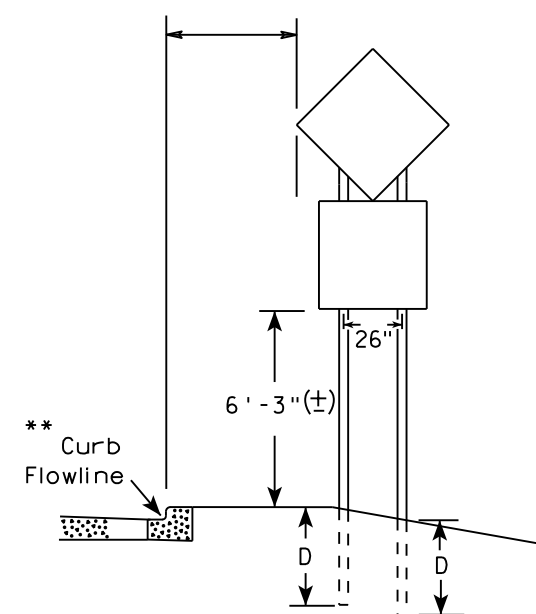
URBAN AREA



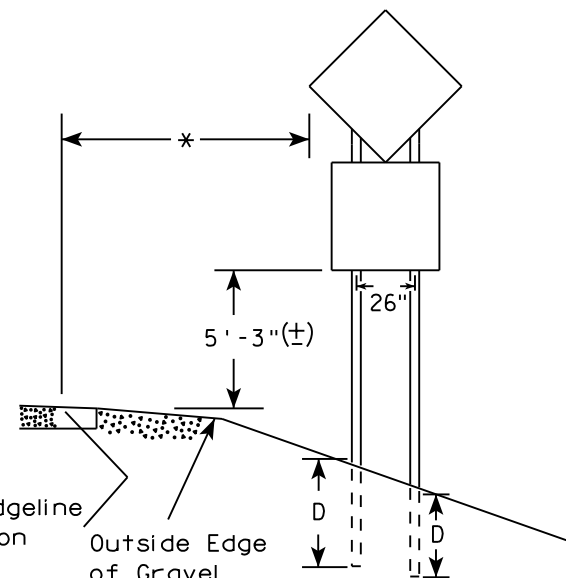
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

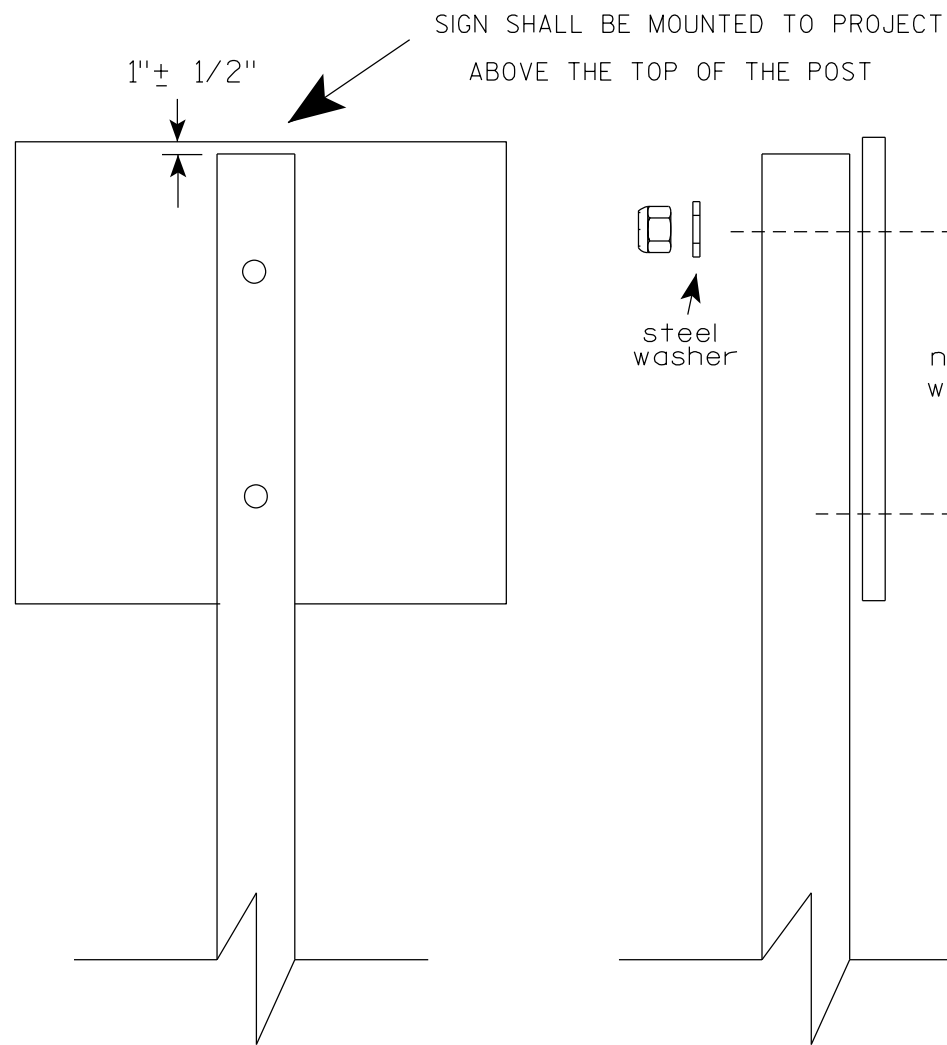
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

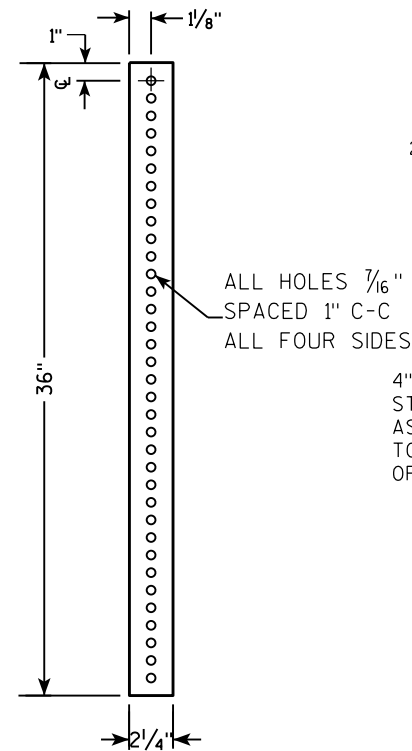
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 4/1/2020 PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

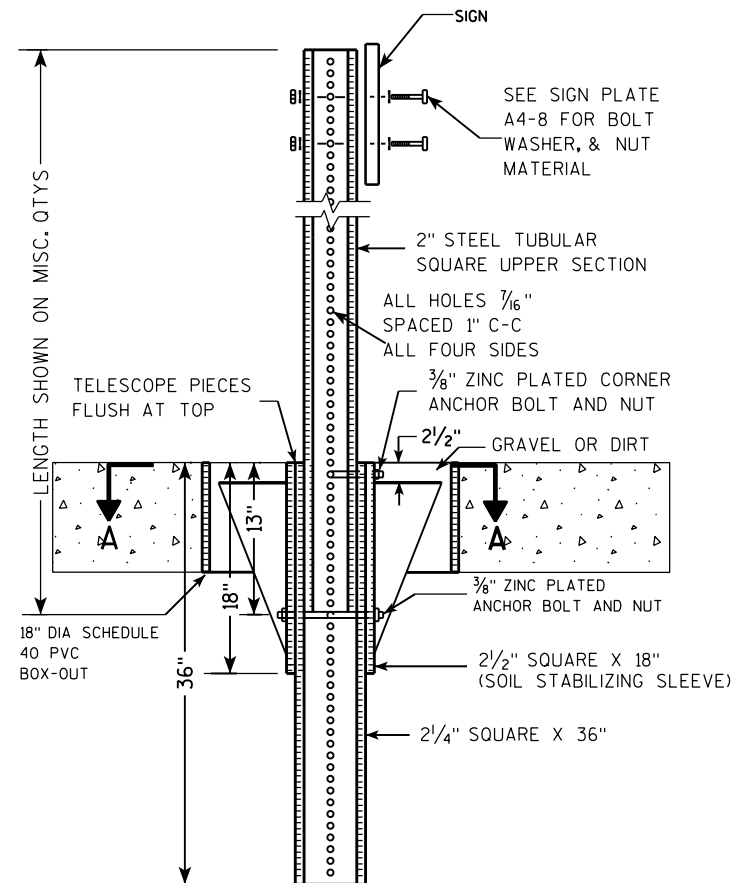
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



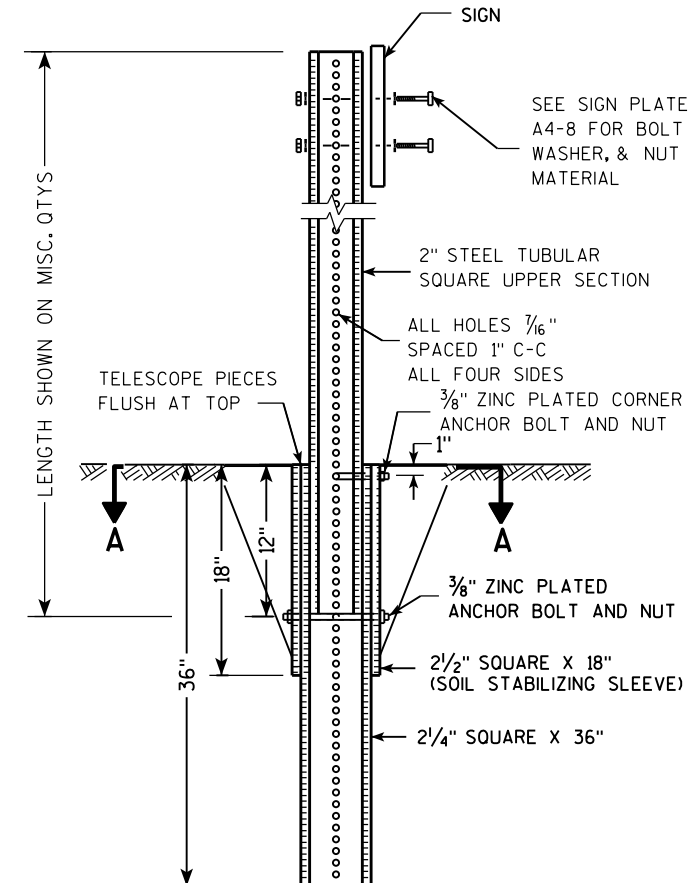
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

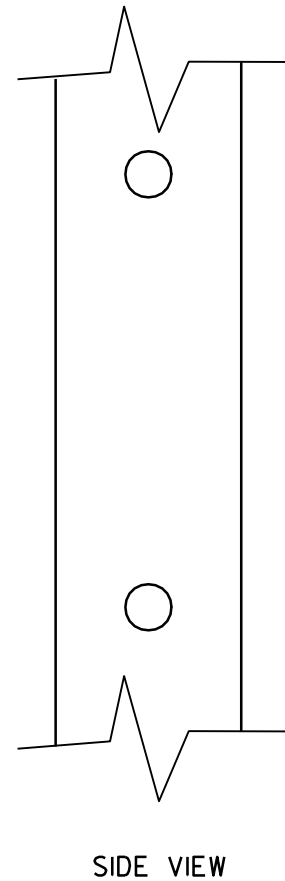
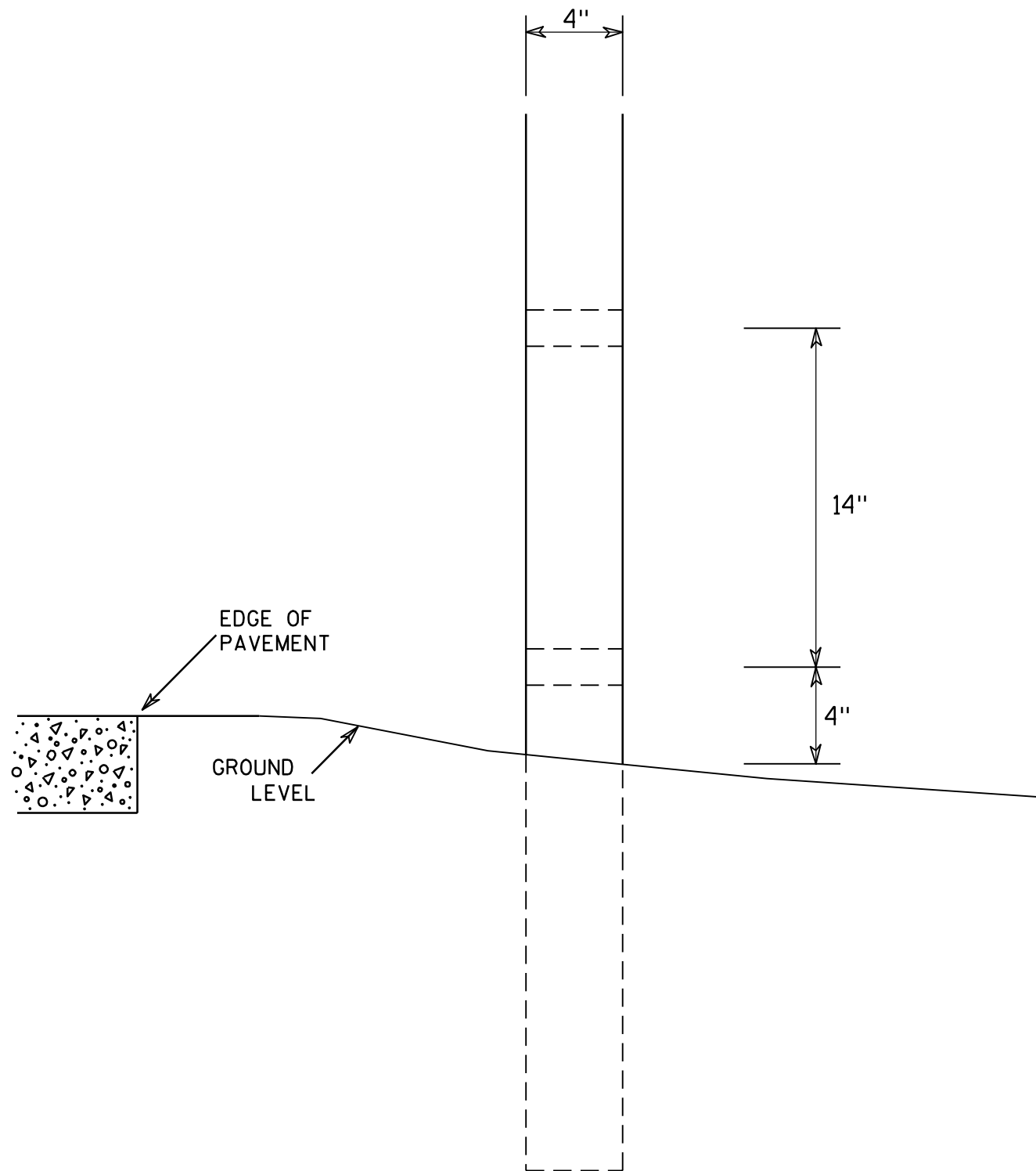
Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

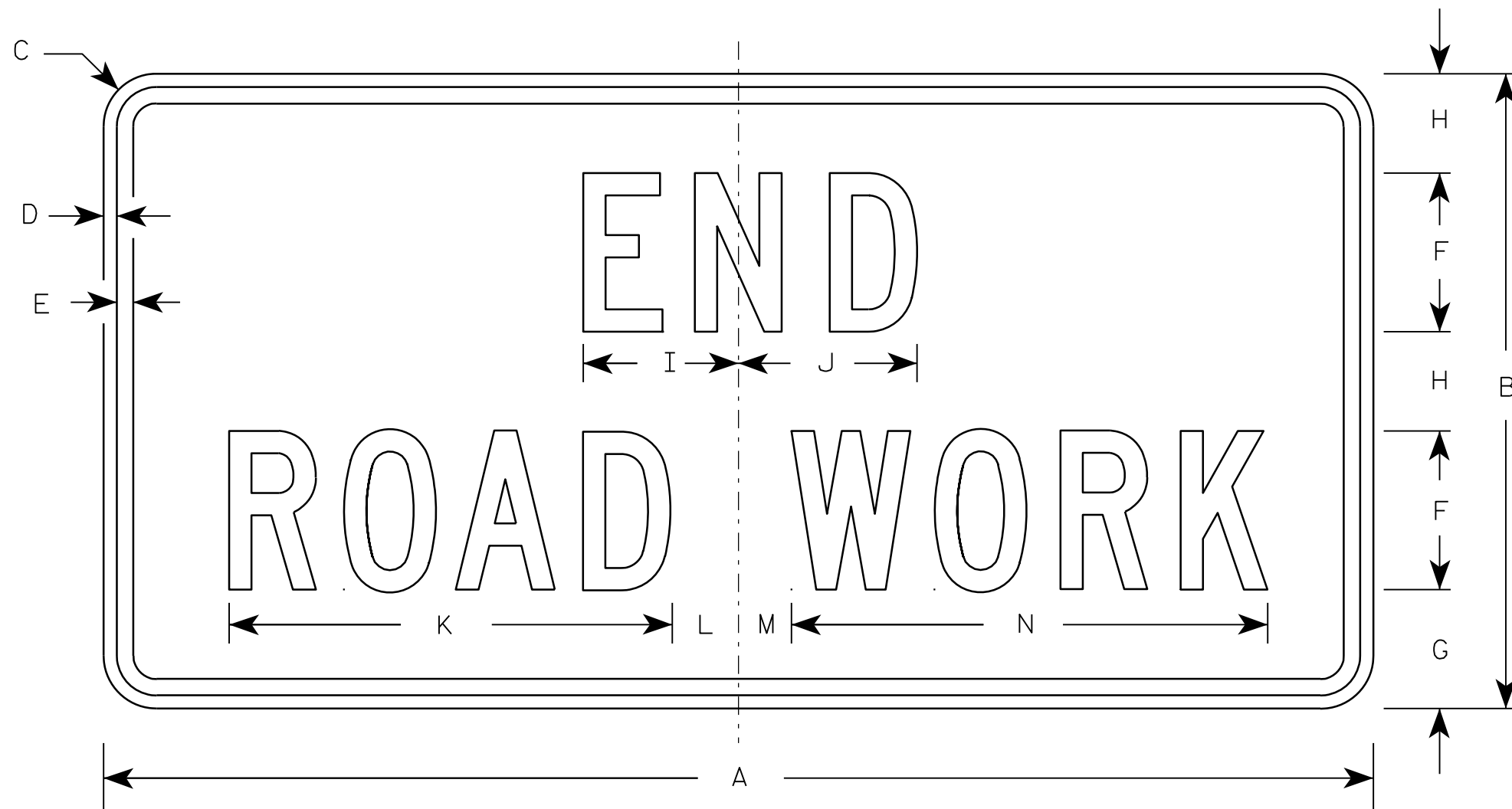
7

7

4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J. Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



G20-2A

7

7

Metric equivalent for this sign is:

SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

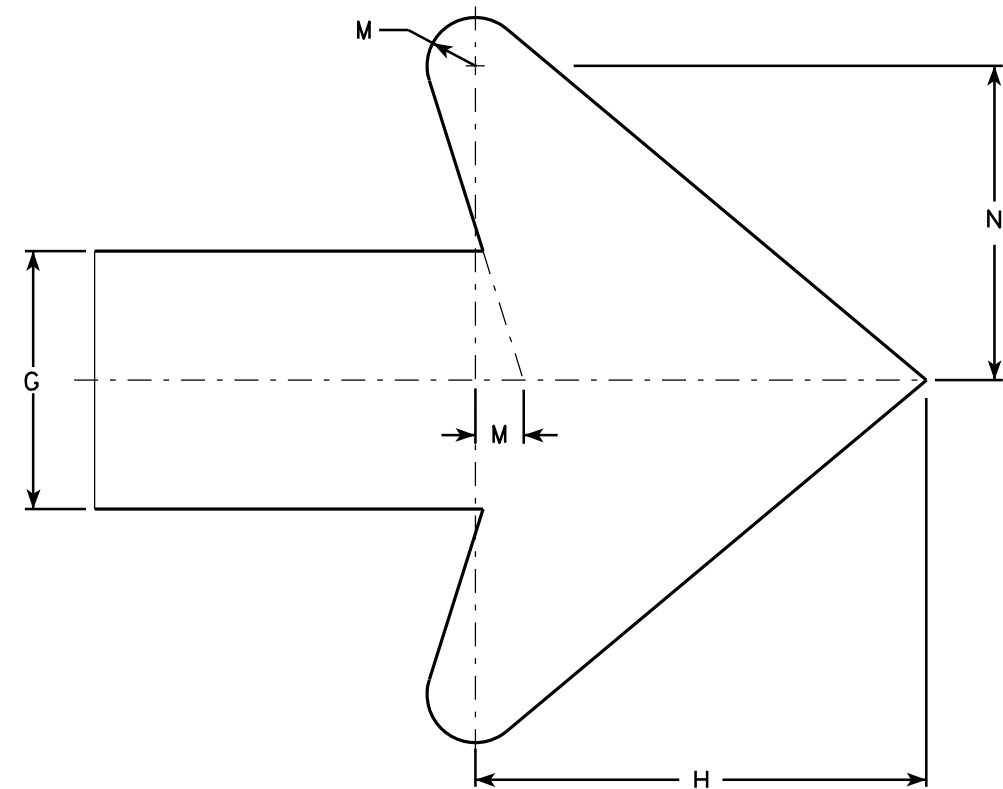
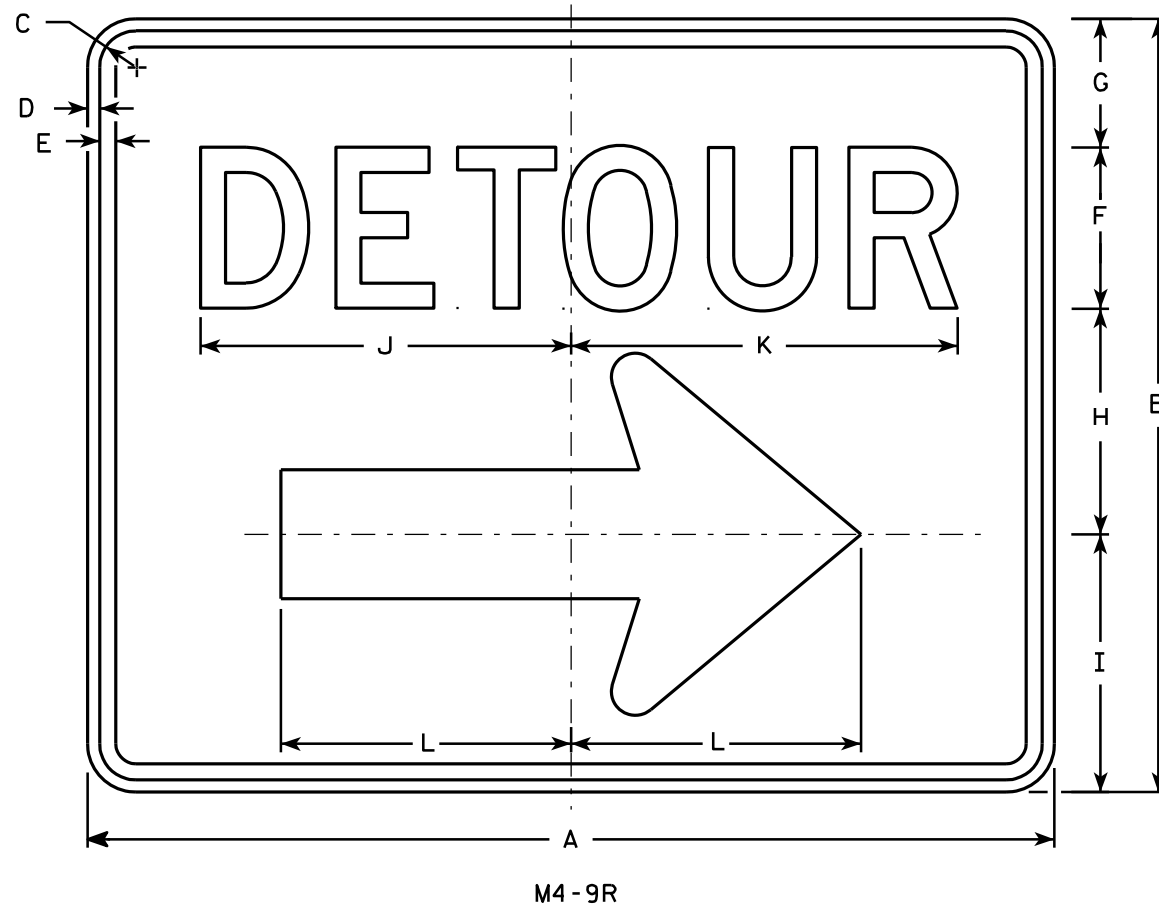
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area sq. m.
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

STANDARD SIGN G20-2A	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 9/30/09	PLATE NO. G20-2A.8

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

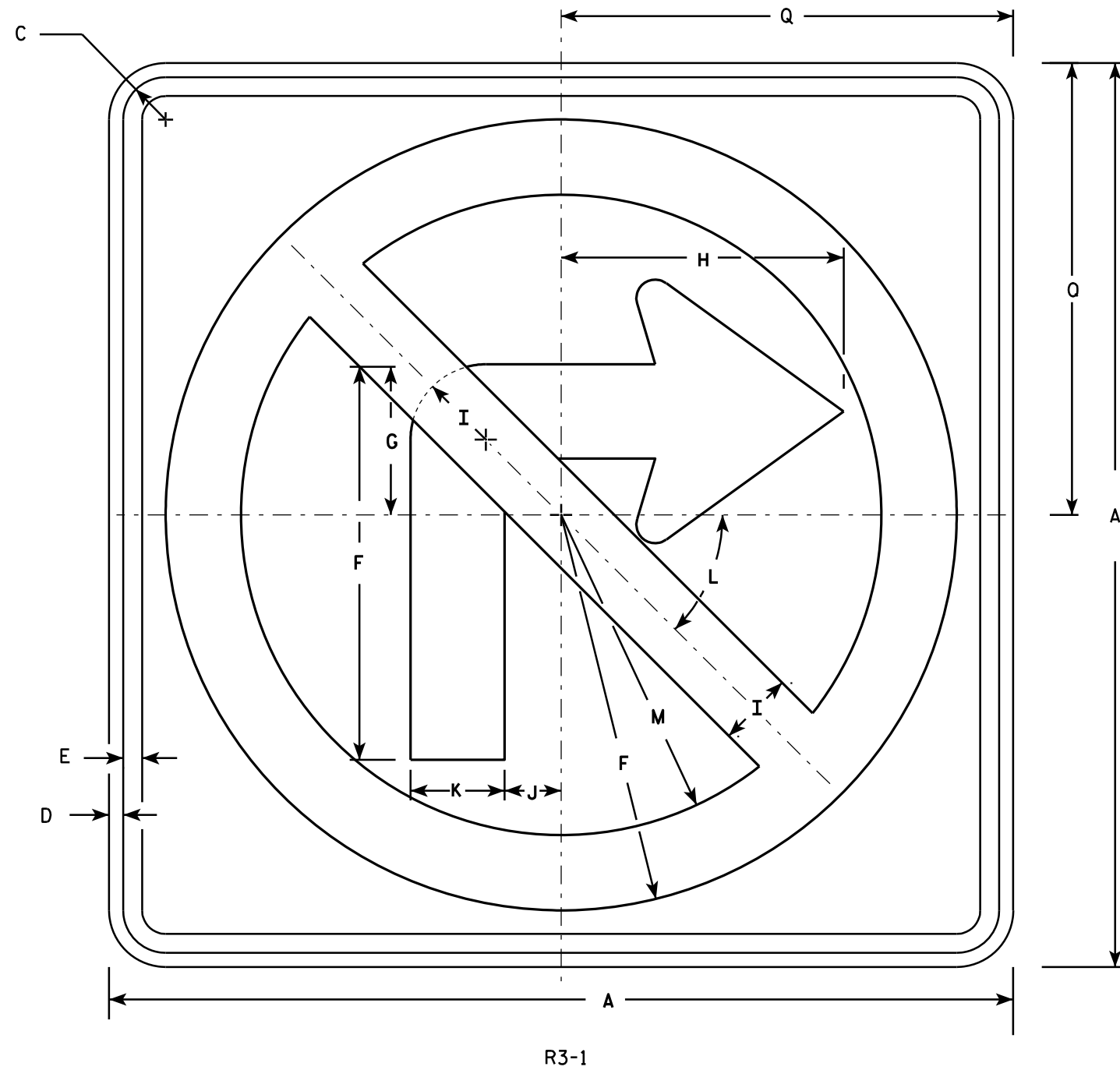
STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

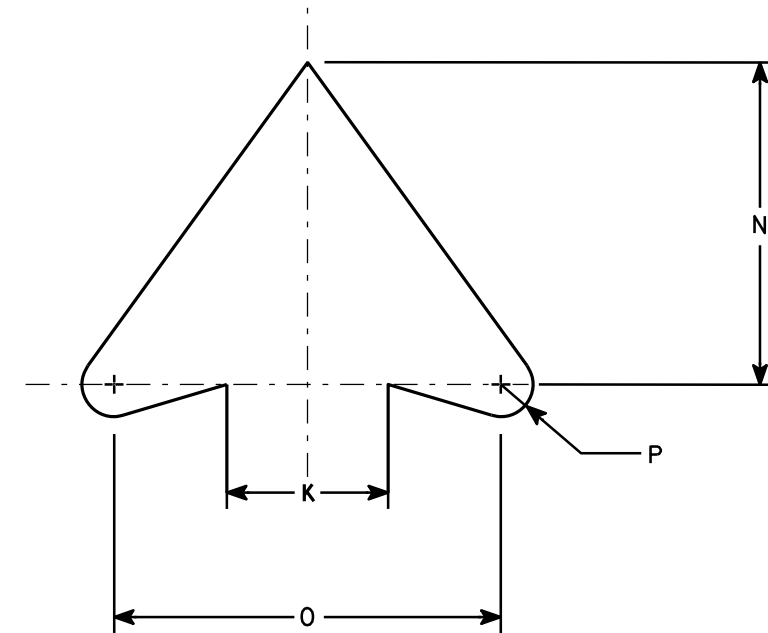
DATE 3/9/11 PLATE NO. M4-9R.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45	8 1/2	5	6	1/2	12										4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2	12										4.0
2M	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
3	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
4	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4	18										9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1	24										16.0

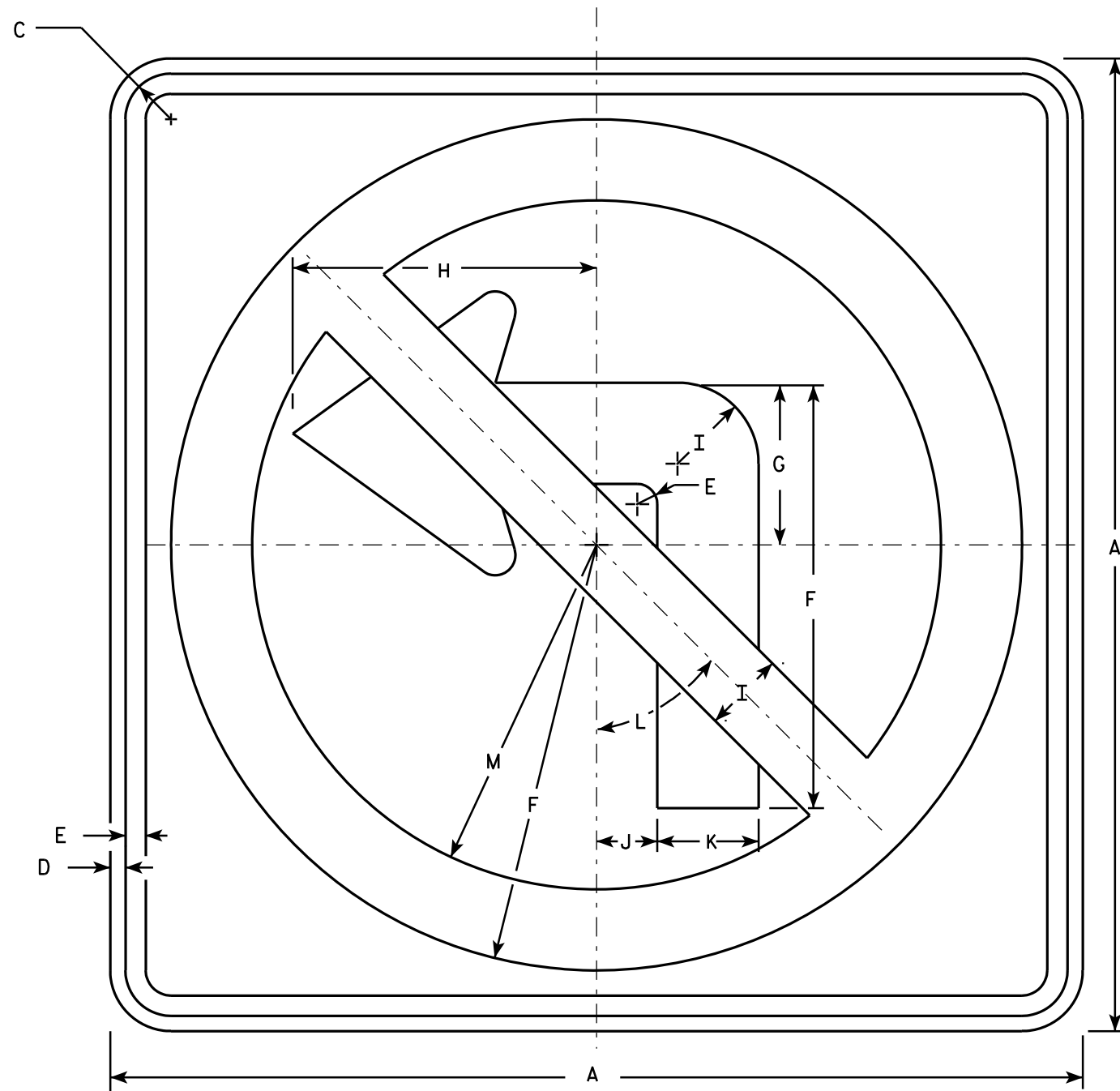
STANDARD SIGN
R3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/08/10 PLATE NO. R3-1.5

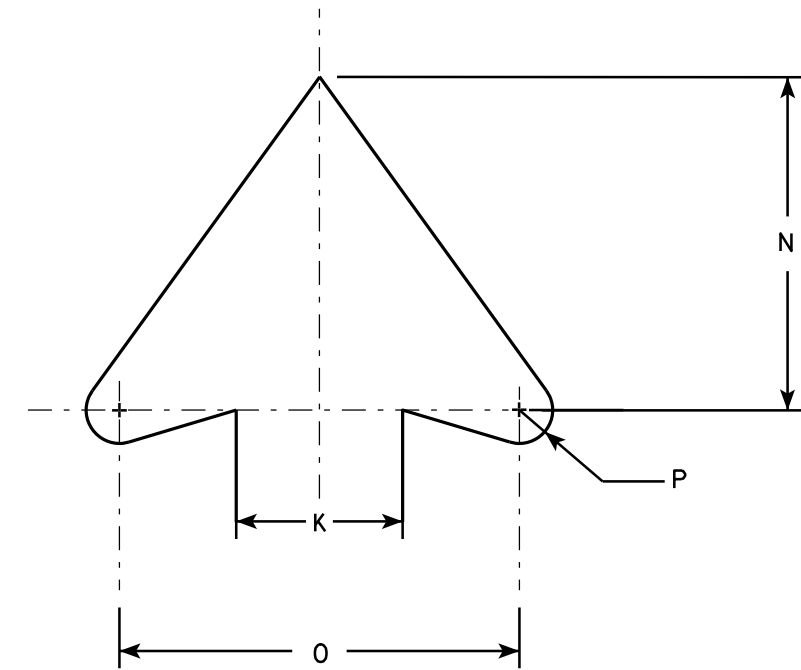
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



R3-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. Ft.
1	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2M	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
3	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
4	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1											16.0

STANDARD SIGN
R3-2

WISCONSIN DEPT OF TRANSPORTATION

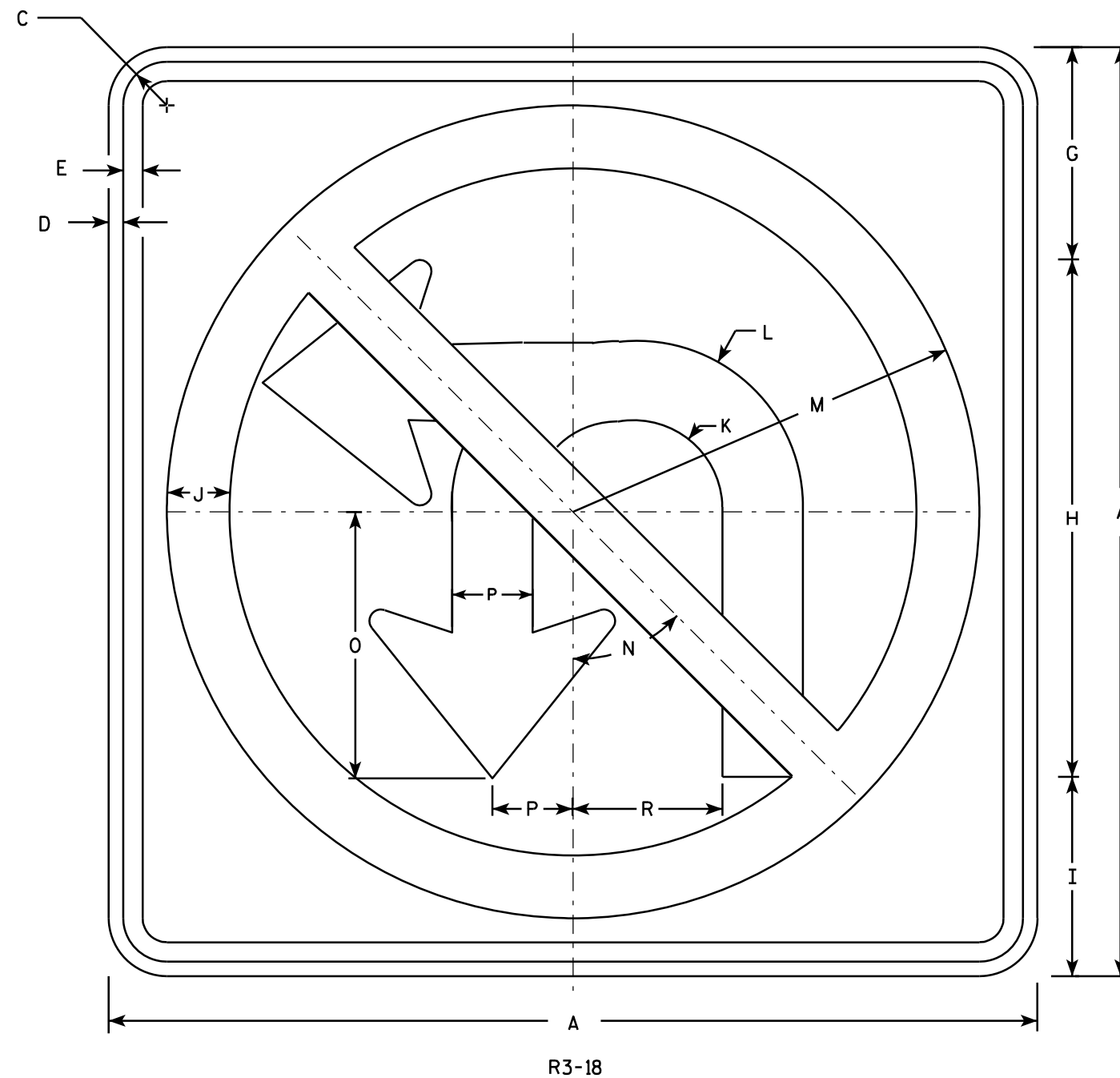
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 12/08/10 PLATE NO. R3-2.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area Sq. Ft.
1																											
2S	24		1 1/8	3/8	1/2		5 1/2	13 3/8	5 1/8	1 5/8	2 1/4	4 1/4	10 1/2	45°	6 7/8	2 1/8		3 7/8									4.0
2M	36		1 5/8	5/8	3/4		8 1/4	20	7 3/4	2 1/2	3 3/8	6 1/2	15 3/4	45°	10 3/8	3 1/8		5 3/4									9.0
3	36		1 5/8	5/8	3/4		8 1/4	20	7 3/4	2 1/2	3 3/8	6 1/2	15 3/4	45	10 3/8	3 1/8		5 3/4									9.0
4	36		1 5/8	5/8	3/4		8 1/4	20	7 3/4	2 1/2	3 3/8	6 1/2	15 3/4	45	10 3/8	3 1/8		5 3/4									9.0
5	48		2 1/4	3/4	1		11	26 3/4	10 1/4	3 1/4	4 5/8	8 5/8	21	45°	13 3/4	4 1/8		7 3/4									16.0

STANDARD SIGN
R3-18

WISCONSIN DEPT OF TRANSPORTATION

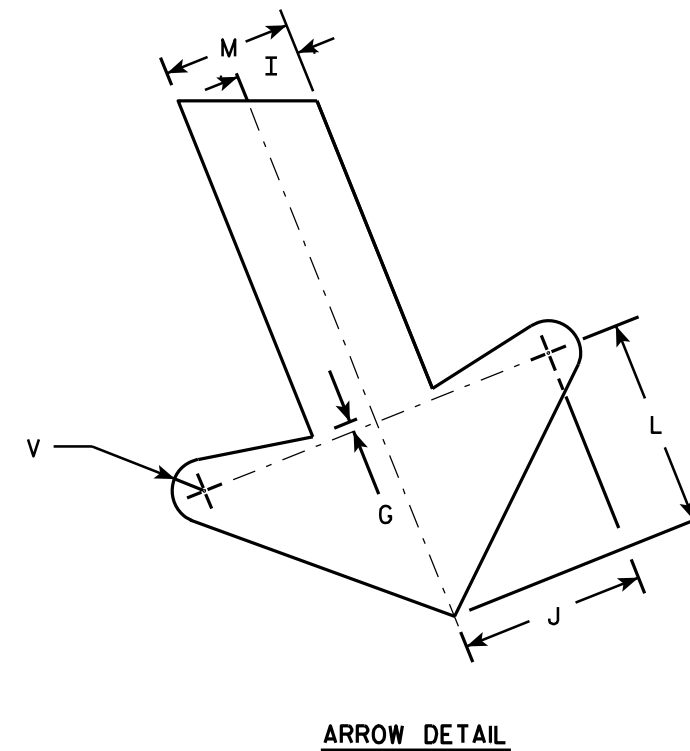
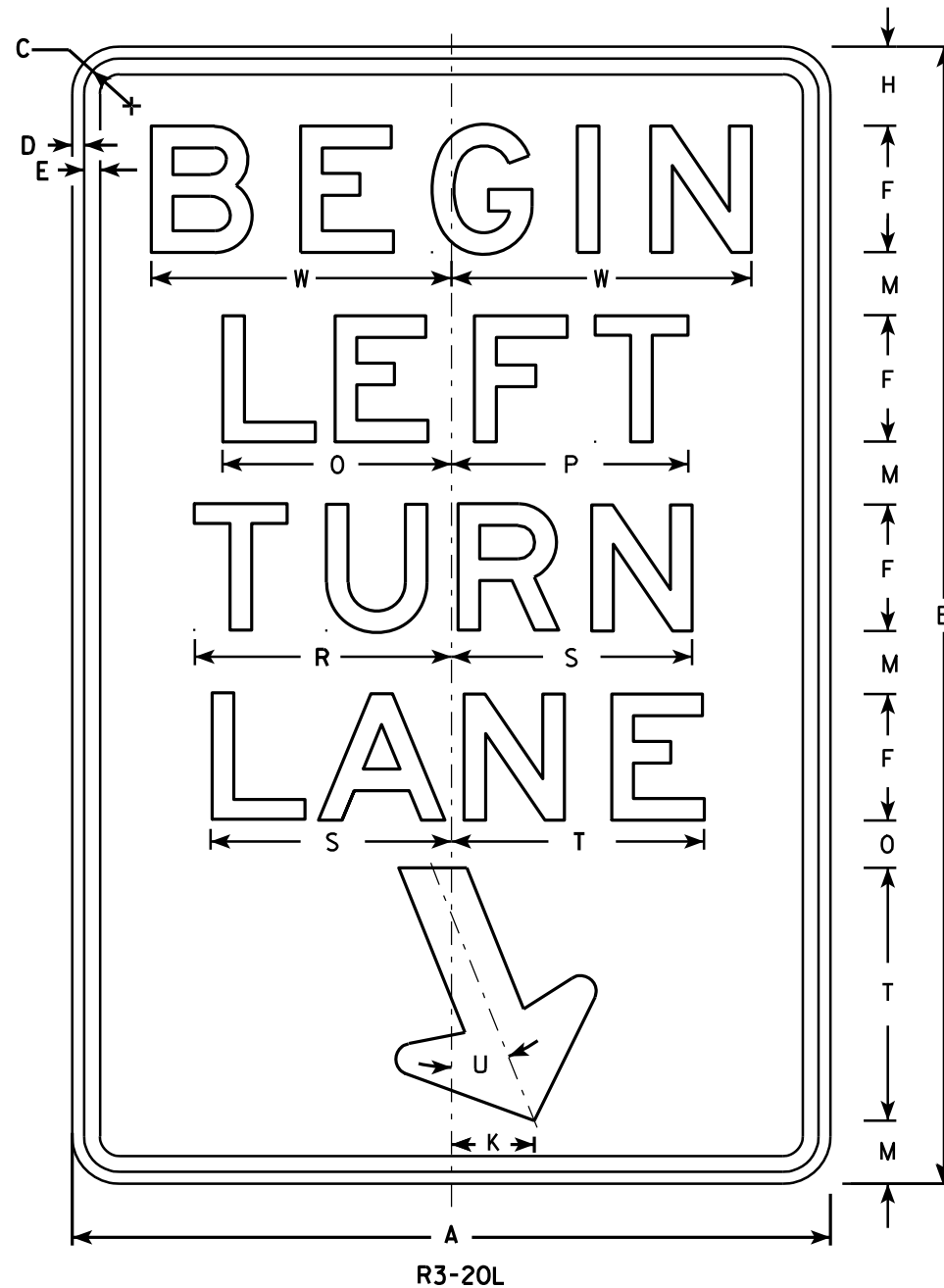
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/21/10 PLATE NO. R3-18.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	7 1/4	7 1/2		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
2M	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	7 1/4	7 1/2		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
3	36	54	1 3/4	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 7/8	3	2 1/4	10 7/8	11 1/4		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5
4																											
5																											

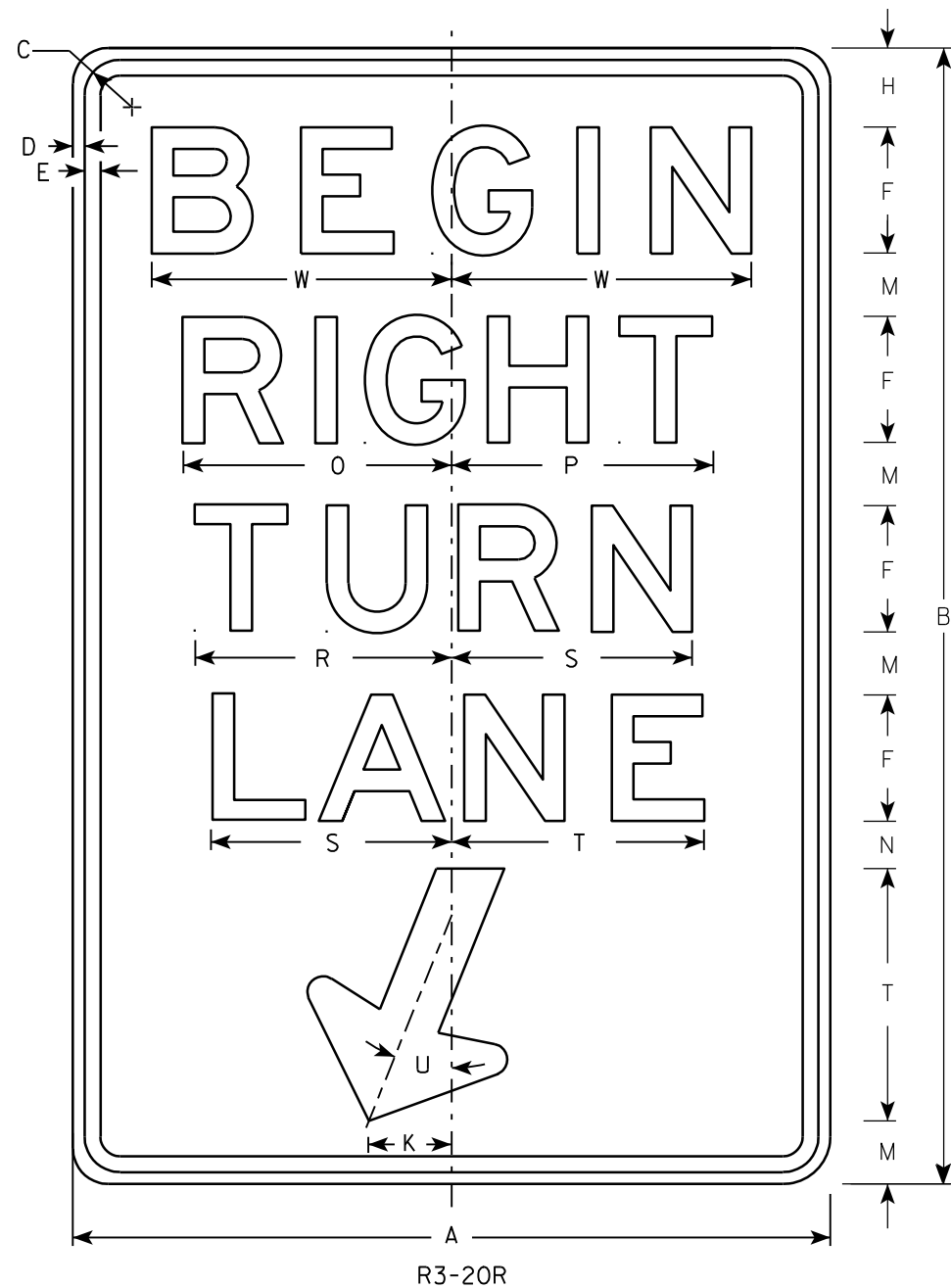
STANDARD SIGN
R3-20L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/18/10 PLATE NO. R3-20L.7

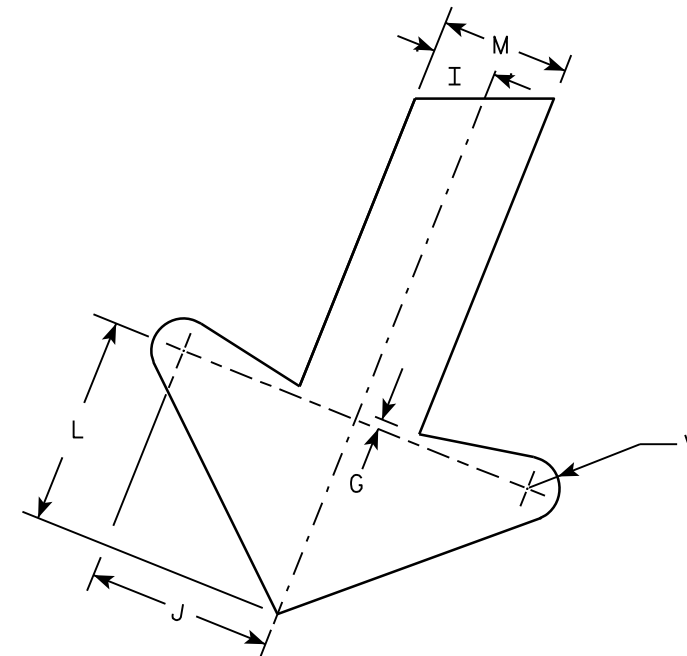
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



R3-20R

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	
1																												
2S	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0	
2M	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0	
3	36	54	1 3/4	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 7/8	3	2 1/4	12 3/4	12 1/2		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5	
4																												
5																												

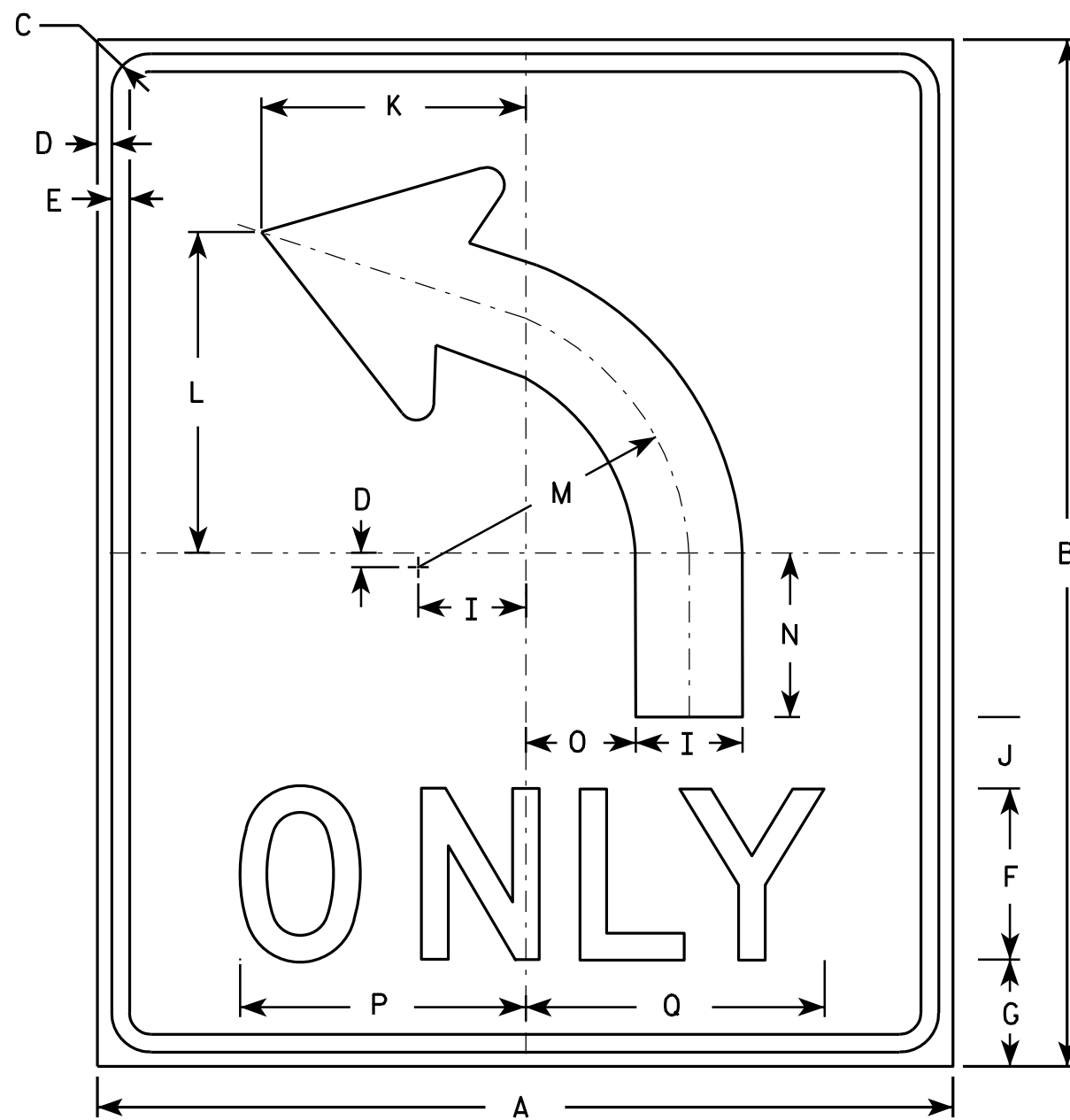
STANDARD SIGN
R3-20R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 10/18/10 PLATE NO. R3-20R.6

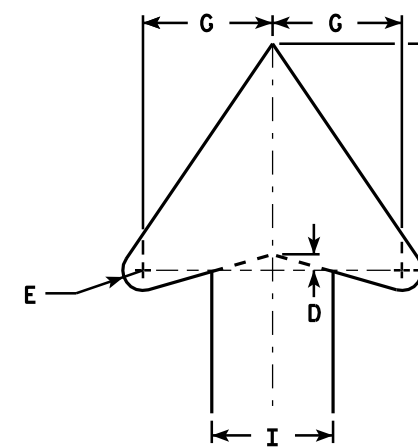
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



R3-50L

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. R3-50R is the same as R3-50L except curved portion of arrow points right.



ARROW DETAIL

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	36	1 3/8	1/2	5/8	6	4	7	3 3/4	2 1/2	9 1/4	11 1/4	9 1/2	5 3/4	3 7/8	10	10 1/2									7.5	
2M	30	36	1 3/8	1/2	5/8	6	4	7	3 3/4	2 1/2	9 1/4	11 1/4	9 1/2	5 3/4	3 7/8	10	10 1/2									7.5	
3																											
4																											
5																											

STANDARD SIGN
R3-50

WISCONSIN DEPT OF TRANSPORTATION

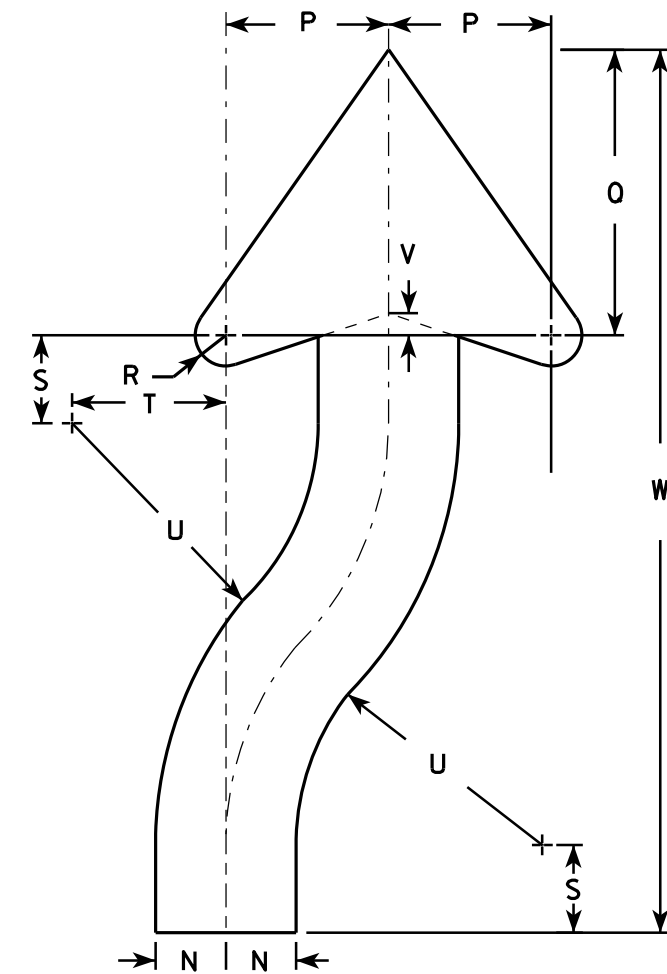
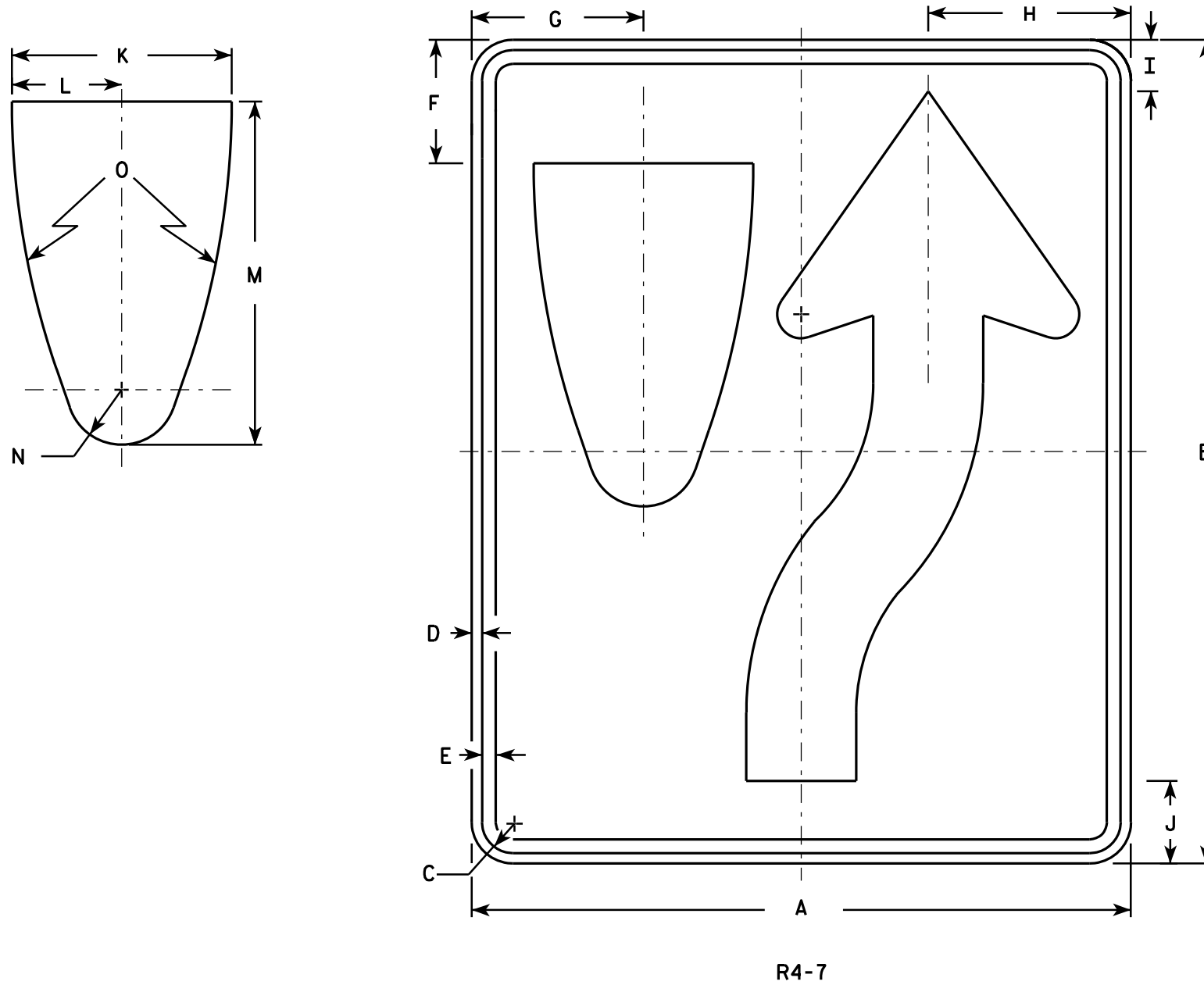
APPROVED *Matthew R. Raush*
for State Traffic Engineer

DATE 3/24/2011 PLATE NO. R3-50.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. material is plywood but borders shall be rounded
2. Color:
Background - White
Message - Black
3. Corners may be square or rounded when base as shown. When base material is metal, the corners and borders shall be rounded.
4. R4-8 is the same as R4-7 except Legend is reversed.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3 3/8	4 3/4	5 1/2	1 3/8	2 1/4	6	3	9 3/8	1 1/2	22 1/2	3 1/2	6 1/8	5/8	1 7/8	3 1/4	6 3/4	1/2	20 3/8				3.0
2S	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
2M	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
3	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
4	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
5	48	60	2 1/4	3/4	1	9	12 1/2	14 3/4	3 3/4	6	16	8	25	4	60	9 1/4	16 1/4	1 5/8	5	8 3/4	18	1 1/4	50 1/4				20.0

STANDARD SIGN
R4-7 & R4-8

WISCONSIN DEPT OF TRANSPORTATION

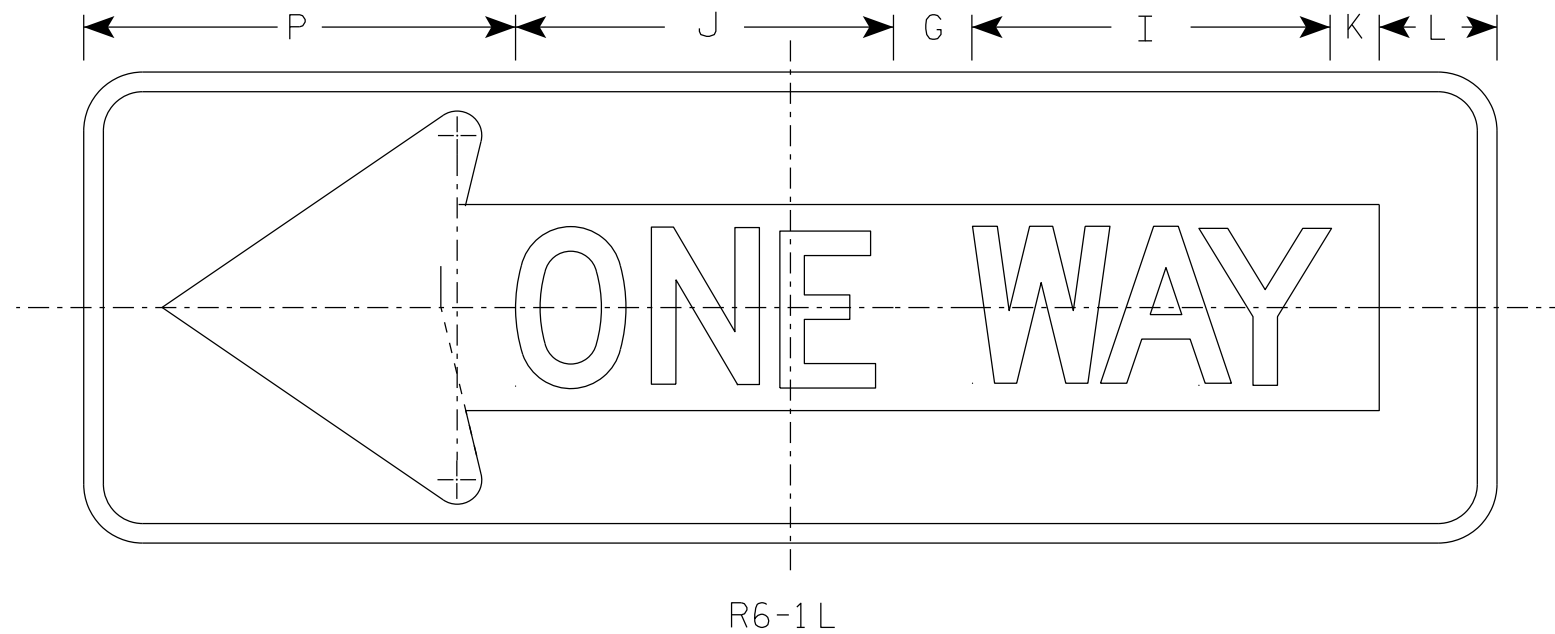
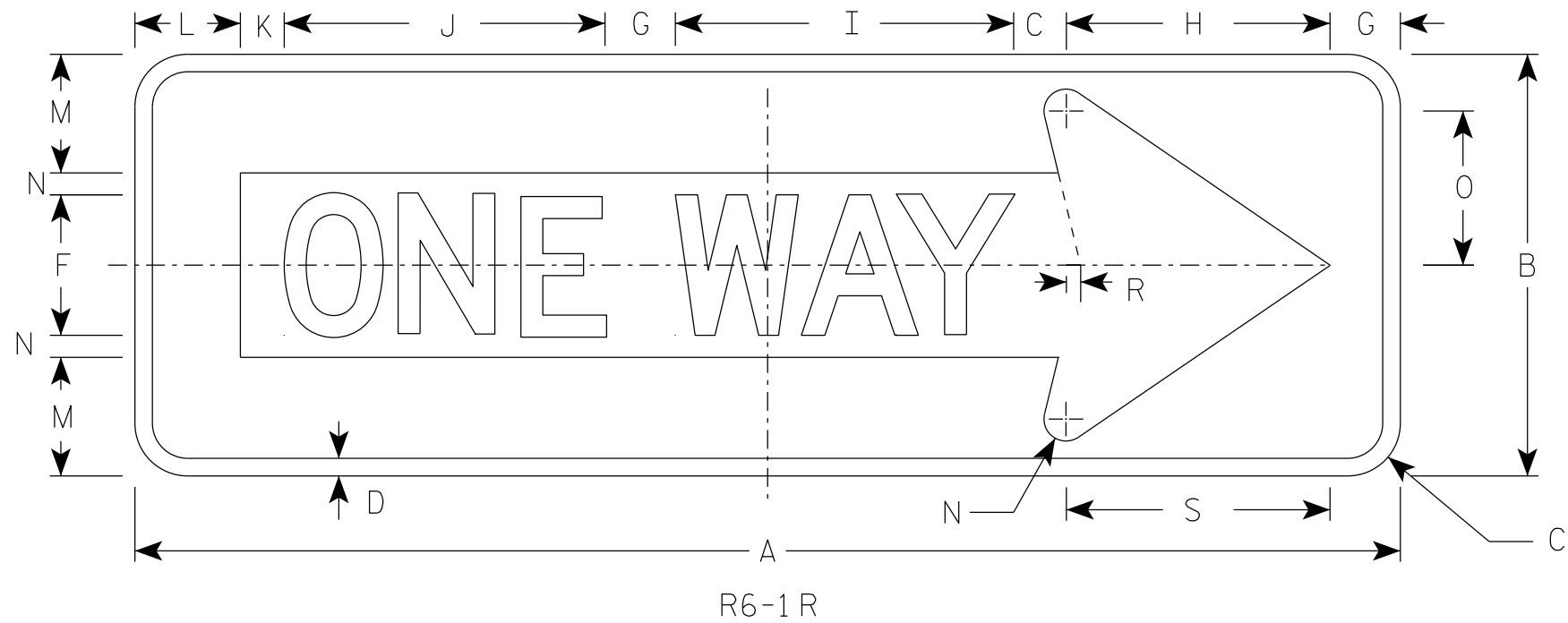
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-7.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - BLACK
Message - BLACK LEGEND & WHITE ARROW & BORDER
3. Message Series - D



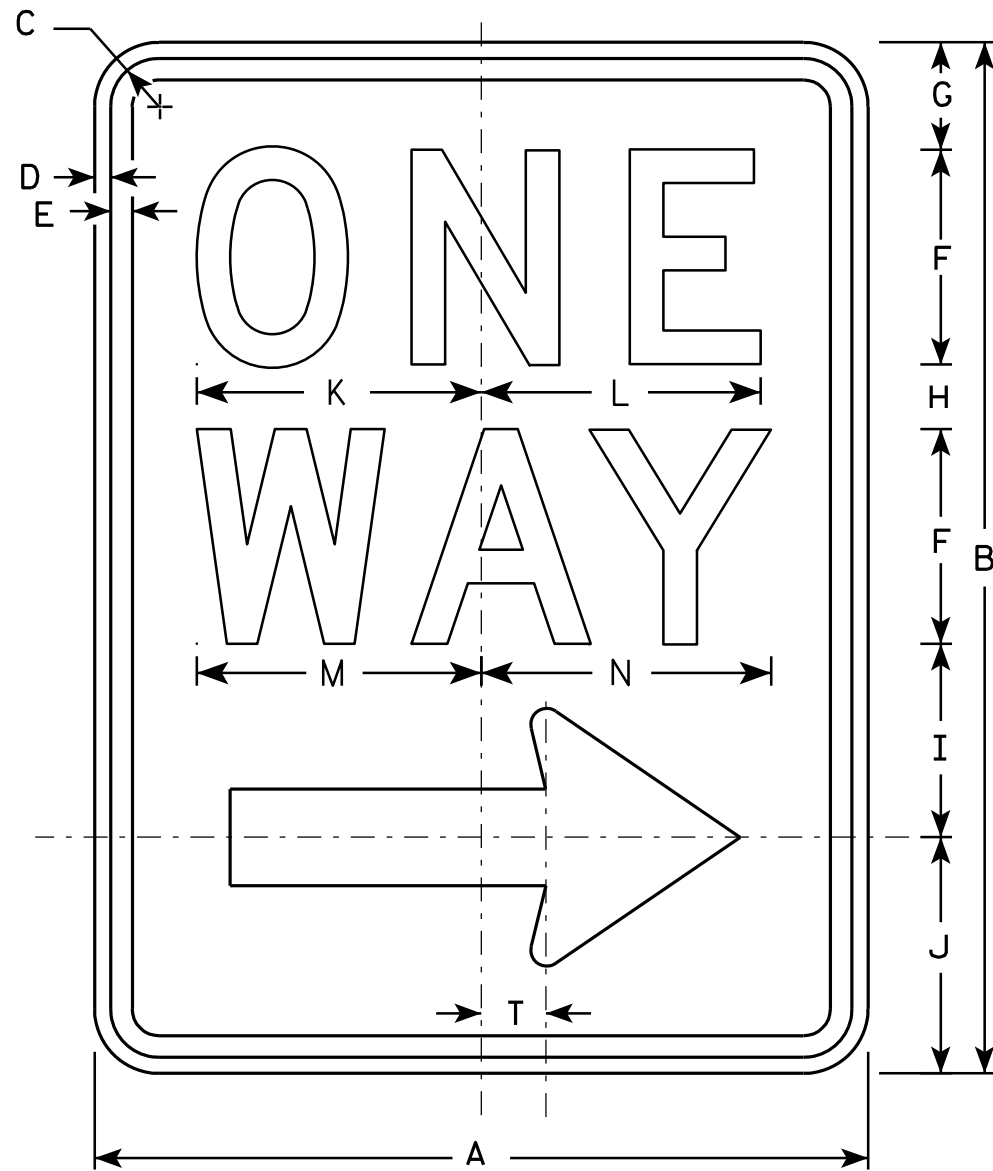
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36	12	1 1/2	1/2		4	2	7 1/2	9 5/8	9 1/8	1 1/4	3	3 3/8	5/8	4 3/8	11		3/8	7 1/2								3.0
2M	54	18	2 1/4	3/4		6	3	11 1/4	13 5/8	14 1/2	1 7/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.75
3	54	18	2 1/4	3/4		6	3	11 1/4	13 5/8	14 1/2	1 7/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.75
4	54	18	2 1/4	3/4		6	3	11 1/4	13 5/8	14 1/2	1 7/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.75
5																											

STANDARD SIGN
R6-1 L & R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

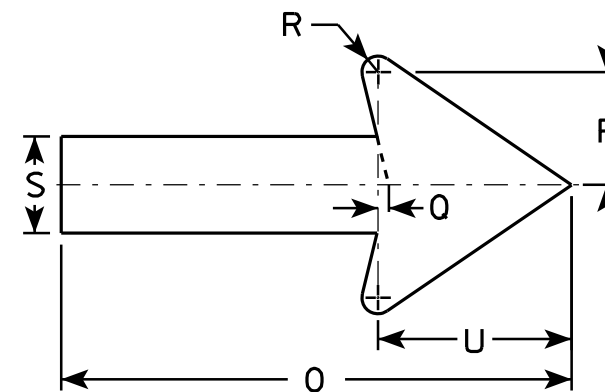
DATE 07/11/18 PLATE NO. R6-1.3



R6-2R

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. R6-2L same as R6-2R except arrow points to the left.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z
1	18	24	1 1/8	3/8	1/2	5	2 1/2	1 1/2	4 1/2	5 1/2	6 5/8	6 1/2	6 5/8	6 3/4	11 7/8	2 5/8	1/4	3/8	2 1/4	1 1/2	4 1/2					
2S	24	30	1 1/8	3/8	1/2	6	3	2 1/2	5 1/2	7	8 1/8	8 1/8	8 1/2	8 5/8	16	3 1/2	3/8	1/2	3	2	6					
2M	30	36	1 3/8	1/2	5/8	8	2 1/2	2 5/8	6 7/8	8	10 1/2	10 1/2	11 1/4	11 1/4	20	4 3/8	1/2	5/8	3 3/4	2 1/2	7 1/2					
3	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 5/8	1/2	3/4	4 3/4	3	9					
4	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 5/8	1/2	3/4	4 3/4	3	9					
5																										

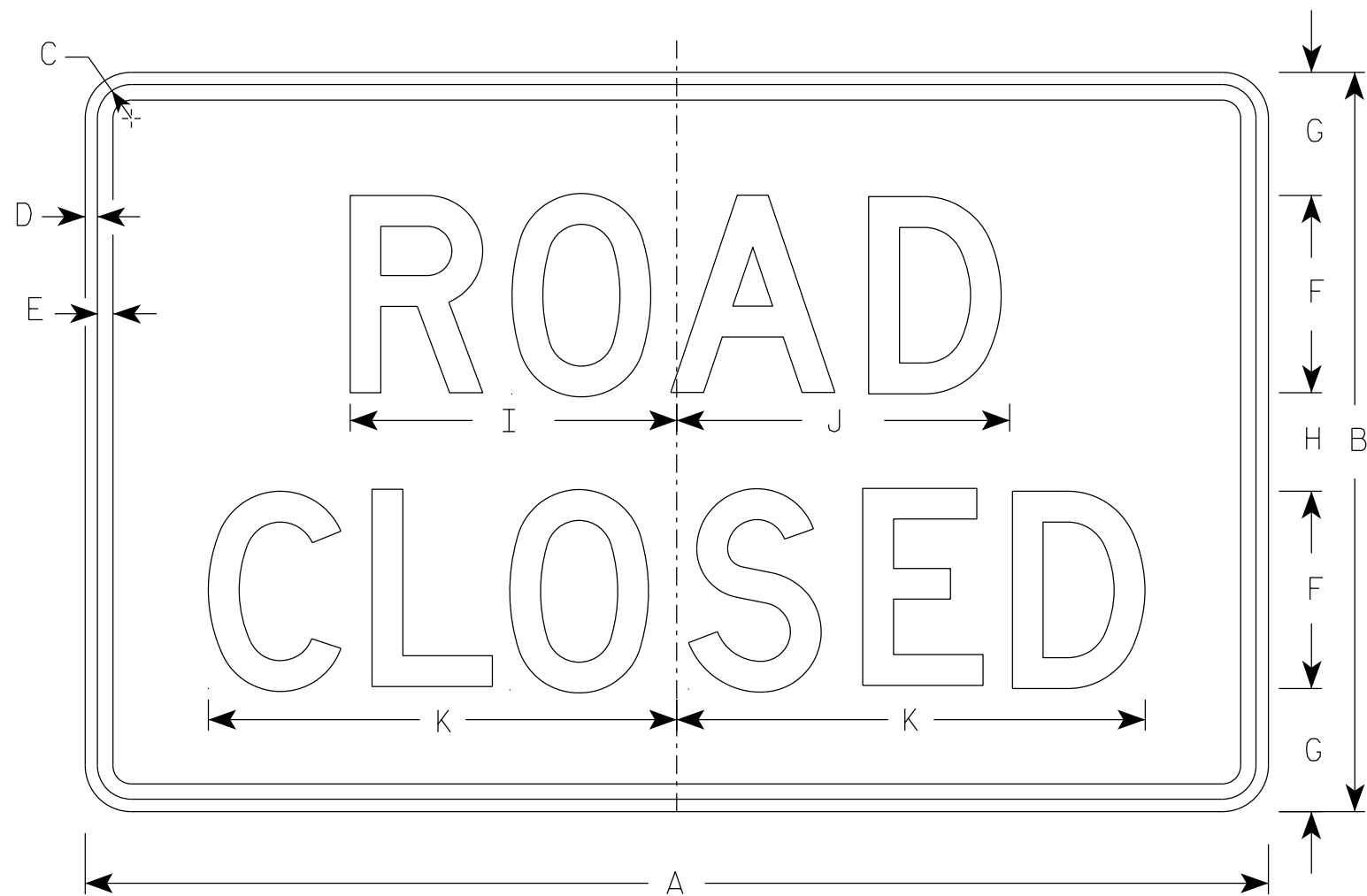
STANDARD SIGN
R6-2 R&L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 11/2/10 PLATE NO. R6-2.8

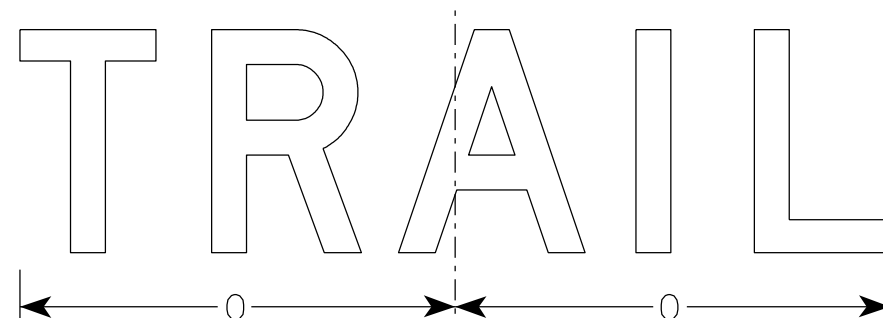
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



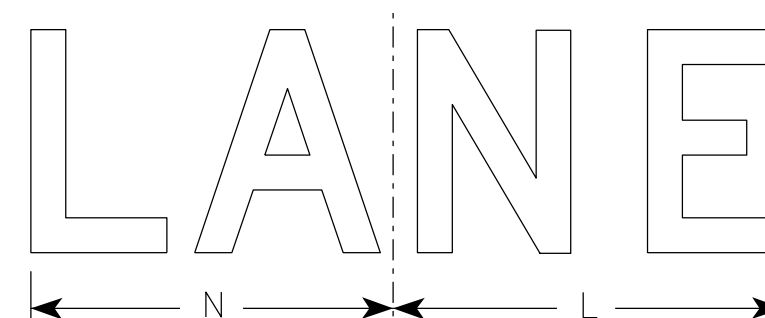
R11-2



R11-2R



R11-2T



R11-2L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

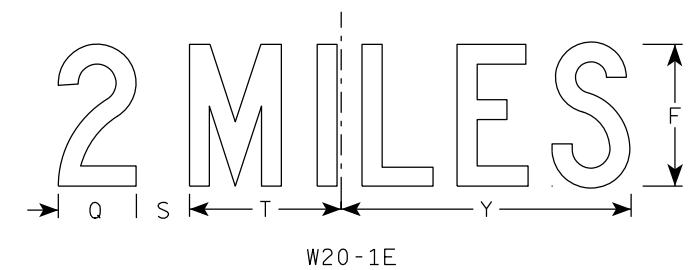
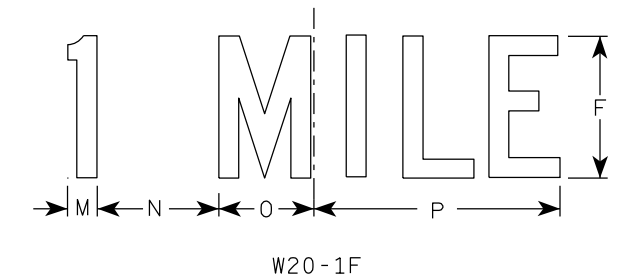
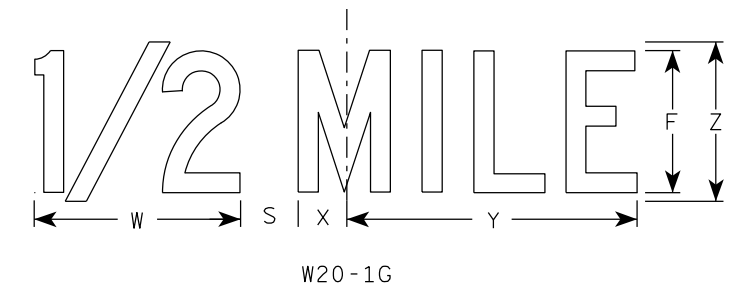
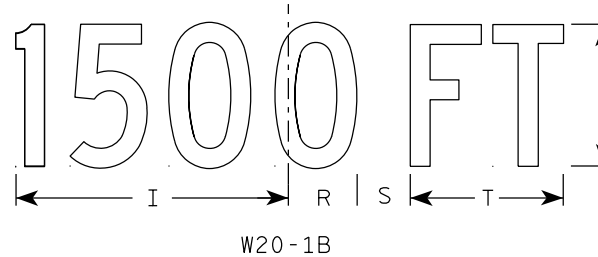
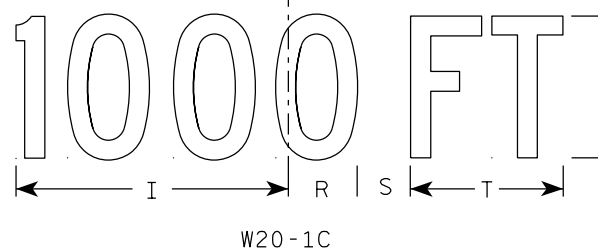
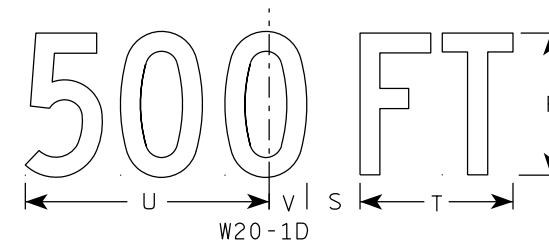
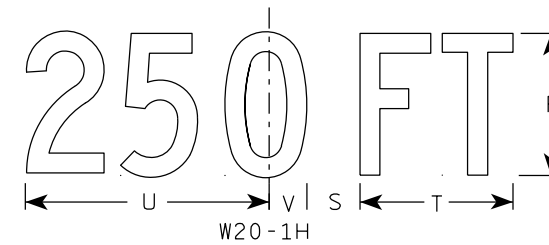
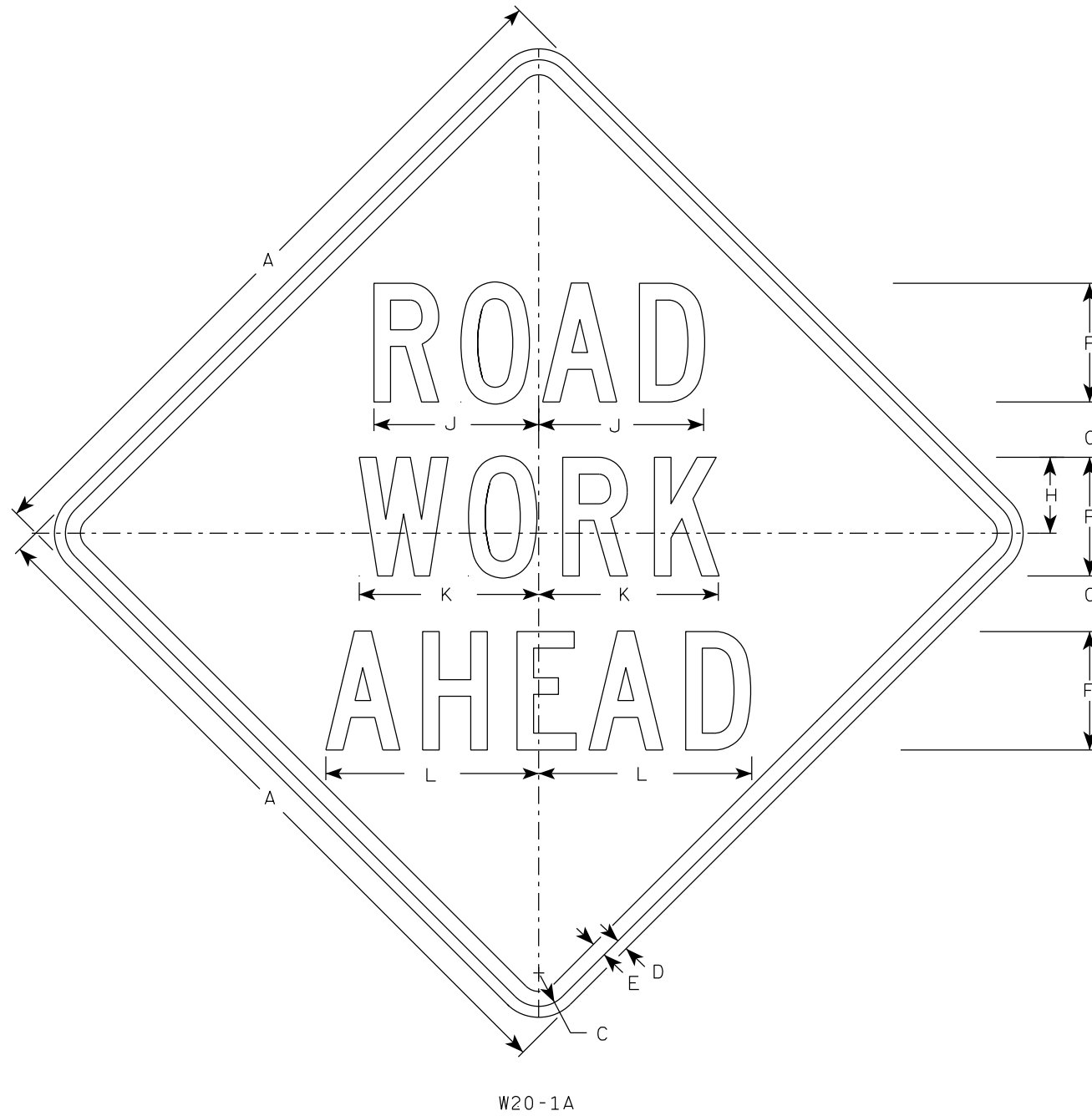
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 3/29/2021 PLATE NO. R11-2.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
 - Background - Orange
 - Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, E, F, G & H

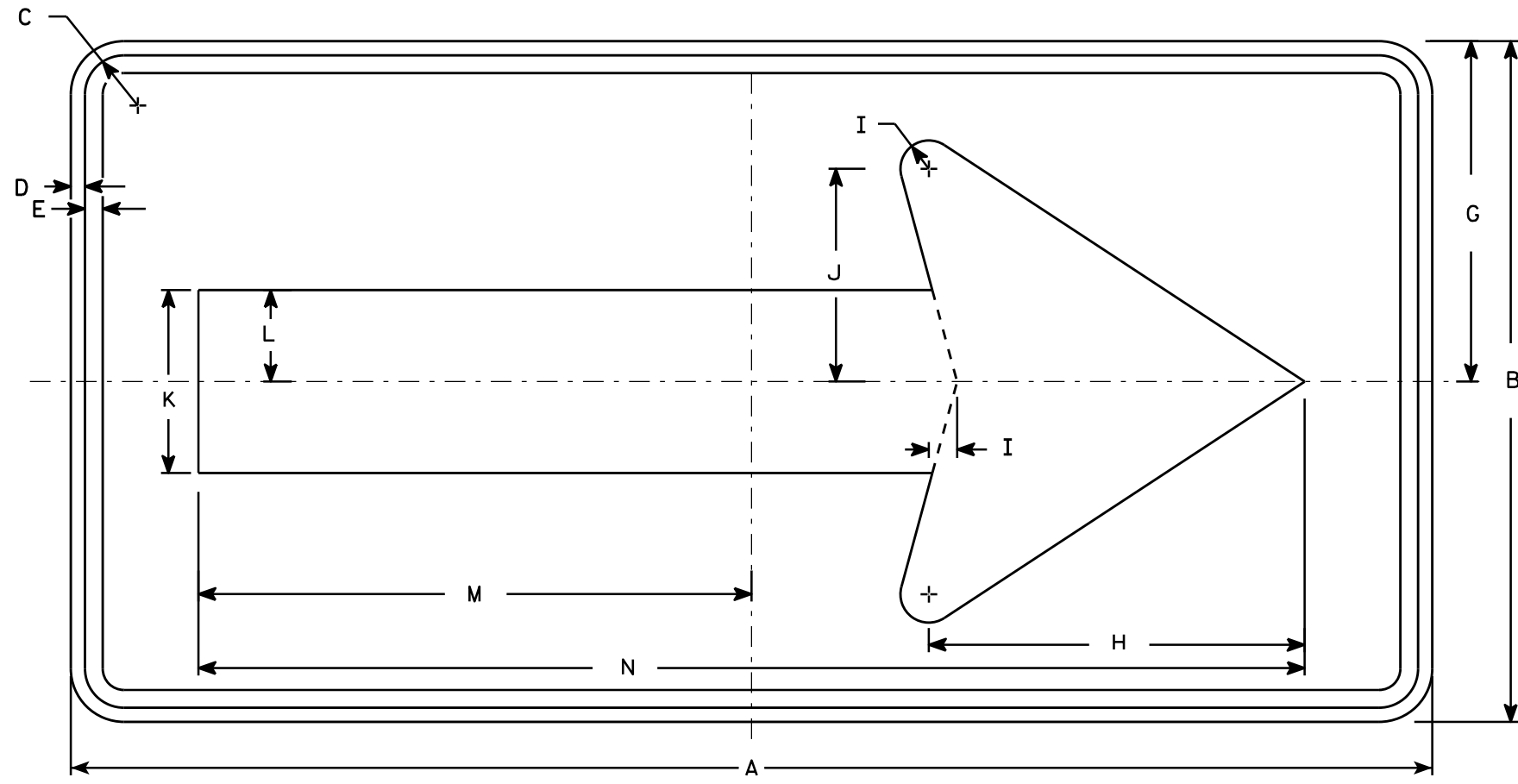
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/25/2020 PLATE NO. W20-1.11

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W01-6

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5

STANDARD SIGN
W01-6

WISCONSIN DEPT OF TRANSPORTATION

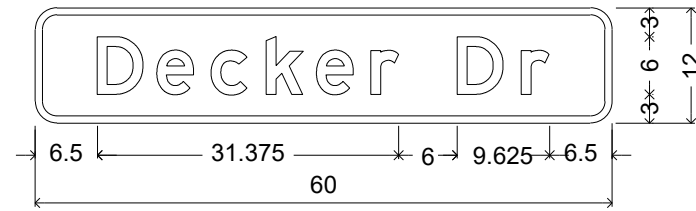
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-6.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES:

1. All Sign Type II - Type H Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - E



D3-1 (Qty 7); 2.250" Radius, 0.750" Border

7

7



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>